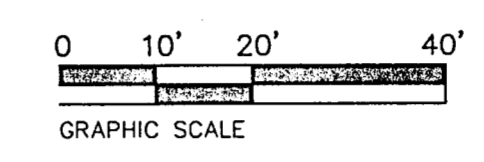
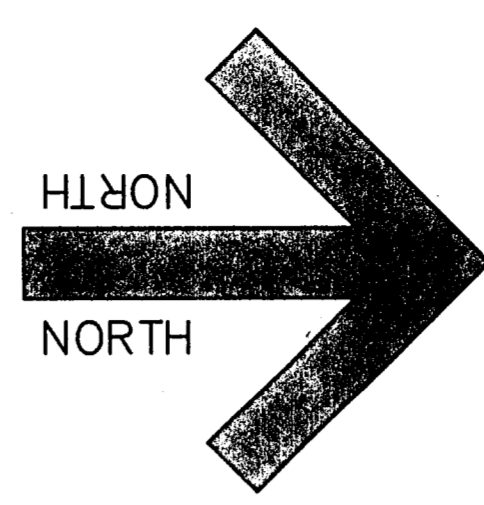
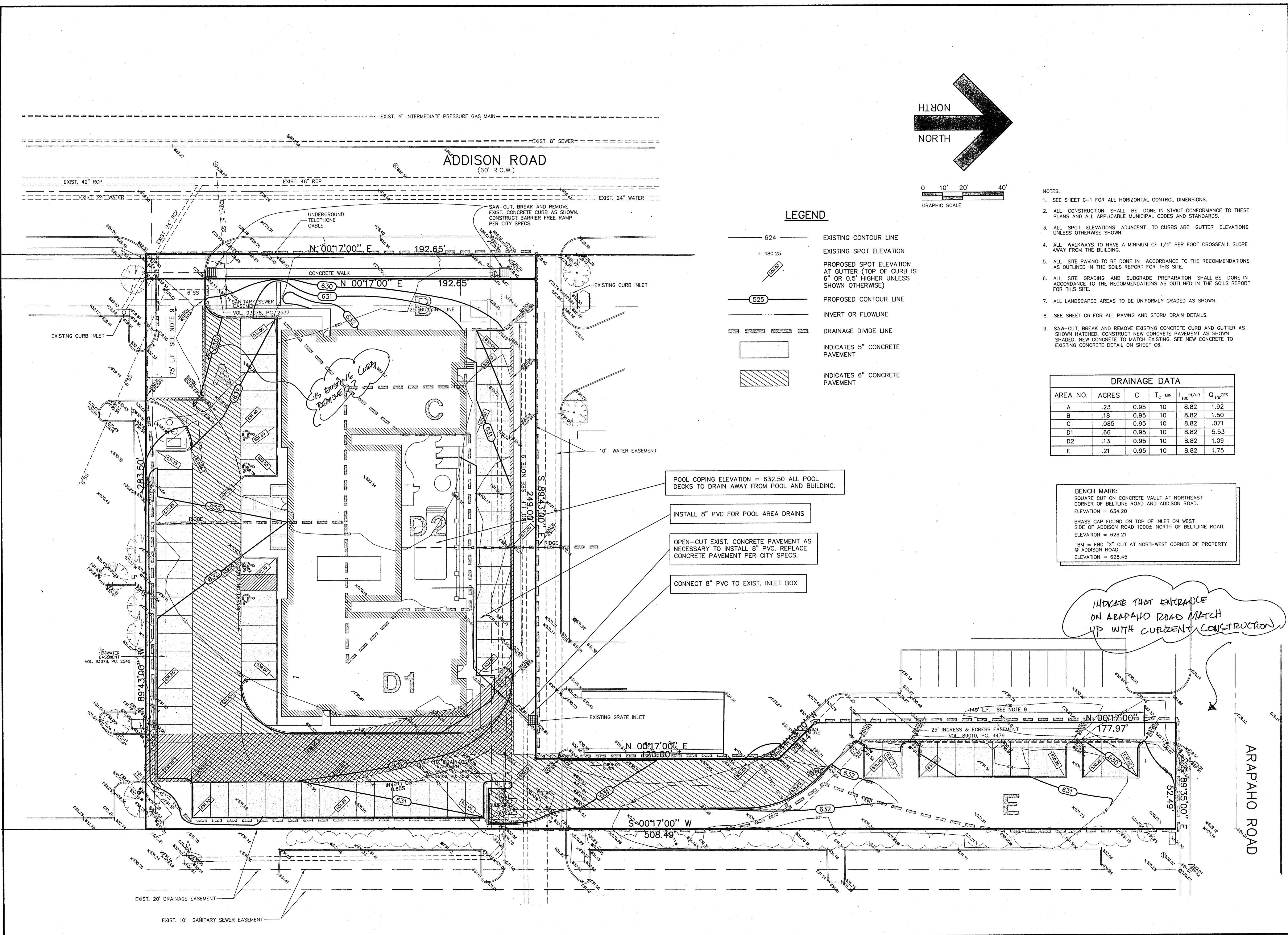


