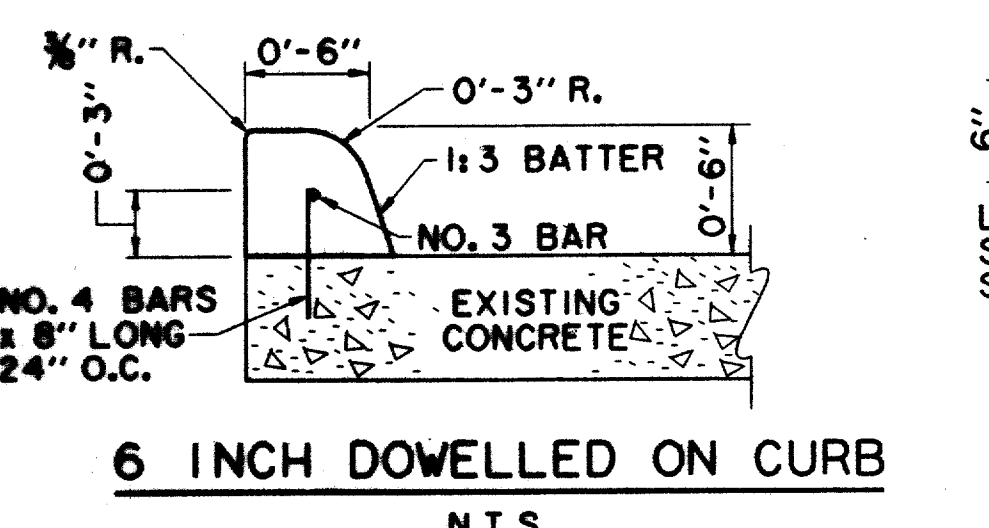
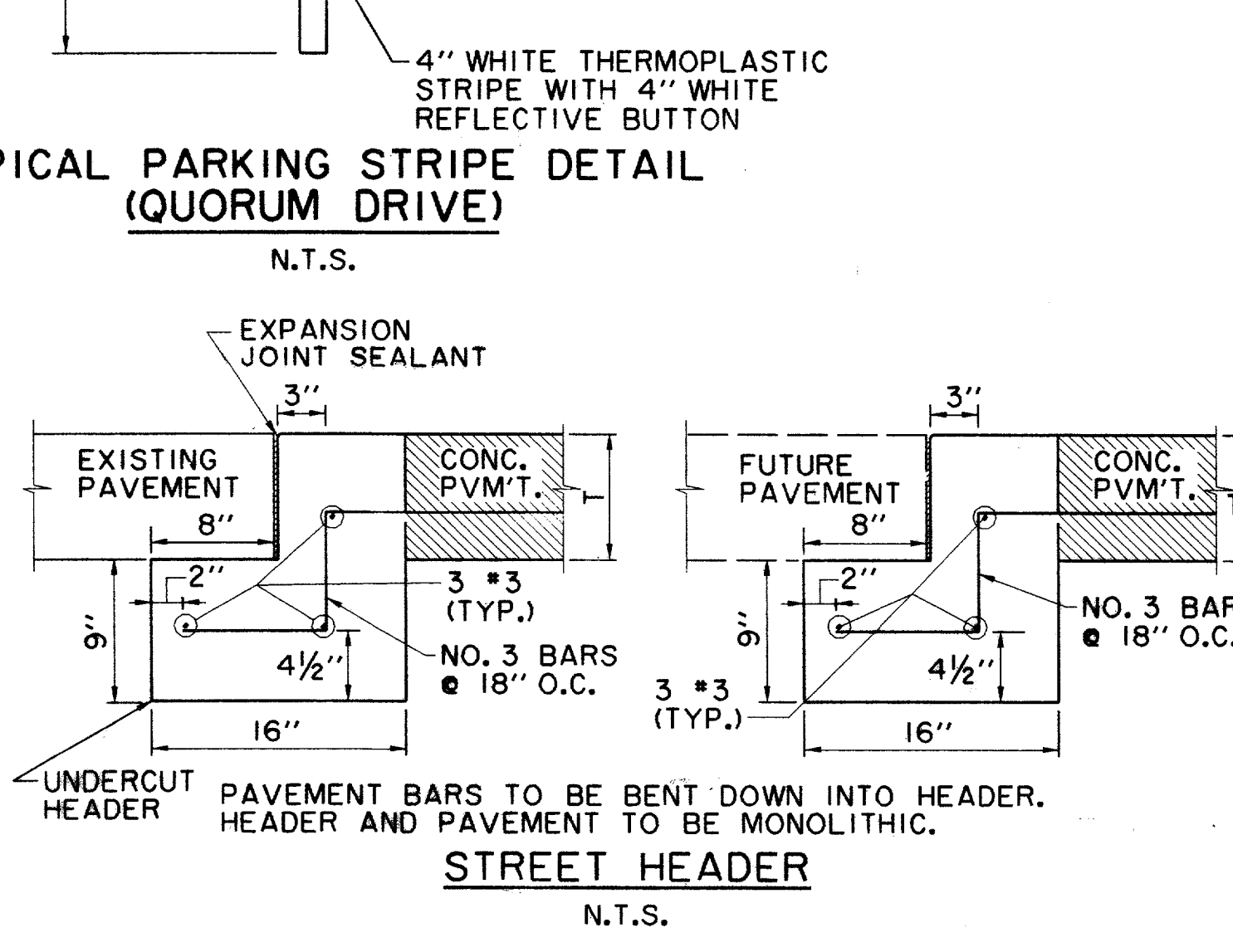
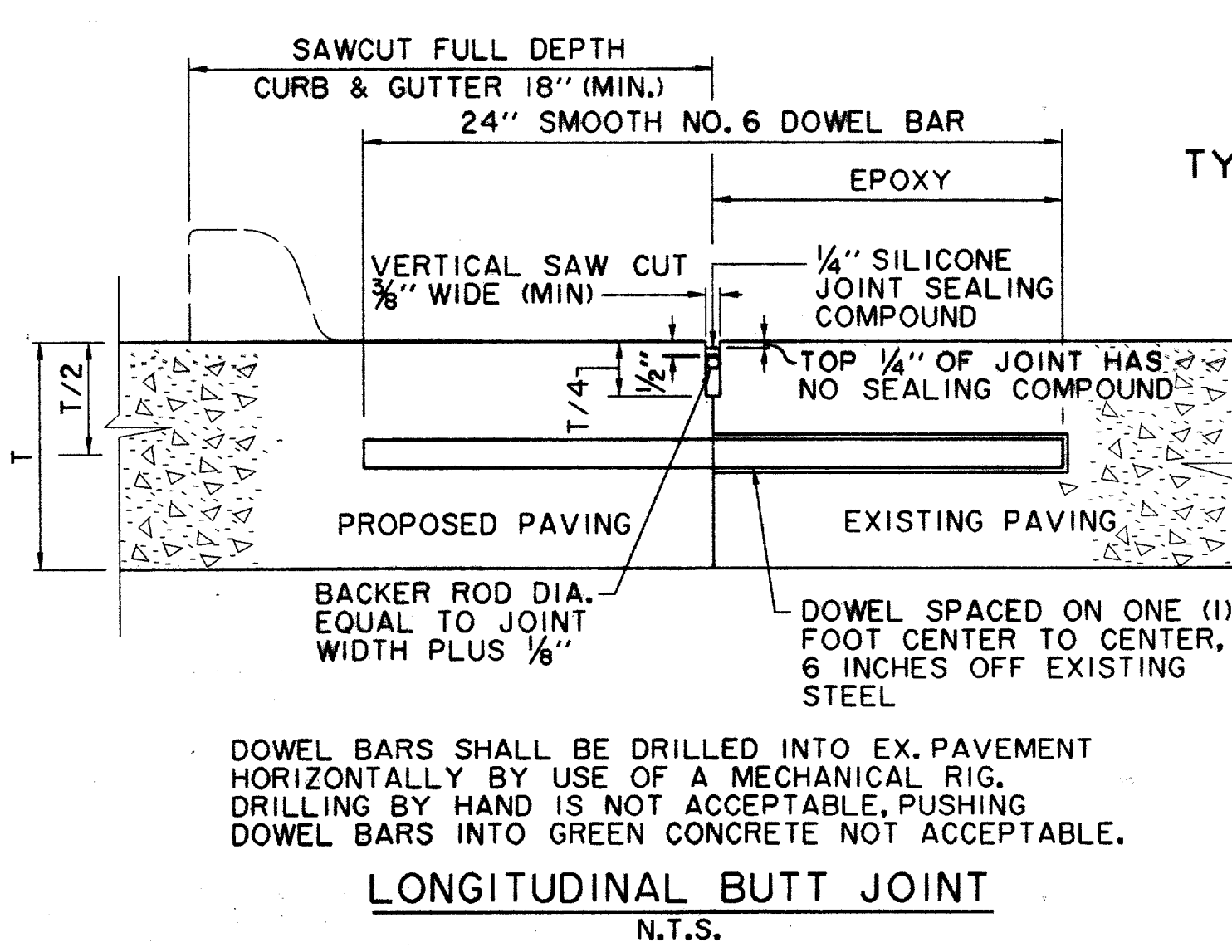
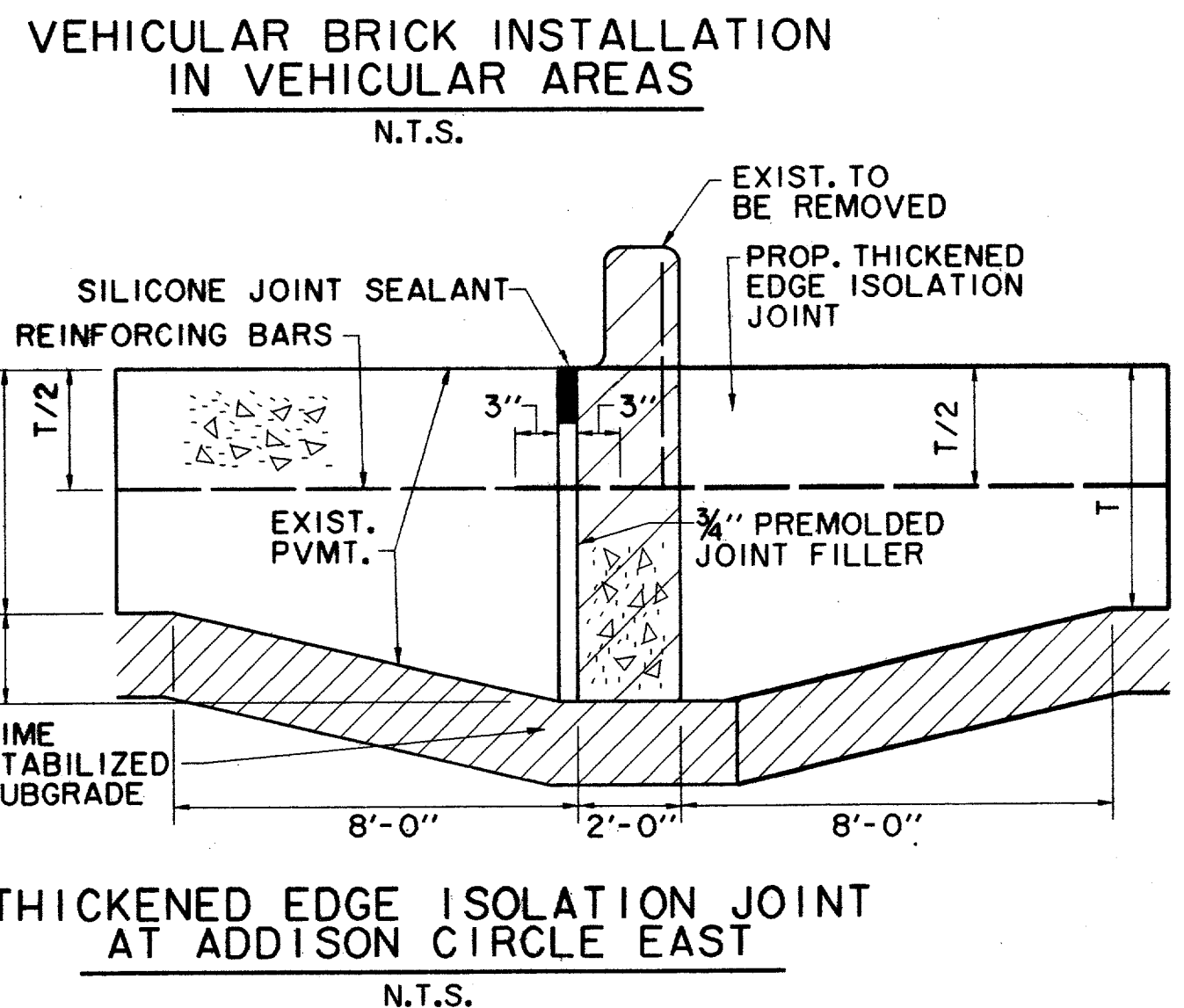
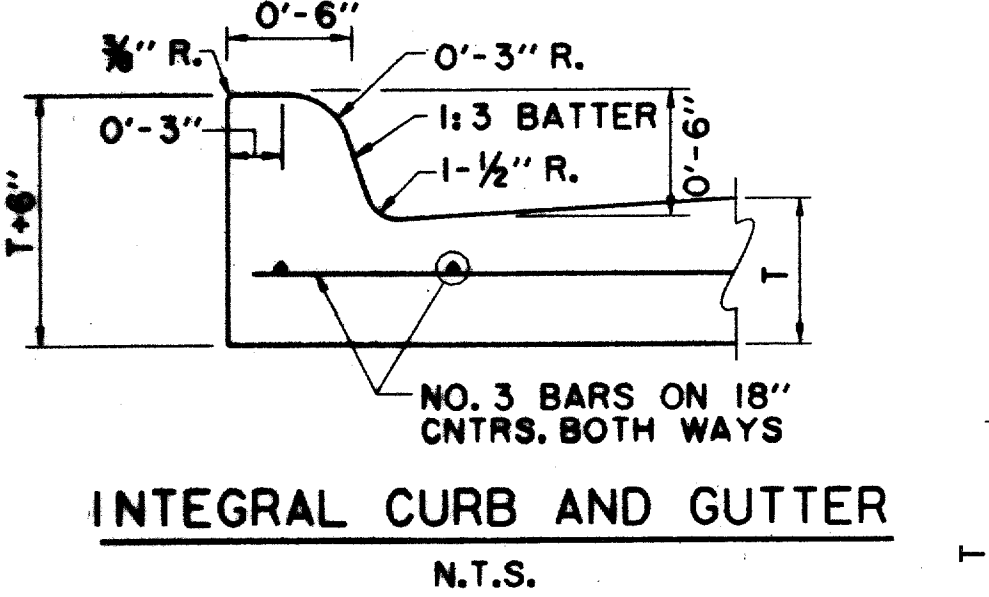
