

ORDINANCE NO. O23-8

AN ORDINANCE OF THE TOWN OF ADDISON, TEXAS, AMENDING THE COMPREHENSIVE ZONING ORDINANCE BY REZONING PROPERTIES COMPRISING 13.7± ACRES OF LAND GENERALLY LOCATED AT THE SOUTHEAST CORNER OF ADDISON ROAD AND FESTIVAL WAY AND THE NORTHEAST CORNER OF QUORUM DRIVE AND ARAPAHO ROAD, FROM COMMERCIAL-1 (C-1), PLANNED DEVELOPMENT (PD), AND URBAN CENTER (UC), TO A NEW PLANNED DEVELOPMENT (PD) ZONING DISTRICT TO ALLOW A TRANSIT-ORIENTED MIXED USE DEVELOPMENT COMPRISED OF MULTIFAMILY RESIDENTIAL, OFFICE, PARKING, RESTAURANT, RETAIL, AND ENTERTAINMENT USES, AND ASSOCIATED PUBLIC AND PRIVATE OPEN SPACE AND COMMON AREAS, THROUGH THE APPROVAL OF A CONCEPT PLAN AND DEVELOPMENT STANDARDS; PROVIDING A PENALTY NOT TO EXCEED TWO THOUSAND AND NO/100 DOLLARS (\$2,000.00); PROVIDING FOR SAVINGS, NO SEVERABILITY AND AN EFFECTIVE DATE.

WHEREAS, at its regular meeting held on January 17, 2023 the Planning & Zoning Commission considered and made recommendations on Case No.1860-Z, being a request to rezone properties comprising 13.7± acres of land generally located at the southeast corner of Addison Road and Festival Way and the northeast corner of Quorum Drive and Arapaho Road, from Commercial-1 (C-1), Planned Development (PD), and Urban Center (UC), to a new Planned Development (PD) Zoning District to allow a transit-oriented mixed use development comprised of multifamily residential, office, parking, restaurant, retail, and entertainment uses, and associated public and private open space and common areas (the “Project”), through the approval of a concept plan and Development Standards (defined herein below); and

WHEREAS, the Project shall be developed in conformance with this Ordinance together with all definitive development agreements executed in connection with the Project, including the Second Amended Memorandum of Understanding between the Town of Addison, AMLI Residential, and Stream Realty for the Addison Circle Area Transit Oriented Development (MOU) dated January 10, 2023, as the same may be amended from time to time (collectively herein, the “Definitive Agreements”); and

WHEREAS, the City Council finds that this change of zoning is in accordance with the adopted Comprehensive Plan of the Town of Addison, as amended; and

WHEREAS, the City Council further finds, after due deliberations and consideration of the recommendation of the Planning and Zoning Commission, the information received at a public hearing, and other relevant information and materials, that this amendment promotes the general welfare and safety of this community.

NOW THEREFORE, BE IT ORDAINED BY THE CITY COUNCIL OF THE TOWN OF ADDISON, TEXAS:

SECTION 1. That the recitals and findings set forth above are hereby found to be true and correct and incorporated as if fully set forth herein.

SECTION 2. The Zoning Ordinance and official zoning map shall be amended so as to rezone the Property, said Property comprising 13.7+/-, and being further described within **Exhibit A** attached hereto, to a new Planned Development (PD) District, subject to the following conditions:

- A. Development Plans for this PD zoning district shall comply with the Permitted Uses and Development Standards (the “Development Standards”), Concept Plan, and Master Streetscape Plan, which are attached hereto as **Exhibit B**, **Exhibit C**, and **Exhibit D** and made a part hereof for all purposes. Additional development conditions may be further defined in the Definitive Agreements.
- B. Lot 4X, Block A (fitness programming) will obtain development plan and civil engineering plan approval prior to issuance of a permit for the construction of site infrastructure and will be completed prior to issuance of a Certificate of Occupancy for Lot 1, Block A (Stream Office).
- C. Lot 1X, Block C (public open space) will obtain development plan and civil engineering plan approval prior to issuance of a permit for the construction of site infrastructure and will be completed prior to issuance of a Certificate of Occupancy for Lot 1, Block B (AMLI Mid-Rise).
- D. Lot 2, Block A will obtain development plan and civil engineering plan approval and be constructed in accordance with the Definitive Agreements.
- E. Right-of-Way on Addison Road will be reserved to accommodate a northbound right-turn lane. If the final traffic impact analysis determines a need for this right-turn lane improvement, it should be constructed concurrent to the first phase of the Project.
- F. The final quantity of parking spaces, operation of the public parking structure, and retail parking accommodations in private parking structures, are subject to completion of a parking study and execution of the Definitive Agreements.
- G. Developer shall provide the City evidence of compliance with FAA Part 77 building height review and final confirmation of residential building sites being situated outside of the 65 DNL noise contour prior to issuance of a building permit for the Project.

SECTION 3. Any person, firm, or corporation violating any of the provisions or terms of this ordinance shall be subject to the same penalty as provided for in the Comprehensive Zoning Ordinance of the city, as heretofore amended, and upon conviction shall be punished by a fine set in accordance with Chapter 1, Section 1-7 of the Code of Ordinances for the Town of Addison.

SECTION 4. The provisions of this Ordinance are severable, and should any section, subsection, paragraph, sentence, phrase or word of this Ordinance, or application thereof to any person, firm, corporation or other business entity or any circumstance, be adjudged or held to be

unconstitutional, illegal or invalid, the same shall not affect the validity of the remaining or other parts or portions of this Ordinance, and the City Council hereby declares that it would have passed such remaining parts or portions of this Ordinance despite such unconstitutionality, illegality, or invalidity, which remaining portions shall remain in full force and effect.

SECTION 5. All ordinances of the City in conflict with the provisions of this ordinance be, and the same are hereby repealed and all other ordinances of the City not in conflict with the provisions of this ordinance shall remain in full force and effect.

SECTION 6. This Ordinance shall become effective from and after its passage and approval and after publication as may be required by law or by the City Charter or ordinance.

PASSED AND APPROVED by the City Council of the Town of Addison, Texas, on this the **13TH** day of **FEBRUARY 2023**.

TOWN OF ADDISON, TEXAS


Joe Chow, Mayor

ATTEST:


Irma G. Parker, City Secretary

APPROVED AS TO FORM:


