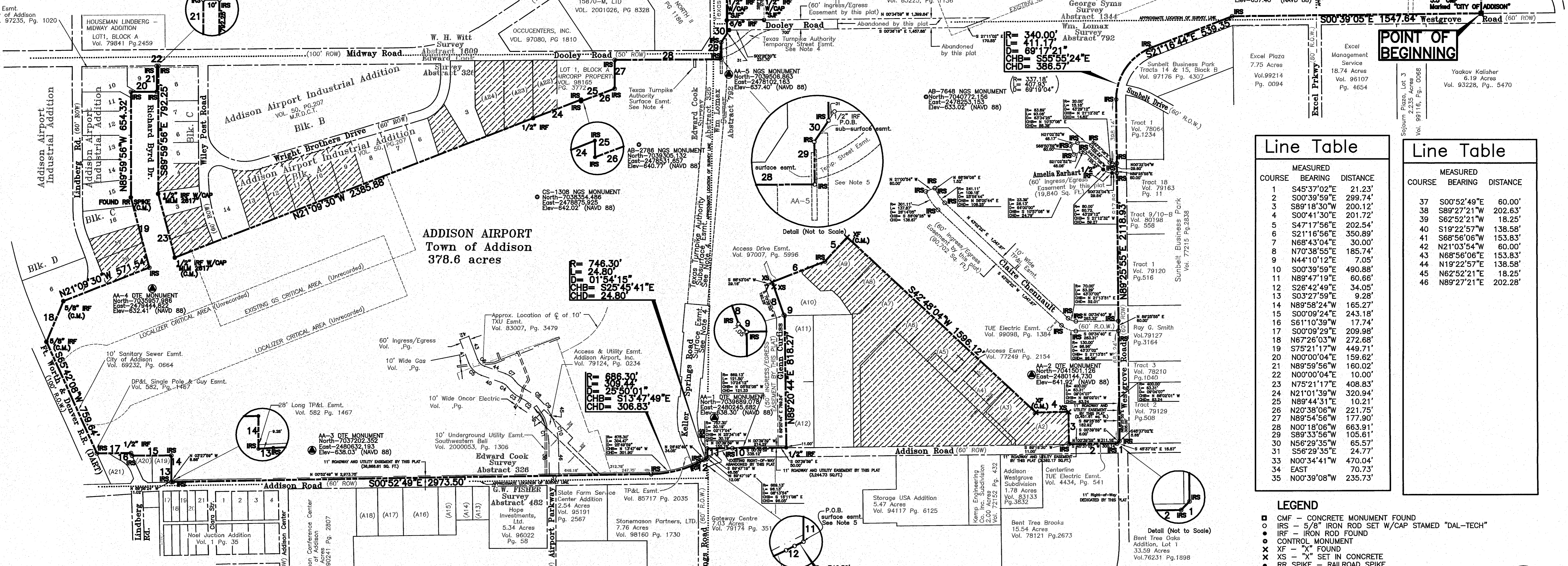
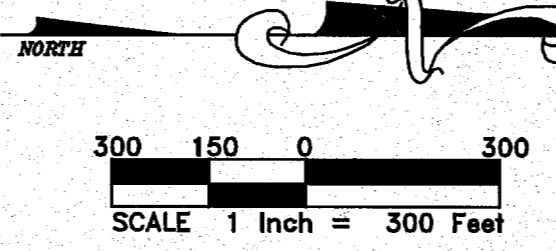


GENERAL NOTES:

- Bearings are referenced to the Texas State Plane coordinate system (NAD83).
- All objects shown hereon as open circles are 5/8-inch iron rods set with a plastic cap that is stamped "DAL-TECH" unless otherwise noted.
- Hatched areas currently have access to the airport property.
- All references to surface and sub-surface easements to the Texas Turnpike Authority are found in Interlocal agreement by and between The Town of Addison and Texas Turnpike Authority (Dallas North Tollway and Addison Airport Toll Tunnel) dated March 10, 1997, as recorded in the D.R.D.C.T. Volume 97084, Page 02628.
- Texas Power & Light Easement recorded in Volume 5021, Page 357 of the D.R.D.C.T., may affect subject tract, but was not locatable by description in the field.



ADDISON AIRPORT
Town of Addison
378.6 acres

R = 746.30'
D = 24.80'
CHB = 01°54'15"
CHD = 25°45'41"E
CHD = 24.80'

R = 686.30'
D = 309.44'
CHB = 51°34'49"E
CHD = 306.63'

CURVE TABLE

NO.	RAD.	L	DELTA	CHB	CHD
C1	300.00'	139.19'	26°35'00"	S76°09'51"W	137.94'
C2	300.00'	188.50'	36°00'00"	S44°52'21"W	185.41'
C3	248.00'	111.76'	25°49'09"	S39°46'56"W	110.81'
C4	70.00'	40.69'	33°18'33"	S36°02'14"W	40.12'
C5	130.00'	112.43'	49°33'09"	N44°09'32"W	108.95'
C6	70.00'	60.54'	49°33'09"	N44°09'32"W	58.67'
C7	130.00'	75.58'	33°18'33"	N36°02'14"W	74.52'
C8	188.00'	84.72'	25°49'09"	N39°46'56"W	84.00'
C9	360.00'	228.19'	36°00'00"	N44°52'21"E	222.49'
C10	360.00'	167.03'	26°35'00"	N76°09'51"E	165.53'

Line Table

COURSE	BEARING	MEASURED DISTANCE
1	S45°37'02"E	21.23'
2	S00°39'59"E	299.74'
3	S89°18'30"W	200.12'
4	S00°41'30"E	201.72'
5	S47°17'56"E	202.54'
6	S21°16'56"E	350.89'
7	N68°43'04"E	30.00'
8	N70°38'55"E	185.74'
9	N44°10'12"E	7.05'
10	S00°39'59"E	490.88'
11	N89°47'19"E	60.66'
12	S26°42'49"E	34.05'
13	S03°27'59"E	9.28'
14	N89°58'24"W	165.27'
15	S00°09'24"E	243.18'
16	S61°10'39"W	17.74'
17	S00°09'29"E	209.98'
18	N67°26'03"W	272.68'
19	S75°21'17"W	449.71'
20	N00°00'04"E	159.62'
21	N89°59'56"W	160.02'
22	N00°00'04"E	10.00'
23	N75°21'17"E	408.83'
24	N21°01'39"W	320.94'
25	N89°44'31"E	10.21'
26	N20°38'06"W	221.75'
27	N89°54'56"W	177.90'
28	N00°18'06"W	663.91'
29	S89°33'56"W	105.61'
30	N56°29'35"W	65.57'
31	S56°29'35"E	24.77'
33	N00°34'41"W	470.04'
34	EAST	70.73'
35	N00°39'08"W	235.73'

Line Table

COURSE	BEARING	MEASURED DISTANCE
37	S00°52'49"E	60.00'
38	S89°27'21"W	202.63'
39	S62°52'21"W	18.25'
40	S19°22'57"W	138.58'
41	S68°56'06"W	153.83'
42	N21°03'54"W	60.00'
43	N68°56'06"E	153.83'
44	N19°22'57"E	138.58'
45	N62°52'21"E	18.25'
46	N89°27'21"E	202.28'

- LEGEND**
- CMF - CONCRETE MONUMENT FOUND
 - IRS - 5/8" IRON ROD SET W/CAP STAMPED "DAL-TECH"
 - IRF - IRON ROD FOUND
 - CONTROL MONUMENT
 - ✕ XF - "X" FOUND
 - ✕ XS - "X" SET IN CONCRETE
 - RR SPIKE - RAILROAD SPIKE
 - DAL-TECH ENGINEERING CONTROL MONUMENT
- (A1) - ADJOINER INDEX
NAVD - NORTH AMERICAN VERTICAL DATUM
DTE - DAL-TECH ENGINEERING
- ▨ PROPERTIES WITH AIRPORT ACCESS

Town of Addison Approval:
City Secretary: Carmen Moran
Scott Wheeler: Mayor (Town of Addison)

DAL-TECH ENGINEERING, INC.
CONSULTING CIVIL ENGINEERS / SURVEYORS
CONSTRUCTION MANAGERS
17311 DALLAS PARKWAY
SUITE 200
DALLAS, TEXAS 75248
(972) 250-2727 (972) 250-4774

FINAL PLAT
ADDISON AIRPORT
TOWN OF ADDISON, TEXAS

DRAWN	JOB NO.	CONTRACT NO.	SCALE	DATE
DTE	0216	-	1"=300'	DEC. 2004

Adjoiner Index

Index	Owner	Vol.	Page
A1	Texas Turnpike Authority	98197	4724
A2	Texas Federal Subdivision, No.2	79039	133
A3	Blakely Airport Addition	84088	4369
A4	Van-Horn Addition	79122	1831
A5	Aweco Subdivision	79136	2610
A6	Replat Aweco Subdivision	97093	2846
A7	Personal Way Aviation Addition	90241	2840
A8	Hanain Out L.L.C.	98245	3525
A9	Airport Land Development Subdivision	79078	1702
A10	1,980-acre Wolfe Subdivision	84108	3030
A11	Airport Plaza	85070	2982
A12	Addison Storage Addition	99018	78
A13	Eleanor W. Dedmon Marital Trust No. 1	98004	3173
A14	Tom J. Shepherd et ux Sandra D. Shepherd	92229	4451
A15	Crossroads Aviation, Inc.	67044	213
A16	D.A. Cody	71165	2799
A17	Barco Mirror & Glass	79020	875
A18	General Public	83083	1059
A19	Town of Addison	97247	4072
A20	White Rock Masonic Lodge #234	39817	418
A21	George P. Kondos and Carol C. Kondos	91084	2102
A22	15800 Dooley Road Ltd.	2000039	4255
A23	John-Mac Joint Venture	82001	4089
A24	Midway Road Condo's	81115	0016

Parent Tracts

Owner: City of Addison
 Address: 15333 Addison Road City of Addison, Texas
 Legal Description: Being all the certain tract of land 373.332 acres, more or less, out of the Edward Cook Survey Abstract 326, the William Lomar Survey Abstract 792, the George Syms Survey Abstract 1344, the William Rowe Survey Abstract 1257, Lot 1R-1, Block A of Coil Addition, Vol. 2002003, Pg. 103, Dallas County, Texas, and part of Lot 1 and Lot 2 of Block "A" of The Carroll Estates Addition, Volume 10 Page 473, Dallas County, Texas

Grantor: Addison Airport, Inc.
 Grantee: City of Addison
 WD Vol.77010 Pg. 1391

Grantor: W. H. Laffity
 Grantee: City of Addison
 Judgment Cause# 80-1894-D
 Vol.91079 Pg.2687

Grantor: Beltwood North Joint Venture, et al
 Grantee: City of Addison
 Vol.82022 Pg.1211

Grantor: Bob Chester and William F. Chester
 Grantee: City of Addison
 Vol.98015 Pg.2143
 Re: Chester Addition Vol.84216 Pg.3066

Grantor: Protecta-Cab, Inc.
 Grantee: Town of Addison
 Vol.97217 Pg.1350

Grantor: Charles Cayce, Independent Executor of the Estate of William L. Gray and Independent Executor of the Estate of Kathy L. Gray
 Grantee: Town of Addison, Texas
 Vol. 2003105, Pg. 11725
 Re: Lot 1R-1, Block A, Coil Addition, Vol. 2002003, Pg. 103

Grantor: North Texas Tollway Authority
 Grantee: Town of Addison
 Vol. 2002150, Pg. 10404

Dedication Statement

That the TOWN OF ADDISON ("Owner") does hereby adopt this plat designating the hereinabove property as ADDISON AIRPORT ADDITION, an addition to the Town of Addison, Texas, and, subject to the conditions, restrictions and reservations stated hereinafter, owner dedicates to the public use forever the streets and alleys shown thereon.

The easements shown on this plat are hereby reserved for the purposes as indicated, including, but not limited to, the installation and maintenance of water, sanitary sewer, storm sewer, drainage, electric, telephones, gas and cable television. Owner shall have the right to use these easements, provided however, that it does not unreasonably interfere or impede with the provision of the services to others. Said utility easements are hereby reserved by mutual use and accommodation of all public utilities using or desiring to use the same. An express easement of ingress and egress is hereby expressly granted on, over and across all such easements for the benefit of the provider of services for which easements are granted.

Any drainage and floodway easement shown hereon is hereby dedicated to the public's use forever, but including the following covenants with regards to maintenance responsibilities. The existing channels or creeks traversing the drainage and floodway easement will remain as an open channel, unless required to be enclosed by ordinance, at all times and shall be maintained by the individual owners of the lot or lots that are traversed by or adjacent to the drainage and floodway easement. The City will not be responsible for the maintenance and operation of said creek or creeks or for any damage or injury of private property or person that results from the flow of water along said creek, or for any damage or injury of private property or person that results from the flow of water along said creek, or for the control of erosion. No obstruction to the natural flow of water run-off shall be permitted by construction of any type building, fence or any other structure within the drainage and floodway easement. Provided, however, it is understood that in the event it becomes necessary for the City to channelize or consider erecting any type of drainage structure in order to improve the storm drainage, then in such event, the City shall have the right, but not the obligation, to enter upon the drainage and floodway easement at any point, or points, with all rights of ingress and egress to investigate, survey, erect, construct or maintain any drainage facility deemed necessary by the City for drainage purposes. Each property owner shall keep the natural drainage channels and creeks traversing the drainage and floodway easement adjacent to his property clean and free of debris, silt, growth, vegetation, weeds, rubbish, refuse matter and any substance which would result in unsanitary conditions or obstruct the flow of water, and the City shall have the right of ingress and egress for the purpose of inspection and supervision and maintenance work by the property owner to alleviate any undesirable conditions which may occur. The natural drainage channels and creeks through the drainage and floodway easement, as in the case of all natural channels, are subject to storm water overflow and natural bank erosion to an extent that cannot be definitely defined. The City shall not be held liable for any damages or injuries of any nature resulting from the occurrence of these natural phenomena, nor resulting from the failure of any structure or structures, within the natural drainage channels, and the owners hereby agree to indemnify and hold harmless the City from any such damages and injuries. Building areas outside the drainage and floodway easement line shall be filled to a minimum elevation as shown on the plat. The minimum floor of elevation of each lot shall be shown on the plat.

The maintenance or paving of the utility and fire lane easements is the responsibility of the property owner. All public utilities shall at all times have the full right of ingress and egress to and from and upon the said utility easements for the purpose of constructing, reconstructing, inspecting, patrolling, maintaining and adding to or removing all or parts of its respective system without the necessity at any time of procuring the permission of anyone. Any public utility shall have the right of ingress and egress to private property for the purpose of reading meters and any maintenance and service required or ordinarily performed by that utility. Buildings, fences, trees, shrubs or other improvements or growth may be constructed, reconstructed or placed upon, over or across the utility easements as shown; provided, however, that owner shall at its sole cost and expense be responsible under any and all circumstances for the maintenance and repair of such improvements or growth, any public utility shall have the right to remove and keep removed all or parts of any buildings, fences, trees, shrubs or other improvements or growth which in any way endanger or interfere with the construction, maintenance or efficiency of its respective system or service.

Water main and sanitary sewer easements shall also include additional area of working space for construction and maintenance of the systems. Additional easement area is also conveyed for installation and maintenance of manholes, cleanouts, fire hydrants, water service and sewer services from the main to curb or pavement line, and the descriptions of such additional easements herein granted shall be determined by their locations as installed.

This plat is approved subject to all platting ordinances, rules, regulations and resolutions of the Town of Addison, Texas.

