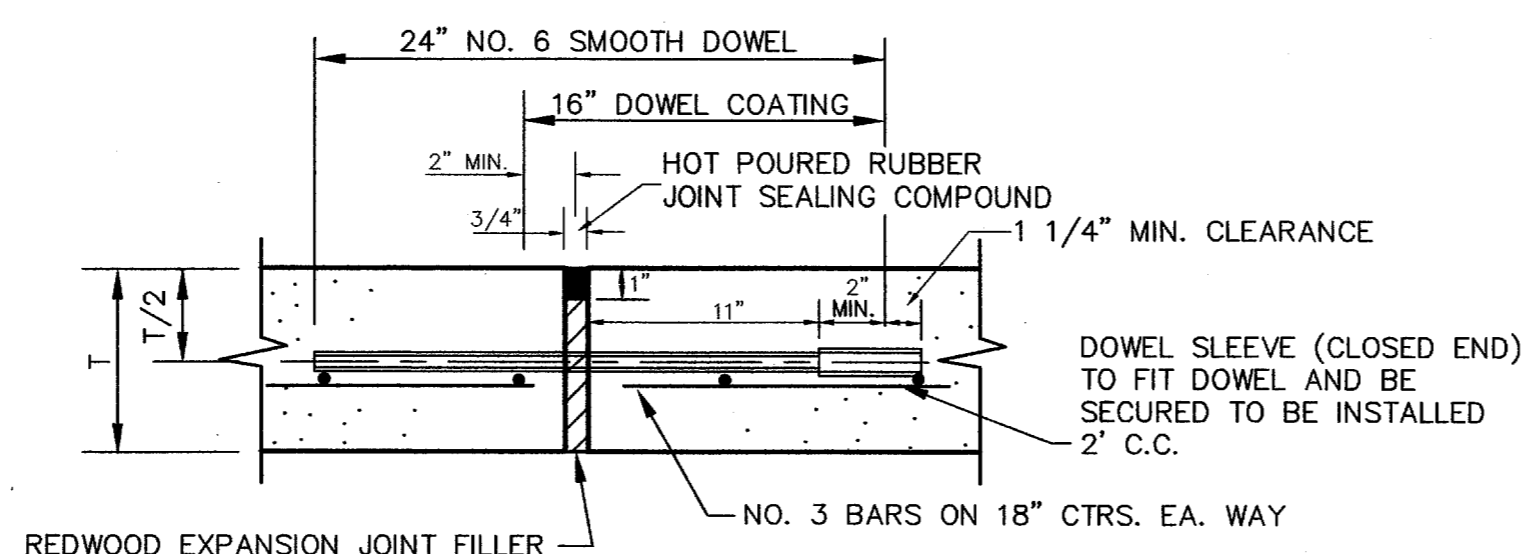


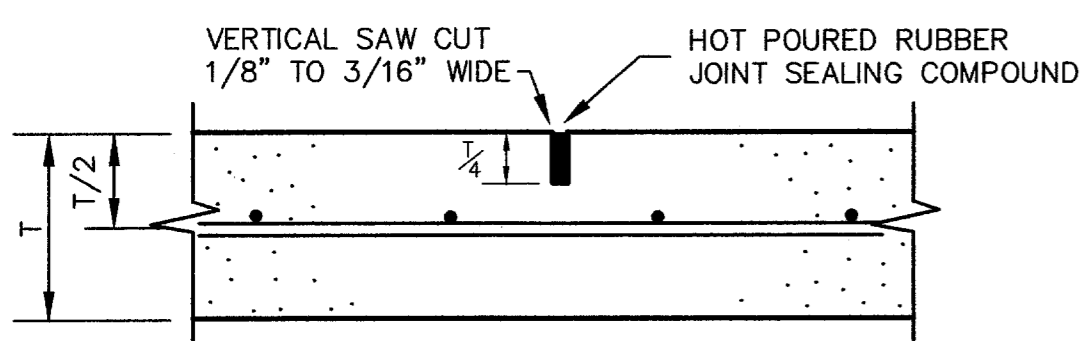
LONGITUDINAL BUTT JOINT
N.T.S.

