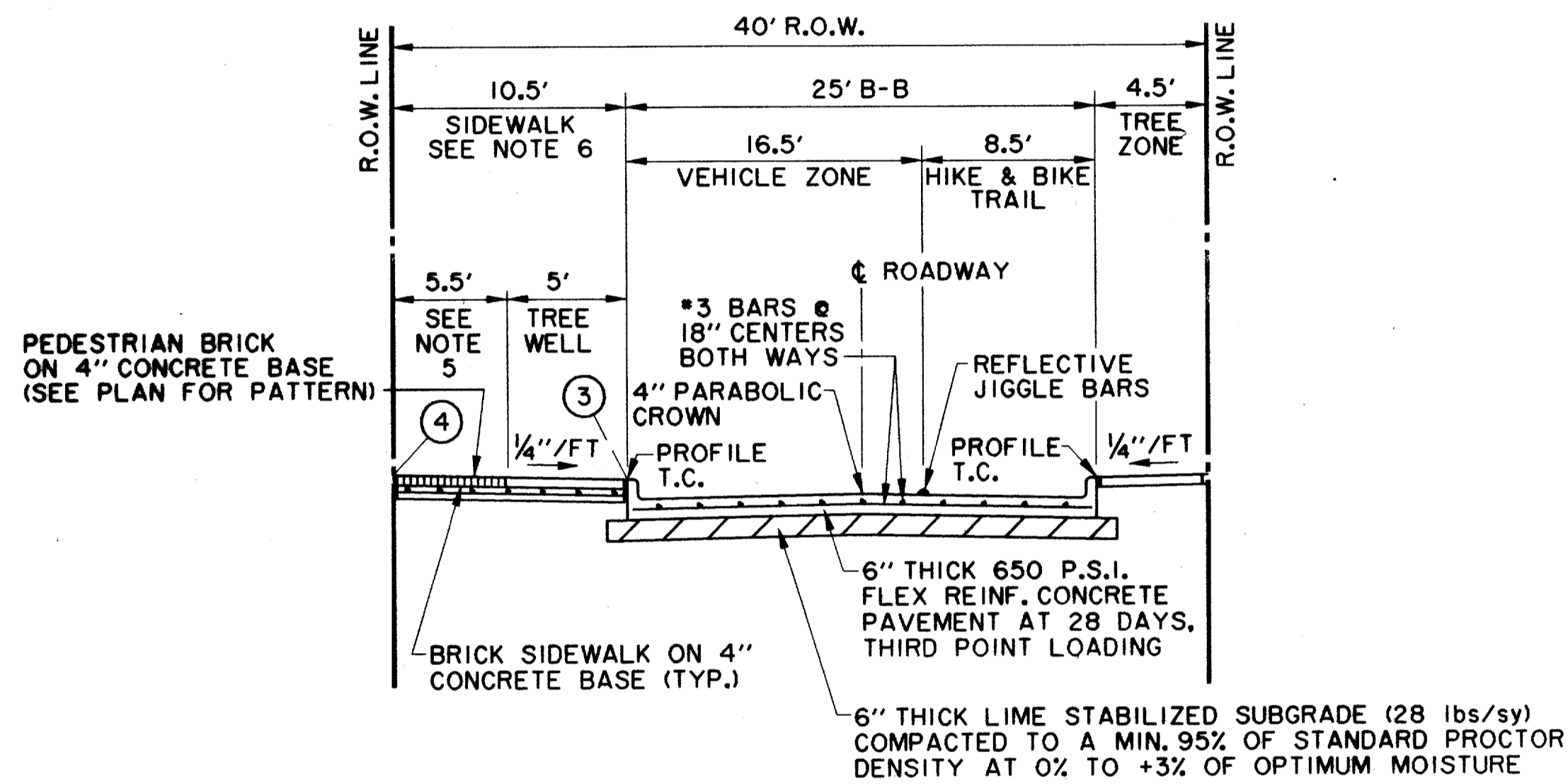