Control Monument

(NAD 83, TEXAS NORTH CENTRAL - 4202)

North	East	Elevation	Desc.
7039689.078	2480245.682	638.30	AA-1
7041501.126	2480144.730	641.92	AA-2
7037202.352	2480632.193	638.03	AA-3
7035957.986	2479444.822	632.41	AA-4
7039506.863	2478102.163	637.40	AA-5
7043849.691	2477497.499	652.26	AA-6
7039305.132	2478531.657	640.77	AB2786
7040772.156	2478253.153	633.02	AB7648
7043042.492	2477532.021	637.40	AH7852
7038354.486	2478875.925	642.02	CS1308

PROPERTY DESCRIPTION

Being a tract of land out of the E. Cook Survey, Abstract No. 326, the William Lomar Survey, Abstract No. 792, the George Syms Survey, Abstract 1344, and the William Rowe Survey, Abstract 1257, and being all of a tract of land described in deed to the City of Addison recorded in Volume 77010, Page 1391 of the Deed Records of Dallas County, Texas (D.R.D.C.T.), all of a tract of land described in Judgment to the City of Addison recorded in Volume 91079, Page 2687, D.R.D.C.T., LOT 7, Block B of Beltwood North Addition, Phase II, an Addition to the City of Dallas, Texas, according to the map thereof recorded in Volume 78201, Page 0001, D.R.D.C.T., as described in deed to the City of Addison recorded in Volume 82022, Page 1211, D.R.D.C.T., a tract of land described in deed to the Town of Addison recorded in Volume 99015, Page 2143, D.R.D.C.T. a tract of land described in deed to the Town of Addison recorded in Volume 97217, Page 1350, D.R.D.C.T., Lot 1, Block 1, of Kincaid Addition, an Addition to the City of Addison, Dallas County, Texas, according to the Plat thereof recorded in Volume 94087, Page 48, D.R.D.C.T., a tract of land described in deed to the Town of Addison recorded in Volume 99249, Page 5436, D.R.D.C.T., a tract of land described in deed to the Town of Addison recorded in Volume 99078, Page 1254, D.R.D.C.T., a tract of land described in Deed to the City of Addison recorded in Volume 5143, Page 162, D.R.D.C.T., and a tract of land described in Deed to the City of Addison recorded in Volume 81150, Page 1985, D.R.D.C.T., said tracts to be known as the Addison Airport tract and being more particularly described as follows:

Beginning at a concrete monument found with a 3 1/2 inch disk stamped "City of Addison" lying in the west right-of-way line of Westgrove Road (a 60 foot wide right-of-way) said point being the most northerly northeast corner of said Addison Airport tract and the southeast corner of Lot 1, Block A, Academy Addition, an addition to the City of Carrollton, Dallas County, Texas according to the Plat thereof recorded in Volume 79186, Page 1, D.R.D.C.T.;

THENCE South 00 degrees 39 minutes 05 seconds East, along the west right-of-way line of said Westgrove Road, a distance of 1547.64 feet to a 5/8 inch iron rod set with cap stamped "DAL-TECH";

THENCE South 21 degrees 16 minutes 44 seconds East, continuing along said west right-of-way line of Westgrove Road, a distance of 539.35 feet to a 5/8 inch iron rod set with cap stamped "DAL-TECH";

THENCE along a curve to the left of said Westgrove Road having a radius of 340.00 feet, a central angle of 69 degrees 17 minutes 21 seconds and an arc length of 411.17 feet, being subtended by a chord of South 55 degrees 55 minutes 24 seconds East for a distance of 386.57 feet to a 5/8 inch iron rod set with cap stamped "DAL-TECH";

THENCE North 89 degrees 25 minutes 55 seconds East, along the southerly right-of-way line of Westgrove Road for a distance of 2,118.93 feet to a 5/8 inch iron rod set with cap stamped "DAL-TECH" at the corner clip of said southerly right-of-way line of Westgrove Road and the west right-of-way line of Addison Road (a 60 foot wide right-of-way);

THENCE South 45 degrees 37 minutes 02 seconds East, along said corner clip, a distance of 21.23 feet to a 5/8 inch iron rod set with cap stamped "DAL-TECH";

THENCE South 00 degrees 39 minutes 59 seconds East, along said west right-of-way line of Addison Road, a distance of 299.74 feet to a 5/8 inch iron rod set with cap stamped "DAL-TECH";

THENCE South 89 degrees 18 minutes 30 seconds West, departing said west right-of-way line of Addison Road, a distance of 200.12 feet to a chiseled "X" set in concrete;

THENCE South 00 degrees 41 minutes 30 seconds East, a distance of 201.72 feet to a found "X";

THENCE South 42 degrees 48 minutes 04 seconds West, a distance of 1,596.12 feet to a found "X";

THENCE South 47 degrees 17 minutes 56 seconds East, a distance of 202.54 feet to a 5/8 inch iron rod set with cap stamped "DAL-TECH";

THENCE South 21 degrees 16 minutes 56 seconds East, a distance of 350.89 feet to a chiseled "X" set in concrete;

THENCE North 68 degrees 43 minutes 04 seconds East, a distance of 30.00 feet to a chiseled cross set in concrete;

THENCE North 70 degrees 38 minutes 55 seconds East, a distance of 185.74 feet to a 5/8 inch iron rod set with cap stamped "DAL-TECH";

THENCE North 44 degrees 10 minutes 12 seconds East, a distance of 7.05 feet to a 5/8 inch iron rod set with cap stamped "DAL-TECH", said point is the common corner between the aforesaid Addison Airport tract and the southwest corner of a tract of land described in deed to D.J. Broughton and E.E. Ericson recorded in Volume 4350, Page 491 of D.R.D.C.T.;

THENCE North 89 degrees 20 minutes 44 seconds East, along the common line of said Addison Airport tract and the south line of said O.J. Broughton tract for a distance of 818.27 feet to a 5/8 inch iron rod set with cap stamped "DAL-TECH" in the west right-of-way line of aforesaid Addison Road;

THENCE South 00 degrees 39 minutes 59 seconds East, a distance of 490.88 feet to a 5/8 inch iron rod set with cap stamped "DAL-TECH" in the apparent common survey line between the aforesaid William Lomar Survey, Abstract No. 792 and the aforesaid E. Cook Survey, Abstract No. 326;

THENCE North 89 degrees 47 minutes 19 seconds East, a distance of 60.66 feet to a 1/2 inch iron rod set with cap stamped "DAL-TECH" in the aforesaid west right-of-way line of Addison Road;

THENCE along a curve to the left in the said west right-of-way line of Addison Road, having a radius of 746.30 feet, a central angle of 01 degree 54 minutes 15 seconds and an arc length of 24.80 feet, being subtended by a chord of South 25 degrees 45 minutes 41 seconds East for a distance of 24.80 feet to a 5/8 inch iron rod set with cap stamped "DAL-TECH";

THENCE South 26 degrees 42 minutes 49 seconds East, along said west right-of-way line of Addison Road, a distance of 34.05 feet to a 5/8 inch iron rod set with cap stamped "DAL-TECH";

THENCE along a curve to the right in the said west right-of-way line of Addison Road having a radius of 886.30 feet, a central angle of 25 degrees 50 minutes 01 second and an arc length of 309.44 feet, being subtended by a chord of South 13 degrees 47 minutes 49 seconds East for a distance of 306.83 feet to a 5/8 inch iron rod set with cap stamped "DAL-TECH";

THENCE South 00 degrees 52 minutes 49 seconds East, along said west right-of-way line of Addison Road, a distance of 2,973.50 feet to a 5/8 inch iron rod set with cap stamped "DAL-TECH";

THENCE South 03 degrees 27 minutes 59 seconds East, along said west right-of-way line of Addison Road, a distance of 9.28 feet to a 5/8 inch iron rod set with cap stamped "DAL-TECH";

THENCE North 89 degrees 58 minutes 24 seconds West, departing said west right-of-way line of Addison Road, a distance of 165.27 feet to a 5/8 inch iron rod set with cap stamped "DAL-TECH";

THENCE South 00 degrees 09 minutes 24 seconds East, a distance of 243.18 feet to a 5/8 inch iron rod set with cap stamped "DAL-TECH";

THENCE South 61 degrees 10 minutes 39 seconds West, a distance of 17.74 feet to a 1/2 inch iron rod found;

THENCE South 00 degrees 09 minutes 29 seconds East, a distance of 209.98 feet to a 5/8 inch iron rod set with cap stamped "DAL-TECH" in the north right-of-way line of the Fort Worth and Denver Railroad spur;

THENCE South 65 degrees 42 minutes 08 seconds West, along said north right-of-way line of said Fort Worth and Denver Railroad spur, a distance of 759.64 feet to a 5/8 inch iron rod found at the most easterly corner of the Addison Airport Industrial District, an Addition to the City of Addison according to the plat thereof recorded in Volume 50, Page 207, D.R.D.C.T.;

THENCE North 67 degrees 26 minutes 03 seconds West, along the said easterly line of Addison Airport Industrial District addition, a distance of 272.68 feet to a 5/8 inch iron rod found;

THENCE North 21 degrees 09 minutes 30 seconds West, along the said easterly line of Addison Airport Industrial District addition, a distance of 571.54 feet to a 5/8 inch iron rod set with cap stamped "DAL-TECH";

THENCE South 75 degrees 21 minutes 17 seconds West, a distance of 449.71 feet to a found railroad spike;

THENCE North 89 degrees 59 minutes 56 seconds West, a distance of 654.32 feet to a 5/8 inch iron rod set with cap stamped "DAL-TECH";

THENCE North 00 degrees 00 minutes 04 seconds East, a distance of 159.62 feet to a 5/8 inch iron rod set with cap stamped "DAL-TECH";

THENCE North 89 degrees 59 minutes 56 seconds West, a distance of 160.02 feet to a 5/8 inch iron rod set with cap stamped "DAL-TECH" in the east right-of-way line of Midway Road (a 100 foot wide right-of-way);

THENCE North 00 degrees 00 minutes 04 seconds East, along said east right-of-way line of Midway Road, a distance of 10.00 feet to a 5/8 inch iron rod set with cap stamped "DAL-TECH";

THENCE South 89 degrees 59 minutes 56 seconds East, a distance of 792.25 feet to a 1/2 inch iron rod found with cap stamped "MLM #2617";

THENCE North 75 degrees 21 minutes 17 seconds East, a distance of 408.83 feet to a 1/2 inch iron rod found with cap stamped "MLM #2617";

THENCE North 21 degrees 09 minutes 30 seconds West, along the aforesaid easterly line of said Addison Airport Industrial District addition, a distance of 2,385.88 feet record to a 1/2 inch iron rod found;

THENCE North 21 degrees 01 minute 39 seconds West, a distance of 320.94 feet to a 5/8 inch iron rod set with cap stamped "DAL-TECH";

THENCE North 89 degrees 44 minutes 31 seconds East, a distance of 10.21 feet to a 5/8 inch iron rod set with cap stamped "DAL-TECH";

THENCE North 20 degrees 38 minutes 06 seconds West, a distance of 221.75 feet to a 5/8 inch iron rod set with cap stamped "DAL-TECH";

THENCE North 89 degrees 54 minutes 56 seconds West, a distance of 177.90 feet to a 5/8 inch iron rod set with cap stamped "DAL-TECH" in the east right-of-way line of Dooley Road (a 50 foot wide right-of-way)

THENCE North 00 degrees 18 minutes 06 seconds West, along the said east right-of-way line of Dooley Road, a distance of 663.91 feet to a 5/8 inch iron rod set with cap stamped "DAL-TECH" for the apparent intersection of said east right-of-way line of Dooley Road and the north right-of-way line of the old Keller Springs Road (a 50 foot wide right-of-way);

THENCE South 89 degrees 33 minutes 56 seconds West, with the said north line of Keller Springs Road, a distance of 105.61 feet to a 5/8 inch iron rod set with cap stamped "DAL-TECH";

THENCE North 56 degrees 29 minutes 35 seconds West, a distance of 65.57 feet to a 5/8 inch iron rod set with cap stamped "DAL-TECH" in the aforesaid east right-of-way line of Dooley Road (a 60 foot wide right-of-way);

THENCE South 89 degrees 19 minutes 27 seconds West, a distance of 361.02 feet to a 5/8 inch iron rod found;

THENCE North 00 degrees 28 minutes 50 seconds West, a distance of 231.53 feet to a 1/2 inch iron rod found;

THENCE North 89 degrees 17 minutes 19 seconds East, a distance of 1.24 feet to a 1/2 inch iron rod found with cap stamped SJ&F

THENCE North 00 degrees 34 minutes 41 seconds West, a distance of 470.04 feet to a 5/8 inch iron rod set with cap stamped "DAL-TECH" in the east line of Beltwood North Phase 2, an addition to the City of Dallas, Texas, according to the Plat thereof recorded in Volume 78201, Page 1, D.R.D.C.T.;

THENCE East, a distance of 70.73 feet along said Beltwood North Phase 2 addition to a 5/8 inch iron rod set with cap stamped "DAL-TECH";

THENCE North 00 degrees 39 minutes 08 seconds West, along said Beltwood North Phase 2 addition, a distance of 235.73 feet to a 5/8 inch iron rod set with cap stamped "DAL-TECH" for the most southerly corner of Lot 7 of said Beltwood North Phase 2 addition;

THENCE North 21 degrees 08 minutes 57 seconds West, passing the north line of said Beltwood North Phase 2 addition at a called distance of 576.04 feet and continuing on for a total distance of 2,125.91 feet to a 5/8 inch iron rod set with cap stamped "DAL-TECH" for the northeast corner of Beltwood North Addition, according to the plat thereof recorded in Volume 78201, Page 1, D.R.D.C.T., same being the southeast corner of Lot 1R-1, Block A, Coil Addition, according to the plat thereof recorded in Volume 2002003, Page 103, D.R.D.C.T.;

THENCE South 89 degrees 35 minutes 51 seconds West, along the common line between the north line of said Beltwood Addition and the south line of said Coil Addition, a distance of 214.48 feet to a 1/2 inch iron rod found in the east right-of-way line of Midway Road (a 100 foot wide right-of-way);

THENCE North 00 degrees 21 minutes 08 seconds West, along said east right-of-way line of Midway Road, passing a 1/2 inch iron rod found for the North corner of said Lot 1R-1, Block A, at a distance of 564.89 feet and continuing for a total distance of 1,756.20 feet to a 1/2 inch iron rod found with cap stamped "SURVCON";

THENCE North 89 degrees 24 minutes 55 seconds East, a distance of 1,165.10 feet to the POINT OF BEGINNING and CONTAINING 16,492,399 square foot or; 378.6 acres of land, more or less.

I, Alan Moore, a Registered Professional Land Surveyor in the State of Texas, do hereby certify that I prepared this Plat from an actual survey of the land under my supervision in accordance with the "Minimum Standards of Practice" approved by the Texas Board of Professional Land Surveyors.

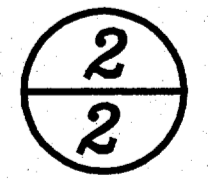
Alan Moore, RPLS No. 5537

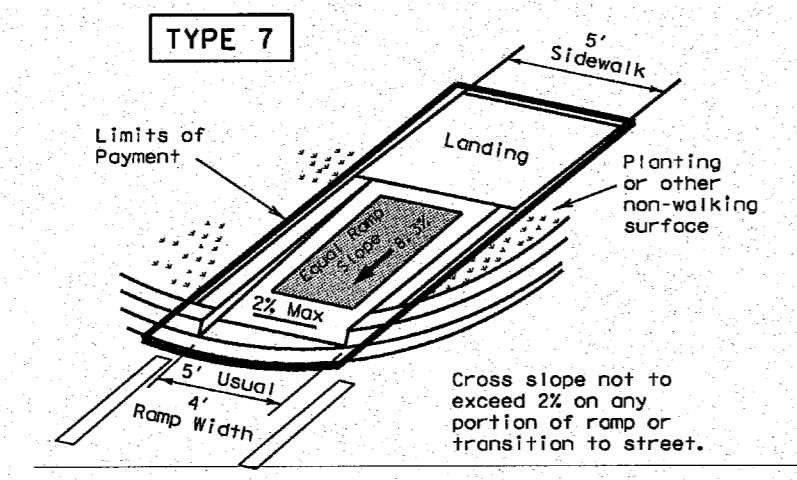
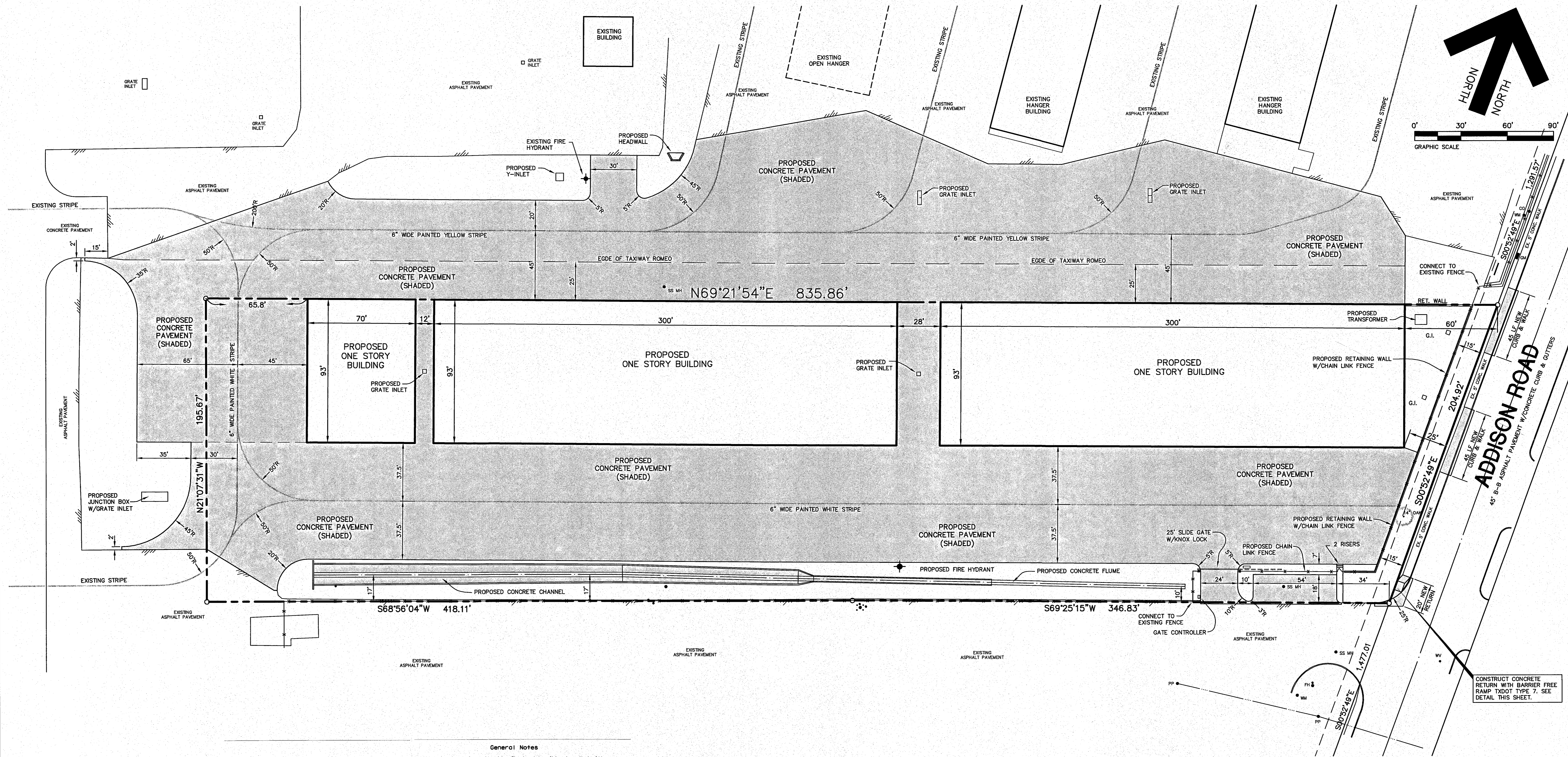
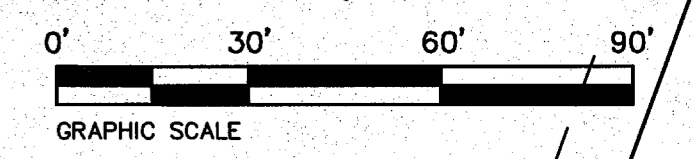
Town of Addison Approval: _____
 City Secretary: Carmen Moran
 Scott Wheeler Mayor (Town of Addison)