- NOTES:
 1. NO. 5 SMOOTH DOWEL BAR MAY BE USED IN 5 INCH AND 6 INCH PAVEMENT THICKNESS.
 2. LONGITUDINAL BUTT CONSTRUCTION MAY BE UTILIZED IN PLACE OF LONGITUDINAL HINGED (KEYWAY) JOINT AT CONTRACTORS OPTION.
 3. DOWEL BARS SHALL BE DRILLED INTO PAVEMENT HORIZONTALLY BY USE OF A MECHANICAL RIG.
 4. DRILLED BY HAND IS NOT ACCEPTABLE, PUSHING DOWEL BARS INTO GREEN CONCRETE NOT ACCEPTABLE.

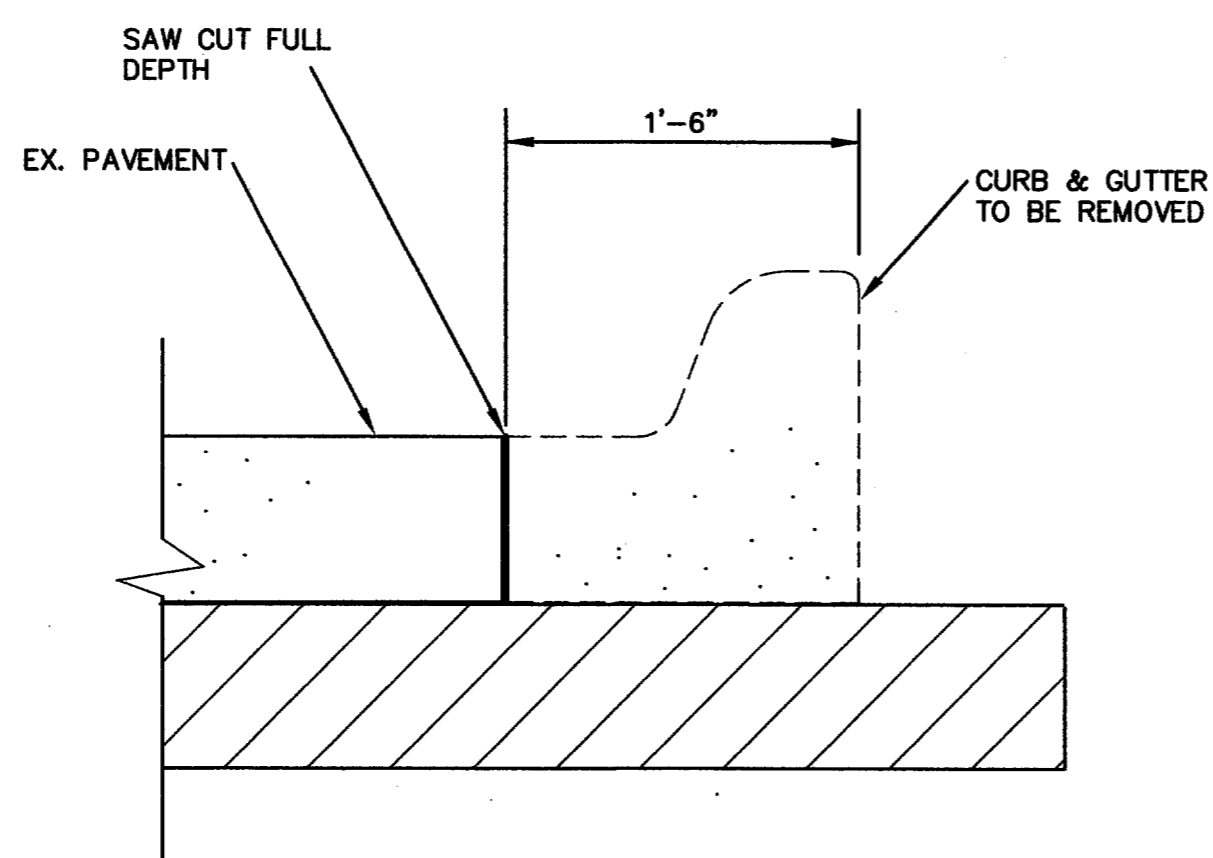


EXPANSION JOINT
N.T.S.