Whitt Wyatt, City Attorney

Published in the Dallas Morning News on Saturday, February 18, 2023

EXHIBIT A

LEGAL DESCRIPTION OF THE PROPERTY

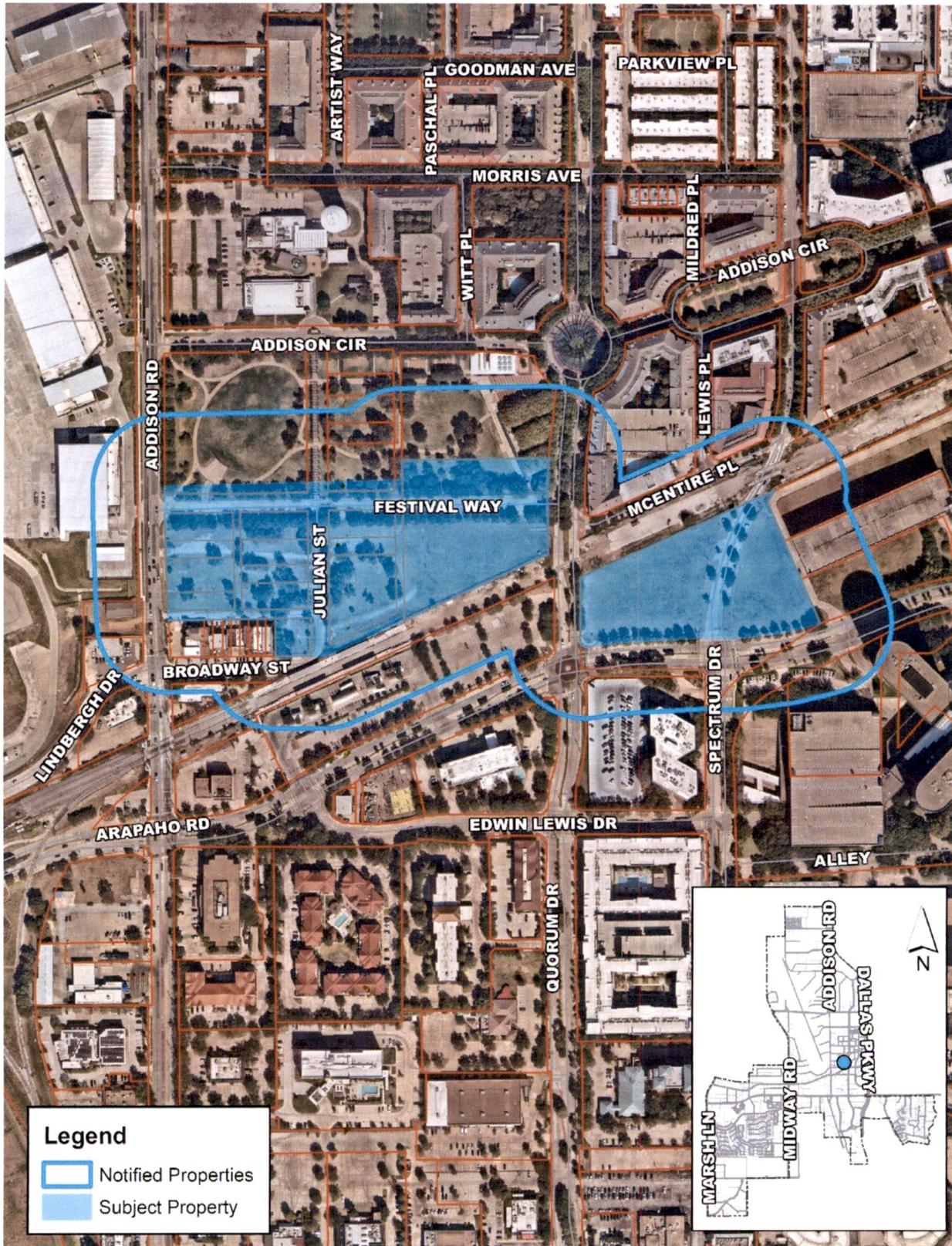


EXHIBIT A

Being a 9.676 acre tract of land situated in the G.W. Fisher Survey, Abstract No. 482, Town of Addison Dallas County, Texas, being all of Lot 1, Block F, Addison Circle Phase II, an addition to the Town of Addison, Dallas County, Texas, according to the plat thereof, recorded in Volume 97217, Page 3056, Deed Records, Dallas County, Texas, and being part of the Plat of Addison, an addition to the Town of Addison, Dallas County Texas, according to the plat thereof, recorded in Volume 1, Page 35, Map Records, Dallas County, Texas, and also being part of that 0.5968 acre tract of land conveyed to the Town of Addison and recorded in Volume 99245, Page 2363 of the Deed Records, Dallas County, Texas and being more particularly described as follows:

BEGINNING at 5/8 inch iron rod with cap stamped “Jones & Carter” (Controlling Monument (CM)) found on at the northwest corner of said Plat of Addison, and also being on the east right-of-way line of Addison Road (variable width right-of-way);

THENCE, North 00 degrees 53 minutes 01 seconds West, along the common west line of said Plat of Addison, and the east right-of-way line of said Addison Road, a distance of 375.25 feet to a 5/8 inch iron rod with cap stamped “Traverse LS Prop Cor” found, from which a 1/2 inch iron rod with cap found on the east right-of-way line of said Addison Road bears North 00 degrees 53 minutes 01 seconds West, a distance of 977.88 feet;

THENCE, North 88 degrees 46 minutes 50 seconds East, departing the east right-of-way line of said Addison Road, and crossing said Plat of Addison, a distance of 666.23 feet to a 5/8 inch iron rod with cap stamped “Traverse LS Prop Cor” found, said point being on the west line of said Lot 1, Block F, Addison Circle Phase II;

THENCE, North 00 degrees 48 minutes 56 seconds West, along the west line of said Lot 1, Block F, a distance of 56.98 feet to a 5/8 inch iron rod with cap stamped “Traverse LS Prop Cor” found on the northwest corner of said Lot 1, Block F, same being the southwest corner of Lot 1, Block A, Addison Circle Phase I, an addition to the Town of Addison, Dallas County Texas, according to the plat thereof, recorded in Volume 97189, Page 1047, Deed Records, Dallas County, Texas;

THENCE, North 89 degrees 01 minutes 55 seconds East, along the common north line of said Lot 1, Block F, and the south line of said Lot 1, Block A, a distance of 413.60 feet to a 5/8 inch iron rod with cap stamped “Traverse LS Prop Cor” found on the northeast corner of said Lot 1, Block F, and the southeast corner of said Lot 1, Block A, and also being on the west right-of-way line of Quorum Drive (variable width right-of-way), said iron rod also being the beginning of a curve to the left, having a central angle of 04 degrees 59 minutes 06 seconds, a radius of 1,485.39 feet and being subtended by a chord bearing of South 01 degrees 37 minutes 49 seconds West, a chord distance of 129.20 feet;

THENCE, in a southwesterly direction, along the common west right-of-way line of said Quorum Drive, and the east line of said Lot 1, Block F, an arc distance of 129.24 feet to a 5/8-inch iron rod with cap stamped “Traverse LS Prop Cor” found;

EXHIBIT A

THENCE, South 00 degrees 51 minutes 44 seconds East, continuing along the common west right-of-way line of said Quorum Drive, and the east line of said Lot 1, Block F, a distance of 136.90 feet to a 5/8 inch iron rod with cap stamped "Traverse LS Prop Cor" found on the southeast corner of said Lot 1, Block F, said iron rod also being on the north line of a 100-foot Dart Rail right-of-way, as recorded in Volume 91008, Page 1390 of the Deed Records, Dallas County, Texas, from which a 5/8 inch iron rod with cap stamped "Jones & Carter" found (CM) bears North 65 degrees 45 minutes 16 seconds East, a distance of 14.16 feet;

THENCE, South 65 degrees 45 minutes 16 seconds West, along the common north line of said Dart rail tract, and the south line of said Lot 1, Block F, a distance of 673.38 feet to a 5/8 inch iron rod with cap stamped "Traverse LS Prop Cor" found on the south line of said Plat of Addison, from which a 5/8 inch iron rod found bears South 72 degrees 36 minutes 28 seconds West, a distance of 4.10 feet;

THENCE, South 88 degrees 38 minutes 43 seconds West, departing the north line of said Dart rail tract, being the southerly line of said Plat of Addison and being the southerly line of a tract of land conveyed to Town of Addison by Special Warranty Deed recorded in Volume 2002057, Page 1296, Deed Records, Dallas County, Texas, a distance of 158.11 feet to a point for corner, said corner being the southwesterly corner of Lot 13 of said Town of Addison tract and being on the southeasterly corner of Lot 12 of said Plat of Addison;

THENCE, North 00 degrees 43 minutes 11 seconds West, along the common westerly line of said Town of Addison tract and the easterly line of said Lot 12, Plat of Addison, a distance of 100.00 feet to a point for corner, said iron rod being on the south line of a 25-foot alley;

THENCE, South 88 degrees 38 minutes 43 seconds West, along the south line of said 25 foot alley, a distance of 298.19 feet to the **POINT OF BEGINNING** and containing 9.676 acres (421,503 square feet) of land, more or less.

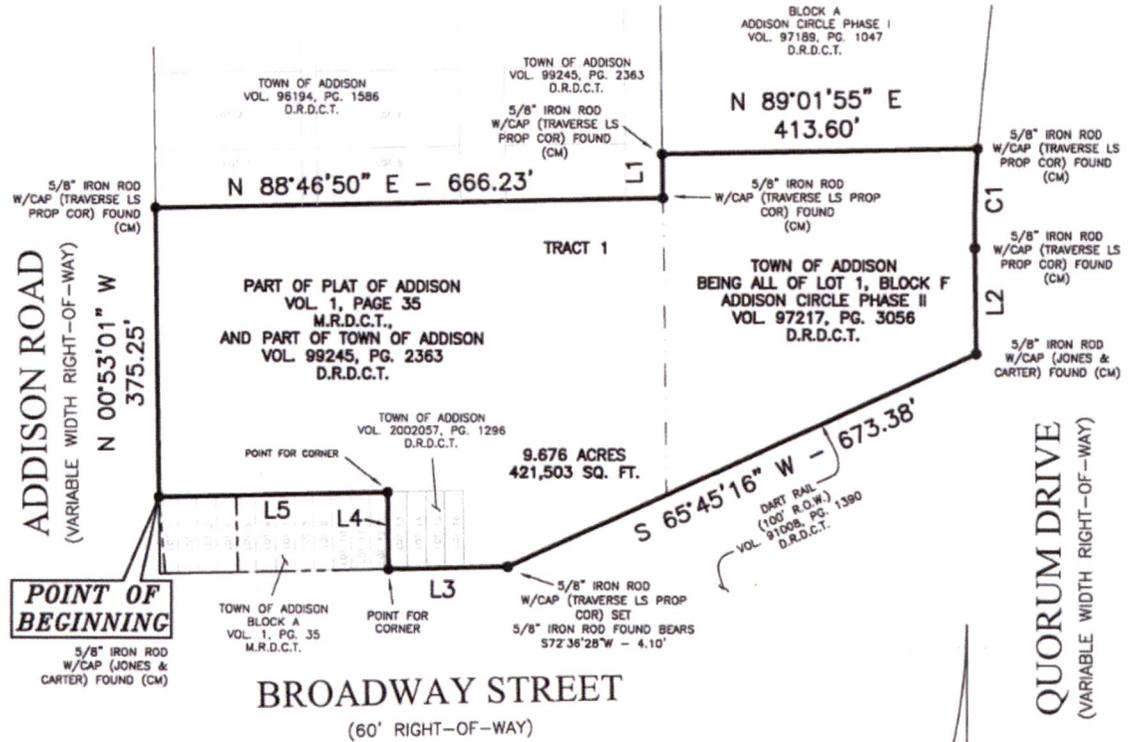
EXHIBIT A

EXHIBIT "A"

