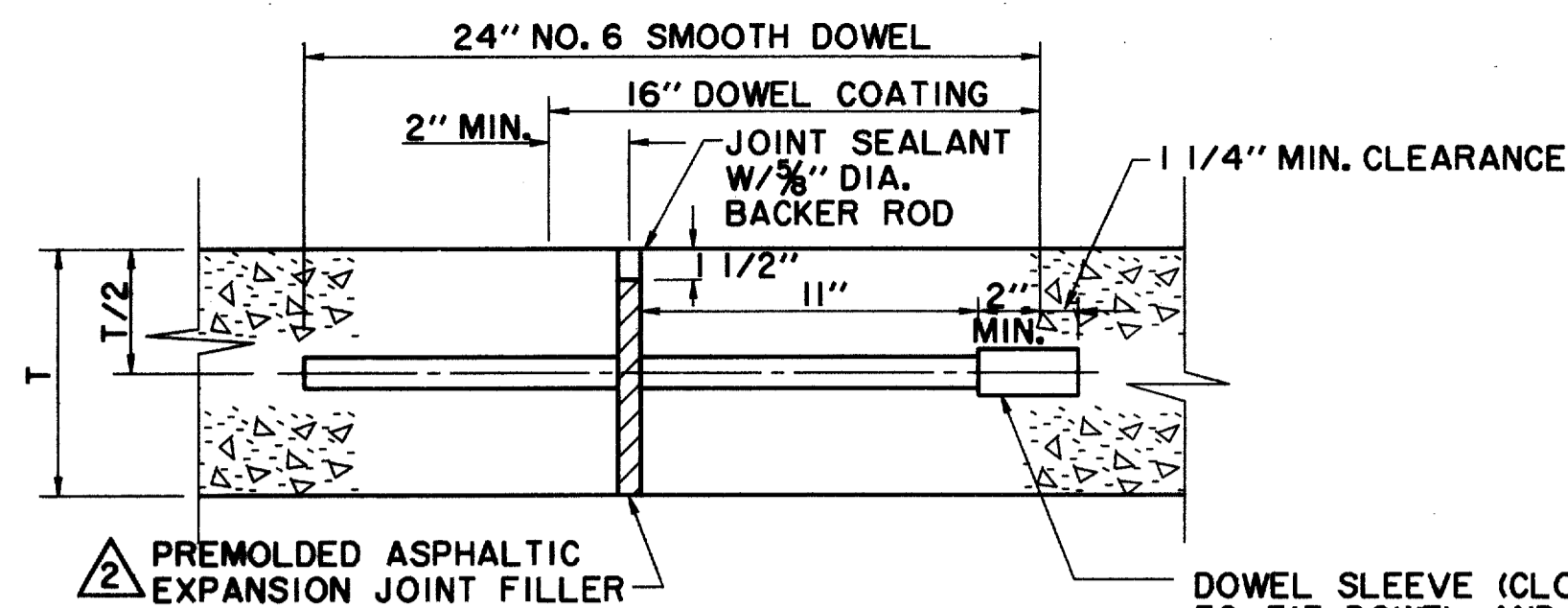


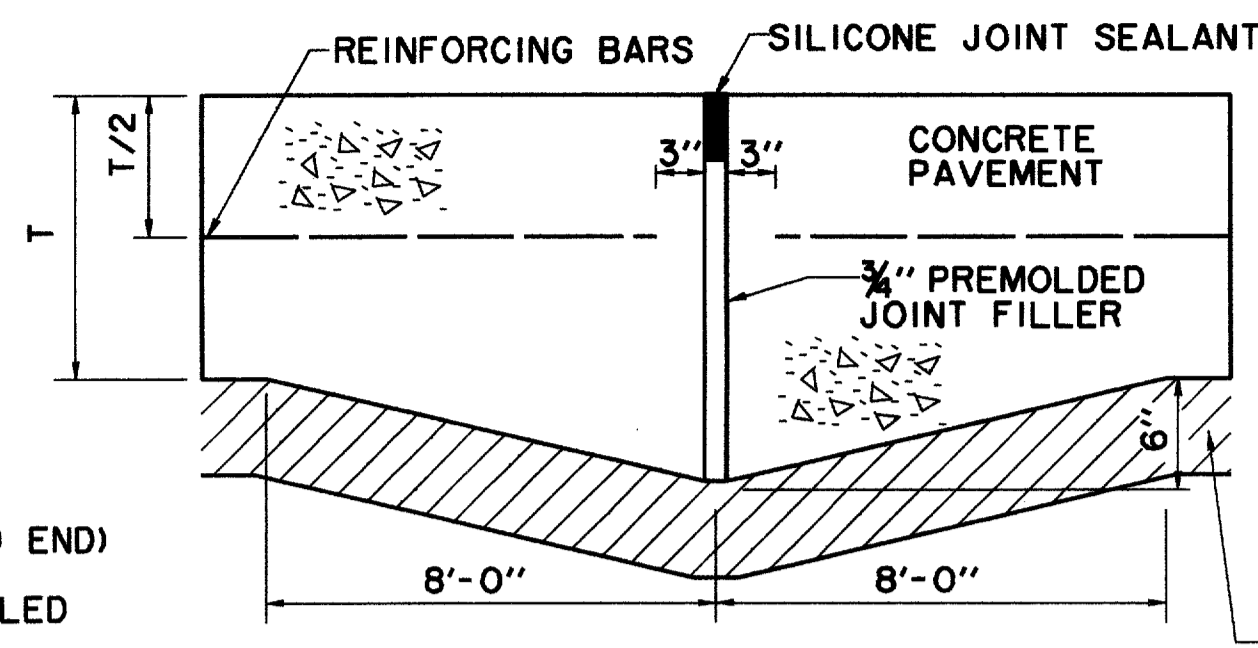
**NOTES-PAVING AND GRADING**

- UNLESS SPECIFICALLY STATED OTHERWISE IN PLANS OR CONTRACT DOCUMENTS, THE CONTROLLING SPECIFICATIONS FOR ALL WORK WITHIN PUBLIC RIGHTS-OF-WAY AND EASEMENTS SHALL BE THE STANDARD SPECIFICATIONS FOR PUBLIC WORKS CONSTRUCTION, SECOND EDITION 1987, AS AMENDED BY THE NORTH CENTRAL TEXAS COUNCIL OF GOVERNMENTS (THE "STANDARD SPECIFICATIONS"). COPIES OF THE STANDARD SPECIFICATIONS MAY BE PURCHASED BY MAIL OR OVER THE COUNTER FROM THE NORTH CENTRAL TEXAS COUNCIL OF GOVERNMENTS, 616 SIX FLAGS DRIVE, P.O. BOX DRAWER COG, ARLINGTON, TEXAS 76005-5888, PHONE METRO 817/640-3300. BULK DISCOUNTS ARE AVAILABLE. THIS DOCUMENT IS COPYRIGHTED.
- ROUGH GRADING SHALL BE ACCOMPLISHED TO WITHIN +/- 0.10 FEET OF PLAN ELEVATION (AFTER ADJUSTMENT FOR FINISH THICKNESS).
- ALL TRAFFIC CONTROL NECESSARY FOR THE WORK SHALL BE PROVIDED BY THE AFFECTED CONTRACTOR. ALL BARRICADES, WARNING SIGNS, LIGHTS, DEVICES, ETC. FOR THE GUIDANCE AND PROTECTION OF TRAFFIC AND PEDESTRIANS MUST CONFORM TO THE INSTALLATIONS SHOWN IN THE LATEST ISSUE OF THE "TEXAS MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES", AS CURRENTLY AMENDED, TEXAS DEPARTMENT OF TRANSPORTATION.
- ALL FILL SHALL BE PLACED IN MAXIMUM 8-INCH LIFTS COMPACTED TO 95% OF STANDARD PROCTOR DENSITY BETWEEN 0% AND +3% OF OPTIMUM MOISTURE CONTENT.
- ALL TREES, STUMPS, BRUSH, GRASSES AND SURFACE ORGANICS WITHIN PROPOSED RIGHT-OF-WAY ARE TO BE REMOVED AND PROPERLY DISPOSED OF OFF-SITE. TREE REMOVAL PERMITS, IF REQUIRED, WILL BE OBTAINED BY THE OWNER.
- EXISTING UTILITY POLES, IF ANY, WILL BE REMOVED OR RELOCATED BY THE UTILITY COMPANIES THROUGH COORDINATION BY THE OWNER. CONTRACTOR SHALL BRING TO THE OWNER'S ATTENTION ANY FACILITIES THAT APPEAR TO BE IN CONFLICT SO THAT THE OWNER HAS SUFFICIENT TIME TO ACCOMPLISH THE NECESSARY RELOCATIONS.