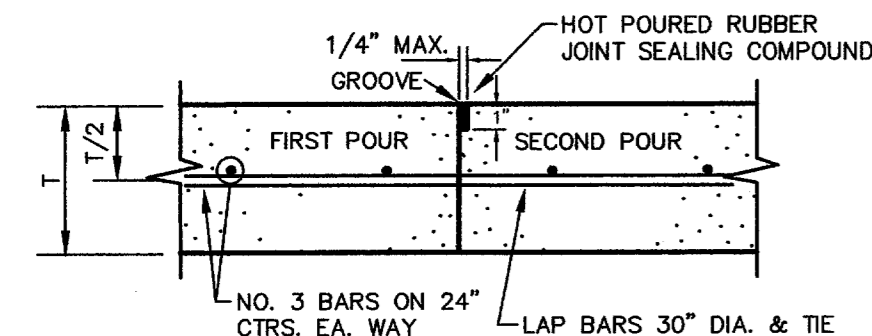
NOTE:
 DOWELS AND REINFORCING BARS SHALL BE SUPPORTED BY AN APPROVED DEVICE.



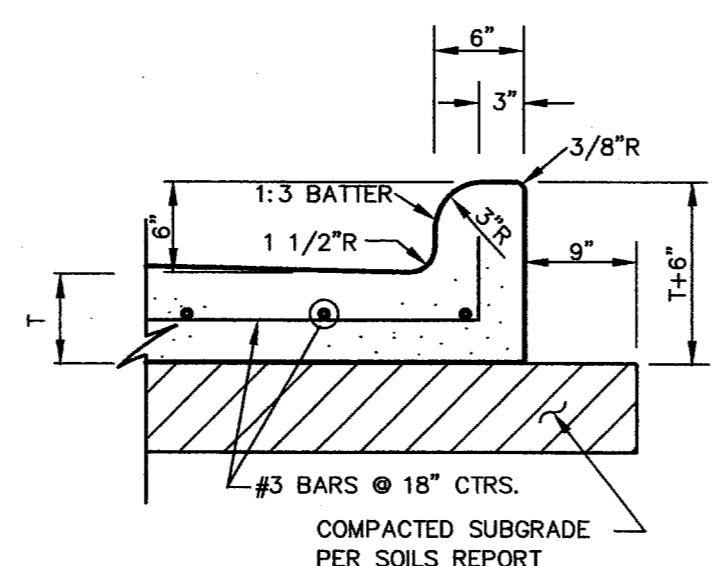
SAWED DUMMY (CONTROL) JOINT
N.T.S.