CURVE TABLE					
Curve No.	Delta	Radius	Length	Chord Bearing	Chord
C1	04°59'06"	1,485.39'	129.24'	S 01°37'49" W	129.20'

The Basis of Bearings is from the Texas State Plane Coordinate System, NAD83, North Central Zone as derived from GPS observations using the Allterra RTK Network and adjusted to surface using a surface scale factor of 1.000136506.

LINE TABLE		
No.	Bearing	Distance
L1	N 00°48'56" W	56.98'
L2	S 00°51'44" W	136.90'
L3	S 88°38'43" W	158.11'
L4	N 00°43'11" W	100.00'
L5	S 88°38'43" W	298.19'



Note: This document was prepared under 22 TAC §663.21, does not reflect the results of an on the ground survey, and is not to be used to convey or establish interests in the real property except those rights and interests implied or established by the creation or reconfiguration of the boundary of the political subdivision for which it was prepared.

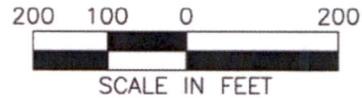


EXHIBIT "A"		2/7/23	
(TRACT 1) 9.676 ACRE TRACT G.W. FISHER SURVEY, ABSTRACT NO. 482 TOWN OF ADDISON, DALLAS COUNTY, TEXAS			
JOB No. TR-05-23	DRAWN BY: DV		
DATE: FEBRUARY 08, 2023	CHECKED BY: MAN		
SCALE: 1"=200'	SHEET 3 OF 3		
MARK ALLAN NACE TEXAS RPLS NO. 5539			

EXHIBIT A

Being a 3.991 acre tract of land situated in the G.W. Fisher Survey, Abstract No. 482, Town of Addison Dallas County, Texas, being part of that 6.272 acre tract of land conveyed to the Town of Addison and recorded in Instrument Volume 93243, Page 6422 of the Deed Records, Dallas County, Texas and being more particularly described as follows:

COMMENCING at an “X” cut (Controlling Monument (CM)) found on the northeast corner of Lot 1, Addison Bus Transit Center, an addition dedicated to the Town of Addison, Dallas County, Texas, according to the plat thereof recorded in Volume 2000130, Page 2574, Deed Records, Dallas County Texas, and also being on the west right-of-way line of Quorum Drive (variable width right-of-way), from which a 5/8 inch rod found with cap stamped “Jones & Carter” (CM) on the south line of a 100-foot Dart Rail right-of-way, as recorded in Volume 91008, Page 1390 of the Deed Records, Dallas County, Texas, said iron rod bears North 00 degrees 45 minutes 26 seconds West, a distance of 76.42 feet;

THENCE, North 87 degrees 50 minutes 28 seconds East, crossing said Quorum Drive, at a distance of 80.16 feet to a 5/8-inch iron rod with cap stamped “Traverse LS Prop Cor” found at the **POINT OF BEGINNING** for the herein described tract;

THENCE, North 65 degrees 45 minutes 10 seconds East, along the common north line of said Town of Addison tract, and the south line of said Dart Rail tract, a distance of 590.07 feet to a point for corner at the most northerly northwest corner of Millennium Phase II, an addition to the Town of Addison, Dallas County, Texas, according to the plat thereof recorded in Volume 98221, Page 00022, Deed Records, Dallas County Texas, and being the beginning of a non-tangent curve to the left, having a central angle of 03 degrees 26 minutes 11 seconds, a radius of 636.00 feet and being subtended by a chord bearing South 32 degrees 09 minutes 42 seconds West, a chord distance of 38.14 feet;

THENCE along said curve to the left along the common east line of said Town of Addison tract and the west line of said Millennium Phase II addition, in a southwesterly direction, an arc distance of 38.15 feet to an angle point at the end of said curve;

THENCE, South 24 degrees 14 minutes 44 seconds East, along the common east line of said Town of Addison tract, and the west line of said Millennium Phase II, passing a 1/2” iron rod found (CM) on the southwest corner of said Millennium Phase II, at a distance of 117.37 feet, and passing a 1/2 inch rod with cap stamped “RPLS 5199” (CM) found on the southwest corner of Millennium Phase I, an addition to the Town of Addison, Dallas County, Texas, according to the plat thereof recorded in Volume 98221, Page 00021, Deed Records, Dallas County Texas, and continuing a total distance of 358.76 feet to a 5/8 inch iron rod with cap stamped “Traverse LS Prop Cor” found on the southeast corner of said Town of Addison tract, same being on the southwest corner of Millennium Phase III, an addition to the Town of Addison, Dallas County, Texas, according to the plat thereof recorded in Volume 99114, Page 0009, Deed Records, Dallas County Texas, and also being on the north right-of-way line of Arapaho Road (variable width right-of-way), said iron rod found being the beginning of a curve to the right, having a central

EXHIBIT A

angle of 14 degrees 55 minutes 52 seconds, a radius of 949.00 feet and being subtended by a chord bearing of South 81 degrees 37 minutes 57 seconds West, a chord distance of 246.61 feet;

THENCE, in a southwesterly direction, along the common south line of said Town of Addison tract, and the north right-of-way line of said Arapaho road, an arc distance of 247.31 feet to the end of said curve;

THENCE, South 89 degrees 05 minutes 53 seconds West, along the common north right-of-way line of said Arapaho Road, and the south line of said Town of Addison tract, a distance of 232.29 feet to a 5/8 inch iron rod with cap stamped "Traverse LS Prop Cor" found at the beginning of a curve to the left, having a central angle of 10 degrees 16 minutes 05 seconds, a radius of 951.00 feet and being subtended by a chord bearing of South 83 degrees 57 minutes 50 seconds West, a chord distance of 170.20 feet;

THENCE, in a southwesterly direction, along the common north right-of-way line of said Arapaho Road, and the south line of said Town of Addison tract, an arc distance of 170.43 feet to a 5/8-inch iron rod with cap stamped "Traverse LS Prop Cor" found on the southwest corner of a corner clip on the north right-of-way line of said Arapaho Road, and the east right-of-way line of said Quorum Drive;

THENCE, North 51 degrees 20 minutes 28 seconds West, along said corner clip, a distance of 26.76 feet to a 5/8 inch iron rod with cap stamped "Traverse LS Prop Cor" found on the northwest corner of said corner clip;

THENCE, North 00 degrees 51 minutes 39 seconds West, along the common east right-of-way line of said Quorum Drive, and the west line of said Town of Addison tract, a distance of 166.40 feet to **POINT OF BEGINNING** and containing 3.991 acres (173,843 square feet) of land, more or less.