- WHERE DEEP VERTICAL EXCAVATIONS (IN EXCESS OF 3 FEET) ARE INDICATED, CUTS SHALL BE LAID BACK AT A STABLE SLOPE (ON OWNER'S PROPERTY) UNTIL WALLS ARE CONSTRUCTED. BACKFILL MATERIAL SHALL BE STOCKPILED ON-SITE AT THE DIRECTION OF THE OWNER.
- ALL REINFORCING STEEL AND DOWEL BARS IN PAVEMENT SHALL BE SUPPORTED AND MAINTAINED AT THE CORRECT CLEARANCES BY THE USE OF BAR CHAIRS OR OTHER APPROVED SUPPORT.
- THE USE OF WOOD FORMS FOR PAVEMENT CONSTRUCTION WILL BE PERMITTED.
- DUMMY JOINTS SHALL BE SAWS IN THE PAVEMENT IN THIS PROJECT. DUMMY JOINTS WILL BE SPACED APPROXIMATELY 20 FEET ON CENTERS UNLESS DIRECTED OTHERWISE BY THE ENGINEER. SPACING VARIATIONS SHALL BE MADE AT BLOCKOUTS, CONSTRUCTION JOINTS, AND STREET INTERSECTIONS TO LINE UP WITH EXISTING PAVING JOINTS AS DIRECTED BY THE ENGINEER. THE SPACING BETWEEN ANY JOINT SHALL NOT BE LESS THAN 10 FEET NOR MORE THAN 30 FEET. ALL DUMMY JOINTS SHALL BE SAWS NOT LATER THAN 12 HOURS AFTER THE PLACEMENT OF THE PAVEMENT.
- BARRIER-FREE RAMPS WILL BE BUILT WITH THIS PROJECT. LOCATIONS MAY BE ADJUSTED AS DIRECTED BY THE ENGINEER TO CLEAR OBSTRUCTIONS.
- VARIABLE HEIGHT CURB AT INTERSECTIONS AND SLOPING CURBS AT DRIVES SHALL BE BUILT TO MATCH FUTURE BARRIER-FREE RAMPS PER PLANS. NO SEPARATE PAY ITEMS ARE PROVIDED.
- TWO-WAY TRAFFIC MUST BE MAINTAINED AT ALL TIMES ON PUBLIC STREETS ADJACENT TO THIS PROJECT UNLESS SPECIFIED OTHERWISE IN THE PLANS AND SPECIFICATIONS. THE TRAVELWAY WIDTH SHALL NOT BE LESS THAN 11 FEET.
- SEE TYPICAL SECTIONS SHEET FOR ADDITIONAL MATERIAL SPECIFICATIONS.
- ALL CURBS SHALL BE PLACED INTEGRAL WITH PAVEMENT.
- CURBS SHALL MEET THE SAME COMPRESSIVE STRENGTH AS SPECIFIED FOR THE CONCRETE PAVEMENT.
- BAR LAPS SHALL BE 30 DIAMETERS.