DAL-TECH ENGINEERING, INC.
 CONSULTING CIVIL ENGINEERS / SURVEYORS
 CONSTRUCTION MANAGERS
 17311 DALLAS PARKWAY SUITE 200
 DALLAS, TEXAS 75248
 (972) 250-2727 (972) 250-4774

FINAL PLAT
ADDISON AIRPORT
TOWN OF ADDISON, TEXAS

DRAWN	JOB NO.	CONTRACT NO.	SCALE	DATE
DTE	0216	-	1" = 300'	DEC. 2004





DIRECTIONAL RAMP WITHIN RADIUS

General Notes

All slopes are maximum allowable. The least possible slope that will still drain properly should be used. Ramp length or grade of approach sidewalks may be adjusted as directed by the Engineer.

The minimum sidewalk width is 5'. Where a 5' sidewalk cannot be provided due to site constraints, a minimum 3' sidewalk with 5' x 5' posting areas at intervals not to exceed 200 ft is required.

Landings shall be 5' x 5' minimum with a maximum 2% slope in any direction. Maneuvering space at the bottom of curb ramps shall be a minimum of 4' x 4' wholly contained within the crosswalk and wholly outside the parallel vehicular travel path.

Maximum allowable cross slope on sidewalk and ramp surfaces is 2%.

Curb ramps with returned curbs may be used only where pedestrians would not normally walk across the ramp. Otherwise, flared sides shall be provided.

All concrete surfaces shall receive a light broom finish unless noted otherwise in the plans.

Ramp textures must consist of truncated domed surfaces. Textures are required to be detectable underfoot. Surfaces that would allow water to accumulate are prohibited.

Additional information on curb ramp location, design, light reflective value and texture may be found in the current edition of the Texas Accessibility Standards (TAS) prepared and administered by the Texas Department of Licensing and Regulation (TDLR).

Raised medians separate opposing directions of traffic and provide a refuge area for pedestrians unable to cross the entire roadway in the allotted signal phase. To serve as a refuge area, the median should be a minimum of 5' wide. Medians should be designed to provide accessible passage over or through them.

Small channelization islands, which can not provide a minimum 5' x 5' landing at the top of ramps, shall be cut through level with the surface of the street.

Crosswalk dimensions, crosswalk markings and stop bar locations shall be as shown elsewhere in the plans. At intersections where crosswalk markings are not required, ramps shall be aligned with theoretical crosswalks, as directed by the Engineer.

Existing features that comply with TAS may remain in place unless otherwise shown on the plans.

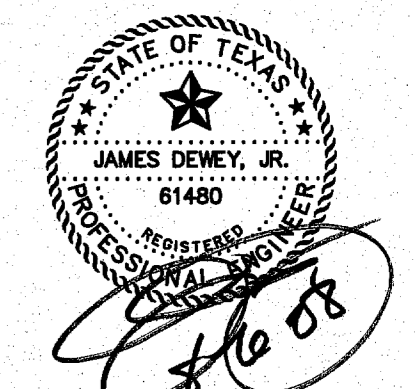
Handrails are not required on curb ramps. Curb ramps shall be provided wherever an accessible route crosses (penetrates) a curb.

Shaded areas indicate locations of detectable warnings. (color / light reflective value and texture contrast)

Curb ramps and landings shall be constructed and paid for in accordance with Item, "Curb Ramp and Landing". Street curb transitions and curb bevels will be paid for in accordance with Item, "Concrete Curb, Gutter and Combined Curb and Gutter".

- NOTES:
1. ALL CONSTRUCTION TO BE DONE IN STRICT ACCORDANCE TO THESE PLANS AND ALL APPLICABLE MUNICIPAL CODES AND STANDARDS.
 2. SEE ARCHITECTURAL PLANS FOR ALL BUILDING DIMENSIONS AND DETAILS.
 3. SEE SHEET C-2 FOR ALL SITE DEMOLITION PLAN.
 4. SEE SHEET C-3 FOR ALL PAVING AND GRADING PLANS.
 5. SEE SHEET C-9 FOR EROSION CONTROL PLAN OF THE SITE.
 6. SEE SHEET C-10 FOR LOCATIONS AND DETAILS OF UTILITY SERVICES.

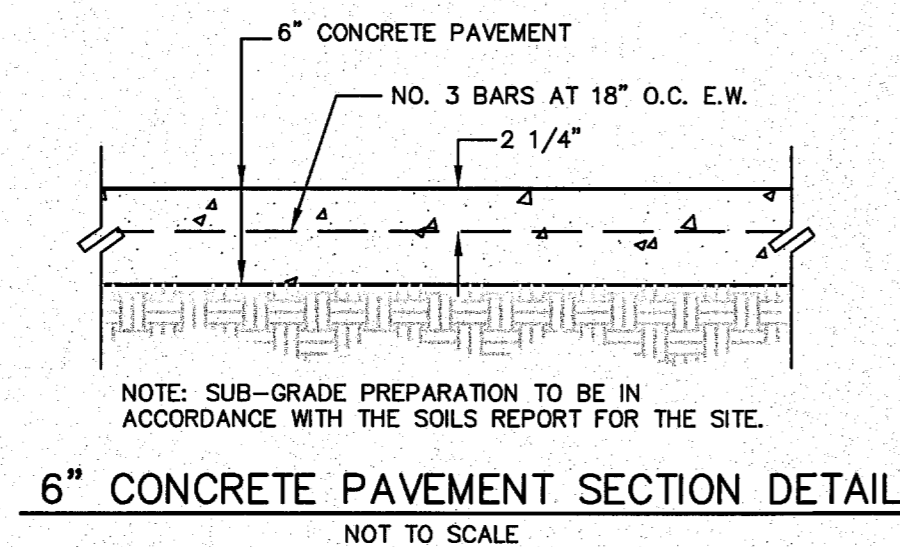
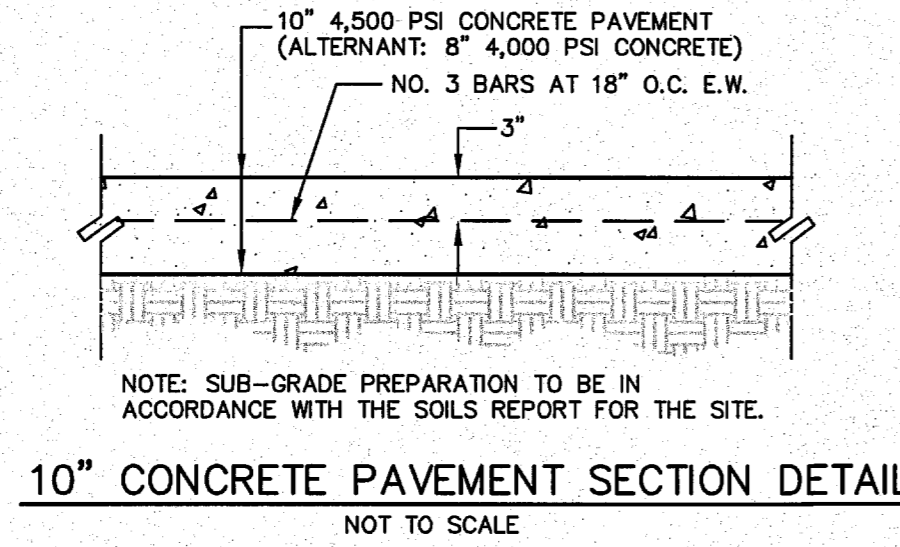
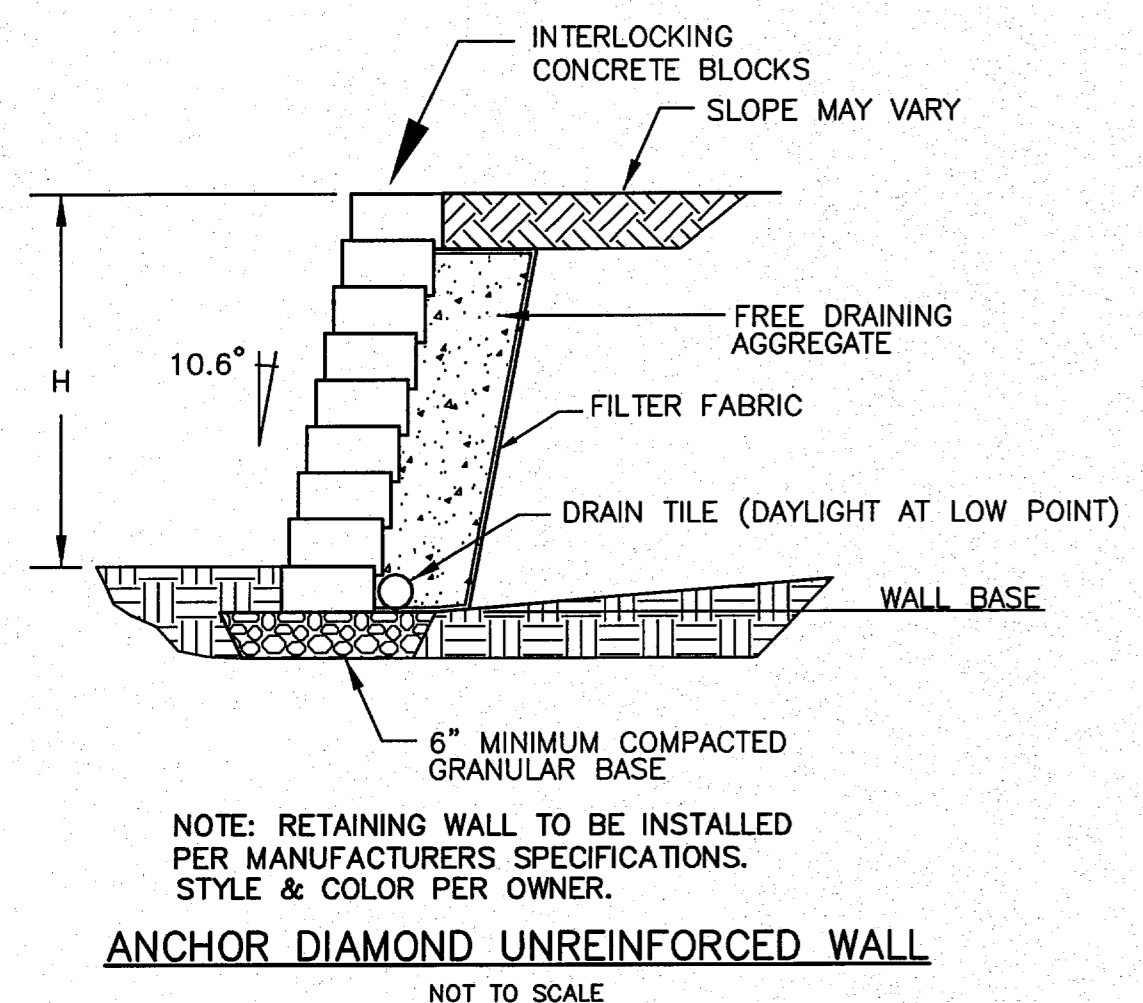
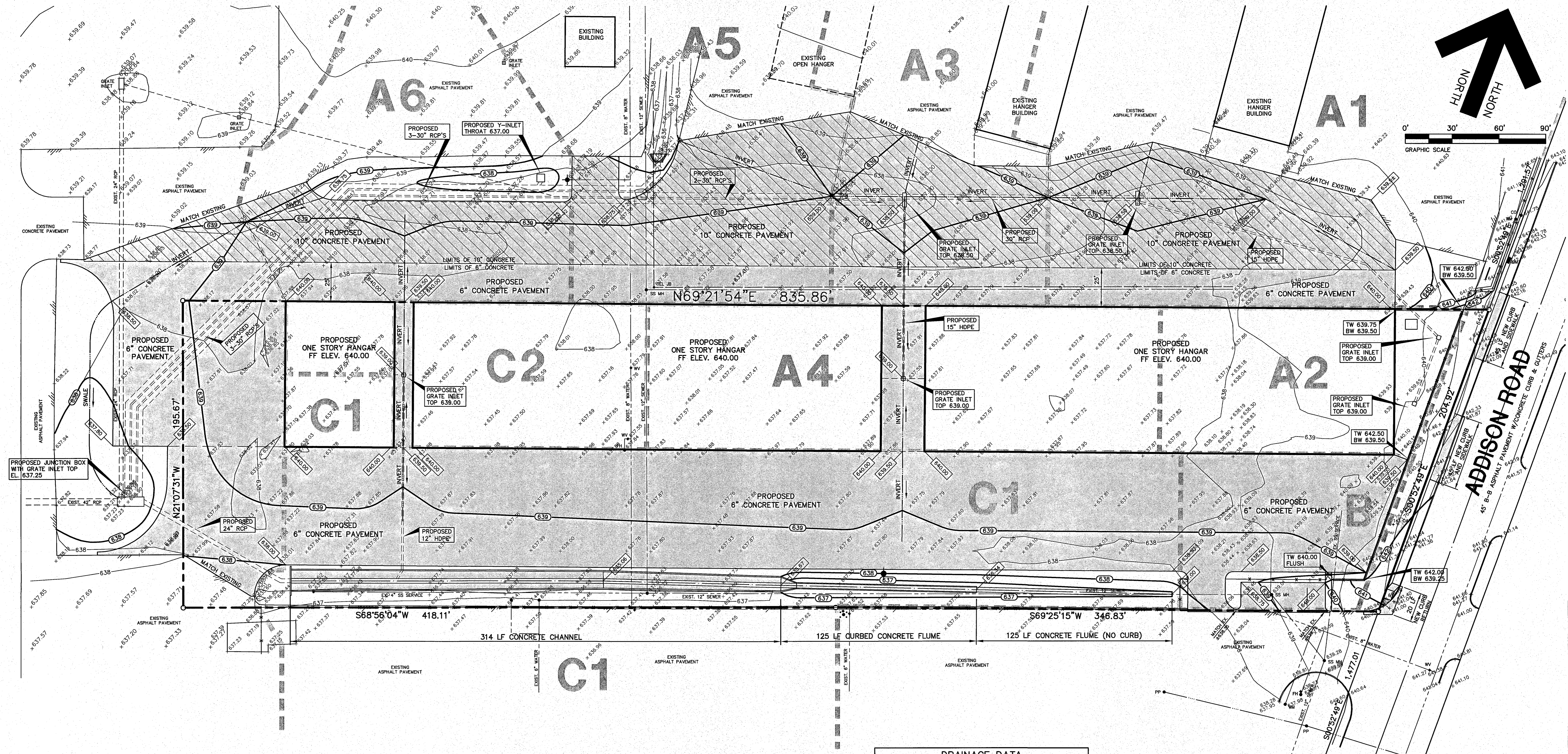
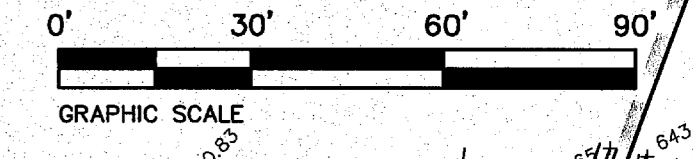
REVISIONS:	
6/04/08	CITY COMMENTS
7/25/08	CITY COMMENTS