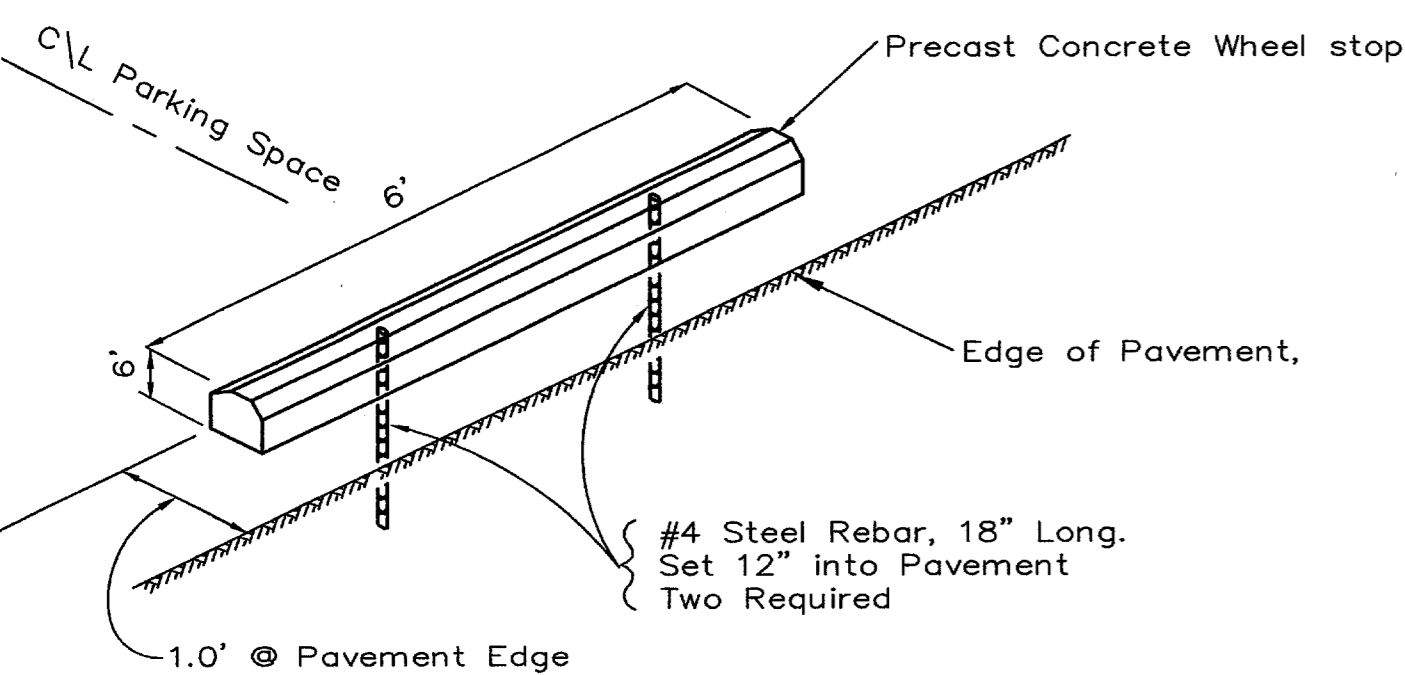
TYPICAL SAWCUT SECTION
NOT TO SCALE