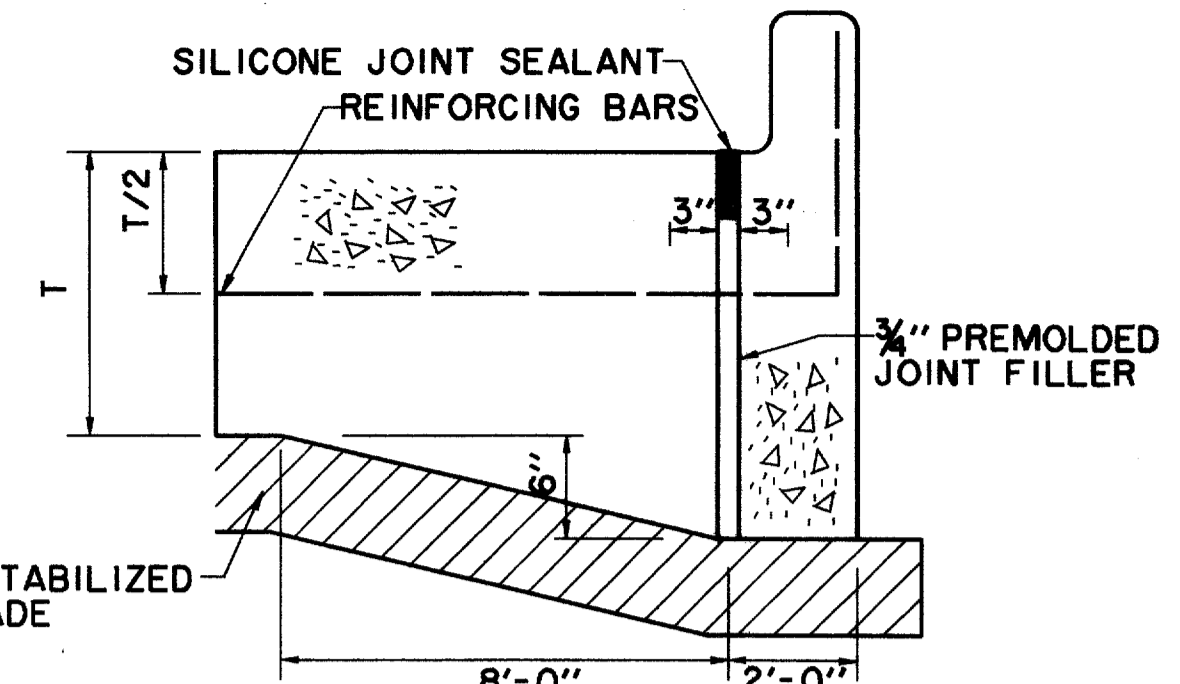
**TRANSVERSE EXPANSION JOINT**

N.T.S.  
(SPACED 600 FT. MAXIMUM, LOCATE AT INTERSECTIONS)  
NOTE:  
DOWELS AND REINFORCING BARS SHALL BE SUPPORTED BY AN APPROVED DEVICE.



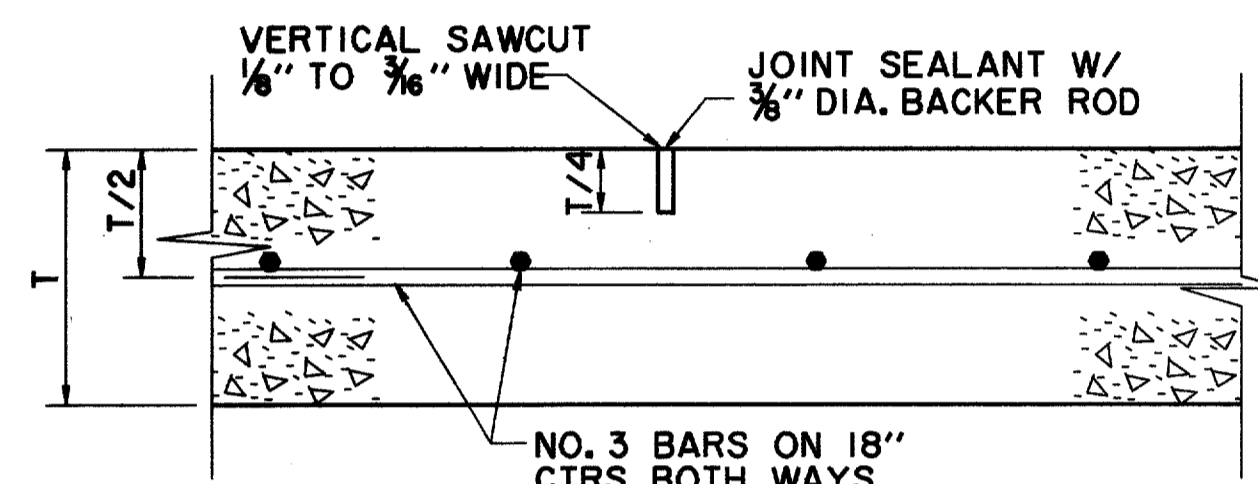
**THICKENED EDGE ISOLATION JOINT**

N.T.S.