SHEET TITLE:
SITE DIMENSIONAL CONTROL PLAN
 HANGER ADDITION PLANS FOR
 EXECUTIVE HANGAR OWNERS ASSOCIATION OF ADDISON
 AIRPORT
 ADDISON, TEXAS

PREPARED BY:
JDJR ENGINEERS AND CONSULTANTS
 ENGINEERS • SURVEYORS • LAND PLANNERS
 2500 Texas Drive Suite 100 Irving, Texas 75062
 Tel 972-252-5357 Fax 972-252-9858

DATE: APR. 8, 2008	DRAWN BY: SAS	SHEET NO.
SCALE: 1" = 30'	CHECKED BY: JDJR	C1 OF 10

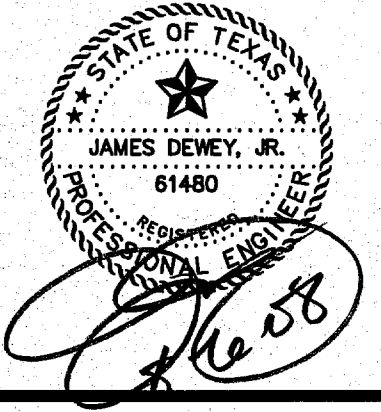


- GENERAL NOTES:**
- ALL CONSTRUCTION SHALL BE DONE IN STRICT CONFORMANCE TO THESE PLANS AND ALL APPLICABLE MUNICIPAL CODES AND STANDARDS.
 - SEE SHEET C-1 FOR ALL HORIZONTAL CONTROL DIMENSIONS.
 - SEE SHEET C2 FOR SITE DEMOLITION PLAN.
 - SEE SHEET C4 FOR OFF-SITE DRAINAGE AREA MAP.
 - SEE SHEET C5 THRU C8 FOR STORM DRAIN PLAN, PROFILES AND DETAILS.
 - SEE SHEET C9 FOR EROSION CONTROL PLAN OF THE SITE.
 - SEE SHEET C10 FOR UTILITY SERVICES PLAN FOR THE SITE.
 - ALL SPOT ELEVATIONS ADJACENT TO CURBS ARE GUTTER ELEVATIONS UNLESS OTHERWISE SHOWN.
 - ALL WALKWAYS TO HAVE A MAXIMUM OF 1/4" PER FOOT CROSSFALL SLOPE AWAY FROM THE BUILDING.
 - ALL SITE PAVING TO BE DONE IN ACCORDANCE TO THE RECOMMENDATIONS AS OUTLINED IN THE SOILS REPORT FOR THIS SITE.
 - ALL SITE GRADING AND SUBGRADE PREPARATION SHALL BE DONE IN ACCORDANCE TO THE RECOMMENDATIONS AS OUTLINED IN THE SOILS REPORT FOR THIS SITE.
 - ALL LANDSCAPED AREAS TO BE UNIFORMLY GRADED AS SHOWN.
- EXISTING UTILITIES NOTES:**
- THE LOCATION OF ALL UNDERGROUND FACILITIES AS INDICATED ON THE PLANS ARE TAKEN FROM PUBLIC RECORDS. JDJR ENGINEERS & CONSULTANTS ASSUMES NO RESPONSIBILITY FOR THE ACCURACY OF SUCH RECORDS AND DOES NOT GUARANTEE THAT ALL UNDERGROUND UTILITIES ARE SHOWN OR ARE LOCATED PRECISELY AS INDICATED.
 - IT IS THE RESPONSIBILITY OF THE CONTRACTOR TO MAKE ARRANGEMENTS WITH THE OWNERS OF SUCH UNDERGROUND FACILITIES PRIOR TO WORKING IN THE AREA TO CONFIRM THEIR EXACT LOCATION AND TO DETERMINE WHETHER ANY ADDITIONAL FACILITIES OTHER THAN THOSE SHOWN ON THE PLANS MAY BE PRESENT.
 - THE CONTRACTOR SHALL PRESERVE AND PROTECT ALL UNDERGROUND FACILITIES FOUND.
 - NOTIFY JDJR ENGINEERS & CONSULTANTS IF ANY UNDERGROUND UTILITIES ARE NOT IN THE LOCATIONS INDICATED ON THESE PLANS (HORIZONTALLY AND VERTICALLY) OR CONFLICT WITH ANY PROPOSED IMPROVEMENTS ASSOCIATED WITH THESE PLANS.

DRAINAGE DATA					
AREA NO.	ACRES	C	T _c MIN	I ₂ IN/HR	Q ₂ CFS
A1	3.42	1.0	10	5.04	17.24
A2	0.39	1.0	10	5.04	1.97
A3	3.33	1.0	10	5.04	16.78
A4	0.71	1.0	10	5.04	3.58
A5	2.62	1.0	10	5.04	13.20
A6	0.80	1.0	10	5.04	4.03
B	0.29	1.0	10	5.04	1.46
C1	2.85	1.0	10	5.04	14.36
C2	0.35	1.0	10	5.04	1.76

- LEGEND**
- 624 ——— EXISTING CONTOUR LINE
 - + 480.25 ——— EXISTING SPOT ELEVATION
 - 625 ——— PROPOSED SPOT ELEVATION
 - PROPOSED CONTOUR LINE
 - INVERT OR FLOWLINE
 - DRAINAGE DIVIDE LINE
 - ▨ INDICATES 6" CONCRETE PAVEMENT
 - ▨ INDICATES 10" CONCRETE PAVEMENT

REVISIONS:	
6/04/08	CITY COMMENTS
7/25/08	CITY COMMENTS



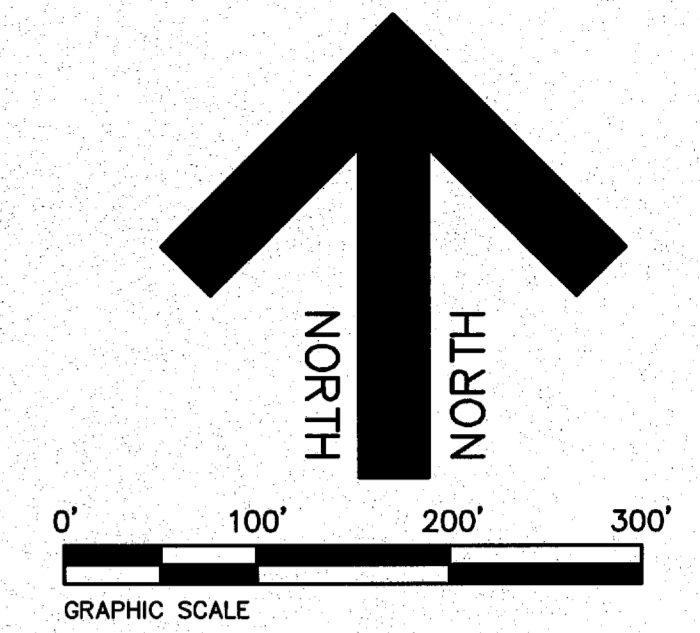
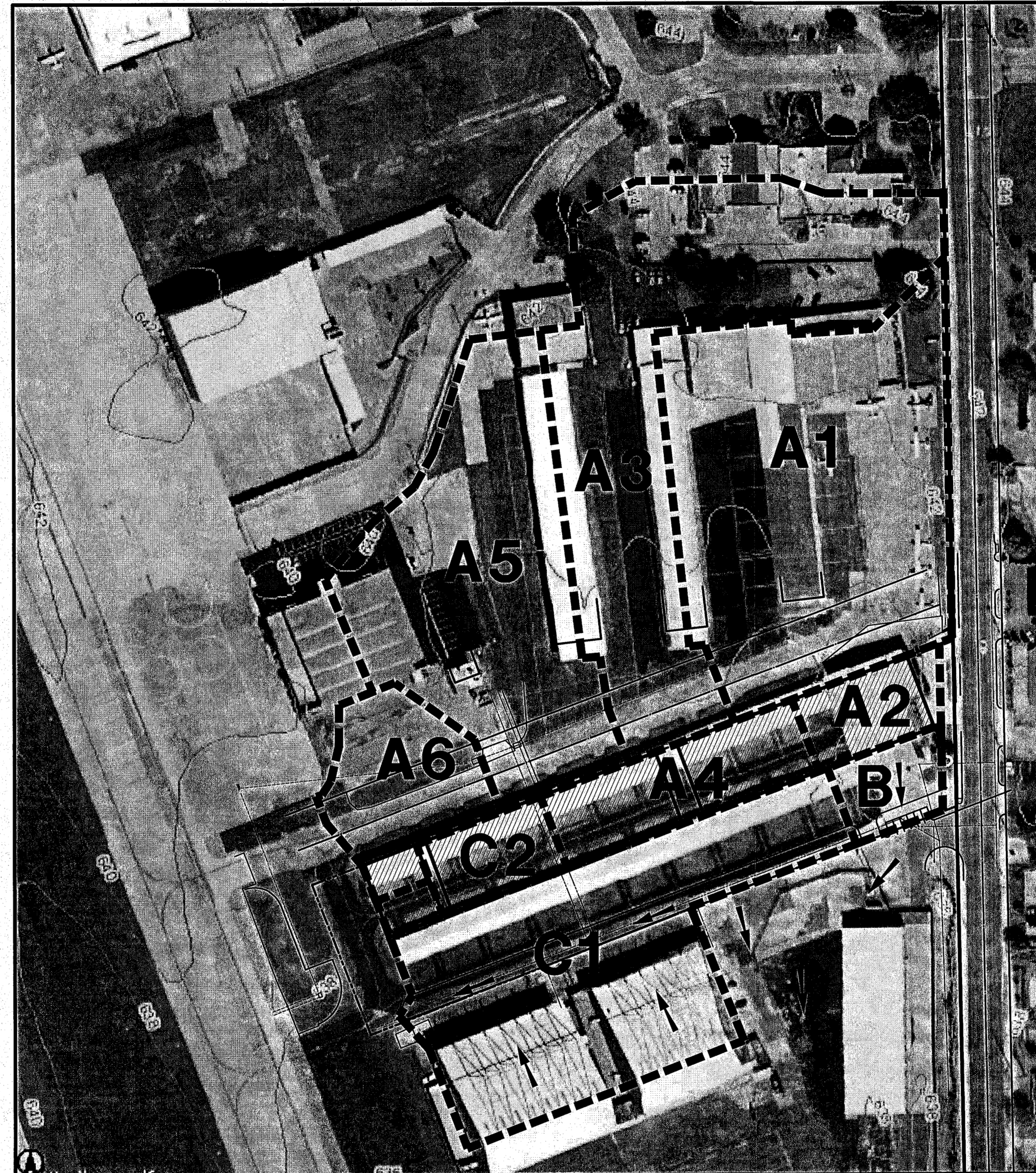
SHEET TITLE:
GRADING AND DRAINAGE PLAN
 HANGER ADDITION PLANS FOR
 EXECUTIVE HANGER OWNERS ASSOCIATION OF ADDISON
 AIRPORT
 ADDISON, TEXAS

PREPARED BY:
JDJR ENGINEERS AND CONSULTANTS
 ENGINEERS • SURVEYORS • LAND PLANNERS
 2500 Texas Drive Suite 100 Irving, Texas 75062
 Tel 972-252-5357 Fax 972-252-8058

BENCH MARK: SQUARE CUT FOUND C.L. OF CURB INLET,
 WEST SIDE OF ADDISON ROAD, 75' SOUTH OF THE C.L.
 OF AIRPORT PKWY.
 ELEVATION 642.51

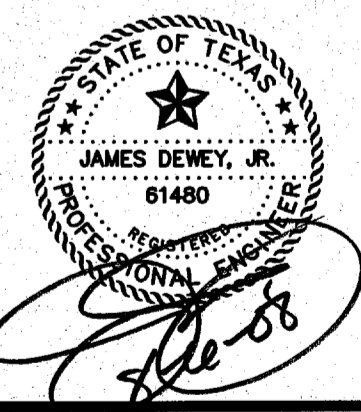
SITE TBM: X-CUT SET IN CONCRETE WALK ±38' NORTH
 OF THE NORTHEAST CORNER OF THIS TRACT.
 ELEVATION 642.86

DATE: APR. 8, 2008	DRAWN BY: SAS	SHEET NO.
SCALE: 1" = 30'	CHECKED BY: JDJR	C3 of 10



DRAINAGE DATA					
AREA NO.	ACRES	C	T _C MIN	I ₂ IN/HR	Q ₂ CFS
A1	3.42	1.0	10	5.04	17.24
A2	0.39	1.0	10	5.04	1.97
A3	3.33	1.0	10	5.04	16.78
A4	0.71	1.0	10	5.04	3.58
A5	2.62	1.0	10	5.04	13.20
A6	0.80	1.0	10	5.04	4.03
B	0.29	1.0	10	5.04	1.46
C1	2.85	1.0	10	5.04	14.36
C2	0.35	1.0	10	5.04	1.76

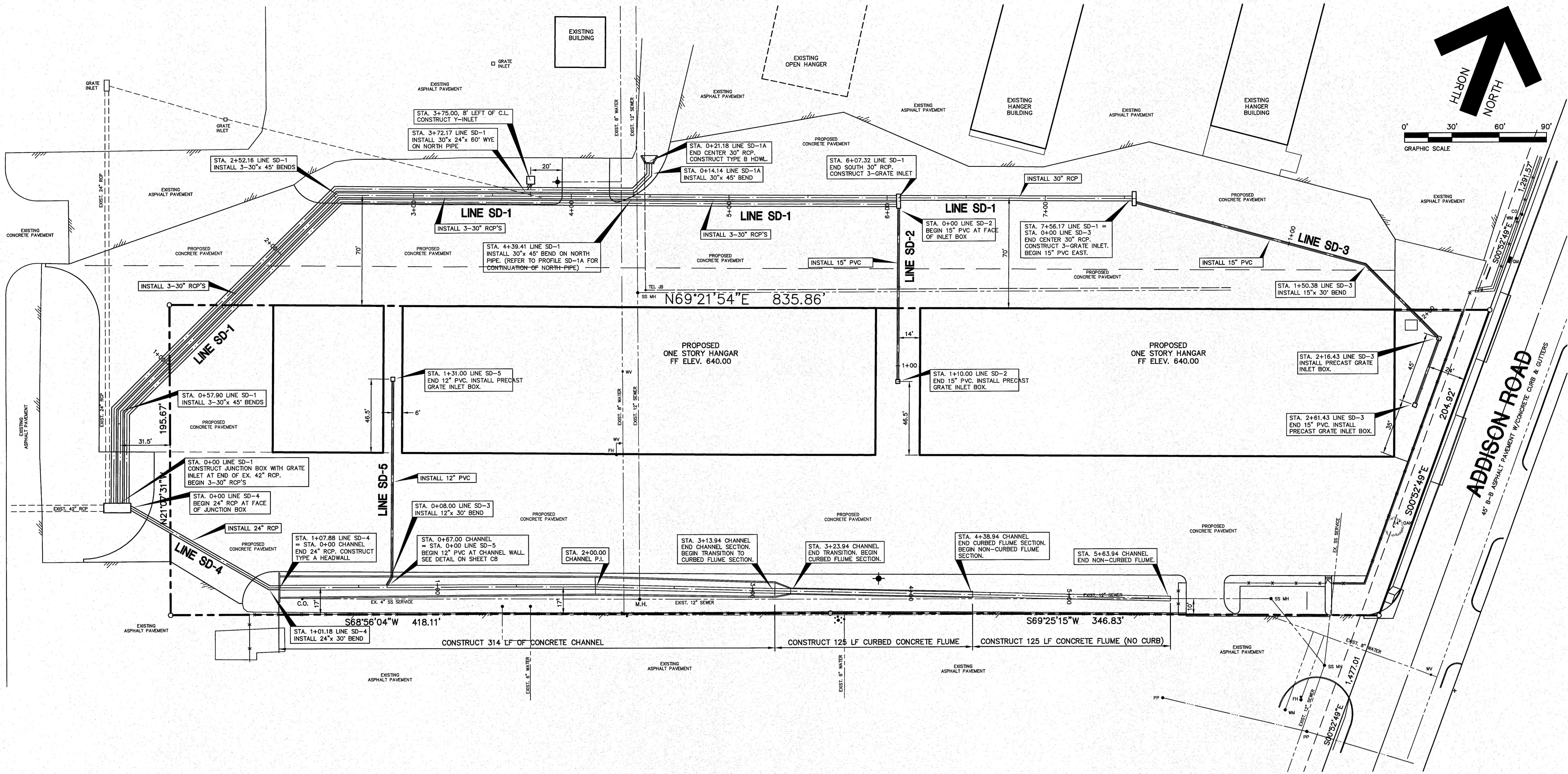
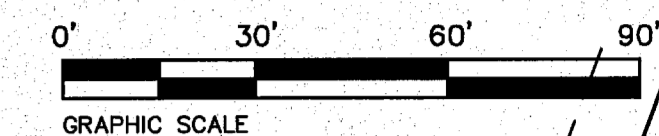
REVISIONS:	