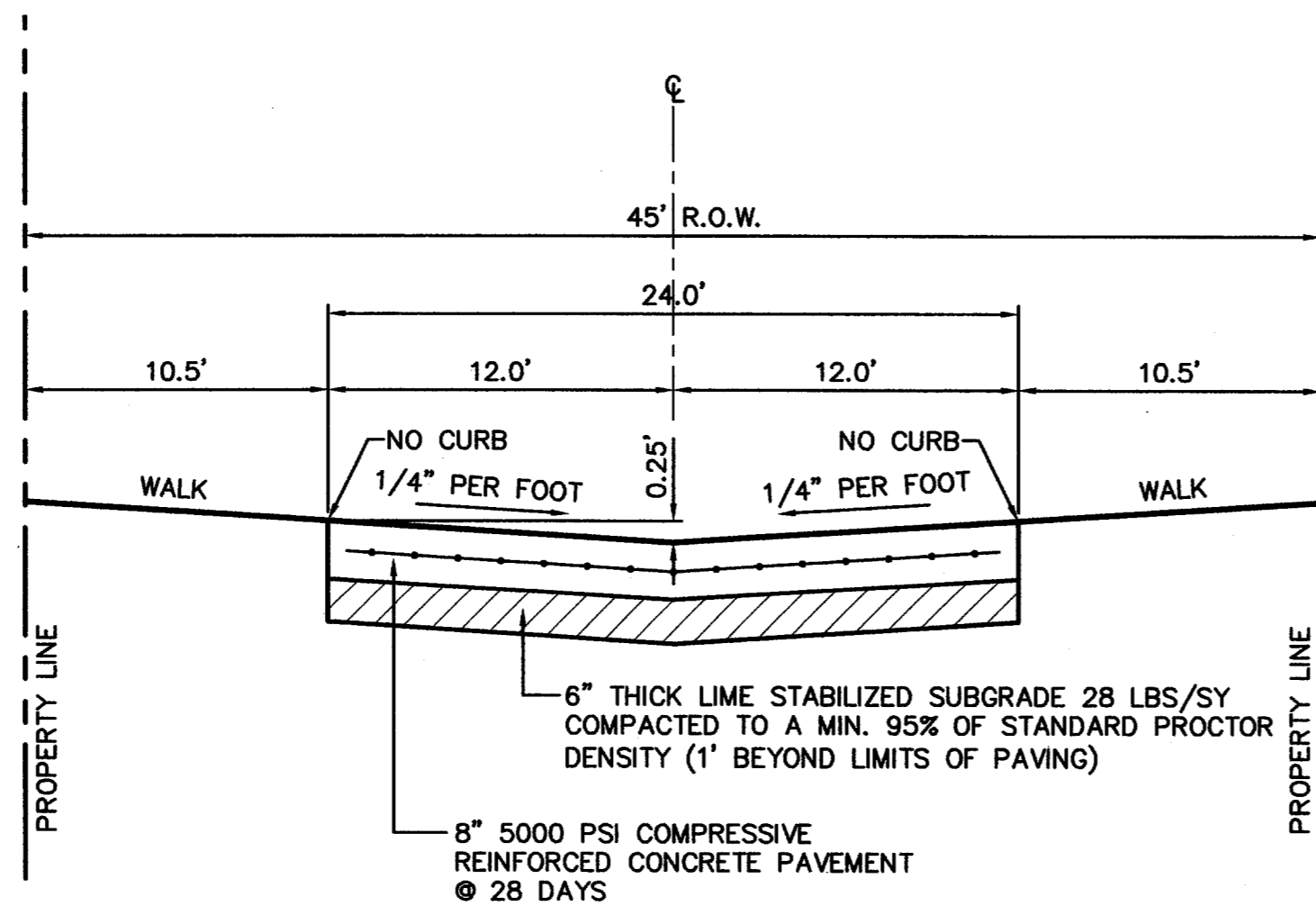
CONSTRUCTION JOINT
N.T.S.