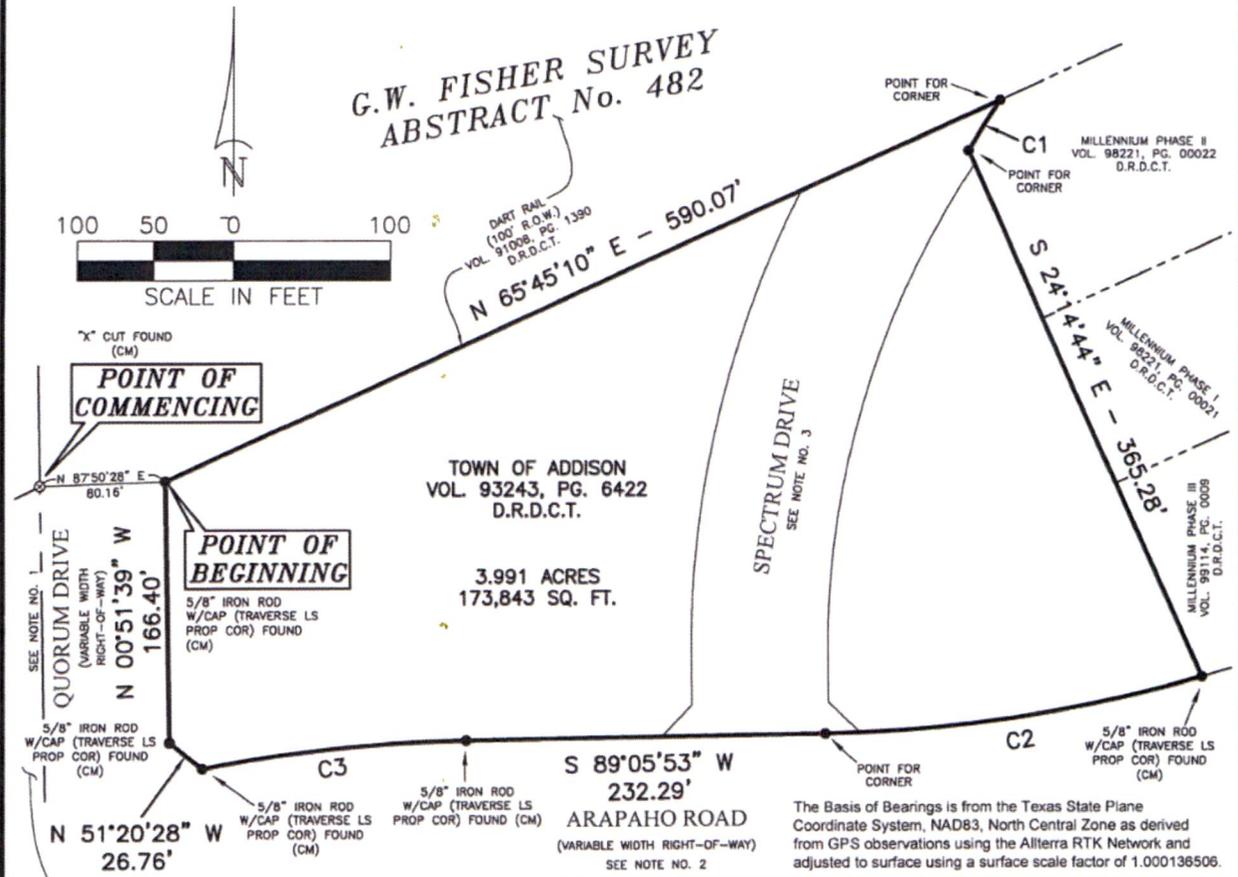
EXHIBIT A

EXHIBIT "A"

Note 1: Quorum Drive between the southerly right-of-way line of the Dart Rail and Arapaho Road does not have a dedicated right-of-way. The right-of-way lines depicted are based on the right-of-way map for Arapaho Road prepared by Huitt-Zollars, Inc. for the Town of Addison, Project No. 1772-01, Sheet No. R2, Revised on 1/16/98.

Note 2: Arapaho Road does not have a dedicated right-of-way. The right-of-way lines depicted for Arapaho Road are based on the right-of-way map for Arapaho Road prepared by Huitt-Zollars, Inc. for the Town of Addison, Project No. 1772-01, Sheet No. R2, Revised on 1/16/98.

Note 3: Spectrum Drive between the southerly right-of-way line of the Dart Rail and Arapaho Road does not have a dedicated right-of-way. The right-of-way lines depicted are based on the Horizontal Control Plan for Spectrum Drive, prepared by Huitt-Zollars, Inc. for the Town of Addison, Project No. 01-3220-01, Sheet No. 2, dated November 2003.



Note: This document was prepared under 22 TAC §663.21, does not reflect the results of an on the ground survey, and is not to be used to convey or establish interests in the real property except those rights and interests implied or established by the creation or reconfiguration of the boundary of the political subdivision for which it was prepared.

CURVE TABLE					
Curve No.	Delta	Radius	Length	Chord Bearing	Chord
C1	03°26'11"	636.00'	38.15'	S 32°09'42" W	38.14'
C2	14°55'52"	949.00'	247.31'	S 81°37'57" W	246.61'
C3	10°16'05"	951.00'	170.43'	S 83°57'50" W	170.20'

EXHIBIT "A"

(TRACT 2) 3.991 ACRE TRACT
G.W. FISHER SURVEY, ABSTRACT NO. 482
TOWN OF ADDISON,
DALLAS COUNTY, TEXAS

JOB No. TR-05-23	DRAWN BY: DV
DATE: FEBRUARY 08, 2023	CHECKED BY: MAN
SCALE: 1"=100'	SHEET 3 OF 3

2/7/23

Mark A. Nace

MARK ALLAN NACE
TEXAS RPLS NO. 5539

TRAVERSE
LAND SURVEYING LLC

Surveying | Construction Staking | Platting

14200 Midway Road, Suite 130
Dallas, TX 75224 T: 469.784.9321
W: TraverseLandSurveying.com
Texas Firm No. 10194631

EXHIBIT B

PERMITTED USES AND DEVELOPMENT STANDARDS

1. Permitted Uses:

- A. Condominium.
- B. Multifamily.
- C. Senior Housing (Special Use Permit (SUP) required).
- D. Live/Work.
 - i. The commercial use is limited to professional services such as accountants, architects, artisans, attorneys, computer and multimedia related professionals, consultants, engineers, hair stylists, insurance, real estate, athletic trainers, travel agents, one-on-one instructors, and similar occupations as may be approved by the Zoning Administrator provided that the use does not detract from, or pose a nuisance to, the adjacent uses.
 - ii. Both the residential and commercial components of the Live/Work unit must be occupied by the same tenant.
- E. Office.
- F. Co-Working.
- G. Retail – as defined in the Residential Subdistrict Use Regulations for the Urban Center (UC) Zoning District.
- H. Restaurant (Special Use Permit (SUP) required).
- I. Food Hall (Special Use Permit (SUP) required).
- J. Hotel (Special Use Permit (SUP) required).
- K. Health Club (Special Use Permit (SUP) required).
- L. Day Care Facility (Special Use Permit (SUP) required).
- M. Entertainment/Event Venue (Indoor or Outdoor).
- N. Sale of alcohol for on-premises or off-premises consumption (Special Use Permit (SUP) required).
- O. Accessory uses encompassing community, social, and recreation facilities customary to multifamily residential and mixed-use development, and home occupations that do not employ persons outside the residence and that do not alter the physical structure or generate additional parking demand, inclusive of any temporary or overnight parking related to such home occupation.

2. Maximum Number of Multifamily Dwelling Units: 700 dwelling units.

3. Development Phasing and Operational Restrictions: The Concept Plan shall be

EXHIBIT B

implemented in accordance with the terms and conditions outlined in the Second Amended Memorandum of Understanding between the Town of Addison, AMLI Residential, and Stream Realty for the Addison Circle Area Transit Oriented Development (the “MOU”) dated January 10, 2023, as the same may be amended, together with the final executed Definitive Agreements (defined in this PD-O23-_____)

4. Landscape (Overall Site):

Minimum Tree Plantings: New canopy tree plantings shall be a minimum of 4 caliper inches in diameter. Tree planting may be achieved in public open space, private open space, or common areas, and at the streetscape.

Trees shall be planted to achieve all required tree mitigation on site. Where mitigated trees are at least 8 caliper inches in diameter, a replacement tree, of at least 6 caliper inches in diameter, must be utilized to fulfill mitigation requirements on an inch for inch basis.

Alternative replacement and transplanted trees shall be considered by the Town where said trees can enhance the diversity of tree planting and positively contribute to the overall aesthetics and screening needs for the site. The Director of Parks and Recreation may, in the Town’s sole discretion, approve alternative tree mitigation standards to account for transplanting of mature trees.

- A. Required Site Landscape: Excluding streets, parking, sidewalks, plazas, and other required hardscape, common areas are to be landscaped and irrigated. Residential ground floor frontages shall be required to landscape the entire area between the edge of sidewalk and the primary building facade, excluding access to sidewalks, stairs, stoops, porches, and patios. This area must be irrigated, and may be landscaped with ground cover, low shrubs, and ornamental trees.