SHEET TITLE:
OFF-SITE DRAINAGE AREA MAP
 HANGER ADDITION PLANS FOR
 EXECUTIVE HANGAR OWNERS ASSOCIATION OF ADDISON
 ADDISON AIRPORT
 ADDISON, TEXAS

PREPARED BY:
JDJR ENGINEERS AND CONSULTANTS
 ENGINEERS • SURVEYORS • LAND PLANNERS
 2800 Texas Drive Suite 100 Irving, Texas 75062
 Tel 972-252-6357 Fax 972-252-8858

DATE: APR. 8, 2008	DRAWN BY: SAS	SHEET NO.
SCALE: 1" = 100'	CHECKED BY: JDJR	C4 OF 10



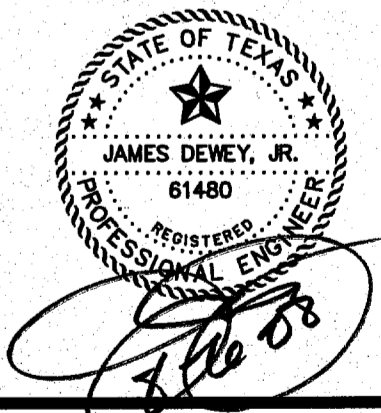
EXISTING UTILITIES NOTES:

- THE LOCATION OF ALL UNDERGROUND FACILITIES AS INDICATED ON THE PLANS ARE TAKEN FROM PUBLIC RECORDS. JDJR ENGINEERS & CONSULTANTS ASSUMES NO RESPONSIBILITY FOR THE ACCURACY OF SUCH RECORDS AND DOES NOT GUARANTEE THAT ALL UNDERGROUND UTILITIES ARE SHOWN OR ARE LOCATED PRECISELY AS INDICATED.
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GENERAL NOTES:

- ALL CONSTRUCTION SHALL BE DONE IN STRICT CONFORMANCE TO THESE PLANS AND ALL APPLICABLE MUNICIPAL CODES AND STANDARDS.
- SEE SHEET C1 FOR ALL HORIZONTAL CONTROL DIMENSIONS.
- SEE SHEET C3 AND C4 FOR GRADING PLAN AND DRAINAGE AREA MAP.
- SEE SHEET C6 AND C7 FOR STORM DRAIN PROFILES.
- SEE SHEET C8 FOR STORM DRAIN DETAILS.
- SEE SHEET C9 FOR EROSION CONTROL PLAN OF THE SITE.
- SEE SHEET C10 FOR UTILITY SERVICES PLAN FOR THE SITE.

REVISIONS:	
6/04/08	CITY COMMENTS
7/25/08	CITY COMMENTS



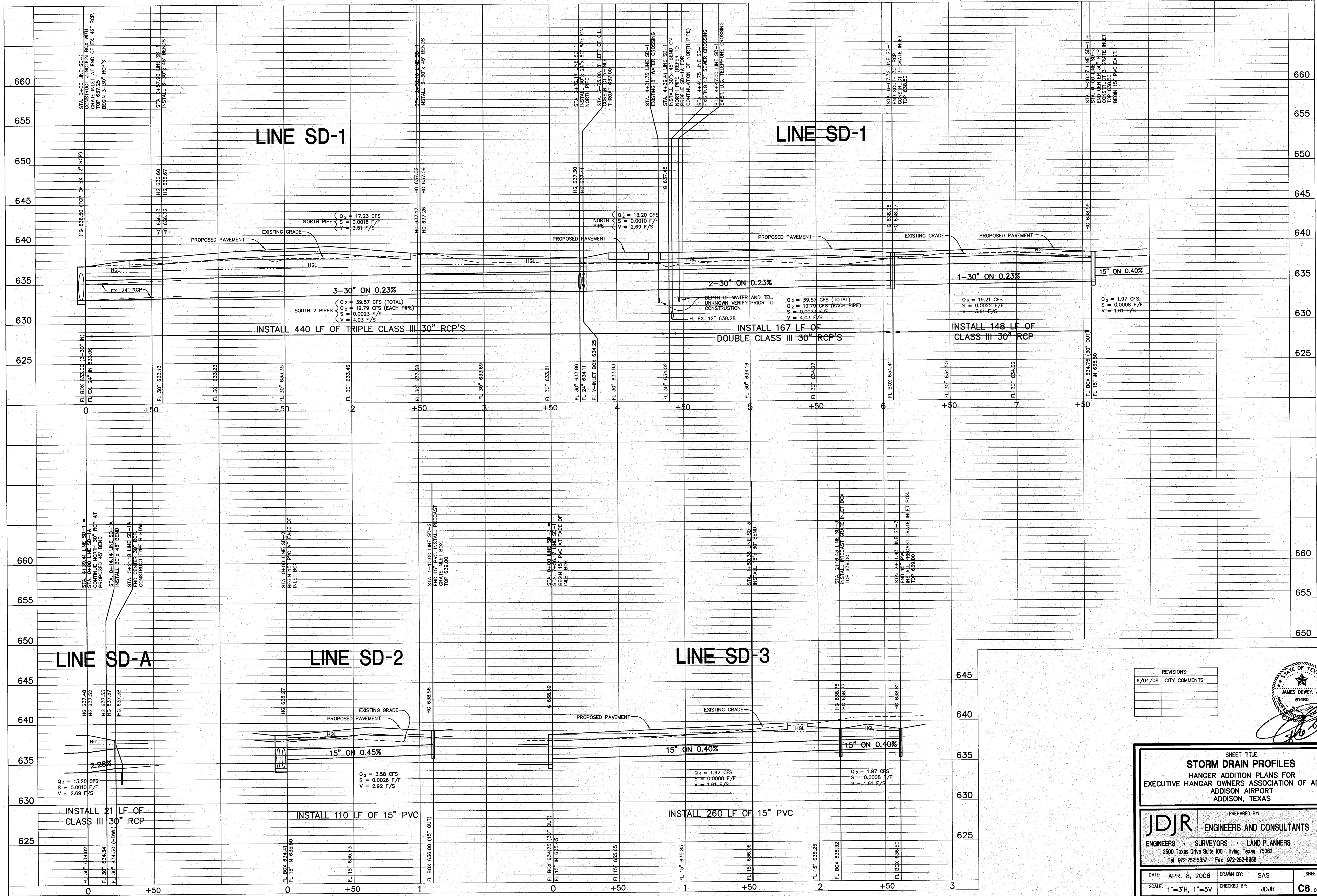
SHEET TITLE:
STORMDRAIN PLAN
 HANGER ADDITION PLANS FOR
 EXECUTIVE HANGER OWNERS ASSOCIATION OF ADDISON
 ADDISON AIRPORT
 ADDISON, TEXAS

PREPARED BY:
JDJR ENGINEERS AND CONSULTANTS

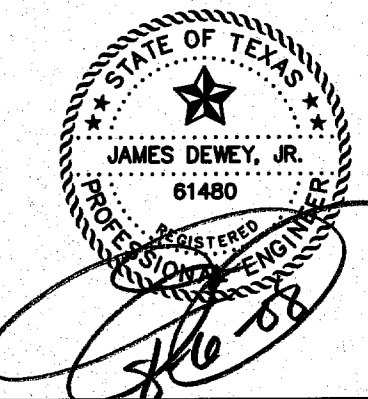
ENGINEERS • SURVEYORS • LAND PLANNERS
 2500 Texas Drive Suite 100 Irving, Texas 75062
 Tel 972-252-5357 Fax 972-252-8858

DATE: APR. 8, 2008 DRAWN BY: SAS SHEET NO.
 SCALE: 1" = 30' CHECKED BY: JDJR C5 OF 10

BENCH MARK: SQUARE CUT FOUND C.L. OF CURB INLET,
 WEST SIDE OF ADDISON ROAD, 75' SOUTH OF THE C.L.
 OF AIRPORT PKWY.
 ELEVATION 642.51
 SITE TM: X-CUT SET IN CONCRETE WALK ±38' NORTH
 OF THE NORTHEAST CORNER OF THIS TRACT.
 ELEVATION 642.86



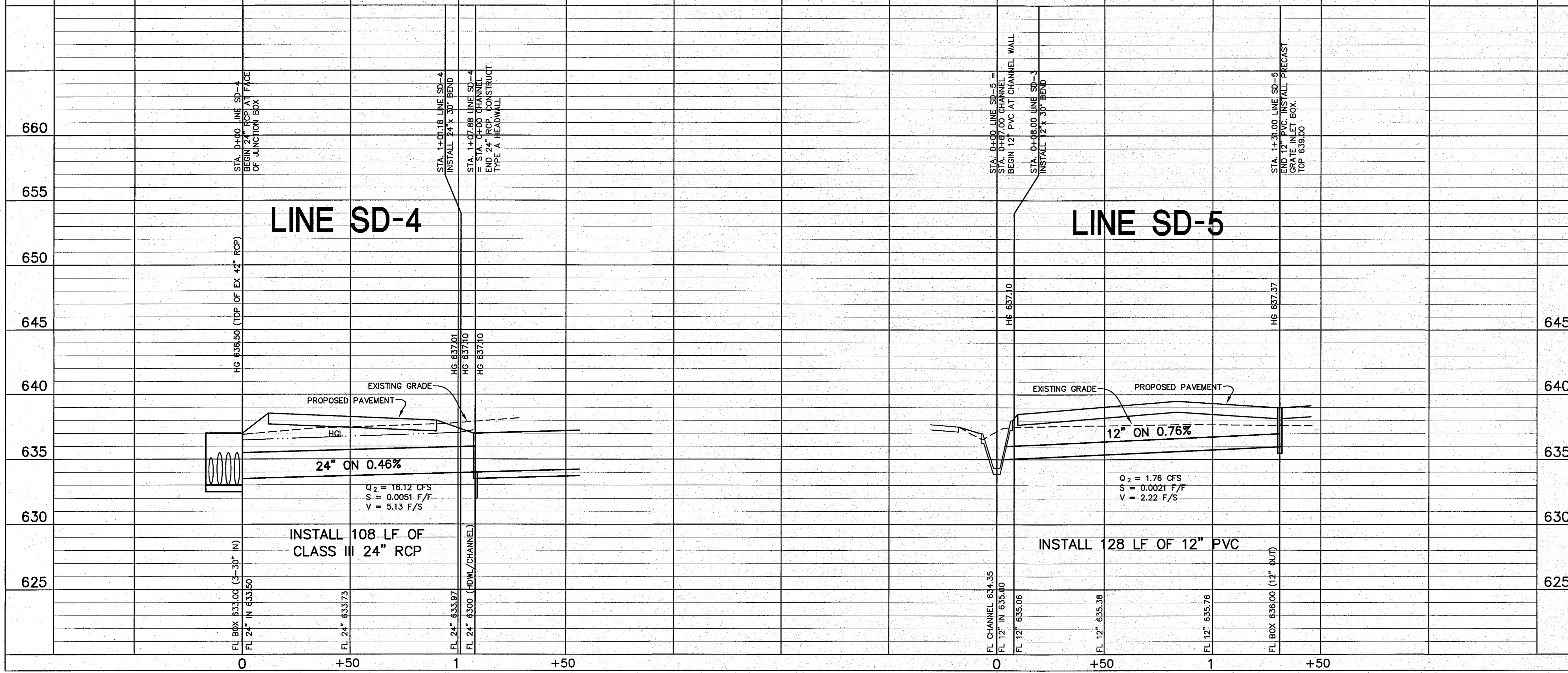
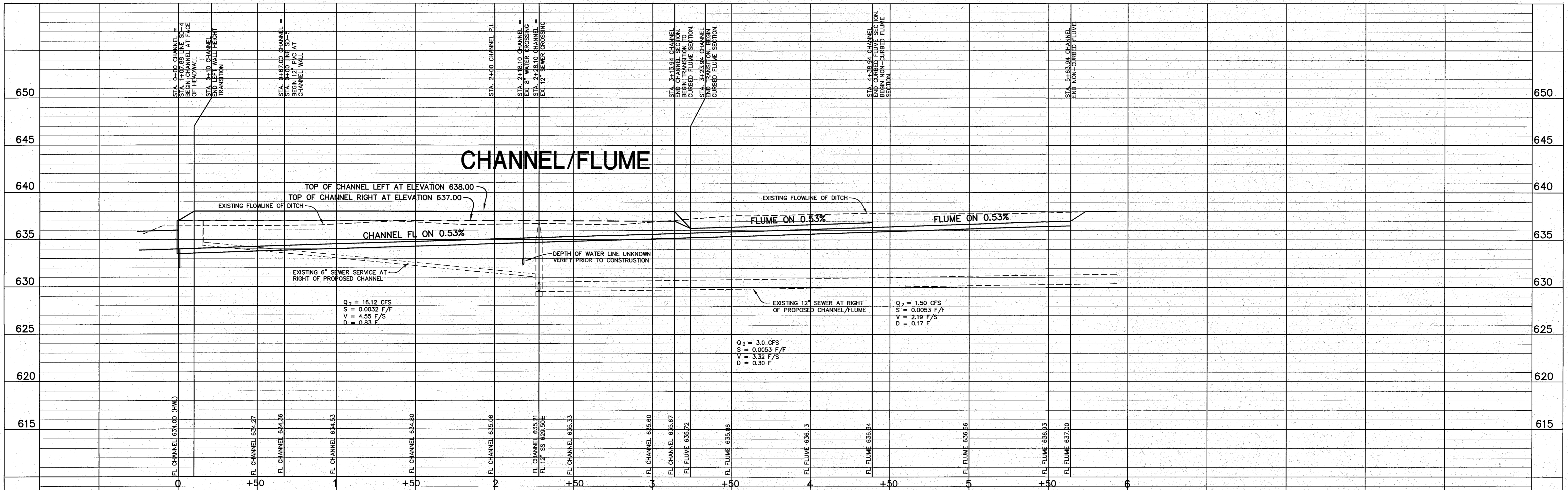
REVISIONS:	
6/04/08	CITY COMMENTS



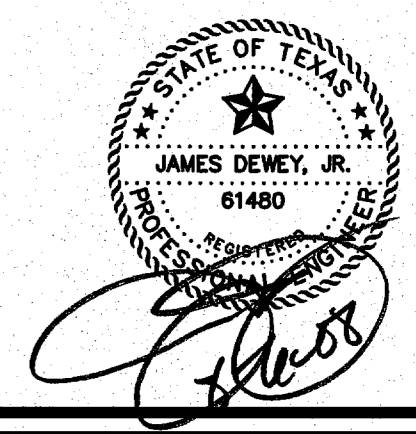
SHEET TITLE:
STORM DRAIN PROFILES
 HANGER ADDITION PLANS FOR
 EXECUTIVE HANGAR OWNERS ASSOCIATION OF ADDISON
 ADDISON AIRPORT
 ADDISON, TEXAS

PREPARED BY:
JDJR ENGINEERS AND CONSULTANTS
 ENGINEERS • SURVEYORS • LAND PLANNERS
 2500 Texas Drive Suite 100 Irving, Texas 75062
 Tel 972-262-6367 Fax 972-262-8958

DATE: APR. 8, 2008	DRAWN BY: SAS	SHEET NO.
SCALE: 1"=3'H, 1"=5'V	CHECKED BY: JDJR	C6 OF 10



REVISIONS:	
6/04/08	CITY COMMENTS

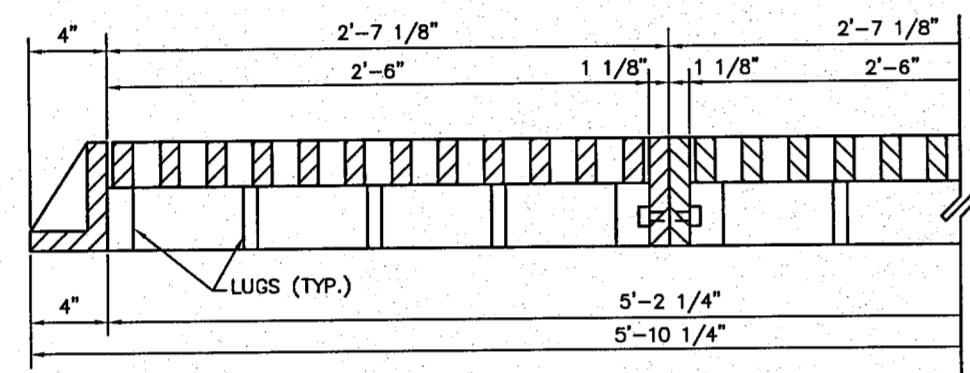


SHEET TITLE:
STORM DRAIN PROFILES
HANGER ADDITION PLANS FOR
EXECUTIVE HANGAR OWNERS ASSOCIATION OF ADDISON
ADDISON AIRPORT
ADDISON, TEXAS