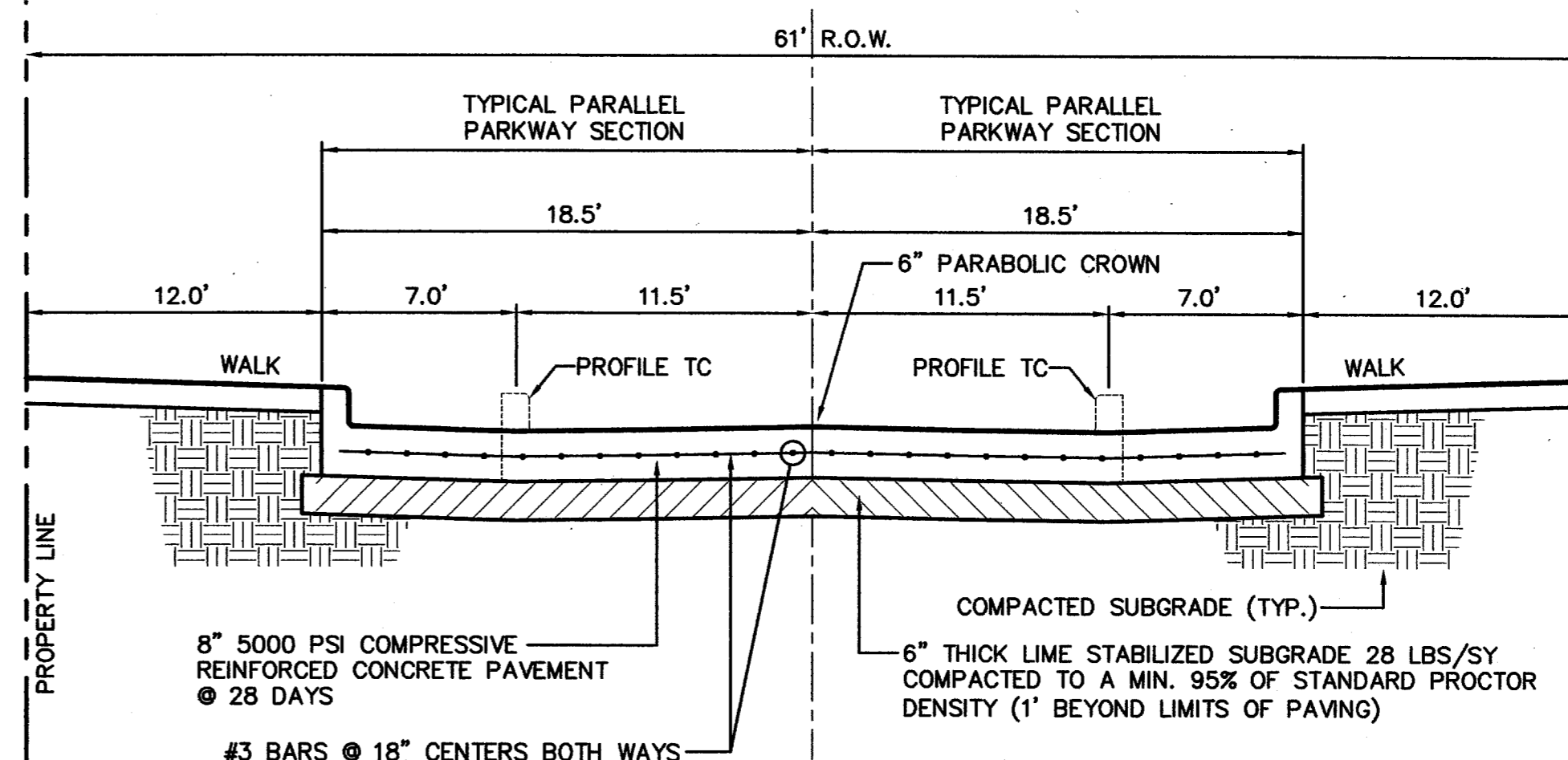
TYPICAL CURB DETAIL
N.T.S.