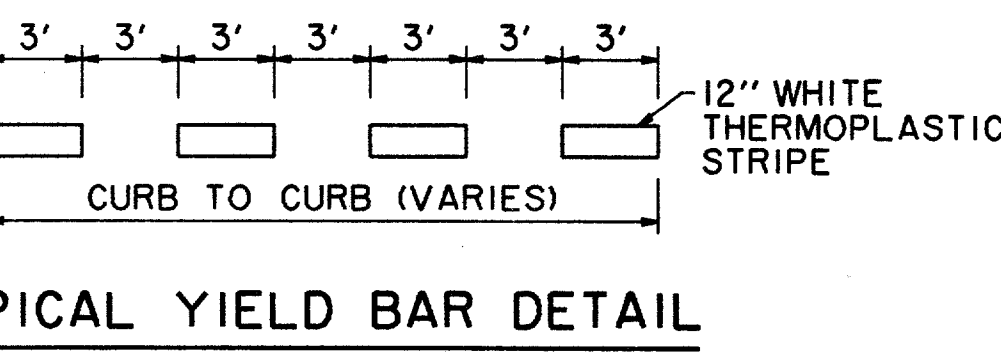
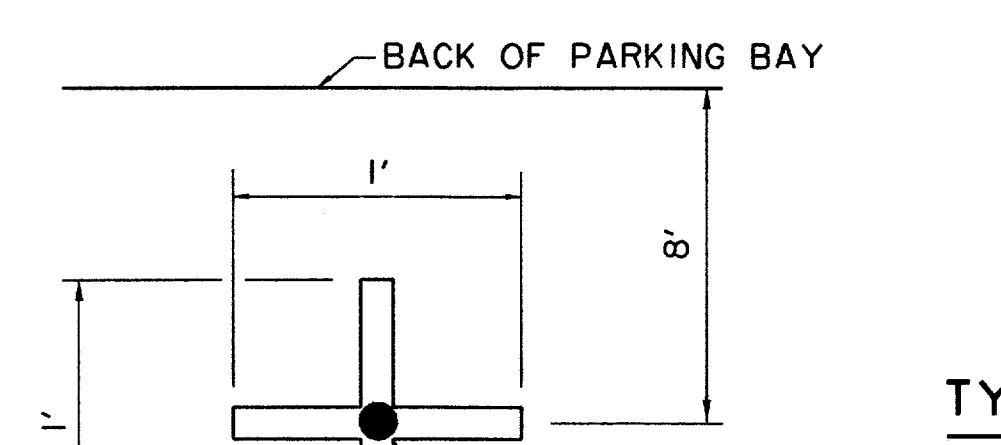
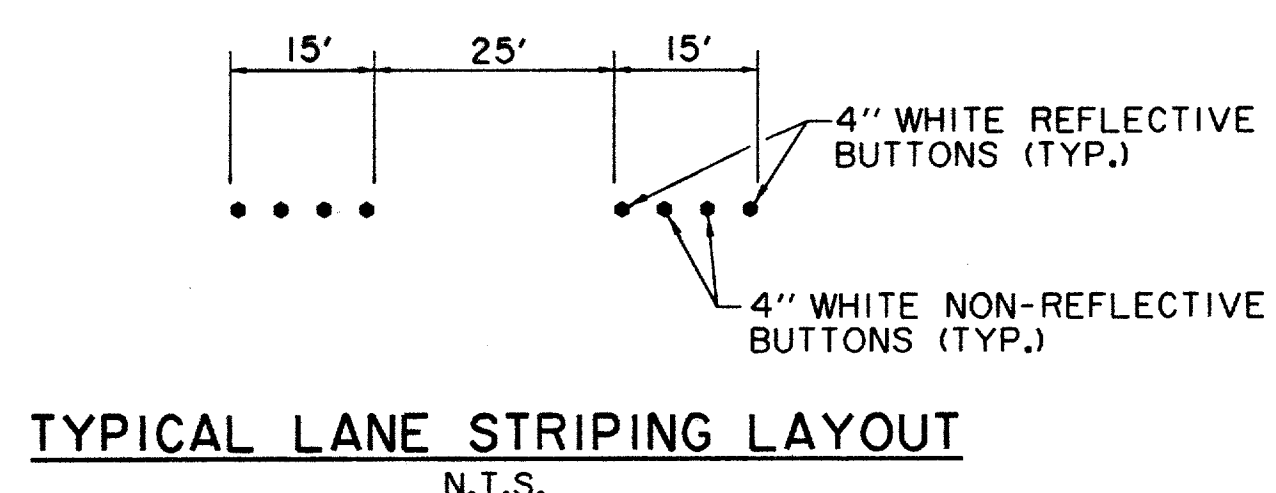
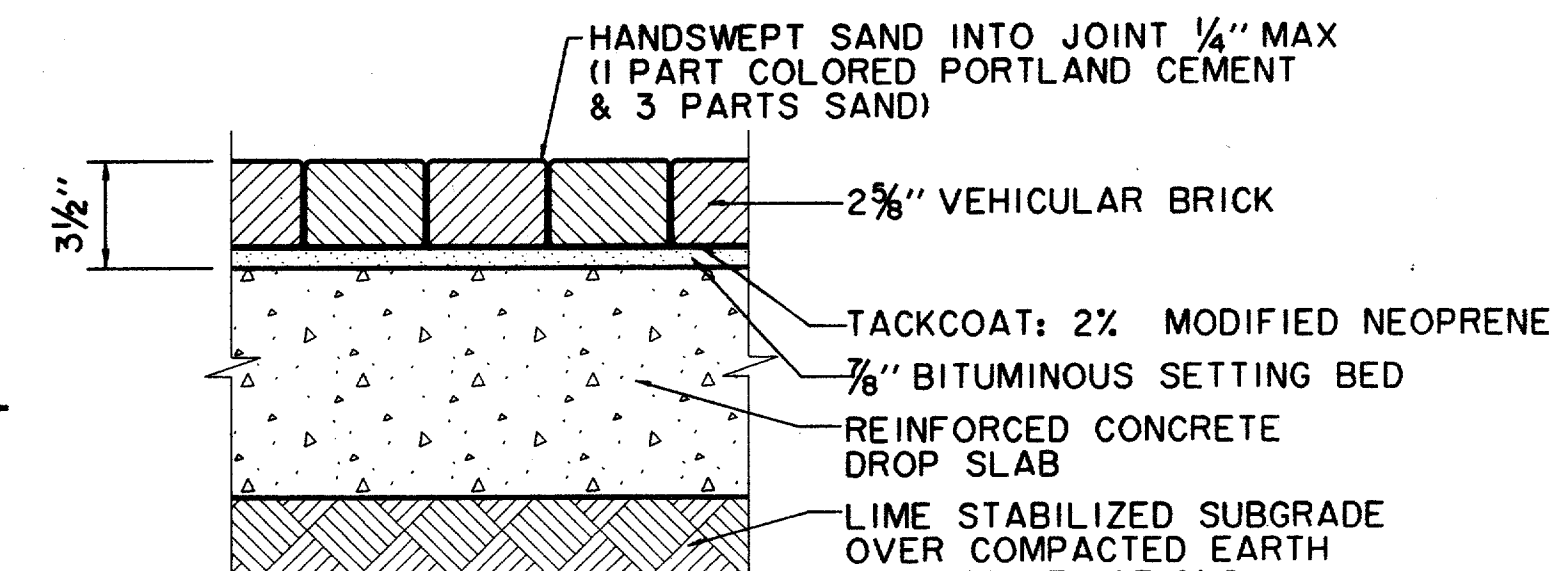
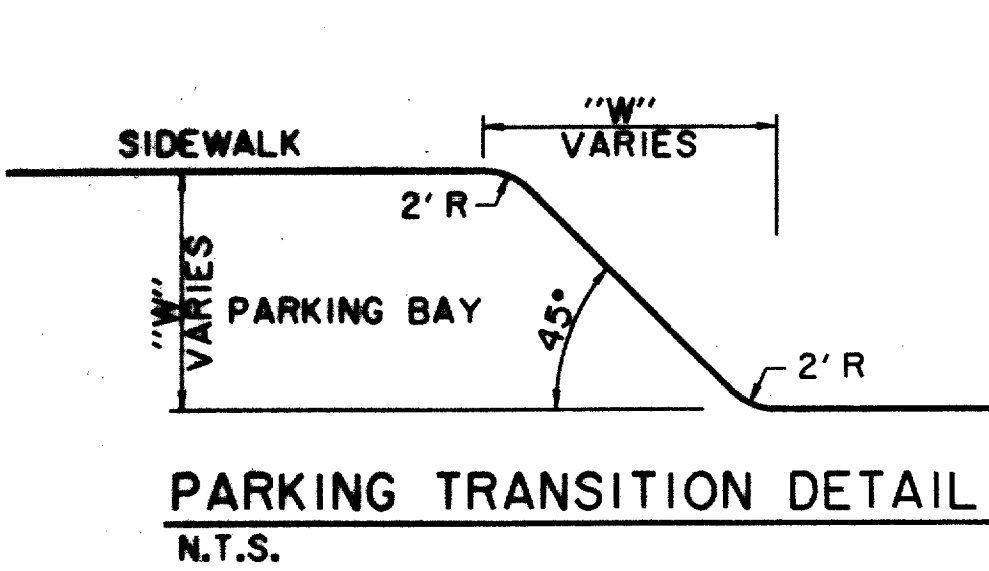