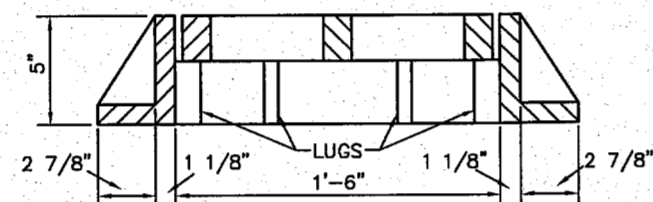
PREPARED BY:
JDJR ENGINEERS AND CONSULTANTS
ENGINEERS • SURVEYORS • LAND PLANNERS
2500 Texas Drive Suite 100 Irving, Texas 75062
Tel 972-252-5357 Fax 972-252-8858

DATE: APR. 8, 2008	DRAWN BY: SAS	SHEET NO.
SCALE: 1"=3'H, 1"=5V	CHECKED BY: JDJR	C7 of 10

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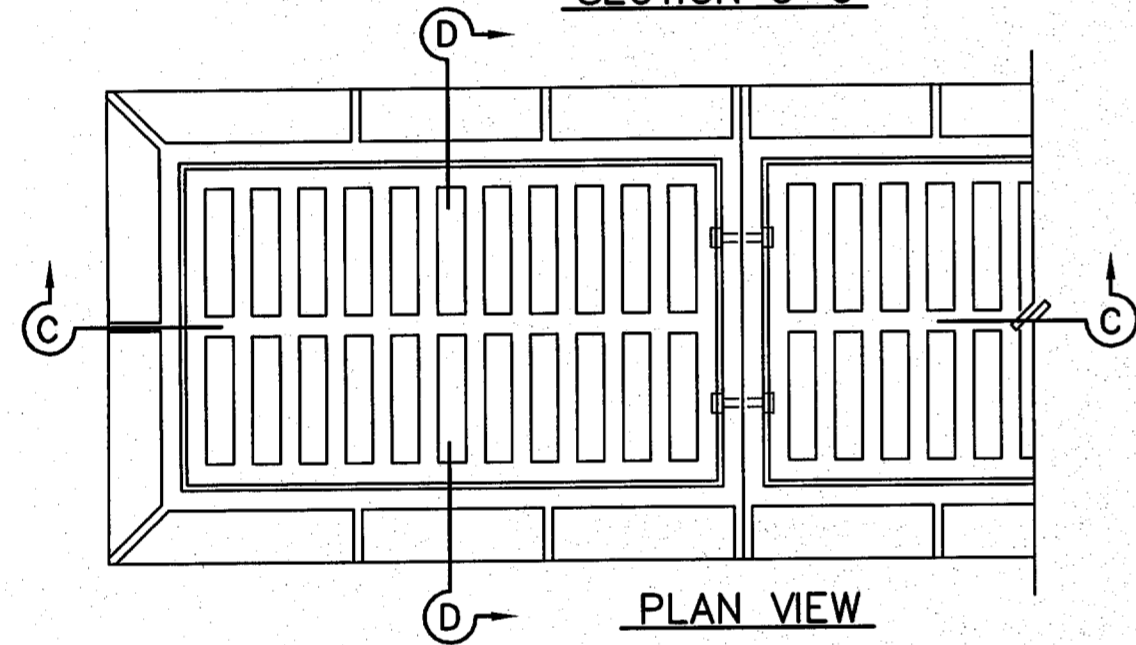


SECTION C-C

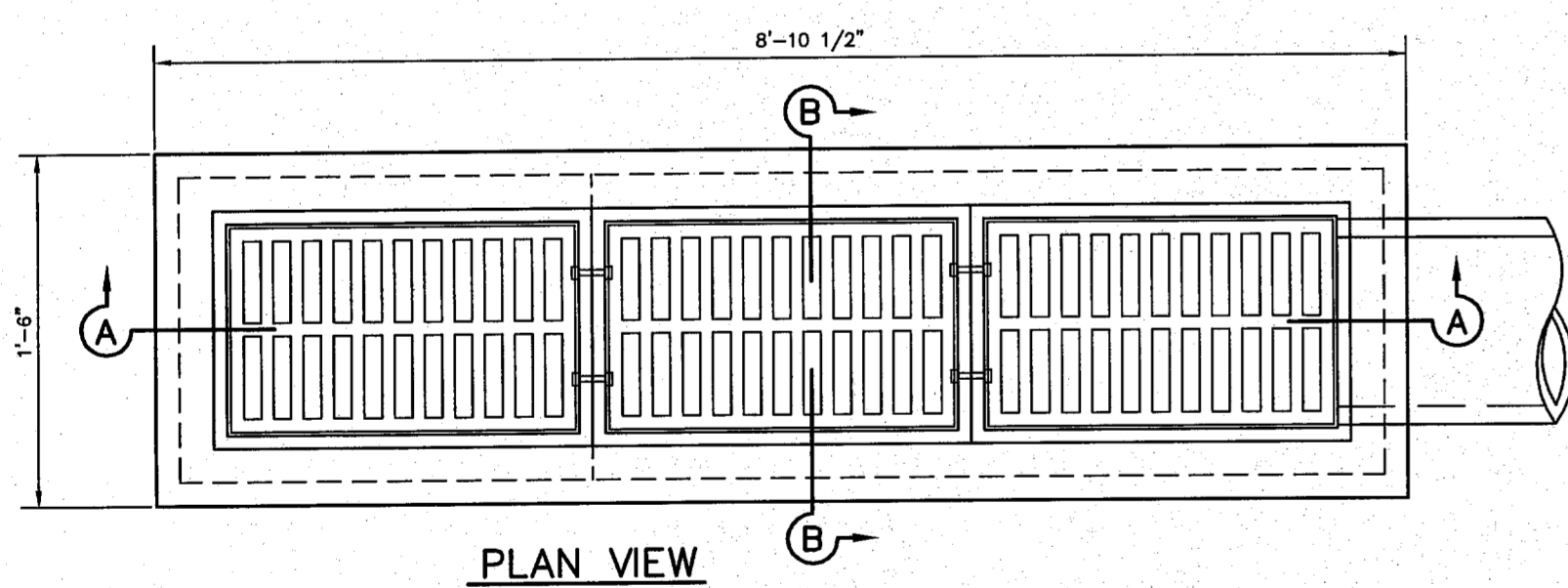


SECTION D-D

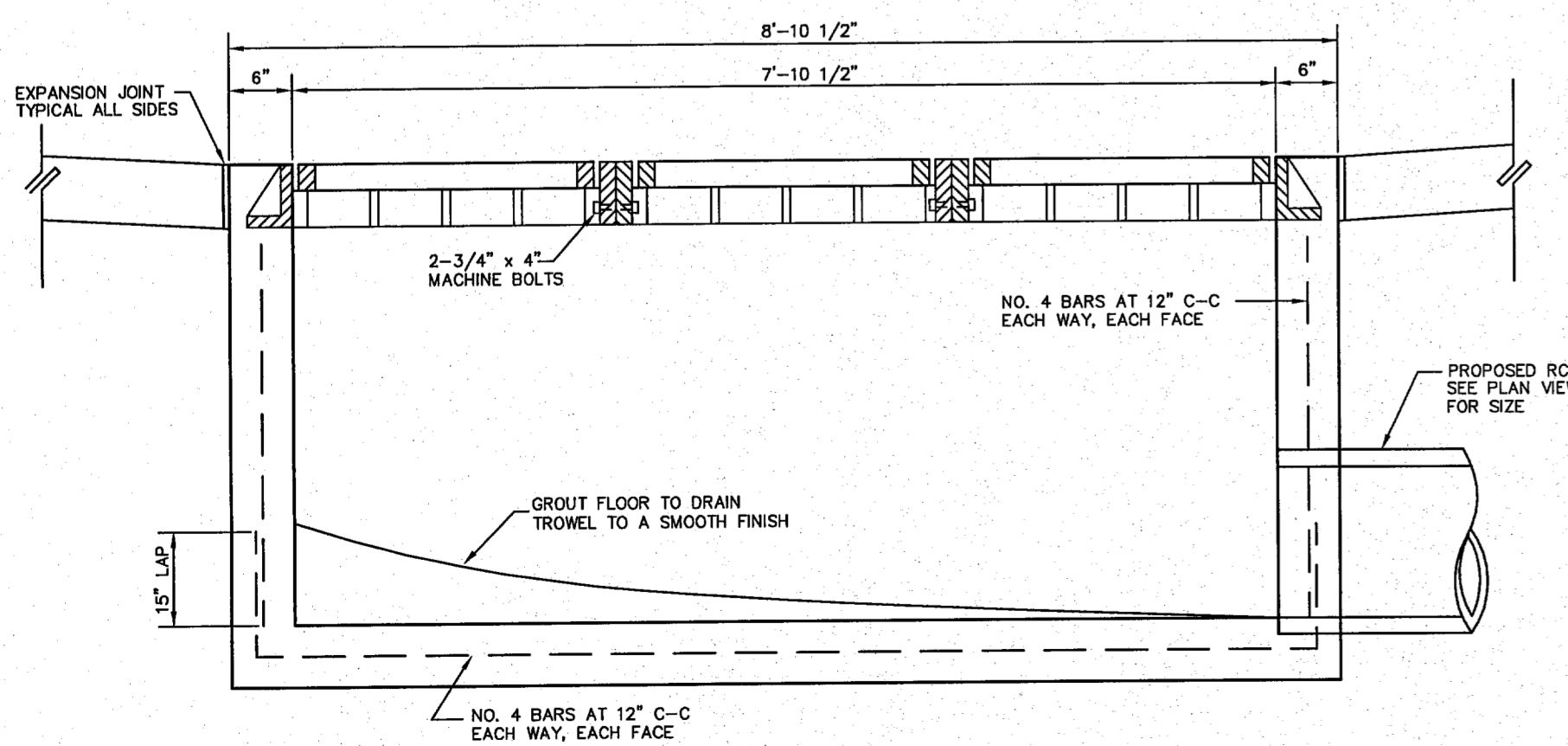
FRAME AND GRATE SHALL BE PATTERN NO. 814 AS MANUFACTURED BY BASS & HAYS FOUNDRY, OR APPROVED EQUAL.



GRATE DETAILS

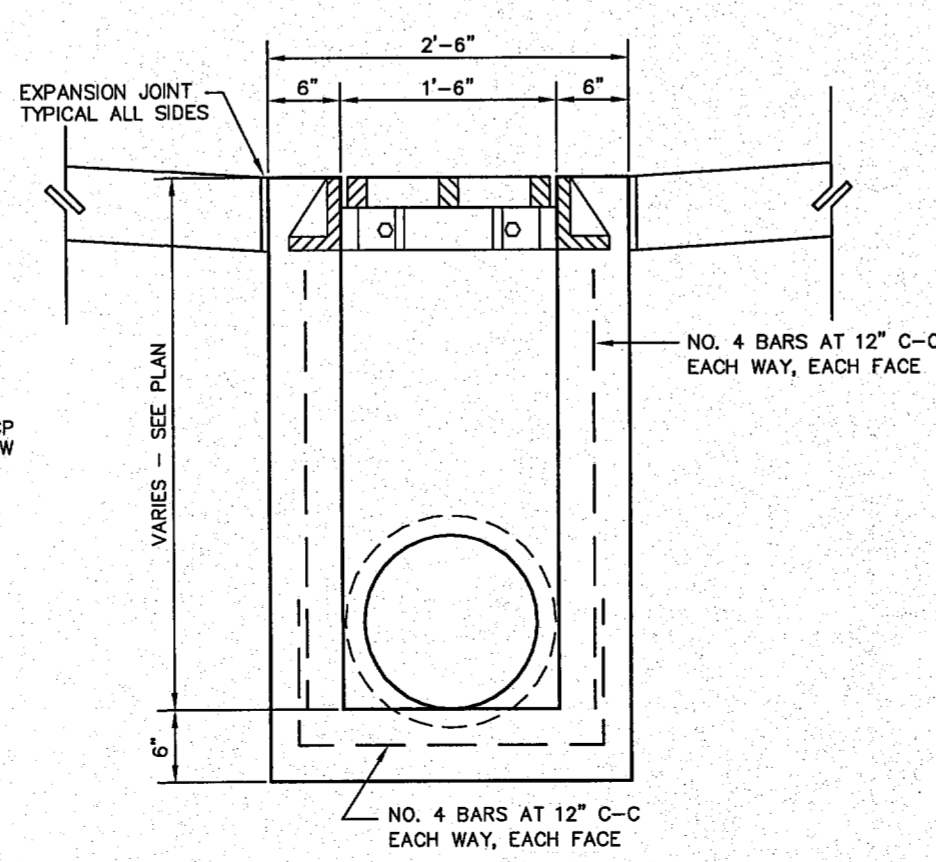


PLAN VIEW



SECTION A-A

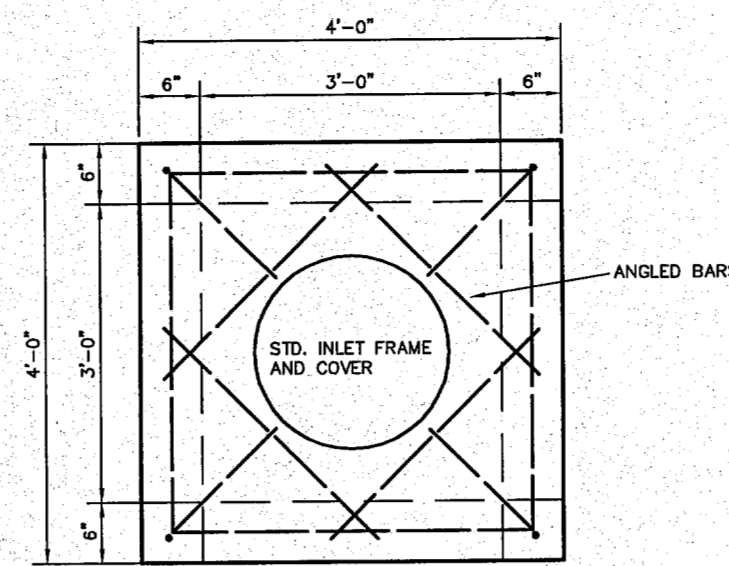
GRATE INLET DETAILS



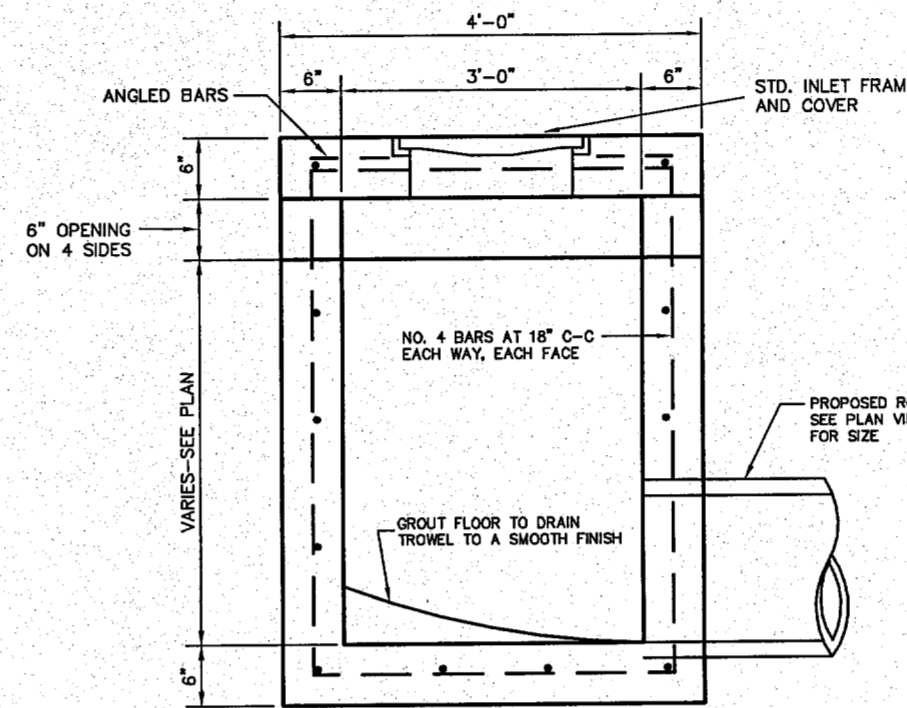
SECTION B-B

NOTES:

1. ALL LAPS AND EXTENSIONS OF REINFORCING BARS SHALL BE 30 BAR DIAMETERS UNLESS NOTED OTHERWISE.
2. TACK WELD GRATES IN PLACE OR USE GRATE LOCK.
3. PIPE MAY BE PLACED IN ANY WALL, BUT SHALL NOT ENTER ANY CORNER, OR BOTTOM.
4. CONCRETE TO BE A MINIMUM OF 4200 P.S.I.