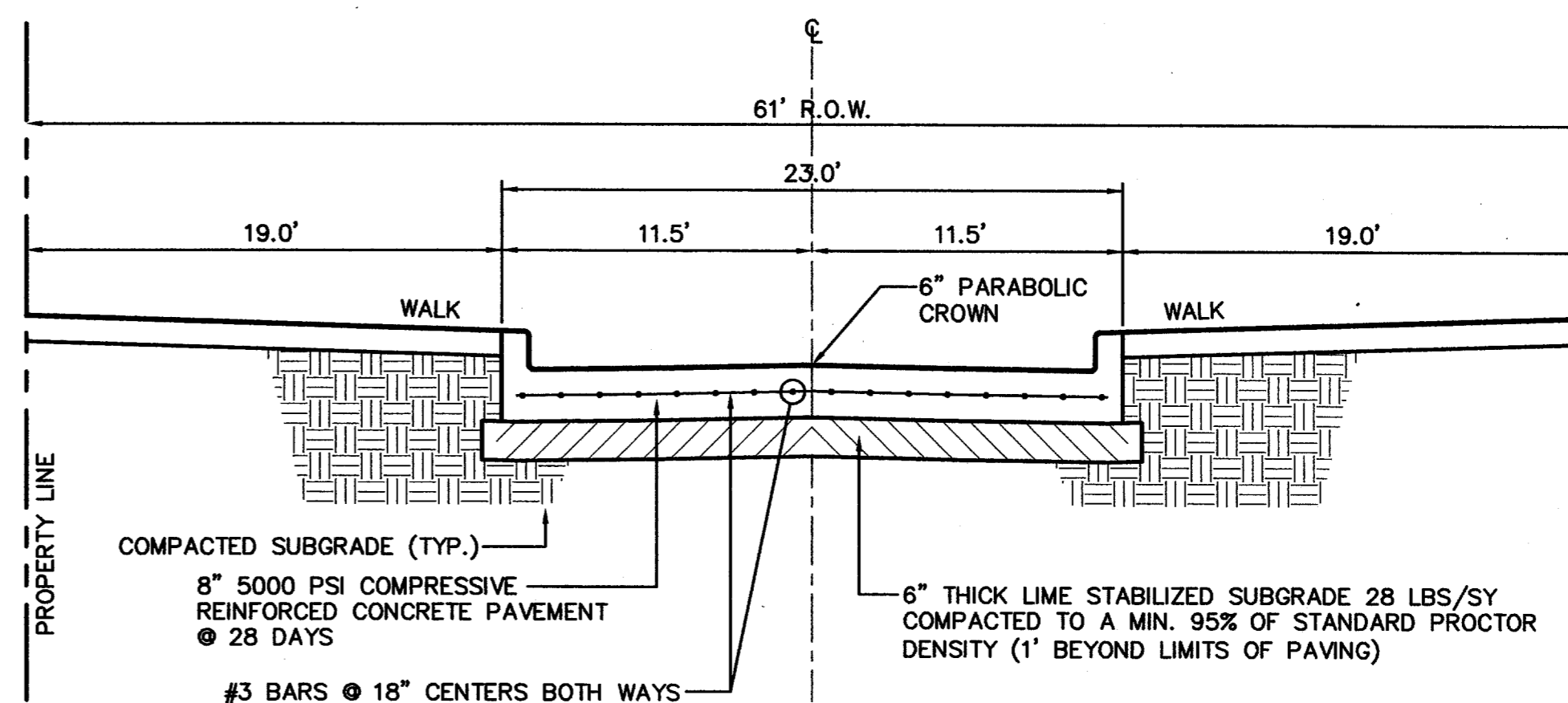
WHEEL STOP DETAIL
N.T.S.