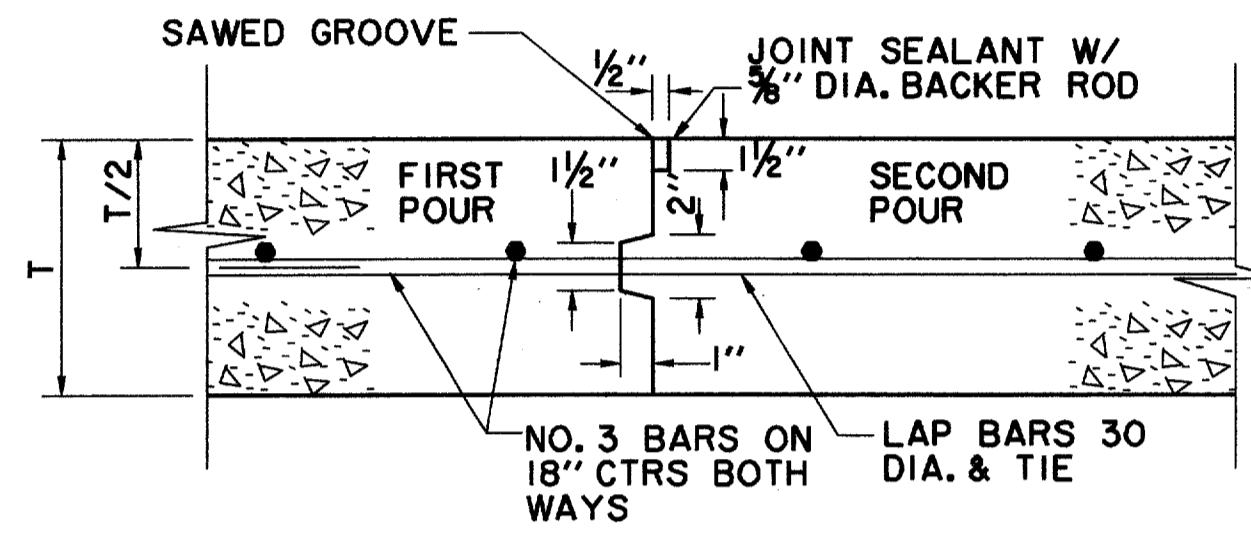
**THICKENED EDGE ISOLATION JOINT AT FUTURE ADDISON CIRCLE**

N.T.S.



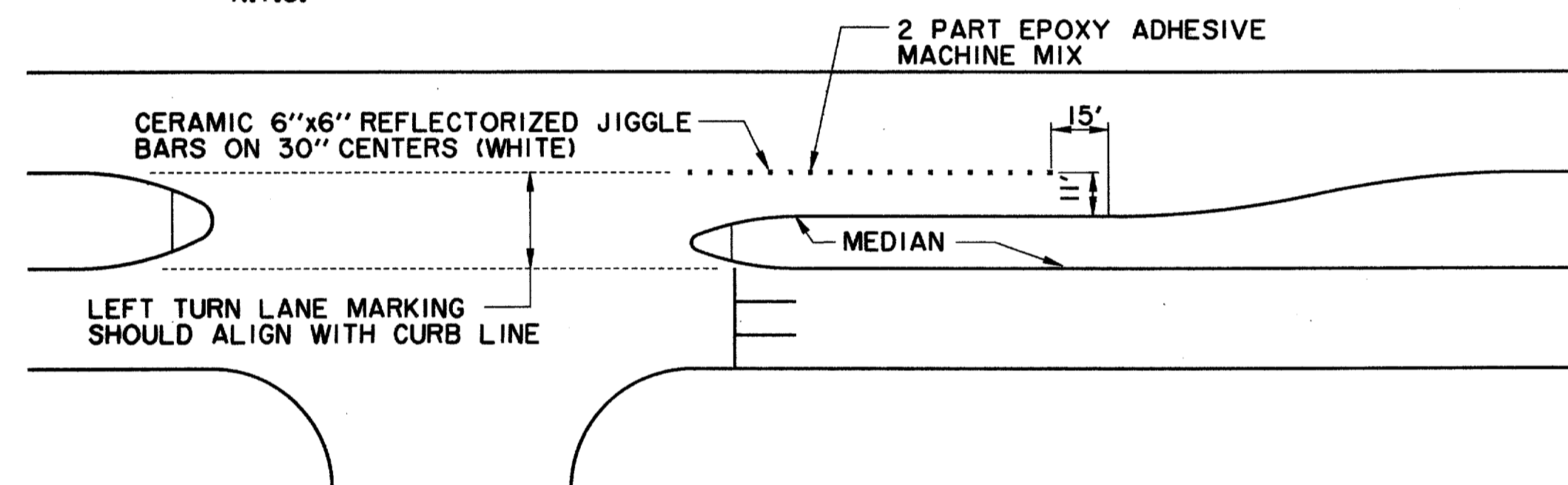
**SAWED DUMMY JOINT**

N.T.S.