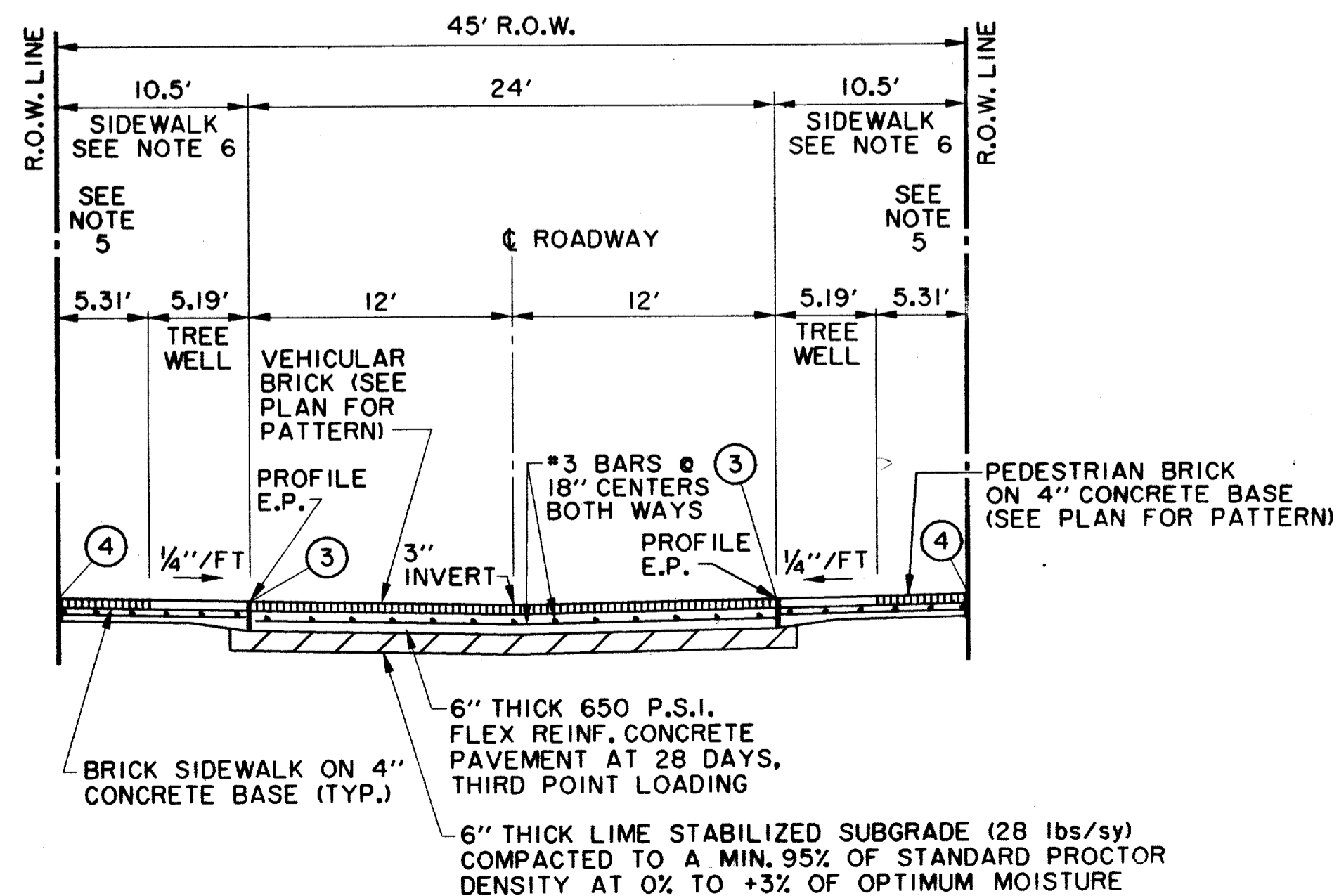