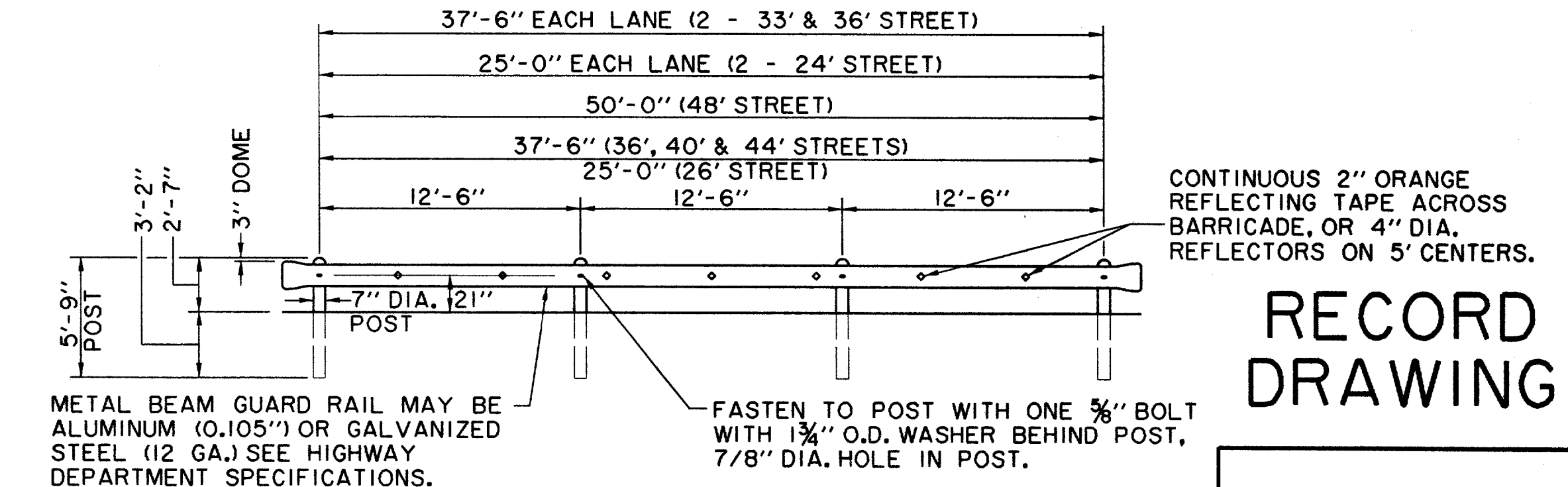
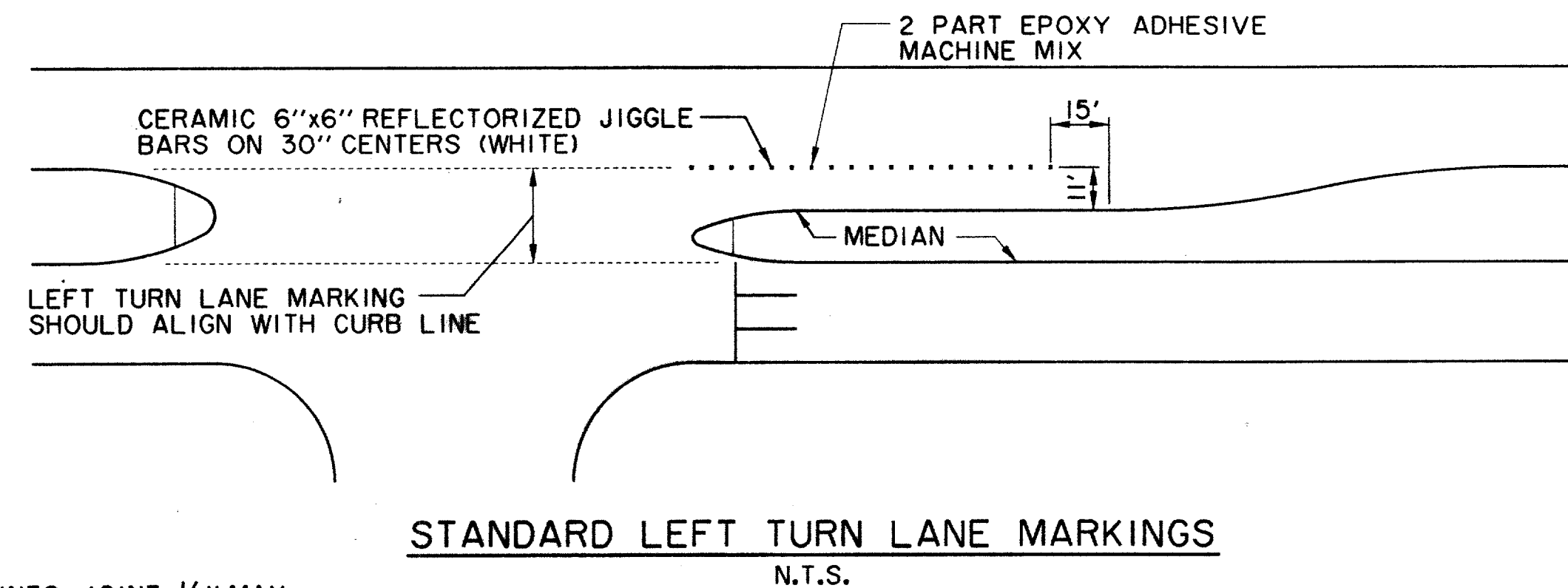
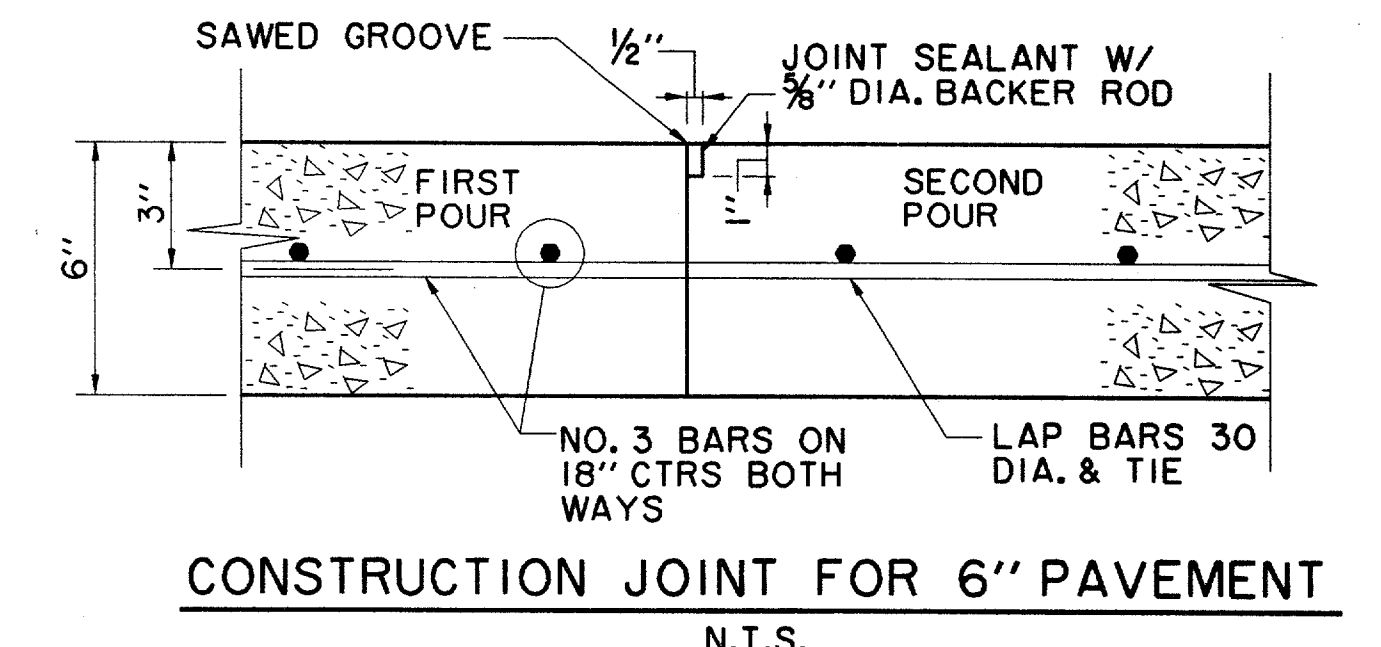
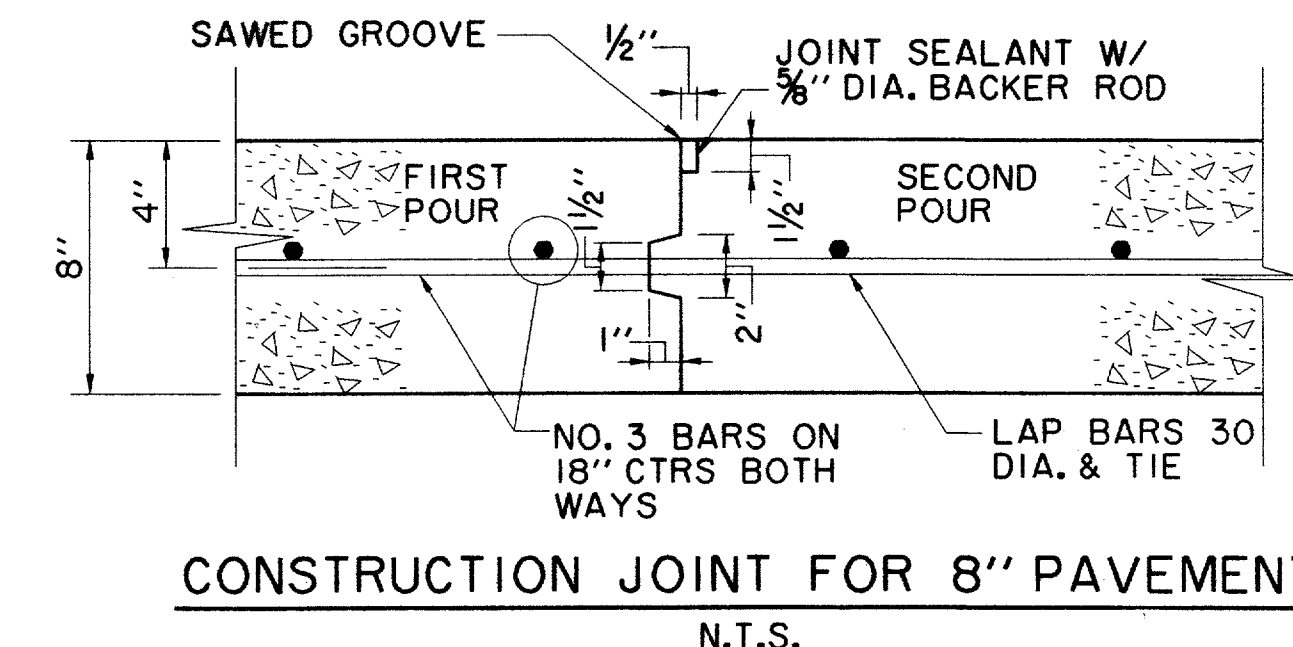
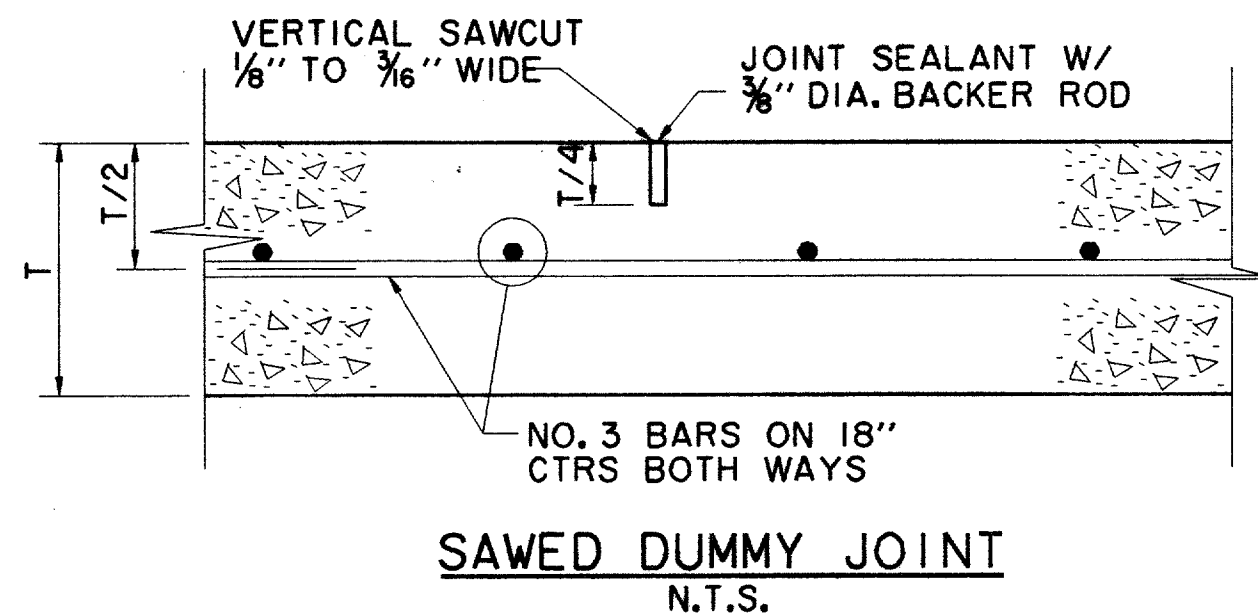