5. Public Open Space:

- A. Minimum Public Open Space: As depicted in the **Concept Plan**.
- B. Public Open Space Furnishings: Public open space shall be amenitized with site furnishings such as benches, trash receptacles, pedestrian lighting, fitness programming, and public art.
- C. Public Open Space Landscape: Landscape shall be irrigated and shall include a mix of evergreen and deciduous canopy and ornamental tree species, ornamental groundcover, and zoysia grass turf, or acceptable alternative.
- D. Public Open Space Installation and Maintenance: All public open space improvements shall be installed by the developer and owned and maintained in accordance with the terms of a future Definitive Agreements between the Town and Co-Developers.

EXHIBIT B

6. **Streetscape Standards:** All streetscapes shall comply with the **Master Streetscape Plan**.

7. **Pavement Treatments:** Enhanced pavement treatments will be provided in accordance with the **Concept Plan**. This will be achieved through the use of pavers, as well as unique concrete treatments such as painting, staining, and patterned sawcuts.

8. **Multifamily (Tower) Form Standards:**

A. Minimum Lot Area: None.

B. Minimum Lot Width: None.

C. Maximum Lot Width (Festival Way Frontage): 450 feet.

D. Minimum Lot Depth: None.

E. Maximum Lot Depth: 300 feet.

F. Minimum Building Setbacks:

i. Front Yard (Festival Way): 30 feet from the centerline of the street.

ii. Front Yard (Quorum Drive): 10 feet.

iii. Side Yard: None.

iv. Rear Yard: None.

G. Minimum Building Height: 13 stories, or the maximum building height permitted under FAA Part 77 review.

H. Maximum Building Height: the maximum building height permitted under FAA Part 77 review.

I. High-Rise Building: A building with an occupied floor located more than 75 feet above the lowest level of fire department access.

J. Minimum Floor Area Per Dwelling Unit: 600 square feet.

K. Minimum Ground Floor Commercial Floor Area: 10,000 square feet.

L. Minimum Ground Floor Commercial Floor Height: 16 feet floor to floor.

9. **Multifamily (Mid-Rise) Form Standards:**

A. Minimum Lot Area: None.

B. Minimum Lot Width: None.

EXHIBIT B

- C. Maximum Lot Width (Arapaho Road Frontage): 600 feet.
- D. Minimum Lot Depth: None.
- E. Maximum Lot Depth (Spectrum Drive Frontage): 400 feet.
- F. Minimum Building Setbacks:
 - i. Front Yard (Arapaho Road): 6 feet.
 - ii. Front Yard (Quorum Drive): 10 feet.
 - iii. Front Yard (Spectrum Drive): 6 feet.
 - iv. Side Yard: None.
 - v. Rear Yard: None.
- G. Minimum Building Height: 6 stories.
- H. Maximum Building Height: 10 stories.
- I. High-Rise Building: A building with an occupied floor located more than 75 feet above the lowest level of fire department access.
- J. Minimum Floor Area Per Dwelling Unit: 600 square feet.
- K. Minimum Ground Floor Commercial Floor Area: 5,000 square feet.
- L. Minimum Ground Floor Commercial Floor Height: 16 feet floor to floor.

10. Office & Parking Structure Form Standards:

- A. Minimum Lot Area: None.
- B. Minimum Lot Width: None.
- C. Maximum Lot Width (Festival Way Frontage): 475 feet.
- D. Minimum Lot Depth: None.
- E. Maximum Lot Depth (Addison Road Frontage): 275 feet.
- F. Minimum Building Setbacks:
 - i. Front Yard (Addison Road): 15 feet.
 - ii. Front Yard (Festival Way): 30 feet from the centerline of the street.

EXHIBIT B

- iii. Side Yard: None.
- iv. Rear Yard: None.
- G. Minimum Building Height: 5 stories (parking garage), 6 stories (office), or, if applicable, the maximum building height permitted under FAA Part 77 review.
- H. Maximum Building Height: the maximum building height permitted under FAA Part 77 review.
- I. Minimum Office Floor Area: 160,000 square feet.
- J. Minimum Ground Floor Retail Floor Area: 3,500 square feet.
- K. Minimum Ground Floor Commercial Floor Height: 15 feet floor to floor.

11. Retail/Restaurant/Entertainment Node Form Standards:

- A. Minimum Lot Area: None.
- B. Minimum Lot Width: 200 feet.
- C. Maximum Lot Width: None.
- D. Minimum Lot Depth: 250 feet.
- E. Maximum Lot Depth: None.
- F. Minimum Building Setbacks:
 - v. Front Yard (Festival Way): 30 feet from the centerline of the street.
 - vi. Side Yard: None
 - vii. Rear Yard: None.
- G. Minimum Building Height: 1 story.
- H. Maximum Building Height: the maximum building height permitted under FAA Part 77 review.
- I. Minimum Building Floor Area: 25,000 square feet.
- J. Minimum Gathering/Civic/Open Space: 20 percent of the gross area of the lot. Physical and visual connections must be made from this designated space(s) to Festival Way and the Silver Line Station.

12. Airport Noise Mitigation: Residential buildings must be located outside of the 65 DNL noise contour for Addison Airport.

EXHIBIT B

13. Parking:

A. Minimum Off-Street Parking Requirements:

- i. Multifamily (Stacked Flat): 1 space per bedroom, not to exceed 2 spaces per dwelling unit.
- ii. Office/Co-Working: 1 space per 300 square feet of floor area.
- iii. Retail: 1 space per 300 square feet of floor area.
- iv. Restaurant: 1 space per 100 square feet of floor area.

B. Shared Parking: The final quantity of parking spaces, operation of the public parking structure, and retail parking accommodations in private parking structures, are subject to completion of a parking study and execution of a development agreement between the Town and Co-Developers.

C. Bicycle Parking: 1 space for every 15 required off-street parking spaces, with at least 50 percent of provided spaces being accommodated within the interior of a structure.

14. Facade: Building facades shall comply with the terms and conditions of the MOU and the Definitive Agreements.

15. Mechanical Equipment Placement and Screening: Mechanical equipment shall be mounted on the roof and be screened from view from all rights-of-way and located to minimize noise intrusion off each lot. Screening must be architecturally compatible with the building design.

16. Solid Waste and Recycling: Solid Waste and recycling services shall be provided for this development. Dumpsters and compactors should be considered in the design of buildings and shall not be permitted as freestanding features within the site, unless enclosed in a masonry dumpster enclosure and situated at least 200 feet away or screened by buildings from view from a public street.

17. Sustainability:

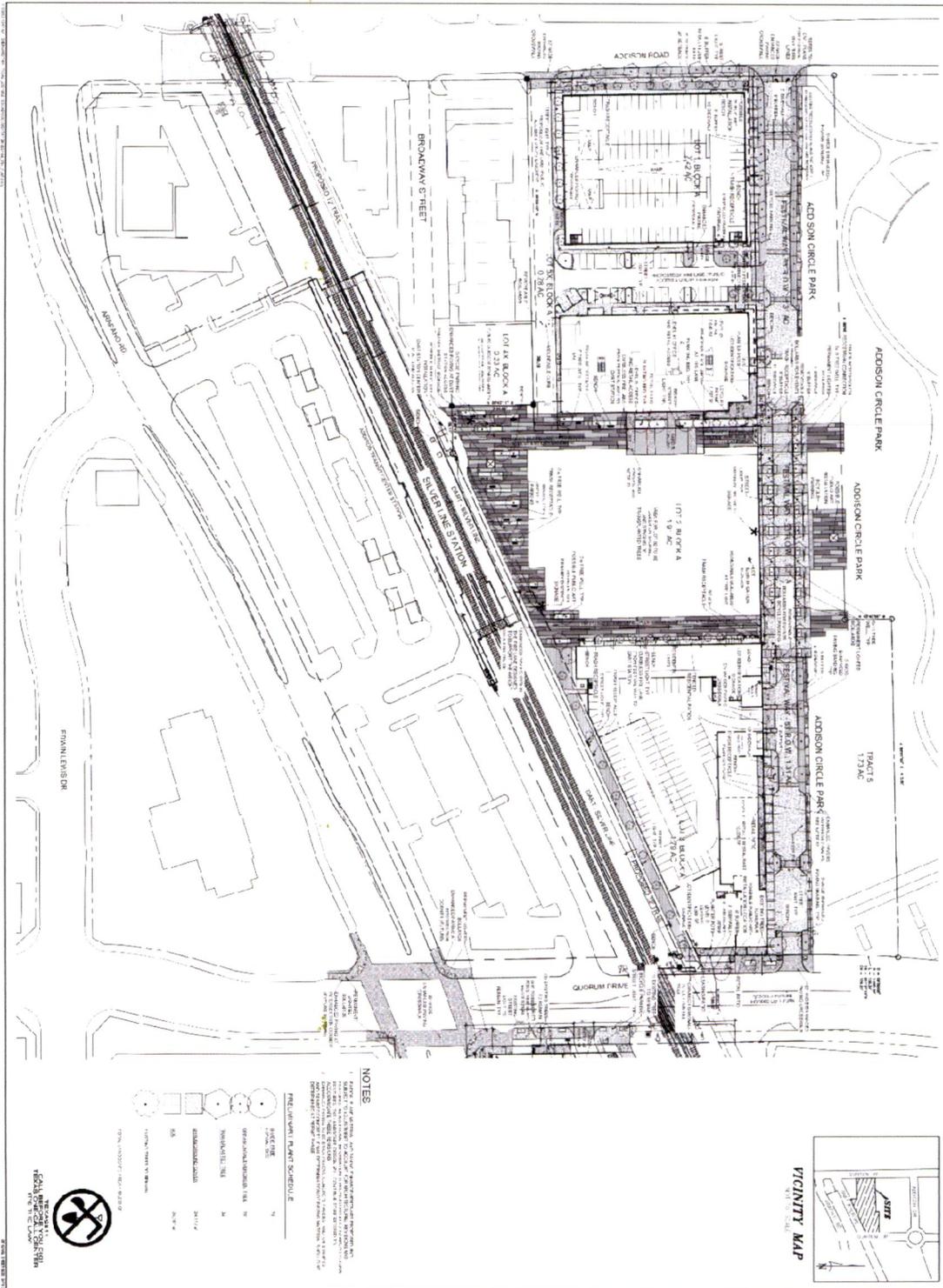
A. The provision of sustainability features such as LEED compliance, electric vehicle charging stations, and public art shall be fully defined by the Definitive Agreements.