PLAN VIEW

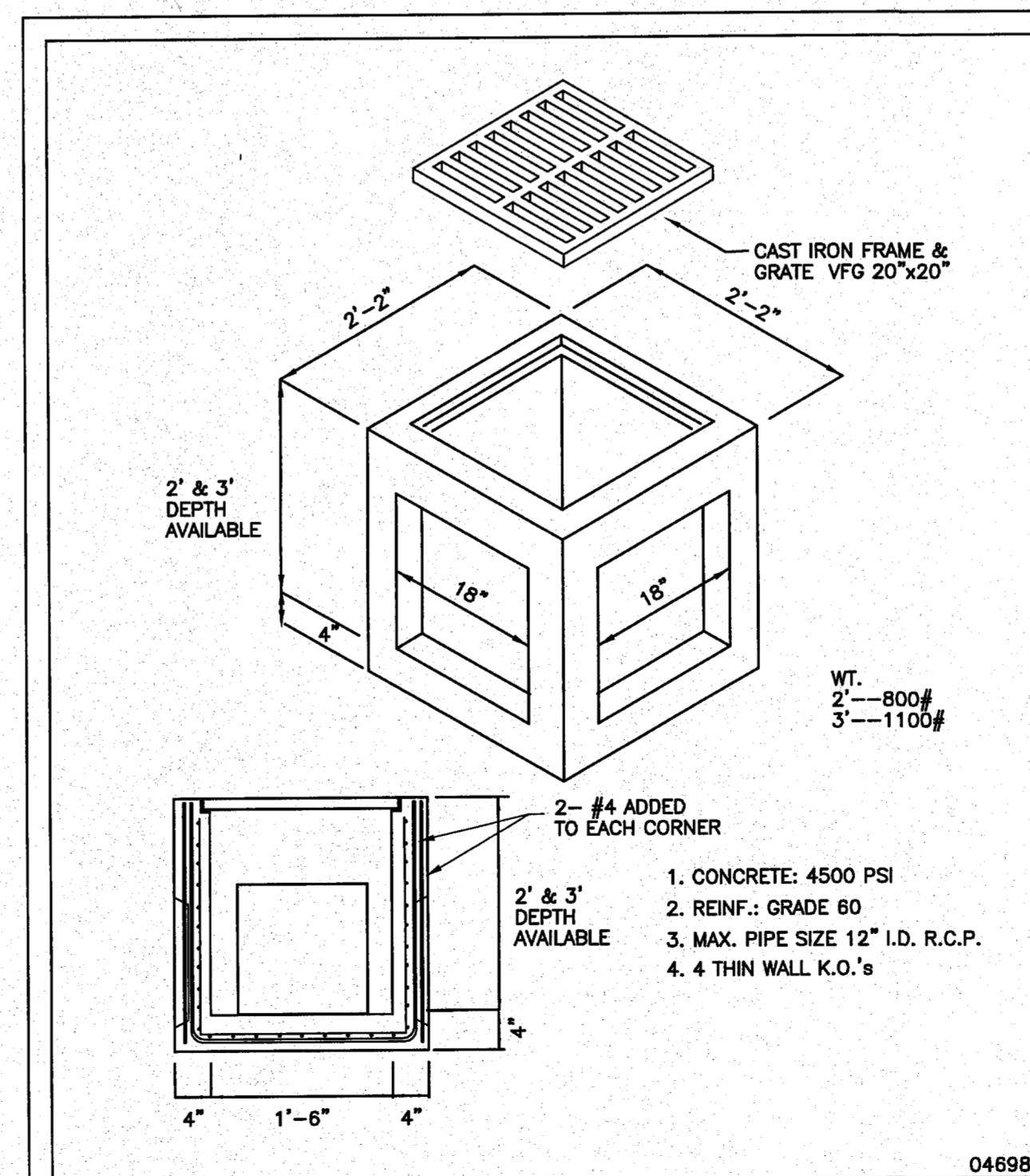


SECTION VIEW

NOTES:

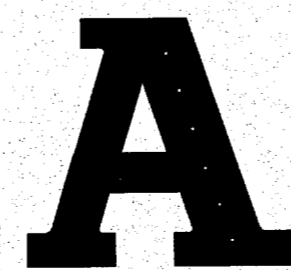
1. ALL CONCRETE SHALL BE CLASS "A"
2. LATERAL PIPE MAY ENTER AT ANY LOCATION.

Y-INLET DETAILS

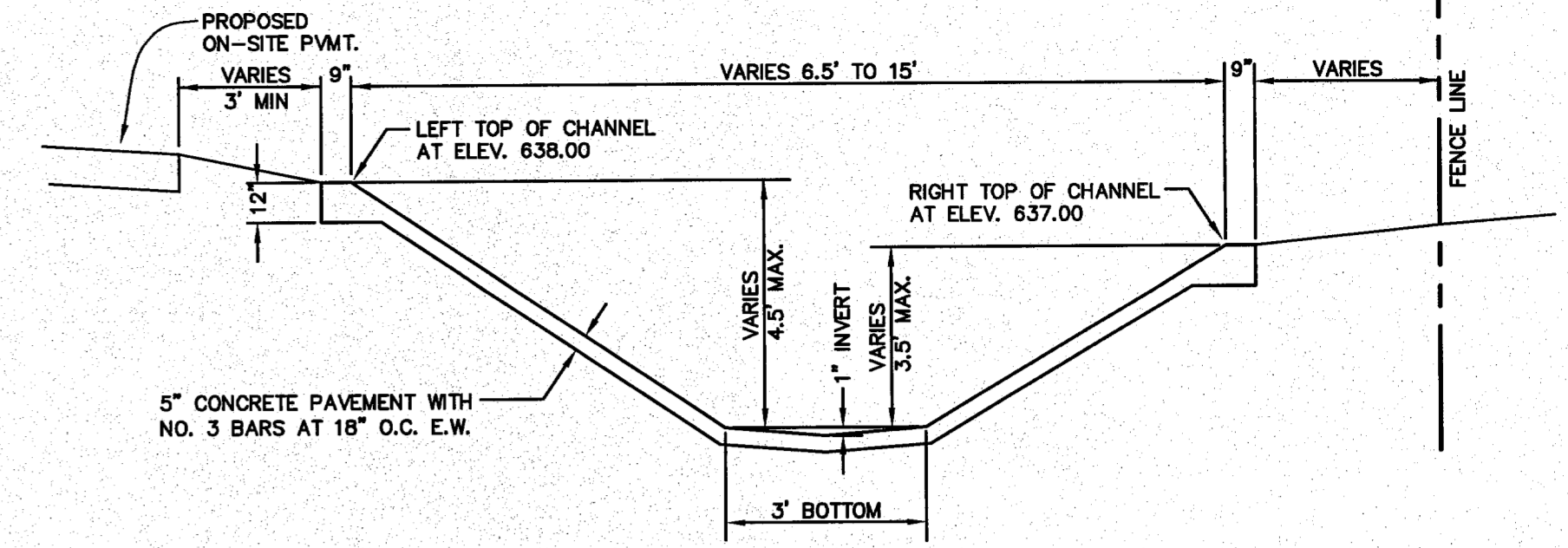


04698

CATCH BASIN NO. 20

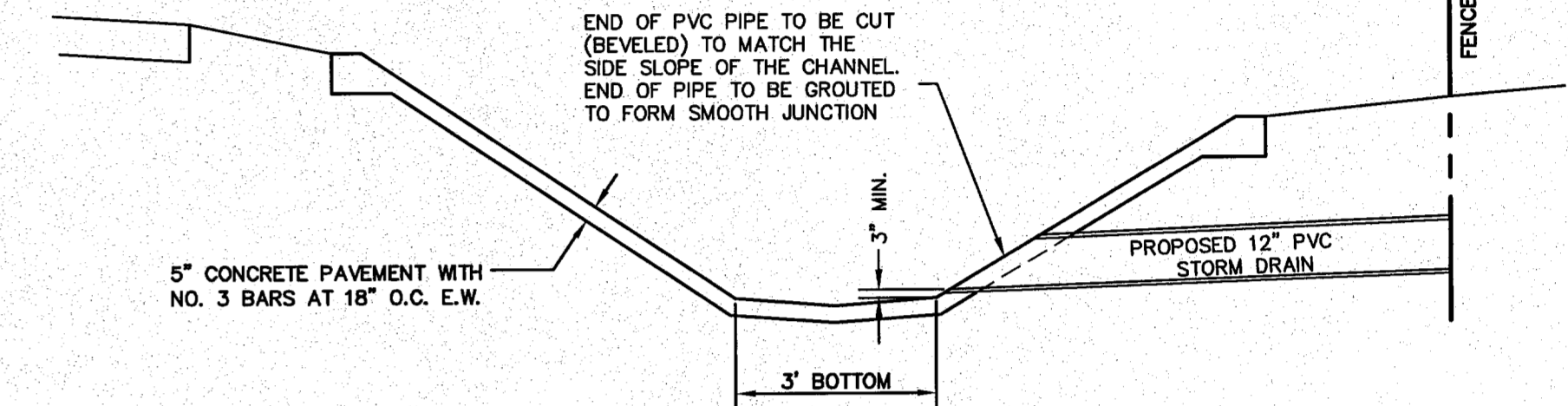


American Industrial Pre-Cast Products, Inc.
P.O. BOX 365 ALVARADO, TEXAS 76009. METRO: 817-477-5286
501 EAST BAXTER SEGUIN, TEXAS 78155 210-401-0555



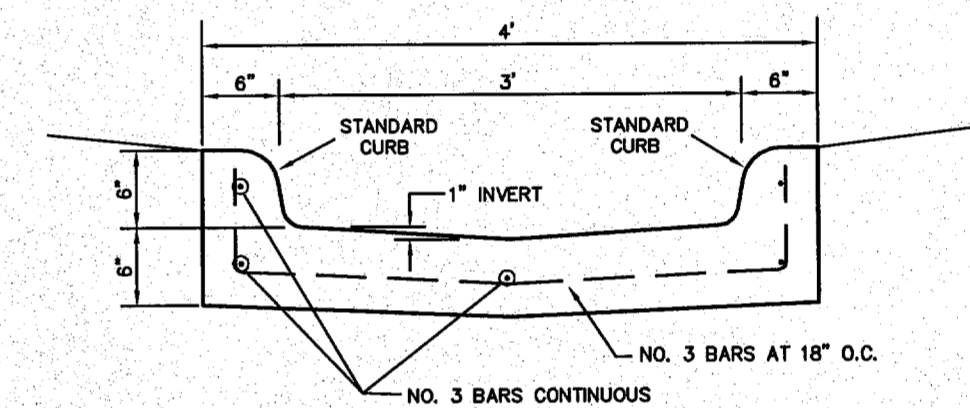
CONCRETE CHANNEL LINER SECTIONAL DETAIL

NOT TO SCALE



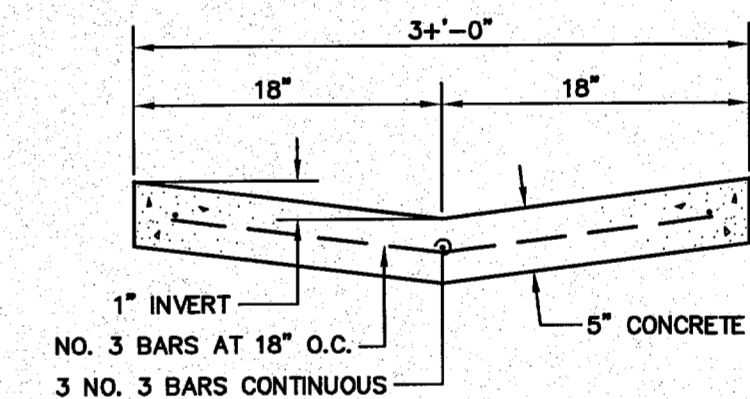
12" PVC OUTFALL AT CONCRETE CHANNEL DETAIL

NOT TO SCALE



CURBED FLUME DETAIL

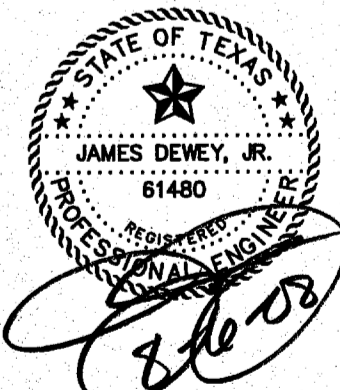
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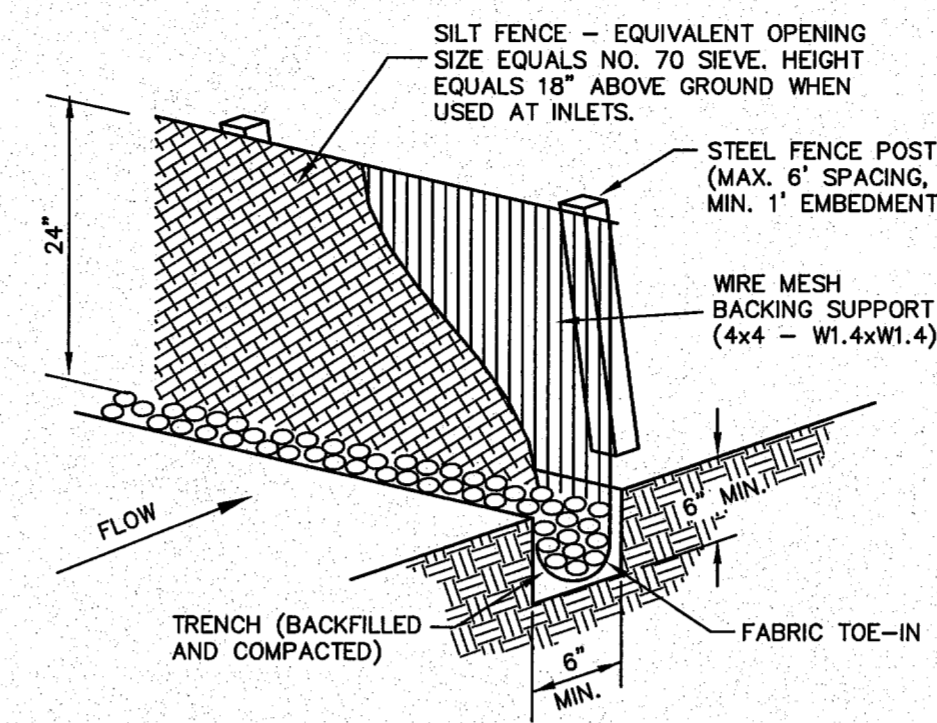
CONCRETE FLUME DETAIL

NOT TO SCALE

REVISIONS:	
7/25/08	CITY COMMENTS



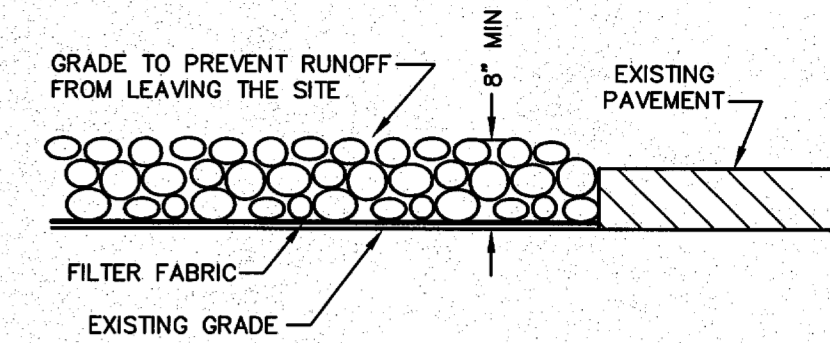
SHEET TITLE: STORM DRAIN DETAILS HANGER ADDITION PLANS FOR EXECUTIVE HANGAR OWNERS ASSOCIATION OF ADDISON ADDISON AIRPORT ADDISON, TEXAS		
PREPARED BY: JDJR ENGINEERS AND CONSULTANTS		
ENGINEERS • SURVEYORS • LAND PLANNERS 2800 Texas Drive Suite 100 Irving, Texas 75082 Tel 972-262-5357 Fax 972-262-8958		
DATE: APR. 8, 2008	DRAWN BY: SAS	SHEET NO.
SCALE: AS SHOWN	CHECKED BY: JDJR	C8 of 10



SILT FENCE DETAIL

SILT FENCE NOTES:

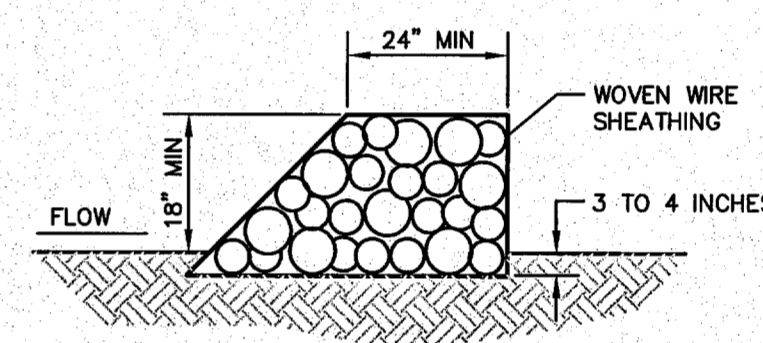
1. STEEL POSTS WHICH SUPPORT THE SILT FENCE SHALL BE INSTALLED ON A SLIGHT ANGLE TOWARD THE ANTICIPATED RUNOFF SOURCE. POST MUST BE EMBEDDED A MINIMUM OF ONE FOOT.
2. THE TOP OF THE SILT FENCE SHALL BE TRENCHED IN WITH A SPADE OR MECHANICAL TRENCHER, SO THAT THE DOWN-SLOPE FACE OF THE TRENCH IS FLAT AND PERPENDICULAR TO THE LINE OF FLOW. WHERE TRENCH CANNOT BE TRENCHED IN (e.g. PAVEMENT), WEIGHT FABRIC FLAP WITH WASHED GRAVEL ON UPHILL SIDE TO PREVENT FLOW UNDER FENCE.
3. THE TRENCH MUST BE A MIN. OF 6" DEEP AND 6" WIDE TO ALLOW FOR THE SILT FENCE FABRIC TO BE LAD IN THE GROUND AND BACKFILLED WITH COMPACTED MATERIAL.
4. SILT FENCE SHOULD BE SECURELY FASTENED TO EACH STEEL SUPPORT POST OR WOVEN WIRE, WHICH IS IN TURN ATTACHED TO THE STEEL FENCE POST. THERE SHALL BE A 6" OVERLAP, SECURELY FASTENED WHERE ENDS OF FABRIC MEET.
5. INSPECTION SHALL BE MADE WEEKLY OR AFTER EACH RAINFALL. REPAIR OR REPLACEMENT SHALL BE MADE PROMPTLY AS NEEDED.
6. SILT FENCE SHALL BE REMOVED WHEN THE SITE IS COMPLETELY STABILIZED SO AS NOT TO BLOCK OR EMPEDE STORM FLOW OR DRAINAGE.
7. ACCUMULATED SILT SHALL BE REMOVED WHEN IT REACHES A DEPTH OF 6". THE SILT SHALL BE DISPOSED OF IN AN APPROVED SITE AND IN SUCH A MANNER AS TO NOT CONTRIBUTE TO ADDITIONAL SILTATION.