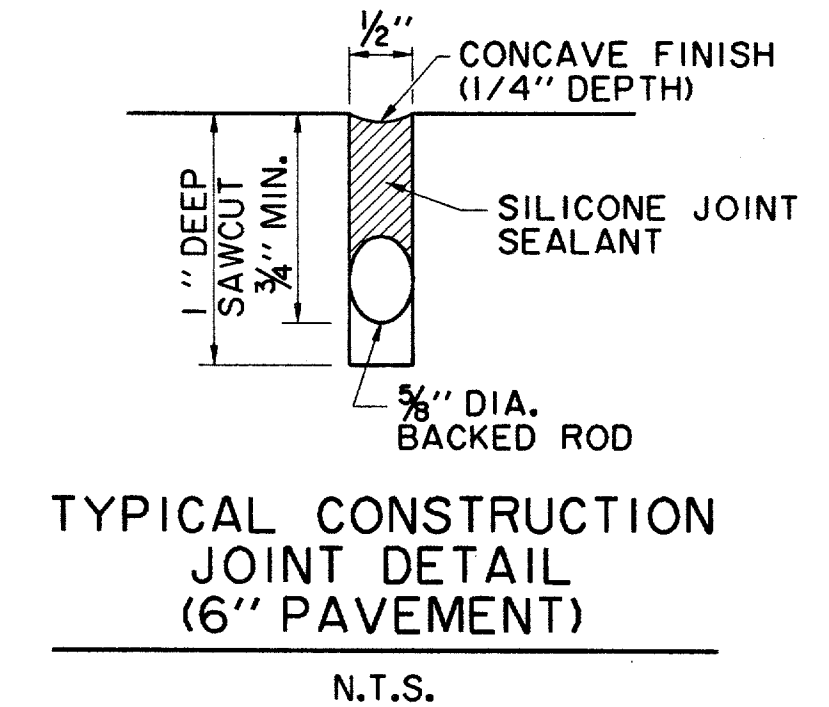
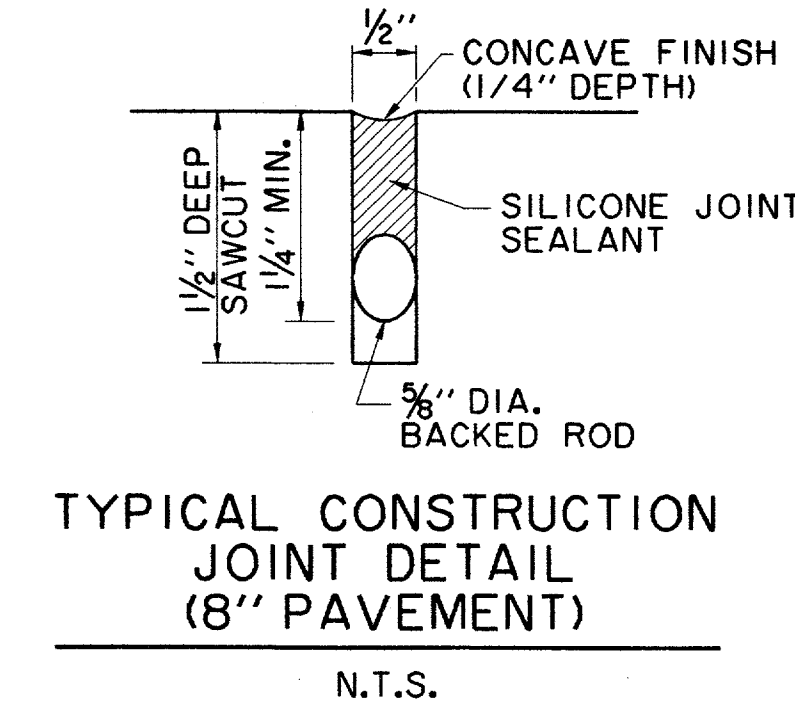
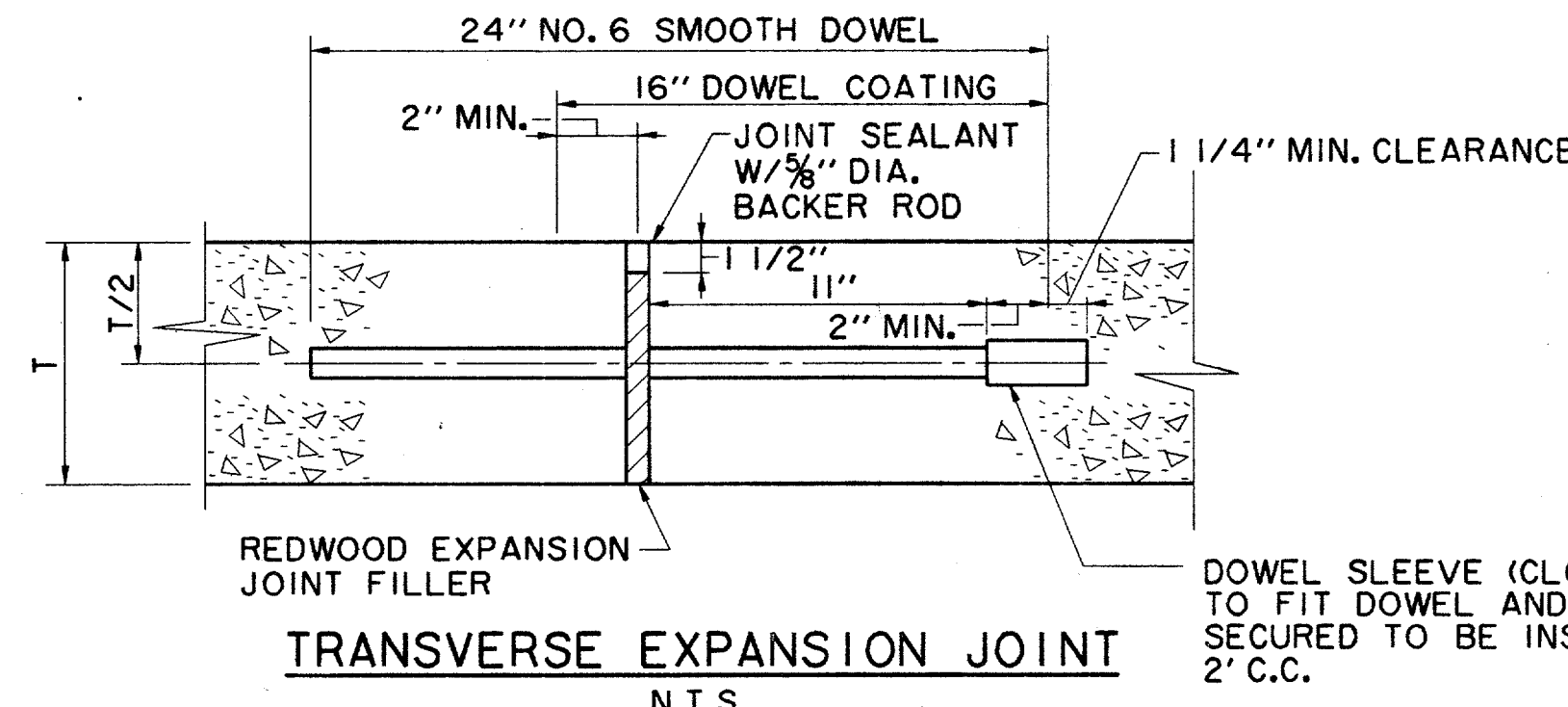
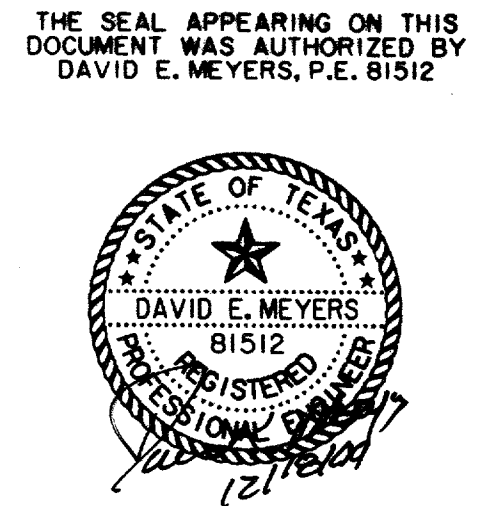


NOTES-PAVING AND GRADING

- UNLESS SPECIFICALLY STATED OTHERWISE IN PLANS OR CONTRACT DOCUMENTS, THE CONTROLLING SPECIFICATIONS FOR ALL WORK WITHIN PUBLIC RIGHTS-OF-WAY AND EASEMENTS SHALL BE THE STANDARD SPECIFICATIONS FOR PUBLIC WORKS CONSTRUCTION, SECOND EDITION 1987, AS AMENDED BY THE NORTH CENTRAL TEXAS COUNCIL OF GOVERNMENTS (THE "STANDARD SPECIFICATIONS"). COPIES OF THE STANDARD SPECIFICATIONS MAY BE PURCHASED BY MAIL OR OVER THE COUNTER FROM THE NORTH CENTRAL TEXAS COUNCIL OF GOVERNMENTS, 616 SIX FLAGS DRIVE, P.O. BOX DRAWER CCG, ARLINGTON, TEXAS 76005-5888, PHONE METRO 817/640-3300. BULK DISCOUNTS ARE AVAILABLE. THIS DOCUMENT IS COPYRIGHTED.