18. Residential Dwelling Unit Features:

A. Interior Finishes: There shall be no linoleum or Formica surfaces in the units. All countertops shall be granite, stone, or a superior surface material.

B. Appliances: All units shall be equipped with energy efficient appliances.

EXHIBIT C



NOTES

1. THIS PLAN IS A PRELIMINARY PLAN AND IS SUBJECT TO THE APPROVAL OF THE CITY ENGINEER AND THE CITY COMMISSIONERS. THE CITY ENGINEER AND CITY COMMISSIONERS SHALL HAVE THE FINAL SAY IN THE MATTER OF THE APPROVAL OF THIS PLAN.

2. THE CITY ENGINEER AND CITY COMMISSIONERS SHALL BE RESPONSIBLE FOR THE ACCURACY OF THE INFORMATION PROVIDED IN THIS PLAN.

3. THE CITY ENGINEER AND CITY COMMISSIONERS SHALL BE RESPONSIBLE FOR THE ACCURACY OF THE INFORMATION PROVIDED IN THIS PLAN.

4. THE CITY ENGINEER AND CITY COMMISSIONERS SHALL BE RESPONSIBLE FOR THE ACCURACY OF THE INFORMATION PROVIDED IN THIS PLAN.

5. THE CITY ENGINEER AND CITY COMMISSIONERS SHALL BE RESPONSIBLE FOR THE ACCURACY OF THE INFORMATION PROVIDED IN THIS PLAN.

REVISIONS

NO.	DESCRIPTION	DATE
1	ISSUED FOR PERMIT	11.14.2018
2	ISSUED FOR PERMIT	11.14.2018
3	ISSUED FOR PERMIT	11.14.2018

DESIGNED BY: [Name]
 CHECKED BY: [Name]
 DRAWN BY: [Name]