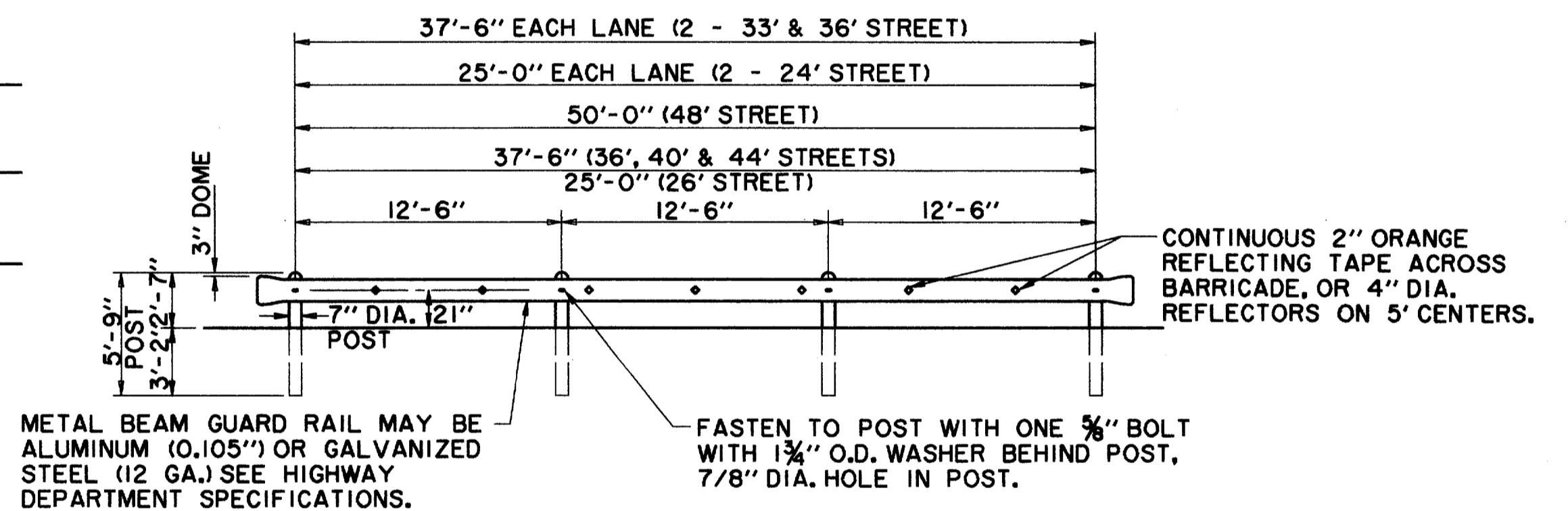
**CONSTRUCTION JOINT**

N.T.S.



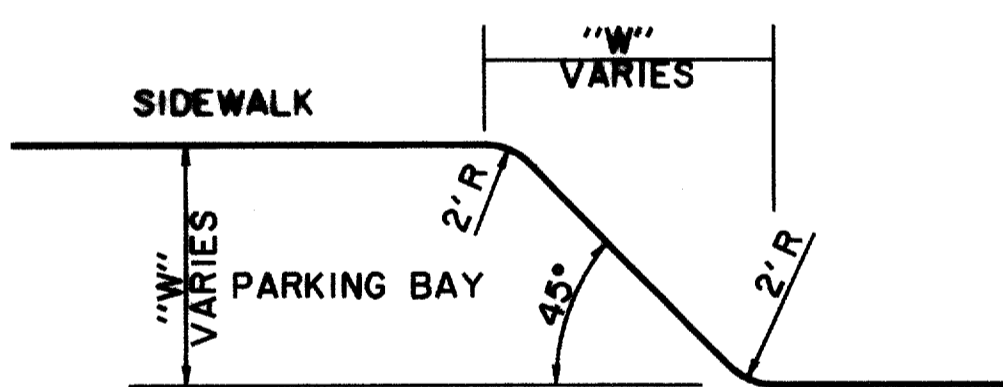
**STANDARD LEFT TURN LANE MARKINGS**

N.T.S.