- THE PAVING AREAS WILL BE GRADED TO +/- .10' OF FINISHED SUBGRADE ELEVATIONS. UNDER A SEPARATE CONTRACT, WITH THE EXCEPTION OF THE DROP SLAB AREAS WHICH WILL BE CUT OUT BY THE PAVING CONTRACTOR PER SECTION C-C ON SHEET P22. SIDEWALK AREAS WILL BE GRADED TO +/- .10' OF FINISHED PAVEMENT ELEVATION UNDER SEPARATE GRADING CONTRACT. AFTER VERIFICATION OF THE GRADE BY THE OWNER, THE UTILITY CONTRACTOR SHALL BE REQUIRED TO ACCEPT THE GRADE PRIOR TO HIS WORK AND RESTORE THE GRADE TO ITS ORIGINAL CONDITION UPON COMPLETION. THE PAVING CONTRACTOR SHALL VERIFY THE GRADE AND BE REQUIRED TO ACCEPT THE GRADE PRIOR TO HIS WORK.
- ALL TRAFFIC CONTROL NECESSARY FOR THE WORK SHALL BE PROVIDED BY THE AFFECTED CONTRACTOR. ALL BARRICADES, WARNING SIGNS, LIGHTS, DEVICES, ETC. FOR THE GUIDANCE AND PROTECTION OF TRAFFIC AND PEDESTRIANS MUST CONFORM TO THE INSTALLATIONS SHOWN IN THE LATEST ISSUE OF THE "TEXAS MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS (MUTCD)", AS CURRENTLY AMENDED, TEXAS DEPARTMENT OF TRANSPORTATION.
- ALL FILL SHALL BE PLACED IN MAXIMUM 8-INCH LIFTS COMPACTED TO 95% OF STANDARD PROCTOR DENSITY BETWEEN 0% AND +3% OF OPTIMUM MOISTURE CONTENT.
- ALL TREES, STUMPS, BRUSH, GRASSES AND SURFACE ORGANICS WITHIN PROPOSED RIGHT-OF-WAY ARE TO BE REMOVED AND PROPERLY DISPOSED OF OFF-SITE. TREE REMOVAL PERMITS, IF REQUIRED, WILL BE OBTAINED BY THE OWNER.
- EXISTING UTILITIES, POLES, ETC. WILL BE REMOVED OR RELOCATED BY THE UTILITY COMPANIES THROUGH COORDINATION BY THE OWNER. CONTRACTOR SHALL BRING TO THE OWNER'S ATTENTION ANY FACILITIES THAT APPEAR TO BE IN CONFLICT SO THAT THE OWNER HAS SUFFICIENT TIME TO ACCOMPLISH THE NECESSARY RELOCATIONS.
- WHERE DEEP VERTICAL EXCAVATIONS (IN EXCESS OF 3 FEET) ARE INDICATED, CUTS SHALL BE LAID BACK AT A STABLE SLOPE (ON OWNER'S PROPERTY) UNTIL WALLS ARE CONSTRUCTED. BACKFILL MATERIAL SHALL BE STOCKPILED ON-SITE AT THE DIRECTION OF THE OWNER.
- ALL REINFORCING STEEL AND DOWEL BARS IN PAVEMENT SHALL BE SUPPORTED AND MAINTAINED AT THE CORRECT CLEARANCES BY THE USE OF BAR CHAIRS OR OTHER APPROVED SUPPORT.
- THE USE OF WOOD FORMS FOR PAVEMENT CONSTRUCTION WILL BE PERMITTED. SAWED DUMMY JOINTS SHALL MATCH EXISTING ON QUORUM DRIVE AND DALLAS PARKWAY. SAWED JOINTS SHALL BE SPACED AT 15' INTERVALS ON ADDISON CIRCLE AND SPECTRUM DRIVE, AND AT 12' INTERVALS ON MORRIS AVENUE AND M-3. SAWED JOINTS FOR M-2 NORTH AND M-2 SOUTH WILL BE PER EXHIBITS 4 AND 5 INCLUDED WITH ADDENDUM NO. 1.
- BARRIER-FREE RAMP SHALL BE BUILT WITH THIS PROJECT. LOCATIONS MAY BE ADJUSTED AS DIRECTED BY THE ENGINEER TO CLEAR OBSTRUCTIONS.
- VARIABLE HEIGHT CURB AT INTERSECTIONS AND SLOPING CURBS AT DRIVES SHALL BE BUILT TO MATCH FUTURE BARRIER-FREE RAMP PER PLANS. NO SEPARATE PAY ITEMS ARE PROVIDED.
- TWO-WAY TRAFFIC MUST BE MAINTAINED AT ALL TIMES ON PUBLIC STREETS ADJACENT TO THIS PROJECT UNLESS SPECIFIED OTHERWISE IN THE PLANS AND SPECIFICATIONS. THE TRAVELWAY WIDTH SHALL NOT BE LESS THAN 11 FEET.
- SEE TYPICAL SECTIONS SHEET FOR ADDITIONAL MATERIAL SPECIFICATIONS.
- ALL CURBS SHALL BE PLACED INTEGRAL WITH PAVEMENT.
- CURBS SHALL MEET THE SAME COMPRESSIVE STRENGTH AS SPECIFIED FOR THE CONCRETE PAVEMENT.
- BAR LAPS SHALL BE 30 DIAMETERS.
- FINISH SHALL BE TRANSVERSE WITH TRAFFIC LANES AND SHALL BE STEEL TINED BROOM FINISH.
- SIDEWALK BEYOND RIGHT-OF-WAY LINE TO BE CONSTRUCTED BY OTHERS WITH THE ADDISON CIRCLE APARTMENTS PHASE II PRIVATE DEVELOPMENT. CONTRACTOR SHALL COORDINATE INTERFACE IN THE FIELD. SEE SIDEWALK DETAILS IN THIS PLAN SET.
- SEE SURFACE PATTERNING PLANS FOR BRICK AND SIDEWALK PATTERNS. POUR SIDEWALK AROUND NOTED LEAVEOUTS PER LOCATIONS ON SURFACE PATTERNING PLANS.



RECORD DRAWING



DATE	DESCRIPTION	REF. NO.
10/3/97	ISSUED FOR CONSTRUCTION	N/A
7/14/97	ISSUED FOR BID	N/A

PAVING DETAILS					
ADDISON CIRCLE					
PHASE II PUBLIC INFRASTRUCTURE					
TOWN OF ADDISON, TEXAS					
Huitt-Zollars, Inc./Engineering/Architecture Dallas, Fort Worth, Houston, El Paso, Phoenix, Tustin, Ontario					
DESIGN	DRAWN	APPR.	SCALE	DATE	PROJECT NO.
HZ1	HZ1	DEM	N.T.S.	OCT. 97	01-1822-21
					P21