PRELIMINARY PLANT SCHEDULE

ITEM NO.	DESCRIPTION	QUANTITY
1	CONCRETE	100
2	STEEL	200
3	WOOD	50
4	PAINT	10
5	LANDSCAPING	5

ADDISON CIRCLE STATION PHASE I

PROJECT NO. 1860-Z/ADDISON CIRCLE STATION PHASE I

DATE: 11/14/2018

LandDesign

1860-Z/ADDISON CIRCLE STATION PHASE I

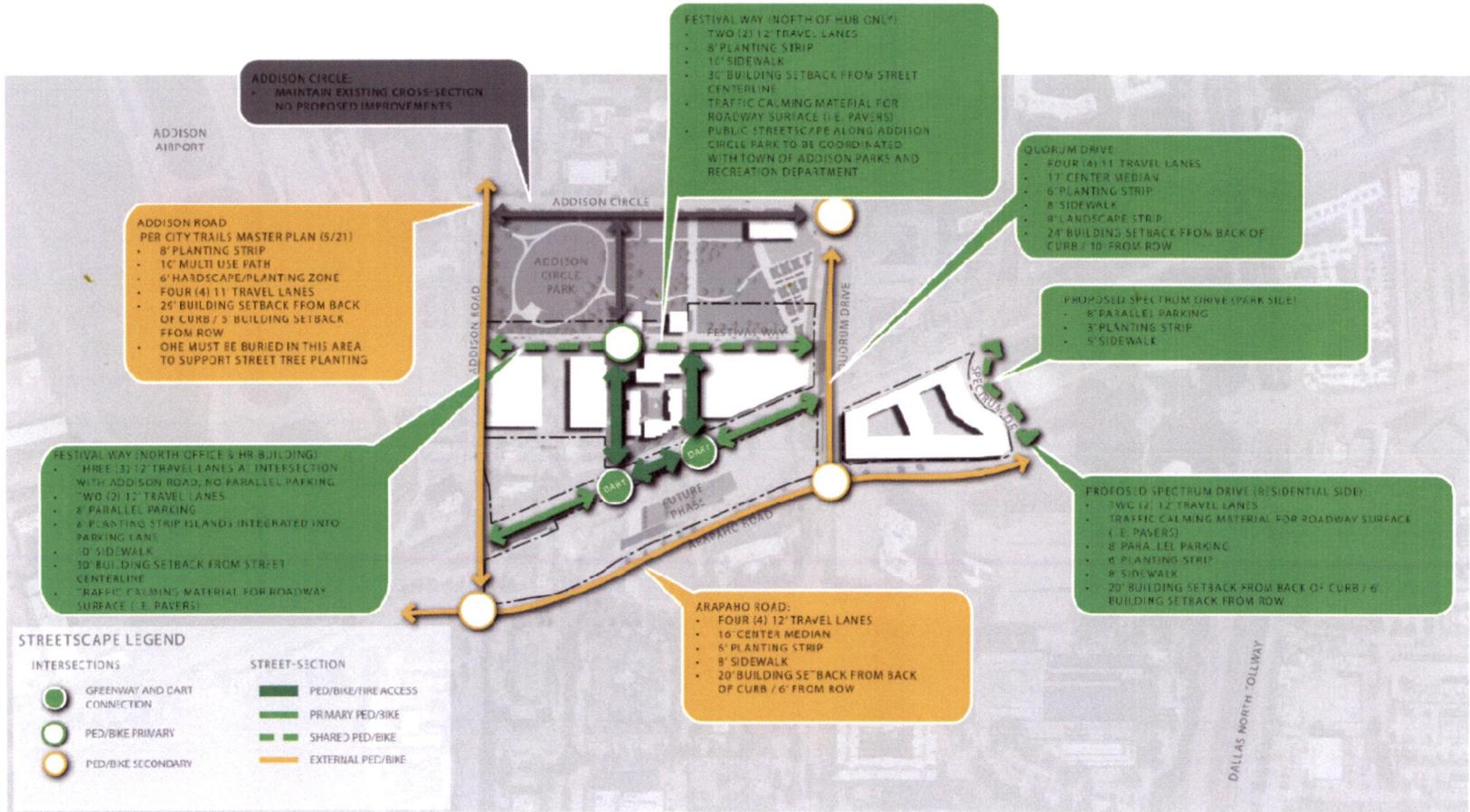
11/14/2018



MASTER STREETSCAPE ADDISON, TX • PEDESTRIAN AND BICYCLE HIERARCHY
 PN 8521168 | 01.13.2023 | AMLI RESIDENTIAL

LandDesign

EXHIBIT D



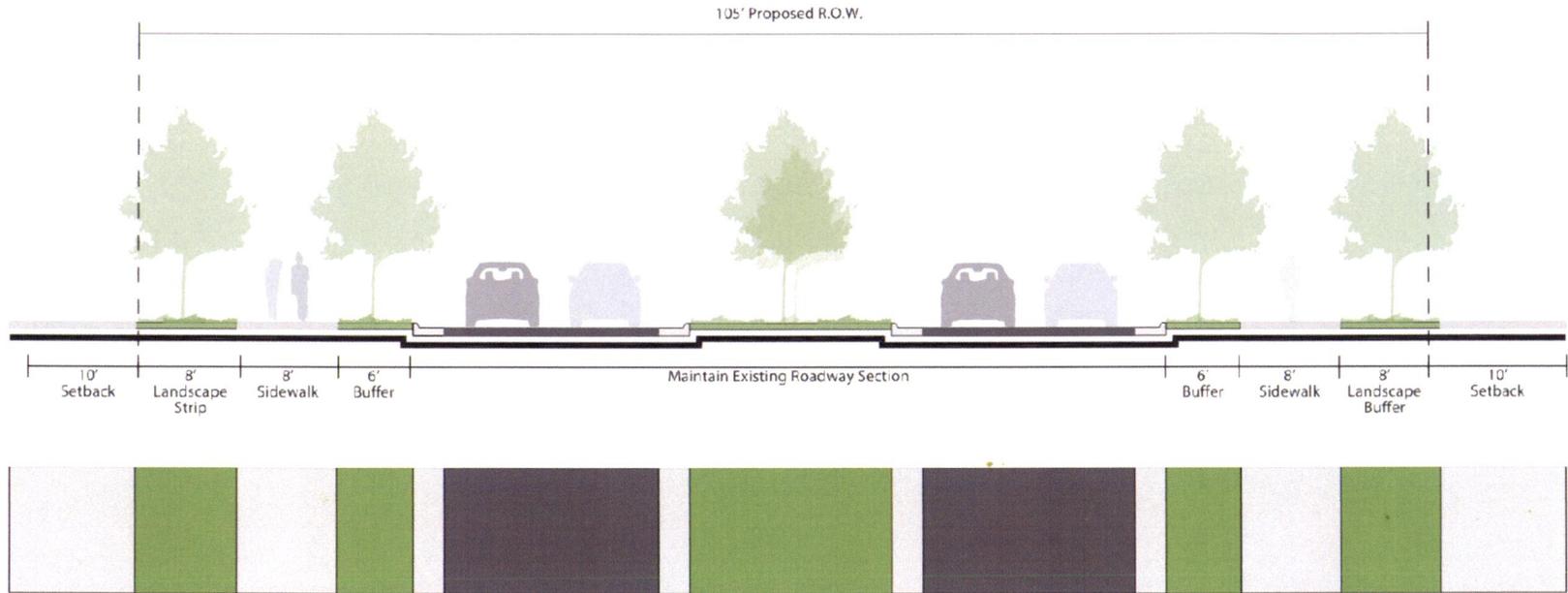
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LandDesign

EXHIBIT D

QUORUM DRIVE

- Overhead Electrical Lines (OHE) to be buried on east side of Quorum Drive.



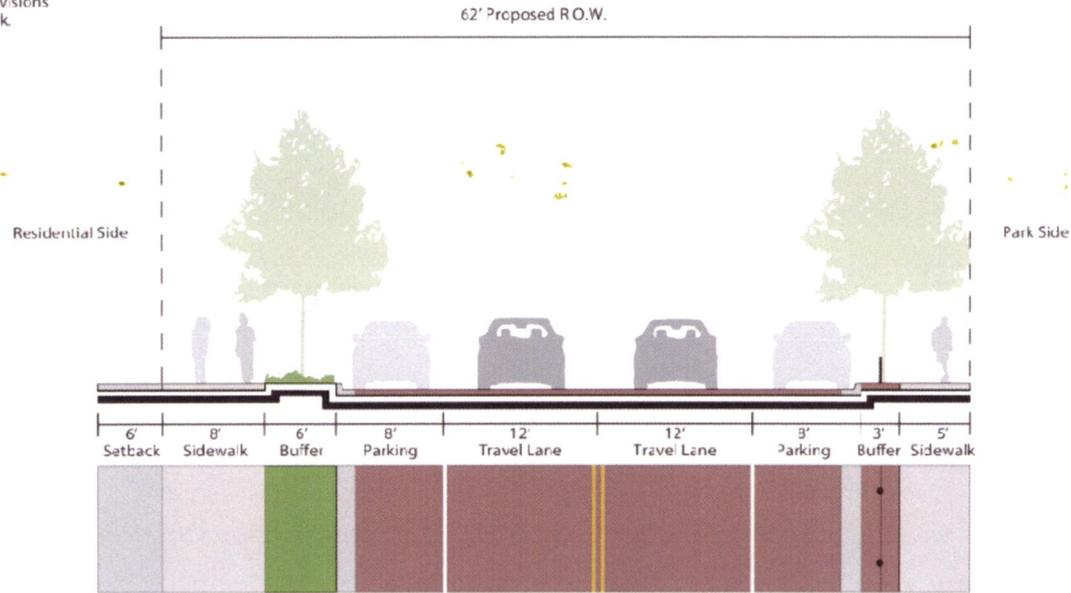
ADDISON STREETScape ADDISON, TX • STREETScape SECTIONS

PN 8521 86 | 01 | 3.2023 | AM11 RESIDENTIAL

LandDesign.

SPECTRUM DRIVE

- Recommended that the Driving Surface of Spectrum Drive be treated with a paving material (i.e.) brick pavers for additional traffic calming
- Integrate staggered landscape islands within parking to further calm traffic
- Sidewalk location and buffer width along east side of Spectrum Drive is subject to revisions based on final design of Spectrum Park.