**2 LANES UNDIVIDED
MORRIS AVENUE**

(SEE PLAN VIEW FOR NECK-DOWN AREAS)



**2 LANES UNDIVIDED
M-3**



**2 LANES UNDIVIDED
M-2 NORTH & M-2 SOUTH**

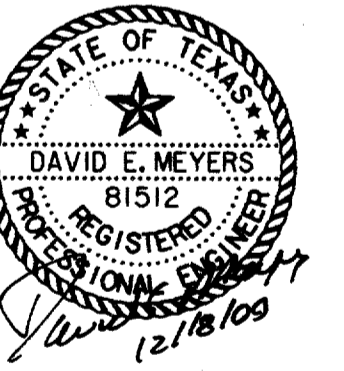
NOTES:

1. BACKFILL MATERIAL IS NOT A SEPARATE PAY ITEM. MATERIAL ACQUIRED FROM EXCAVATION MAY BE USED IF APPROVED BY ENGINEER.
2. PAY LINE FOR UNCLASSIFIED STREET EXCAVATION IS THE RIGHT-OF-WAY LINE WHEN A BUILDING IS BEING CONSTRUCTED ADJACENT TO THE RIGHT-OF-WAY. EXCAVATION BEYOND THE RIGHT-OF-WAY LINE IS ACCOUNTED FOR IN THE BUILDING EXCAVATION. PAY LINE FOR UNCLASSIFIED STREET EXCAVATION CAN BE BEYOND THE RIGHT-OF-WAY LINE WHEN ADJACENT TO UNDEVELOPED TRACTS.
3. SAWED DUMMY JOINTS SHALL MATCH EXISTING ON QUORUM DRIVE AND DALLAS PARKWAY. SAWED JOINTS SHALL BE SPACED AT 15' INTERVALS ON ADDISON CIRCLE AND SPECTRUM DRIVE, AND AT 12' INTERVALS ON MORRIS AVENUE AND M-3. SAWED JOINTS FOR M-2 NORTH AND M-2 SOUTH WILL BE PER EXHIBITS 4 AND 5 OF ADDENDUM NO. 1.
4. PROFILE GRADES SHOWN ON THE PAVING PLAN AND PROFILE SHEETS ARE TOP OF CURB AT BACK OF CURB FOR A NORMAL ROADWAY CROSS-SECTION AS INDICATED ON THE TYPICAL SECTIONS BY "PROFILE T.C.". PROFILE GRADES ON THE MEWS ARE EDGE OF PAVEMENT 12' LEFT AND RIGHT OF CENTERLINE AS INDICATED ON THE TYPICAL SECTION AS "PROFILE E.P.".
5. THE 1/4" / FT. CROSS-SLOPE IN SIDEWALK AND HEAD-IN PARKING AREAS IS THE STANDARD CONDITION ADJACENT TO MOST LAND USES SUCH AS RESIDENTIAL AND PARK PROPERTIES. A VARIABLE SLOPE MAY BE REQUIRED ADJACENT TO RETAIL USES IN ORDER TO MATCH THE FINISHED FLOOR ELEVATION FOR BARRIER-FREE ACCESS. SEE PLANS FOR THESE LOCATIONS.
6. SIDEWALK TYPICAL SECTIONS VARY ALONG EACH STREET AND FROM STREET TO STREET. THERE ARE SOME ENHANCED SIDEWALK SECTIONS AT TRASH LOCATIONS. SEE PAVING AND SURFACE PATTERNING PLANS FOR LOCATIONS AND DETAILS.
7. SIDEWALKS MAY CONTINUE INTO FRONT YARD UNDER SEPARATE CONTRACT. SEE SURFACE PATTERNING PLANS FOR PAY LIMITS. †
8. THIS PROJECT HAS BEEN ROUGH GRADED TO 0.10' OF PLAN ELEVATION (AFTER ADJUSTMENT FOR FINISH THICKNESS) AS A PART OF THE ADDISON CIRCLE PHASE II MASS GRADING PROJECT.

JOINT LEGEND:

- ① - SAWED LONGITUDINAL DUMMY JOINT
- ② - CONSTRUCTION JOINT (FULL WIDTH PVM'T. IS ALLOWED WHERE APPROVED BY THE TOWN OF ADDISON). FULL WIDTH PAVEMENT REQUIRES A SAWED LONGITUDINAL JOINT AT THE CENTERLINE OF THE PAVEMENT.
- ③ - EXPANSION JOINT
- ④ - ISOLATION JOINT
- ⑤ - LONGITUDINAL BUTT JOINT

THE SEAL APPEARING ON THIS DOCUMENT WAS AUTHORIZED BY DAVID E. MEYERS, P.E. 81512



**RECORD
DRAWING**

DATE	DESCRIPTION	REF NO.
10/3/97	ISSUED FOR CONSTRUCTION	N/A
8/4/97	ADDENDUM NO. 1	▲
7/14/97	ISSUED FOR BID	N/A

PAVING TYPICAL SECTIONS						
ADDISON CIRCLE						
PHASE II PUBLIC INFRASTRUCTURE						
TOWN OF ADDISON, TEXAS						
Huitt-Zollars, Inc./Engineering/Architecture Dallas, Fort Worth, Houston, El Paso, Phoenix, Tustin, Ontario						
DESIGN	DRAWN	APPR.	SCALE	DATE	PROJECT NO.	NO.
HZI	HZI	DEM	N.T.S.	OCT. 97	01-1822-21	P2