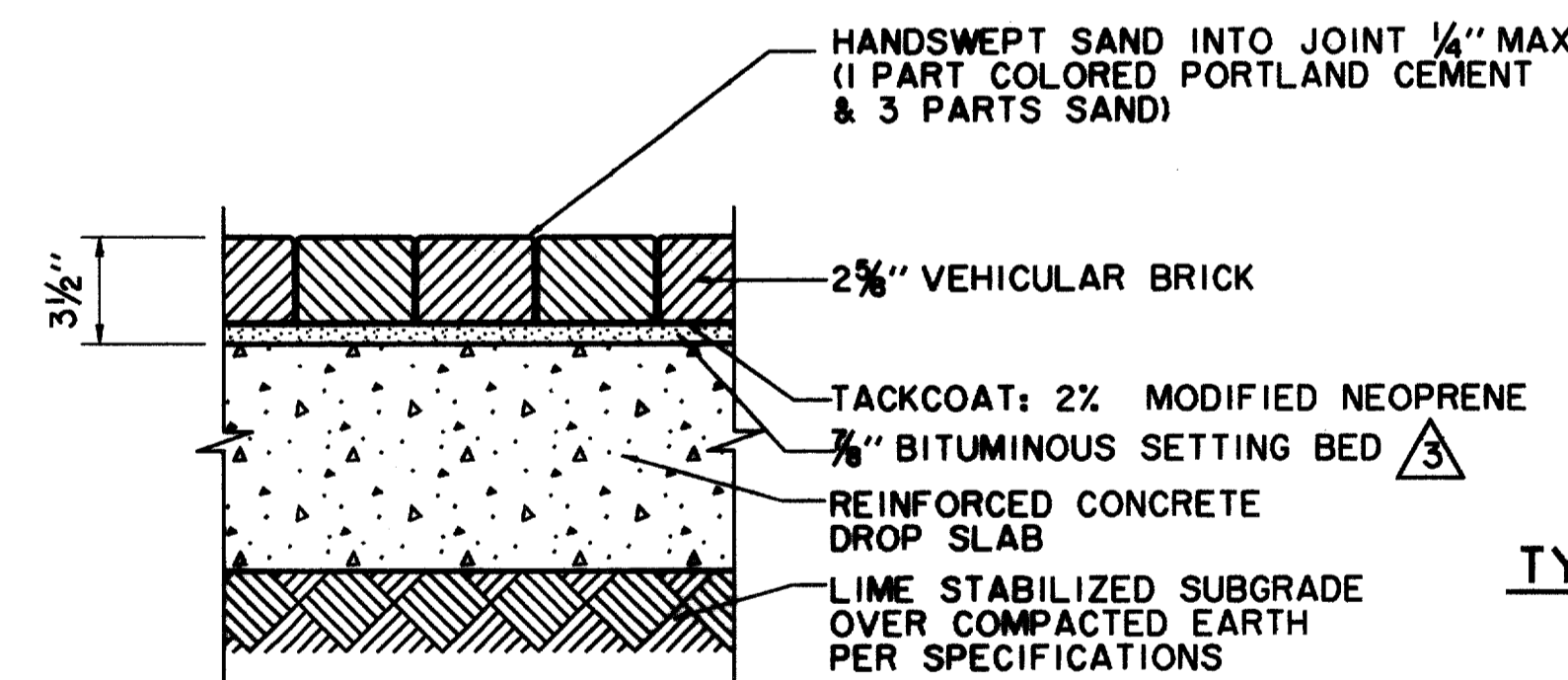
**BARRICADE DETAIL FRONT ELEVATION**

N.T.S.



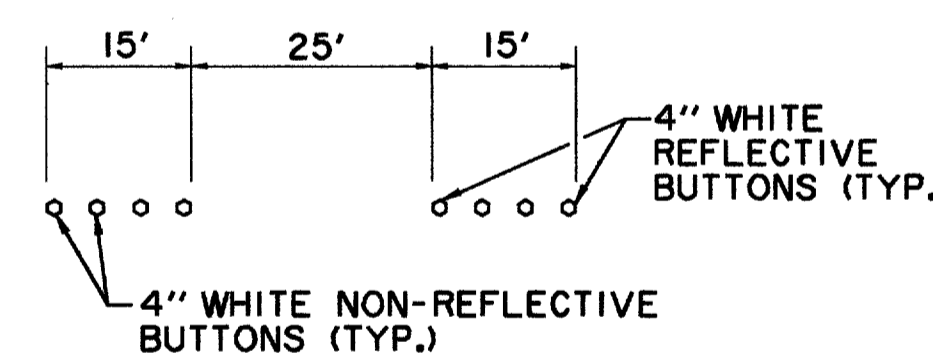
**PARKING TRANSITION DETAIL**

N.T.S.



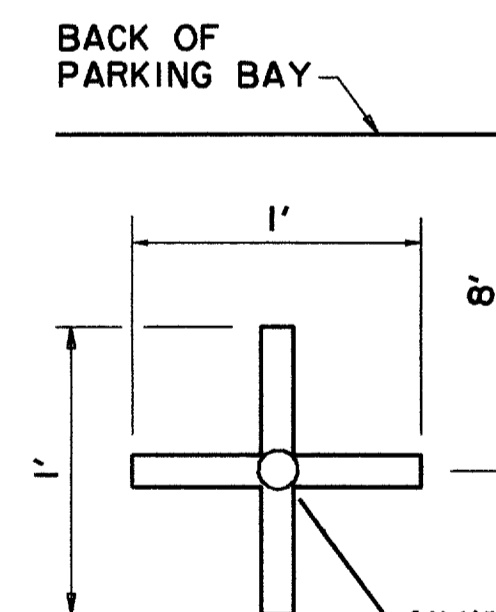
**VEHICULAR BRICK INSTALLATION IN VEHICULAR AREAS**

N.T.S.



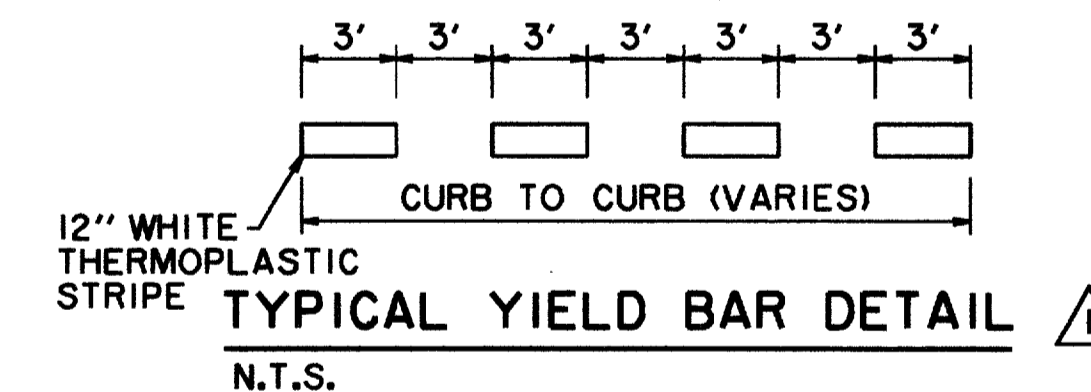
**TYPICAL LANE STRIPING LAYOUT**

N.T.S.