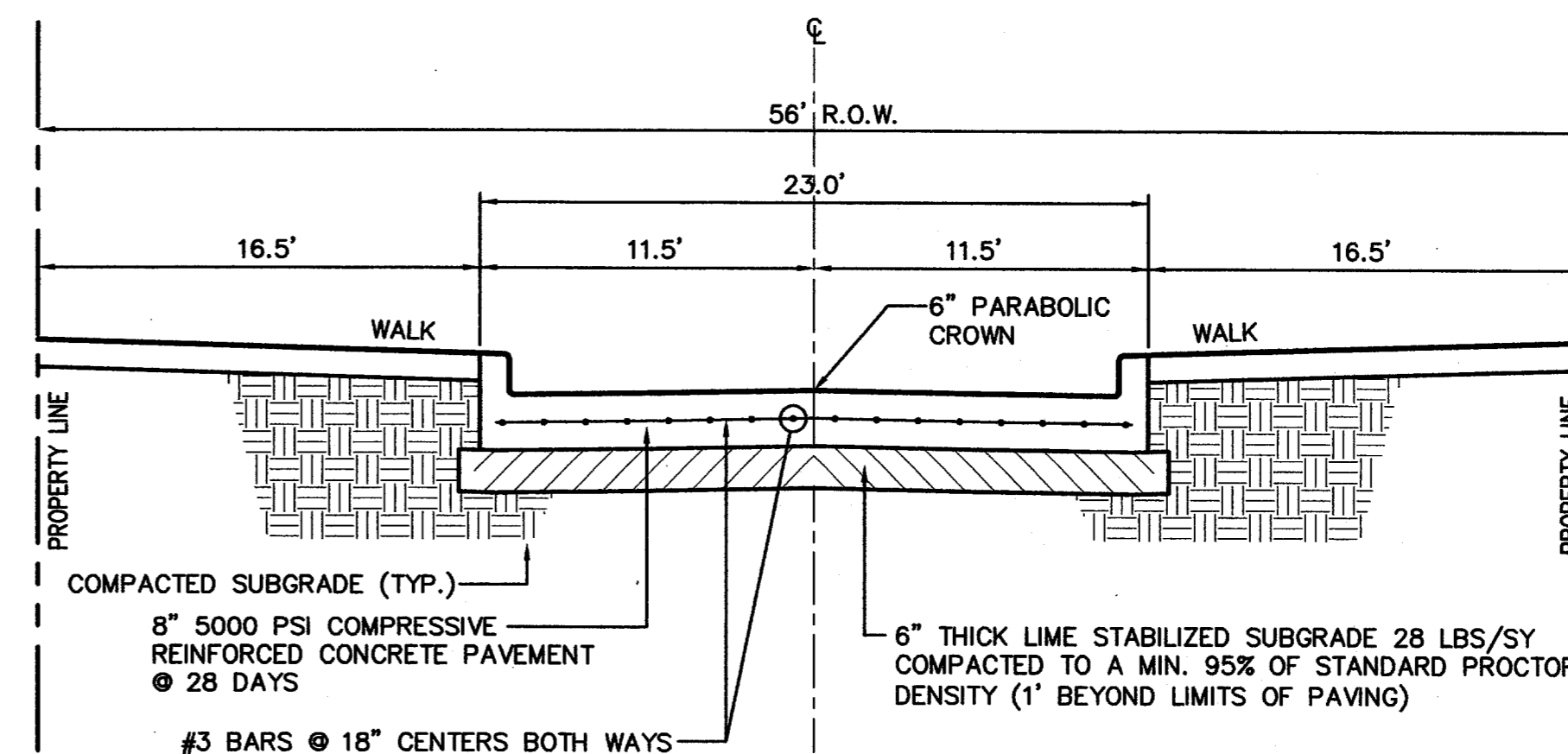
TYPICAL SECTION OF MEWS STREETS
NOT TO SCALE



2 LANES UNDIVIDED (CALLOWAY STREET)
NOT TO SCALE



2 LANES UNDIVIDED (PARKVIEW STREET - EAST)
NOT TO SCALE

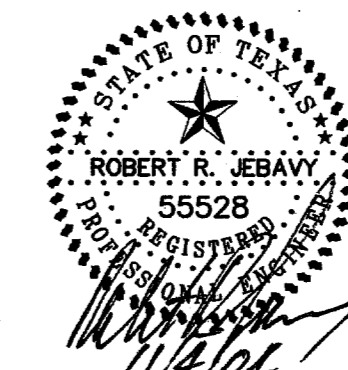


2 LANES UNDIVIDED (PARKVIEW STREET - WEST)
NOT TO SCALE

NOTE:
 ALL ADA ACCESSIBLE ROUTES SHALL HAVE MAXIMUM 2% CROSS SLOPES AND MAXIMUM 5% LONGITUDINAL SLOPE.

AS-BUILT
 NOVEMBER 4, 2004

NOTE:
 THESE PLANS HAVE BEEN REVISED TO CONFORM WITH CONSTRUCTION RECORDS PROVIDED BY CONTRACTOR.



PAVING DETAILS						
PARKVIEW AT ADDISON CIRCLE						
TOWN OF ADDISON, TEXAS						
DALLAS COUNTY						
BROCKETTE · DAVIS · DRAKE, inc.						
consulting engineers						
Civil & Structural Engineering · Surveying						
4144 North Central Expressway, Suite 1100, Dallas, Texas 75204						
(214) 824-3647, fax (214) 824-7064						
DESIGNED	DRAWN	DATE	SCALE	NOTES	FILE	NO.
RRJ	BDD	04/04	AS NOTED	BDD	C03393	C28

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C03393 PLAN C03393-PAV-DET-SPV.DWG