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LEGEND

- 624 — EXISTING CONTOUR LINE
- + 480.25 EXISTING SPOT ELEVATION
- PROPOSED SPOT ELEVATION AT GUTTER (TOP OF CURB IS 6" OR 0.5' HIGHER UNLESS SHOWN OTHERWISE)
- 625 — PROPOSED CONTOUR LINE
- — — INVERT OR FLOWLINE
- — — DRAINAGE DIVIDE LINE
- [Hatched Box] INDICATES 5" CONCRETE PAVEMENT
- [Diagonal Hatched Box] INDICATES 6" CONCRETE PAVEMENT

NOTES:

1. SEE SHEET C-1 FOR ALL HORIZONTAL CONTROL DIMENSIONS.
2. ALL CONSTRUCTION SHALL BE DONE IN STRICT CONFORMANCE TO THESE PLANS AND ALL APPLICABLE MUNICIPAL CODES AND STANDARDS.
3. ALL SPOT ELEVATIONS ADJACENT TO CURBS ARE GUTTER ELEVATIONS UNLESS OTHERWISE SHOWN.
4. ALL WALKWAYS TO HAVE A MINIMUM OF 1/4" PER FOOT CROSSFALL SLOPE AWAY FROM THE BUILDING.
5. ALL SITE PAVING TO BE DONE IN ACCORDANCE TO THE RECOMMENDATIONS AS OUTLINED IN THE SOILS REPORT FOR THIS SITE.
6. ALL SITE GRADING AND SUBGRADE PREPARATION SHALL BE DONE IN ACCORDANCE TO THE RECOMMENDATIONS AS OUTLINED IN THE SOILS REPORT FOR THIS SITE.
7. ALL LANDSCAPED AREAS TO BE UNIFORMLY GRADED AS SHOWN.
8. SEE SHEET C6 FOR ALL PAVING AND STORM DRAIN DETAILS.
9. SAW-CUT, BREAK AND REMOVE EXISTING CONCRETE CURB AND GUTTER AS SHOWN HATCHED. CONSTRUCT NEW CONCRETE PAVEMENT AS SHOWN SHADED. NEW CONCRETE TO MATCH EXISTING. SEE NEW CONCRETE TO EXISTING CONCRETE DETAIL ON SHEET C6.

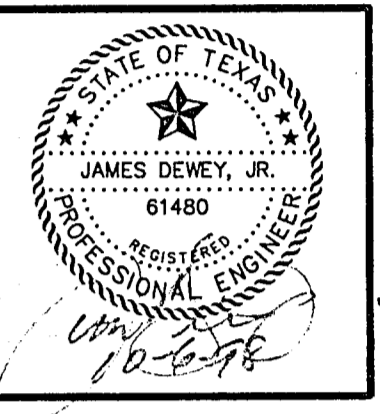
DRAINAGE DATA					
AREA NO.	ACRES	C	T _c MIN	I ₁₀₀ N/HR	Q ₁₀₀ CFS
A	.23	0.95	10	8.82	1.92
B	.18	0.95	10	8.82	1.50
C	.085	0.95	10	8.82	.071
D1	.66	0.95	10	8.82	5.53
D2	.13	0.95	10	8.82	1.09
E	.21	0.95	10	8.82	1.75

BENCH MARK:
 SQUARE CUT ON CONCRETE VAULT AT NORTHEAST CORNER OF BELTLINE ROAD AND ADDISON ROAD.
 ELEVATION = 634.20
 BRASS CAP FOUND ON TOP OF INLET ON WEST SIDE OF ADDISON ROAD 1000'S NORTH OF BELTLINE ROAD.
 ELEVATION = 628.21
 TBM = FIND "X" CUT AT NORTHWEST CORNER OF PROPERTY @ ADDISON ROAD.
 ELEVATION = 628.45

- POOL COPING ELEVATION = 632.50 ALL POOL DECKS TO DRAIN AWAY FROM POOL AND BUILDING.
- INSTALL 8" PVC FOR POOL AREA DRAINS
- OPEN-CUT EXIST. CONCRETE PAVEMENT AS NECESSARY TO INSTALL 8" PVC. REPLACE CONCRETE PAVEMENT PER CITY SPECS.
- CONNECT 8" PVC TO EXIST. INLET BOX

INDICATE THAT ENTRANCE ON ARAPAHO ROAD MATCH UP WITH CURRENT CONSTRUCTION

JDJR ENGINEERS & CONSULTANTS, INC.
 ENGINEERS • LAND PLANNERS • CAD DESIGNS
 2505 Texas Drive Suite 109 Irving, Texas 75062
 Tel. 972-252-0018 (6357) Fax. 972-273-8880



PROJECT: MAINSTAY SUITES OF ADDISON ADDITION
 BELTLINE ROAD
 ADDISON, TEXAS

REVISIONS:

DATE	REVISION
10/02/98	ARCH. COMMENTS

SHEET TITLE
 GRADING AND PAVING PLAN

DATE: 09/11/98
 SCALE: 1" = 20'
 DRAWN BY: J.N.M.
 CHECKED BY: JDJR
 SHEET NO. C2 OF 6
 JDJR FILE NO. 98-045