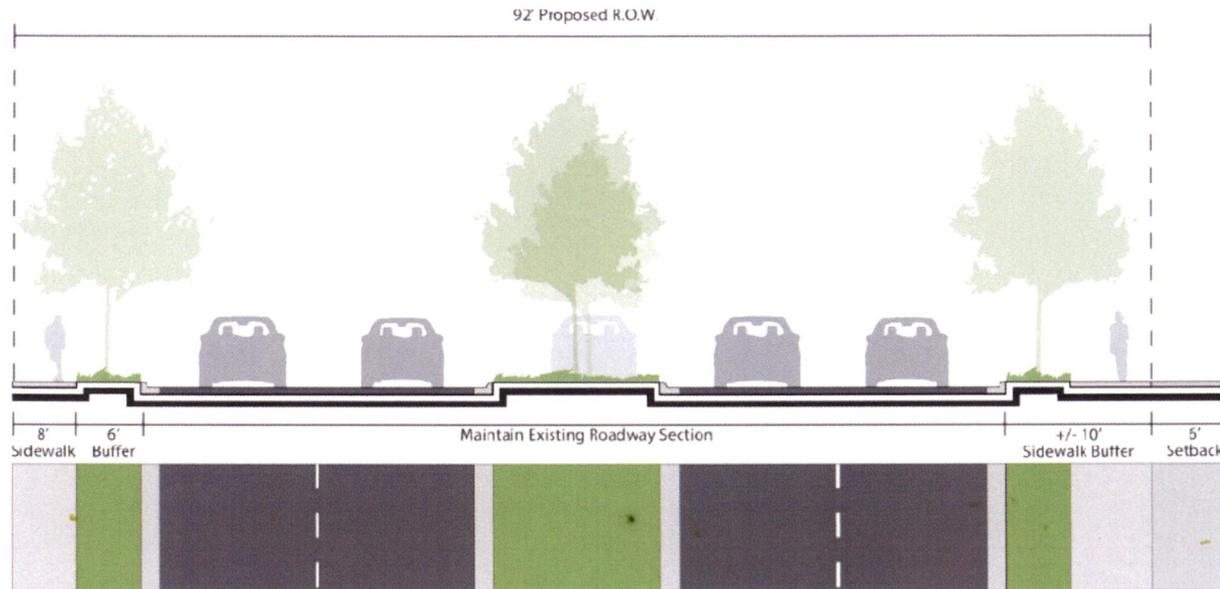


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LandDesign

EXHIBIT D

ARAPAHO ROAD

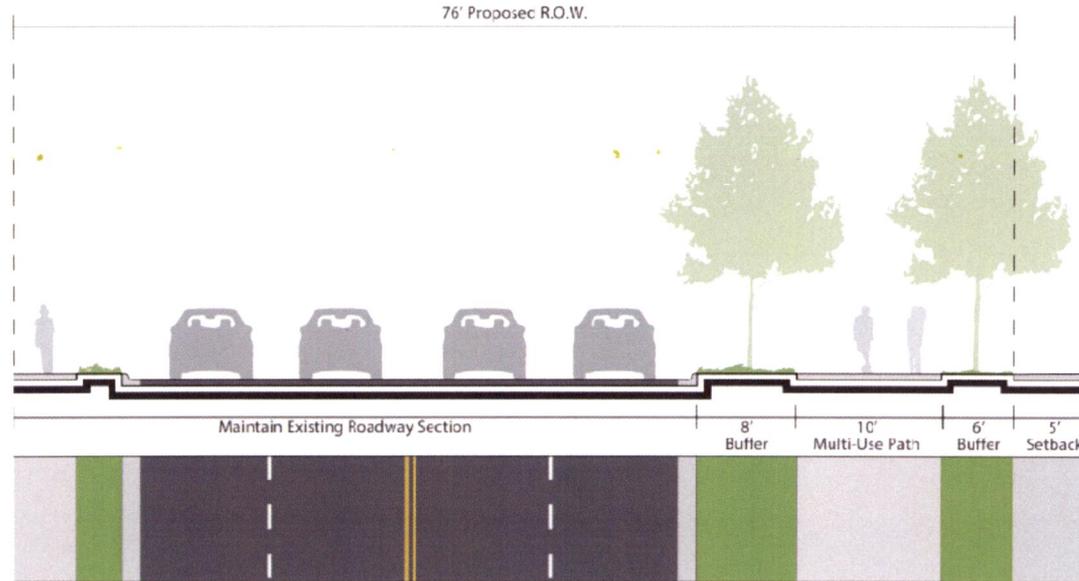


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PN E521186 | 01.13.2023 | AMLI RESIDENTIAL

LandDesign

ADDISON ROAD

- Bury existing OHE below 3' buffer strip and multi-use path



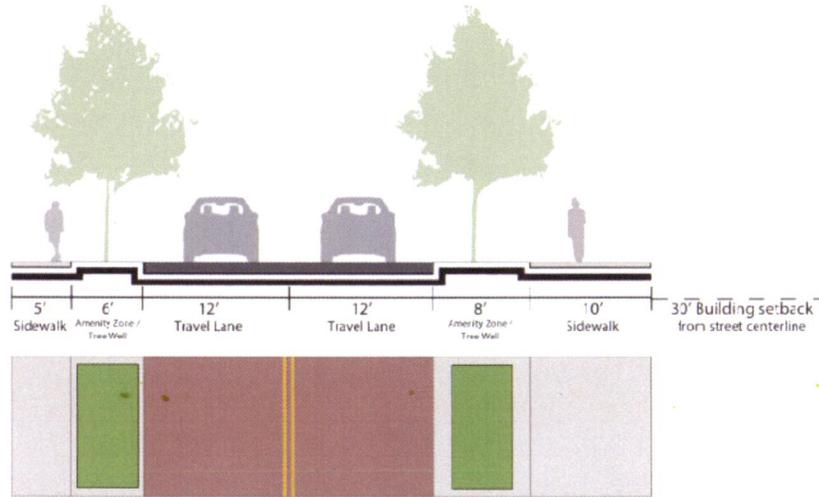
ADDISON STREETScape ADDISON, TX • STREETScape SECTIONS

PN 8521186 | 01.13.2023 | AML: RESIDENTIAL

LandDesign

FESTIVAL WAY

- Block to the north of The Hub only
- Recommended that the driving surface be treated with a paving material (ie brick pavers) for additional traffic calming
- Block is intended to be a speed table/table top condition to reduce traffic speeds for pedestrian safety.



ADDISON STREETSCAPE ADDISON, TX • STREETSCAPE SECTIONS

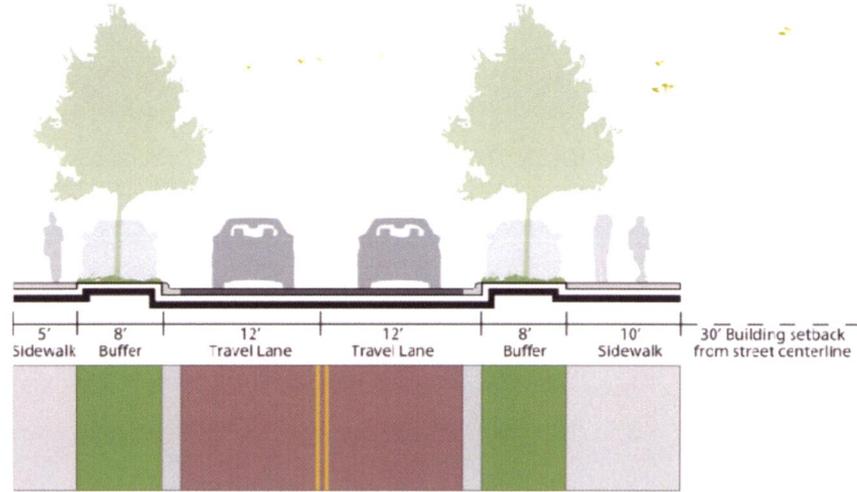
PH 8521186 | 01.13.2023 | AMLI RESIDENTIAL

LandDesign

EXHIBIT D

FESTIVAL WAY

- Block to the north of office and high rise development only
- Planting strip can fall in line with the parallel parking spaces
- Recommended that the driving surface be treated with a paving material (i.e. brick pavers) for additional traffic calming



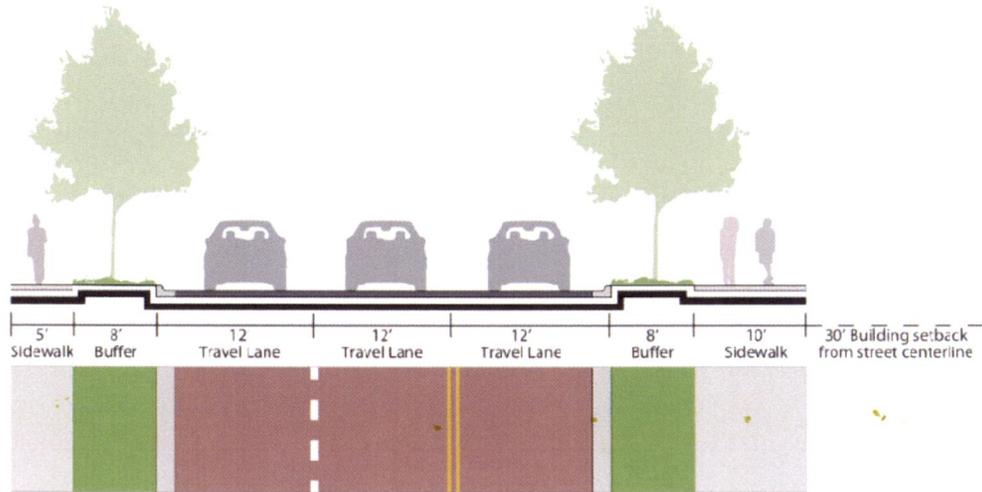
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LandDesign

FESTIVAL WAY

- Intersection of Festival Way and Addison Road
- No parallel parking spaces
- Recommended that the driving surface be treated with a paving material (i.e. brick pavers) for additional traffic calming



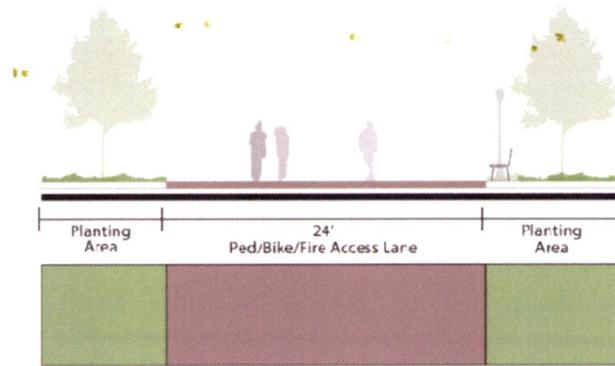
ADDISON STREETSCAPE ADDISON, TX • STREETSCAPE SECTIONS

FN 8521186 | 01.13.2023 AMLI RESIDENTIAL

LandDesign.

PED/BIKE/FIRE ACCESS

- Access points to the east and west of the entertainment block
- Recommended that the driving surface be treated with a paving material (i.e. concrete pavers, enhanced hardscape) to relate to the pedestrian intent



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LandDesign

PED/BIKE/FIRE ACCESS

- Trail adjacent to Rail Corridor at Lot 03, Block A
- Access points to the east and west of the entertainment block
- Recommended that the driving surface be treated with a paving material (i.e. concrete pavers, enhanced hardscape) to relate to the pedestrian intent