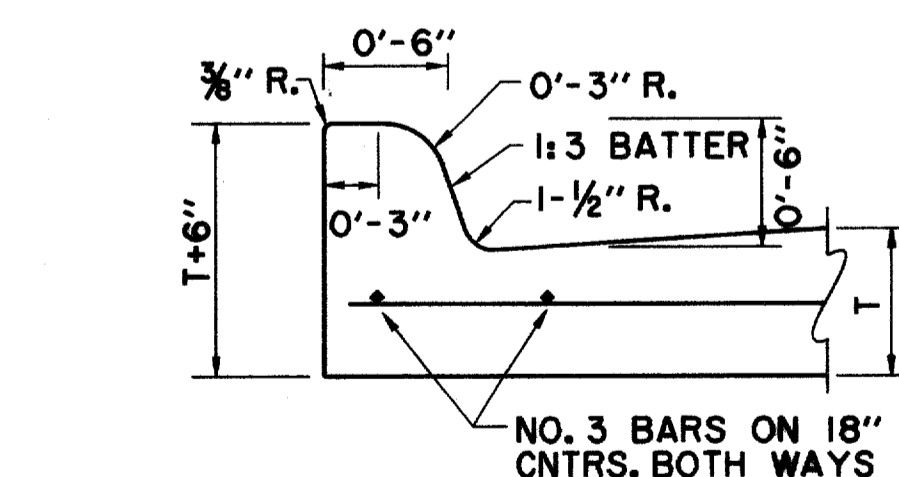
**TYPICAL PARKING STRIPE DETAIL (QUORUM DRIVE)**

N.T.S.



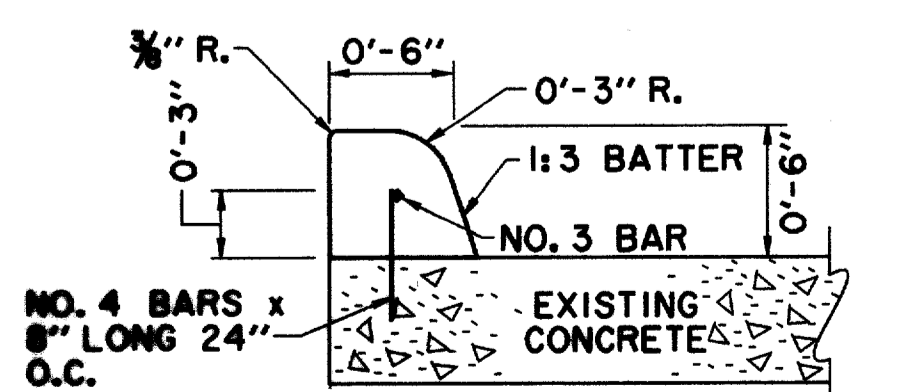
**TYPICAL YIELD BAR DETAIL**

N.T.S.



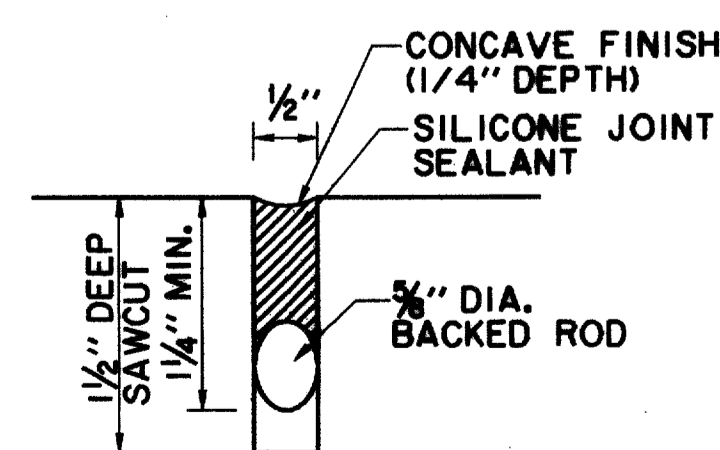
**INTEGRAL CURB AND GUTTER**

N.T.S.



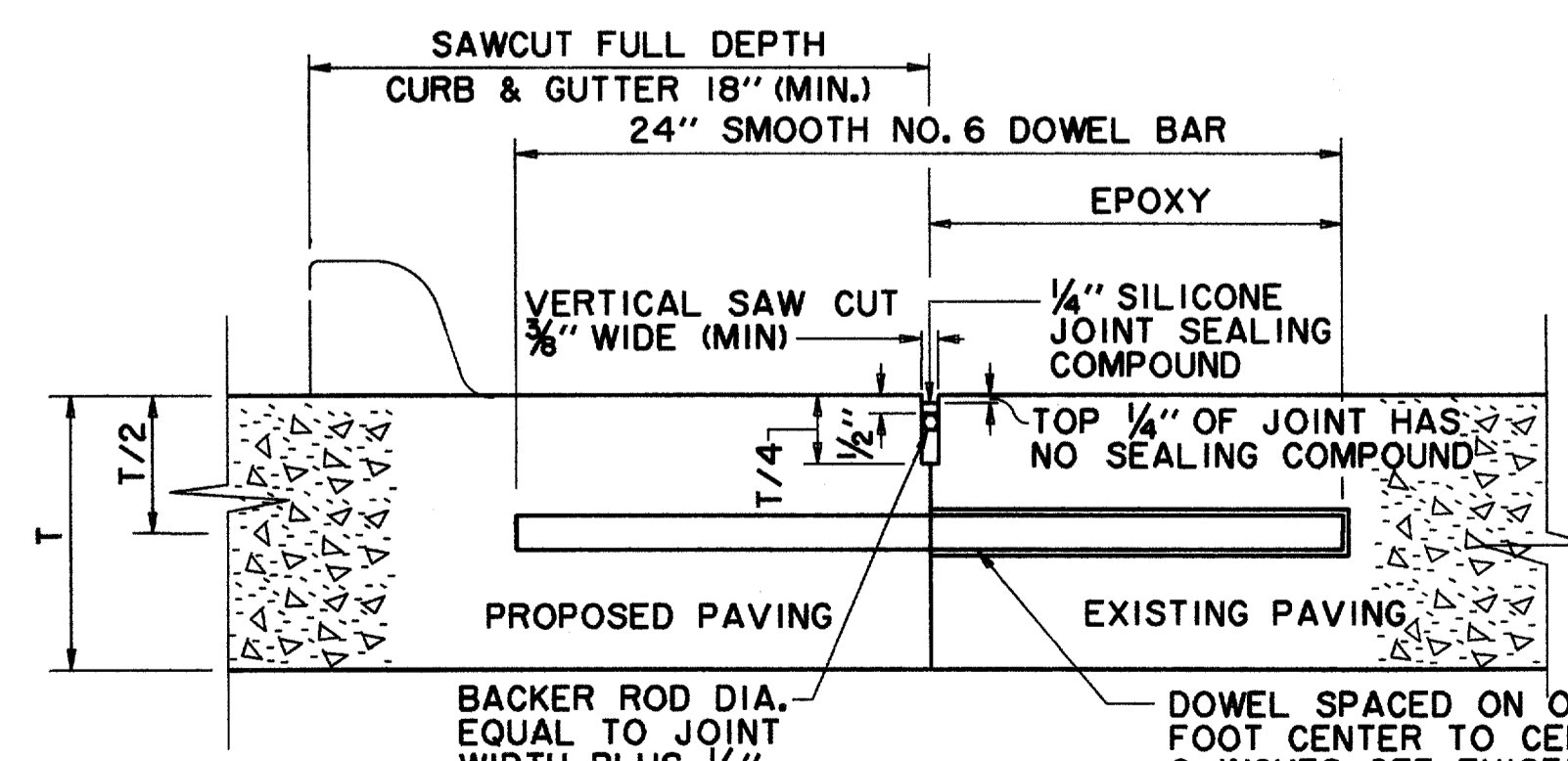
**6 INCH DOWELLED ON CURB**

N.T.S.



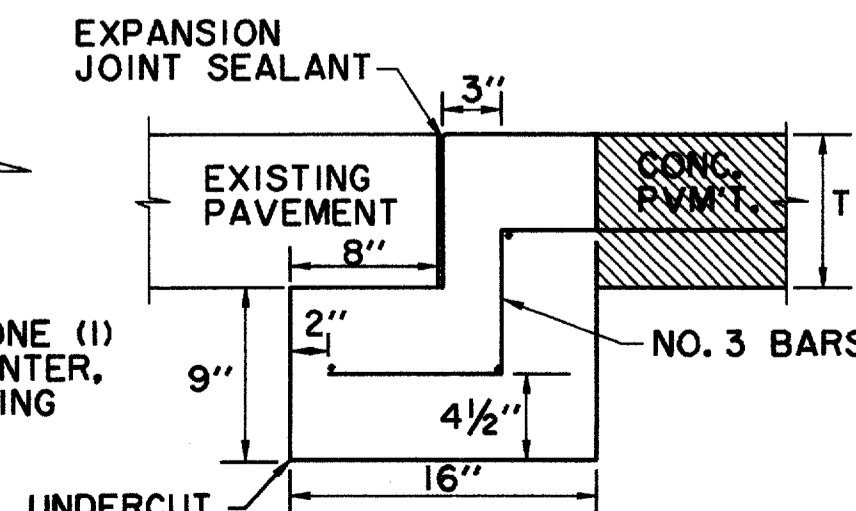
**TYPICAL JOINT DETAIL**

N.T.S.



**LONGITUDINAL BUTT JOINT**

N.T.S.



**STREET HEADER**

N.T.S.

RECORD DRAWING 5/1/98

3 CORRECTED BITUMINOUS SETTING BED THICKNESS 11/15/96

2 ADDENDUM #4 2/16/96

1 ADDENDUM #2 1/22/96

ISSUED FOR CONSTRUCTION MARCH 28, 1996

THE SEAL APPEARING ON THIS DOCUMENT WAS AUTHORIZED BY KENNETH A. ROBERTS, P.E. 55446 ON MARCH 28, 1996.



**PAVING DETAILS**

**ADDISON CIRCLE**

**PHASE I PUBLIC INFRASTRUCTURE**

**TOWN OF ADDISON, TEXAS**

Huitt-Zollars, Inc./Engineering/Architecture  
Dallas, Fort Worth, Houston, El Paso, Phoenix, Tustin, Ontario, San Clemente

DESIGN	DRAWN	APPR.	SCALE	DATE	PROJECT NO.	NO.
HZI	HZI	KAR	N.T.S.	JAN 96	1822-04	32