NOTES:

1. STONE SHALL BE 3 TO 5 INCH DIAMETER CRUSHED ROCK OR ACCEPTABLE CRUSHED PORTLAND CEMENT CONCRETE.
2. LENGTH SHALL BE SHOWN ON PLANS, WITH A MINIMUM LENGTH OF 30 FEET FOR LOTS WHICH ARE LESS THAN 150 FEET FROM THE EDGE OF PAVEMENT. THE MINIMUM DEPTH IN ALL OTHER CASES SHALL BE 50 FEET.
3. THE THICKNESS SHALL NOT BE LESS THAN 6 INCHES.
4. THE WIDTH SHALL BE NO LESS THAN THE FULL WIDTH OF ALL POINTS OF INGRESS OR EGRESS.
5. WHEN NECESSARY, VEHICLES SHALL BE CLEANED TO REMOVE SEDIMENT PRIOR TO ENTRANCE ONTO A PUBLIC ROADWAY. WHEN WASHING IS REQUIRED, IT SHALL BE DONE ON AN AREA STABILIZED WITH CRUSHED STONE WITH DRAINAGE FLOWING AWAY FROM BOTH THE STREET AND THE STABILIZED ENTRANCE. ALL SEDIMENT SHALL BE PREVENTED FROM ENTERING ANY STORM DRAIN, OR WATERCOURSE USING APPROVED METHODS.
6. THE ENTRANCE SHALL BE MAINTAINED IN A CONDITION WHICH WILL PREVENT TRACKING OR FLOWING OF SEDIMENT ONTO PAVED SURFACES. THIS MAY REQUIRE PERIODIC TOP DRESSING WITH ADDITIONAL STONE AS CONDITIONS DEMAND. ALL SEDIMENT SPILLED, DROPPED, WASHED OR TRACKED ONTO PAVED SURFACES, MUST BE REMOVED IMMEDIATELY.
7. THE ENTRANCE MUST BE PROPERLY GRADED OR INCORPORATE A DRAINAGE TO PREVENT RUNOFF FROM LEAVING THE CONSTRUCTION SITE.

**STABILIZED CONSTRUCTION ENTRANCE
NOT TO SCALE**

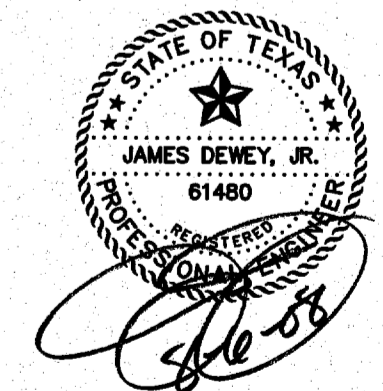


ROCK BERM DETAIL

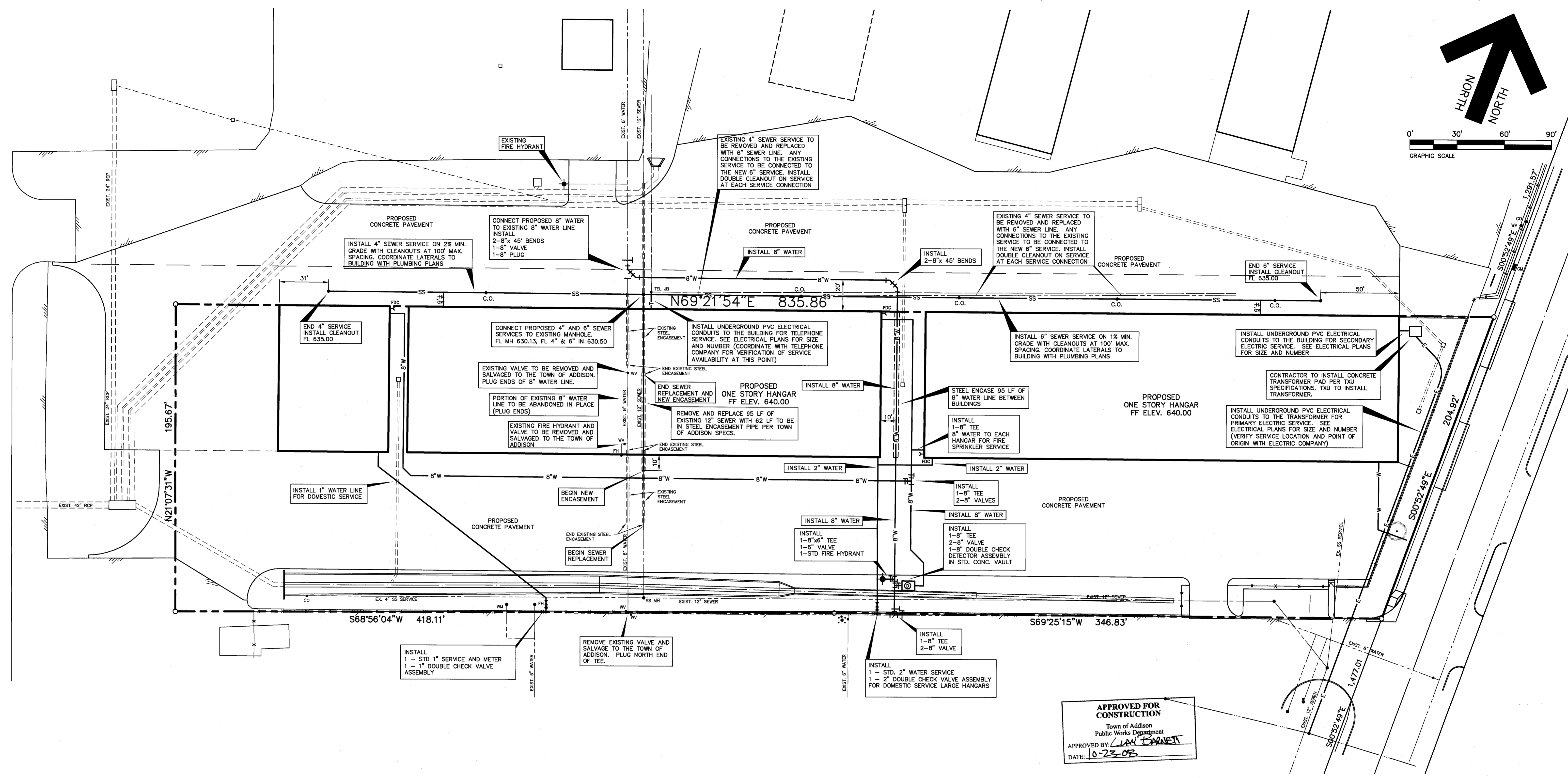
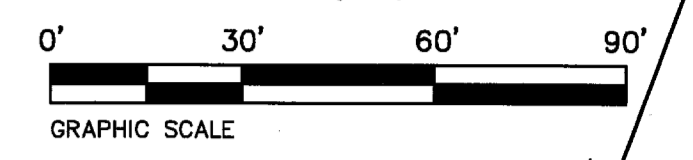
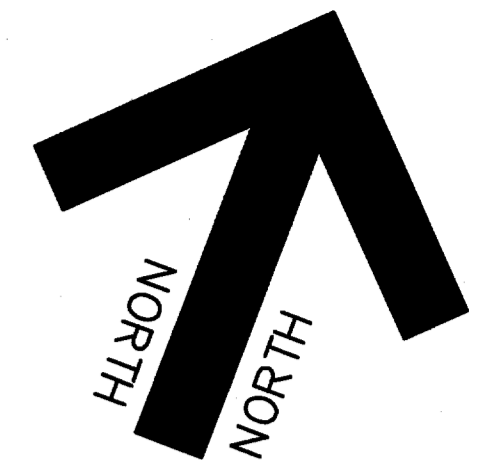
ROCK BERM NOTES:

1. USE ONLY OPEN GRADED 4-8 INCHES IN DIAMETER FOR STREAM FLOW CONDITIONS. USE OPEN GRADED ROCK 3-5 INCHES IN DIAMETER FOR OTHER CONDITIONS.
2. THE ROCK BERM SHALL BE SECURED WITH A WOVEN WIRE SHEATHING HAVING A MAXIMUM OPENING OF 1 INCH AND A MINIMUM WIRE SIZE OF 20 GAUGE AND SHALL BE BURIED IN A TRENCH APPROXIMATELY 3 TO 4 INCHES DEEP.
3. THE ROCK BERM SHALL BE INSPECTED EVERY TWO WEEKS OR AFTER EACH 1/2" RAIN EVENT AND SHALL BE REPLACED WHEN THE STRUCTURE CEASES TO FUNCTION AS INTENDED DUE TO SILT ACCUMULATION AMONG THE ROCKS, WASHOUT, CONSTRUCTION TRAFFIC DAMAGE, ETC.
4. WHEN SILT REACHES A DEPTH EQUAL TO ONE-THIRD OF THE HEIGHT OF THE BERM OR ONE FOOT, WHICHEVER IS LESS, THE SILT SHALL BE REMOVED AND DISPOSED OF PROPERLY.
5. WHEN THE SITE IS COMPLETELY STABILIZED, THE BERM AND ACCUMULATED SILT SHALL BE REMOVED AND DISPOSED OF IN AN APPROVED MANNER.
6. ROCK BERM SHOULD BE USED AS CHECK DAMS FOR CONCENTRATED FLOW AND ARE NOT INTENDED FOR USE IN PERIMETER PROTECTION.

REVISIONS:	
6/04/08	CITY COMMENTS



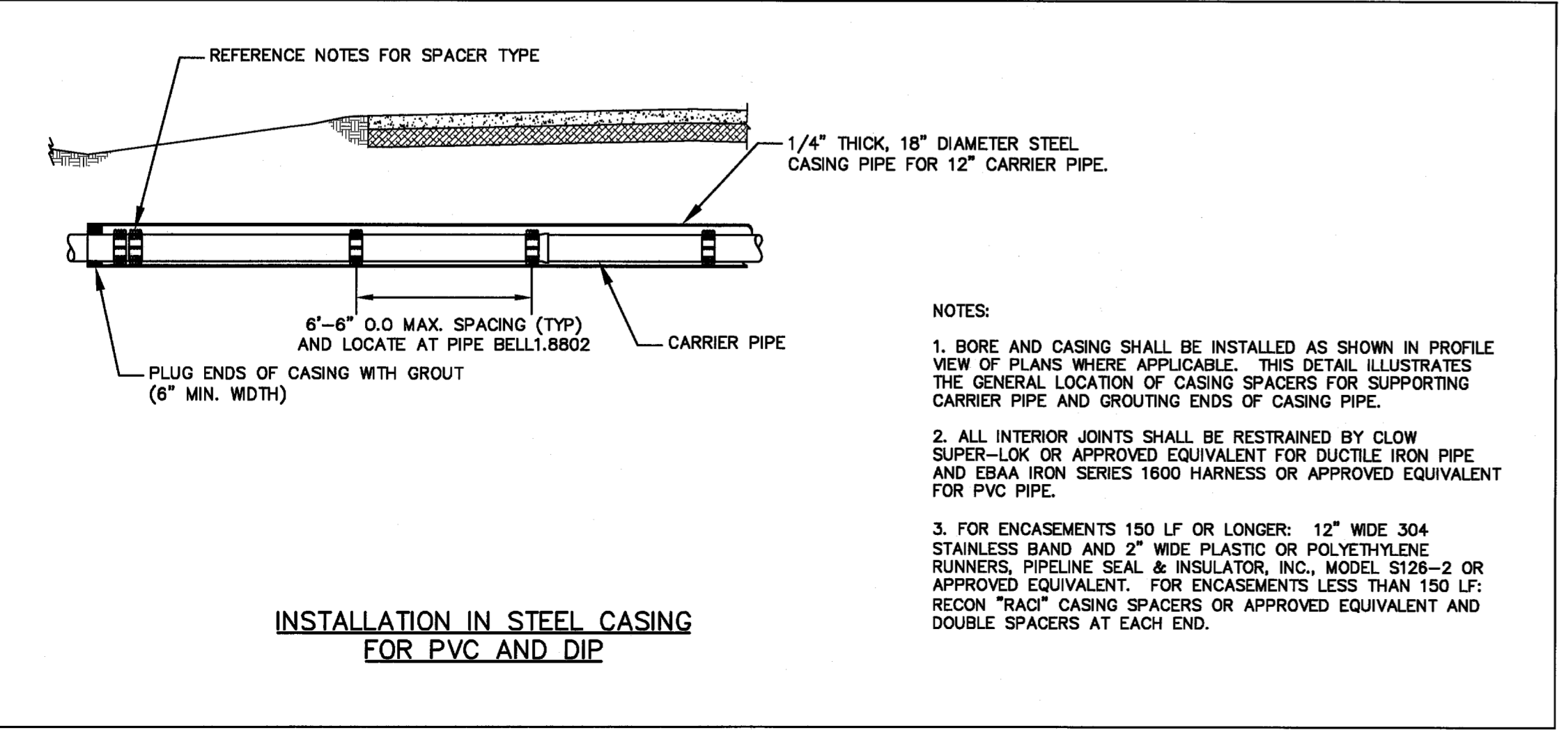
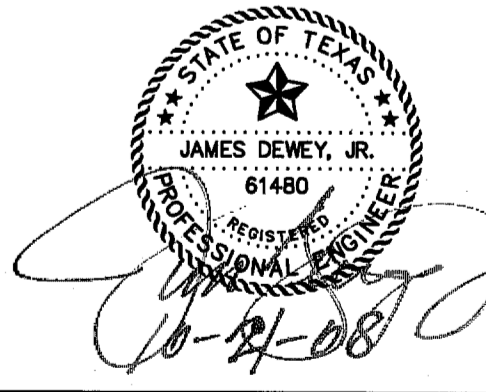
SHEET TITLE: EROSION CONTROL DETAILS HANGER ADDITION PLANS FOR EXECUTIVE HANGAR OWNERS ASSOCIATION OF ADDISON ADDISON AIRPORT ADDISON, TEXAS		
PREPARED BY: JDJR ENGINEERS AND CONSULTANTS		
ENGINEERS • SURVEYORS • LAND PLANNERS 2500 Texas Drive Suite 100 Irving, Texas 75082 Tel: 972-252-5357 Fax: 972-252-8858		
DATE: APR. 8, 2008	DRAWN BY: SAS	SHEET NO.
SCALE: AS SHOWN	CHECKED BY: JDJR	C9A of 10



APPROVED FOR CONSTRUCTION
 Town of Addison
 Public Works Department
 APPROVED BY: *LAY GARNETT*
 DATE: 10-23-08

All responsibility for the adequacy of these plans remains with the Engineer who prepared them. In approving these plans, the Town of Addison makes no representation of adequacy of the work of the Design Engineer.

REVISIONS:	
6/04/08	CITY COMMENTS
7/25/08	CITY COMMENTS
8/01/08	CITY COMMENTS
8/05/08	CITY COMMENTS
9/09/08	REV. WATER SERV.
9/23/08	REV. WATER SERV.
10/21/08	REV. FIRE SERV.



EXISTING UTILITIES NOTES:

1. THE LOCATION OF ALL UNDERGROUND FACILITIES AS INDICATED ON THE PLANS ARE TAKEN FROM PUBLIC RECORDS. JDJR ENGINEERS & CONSULTANTS ASSUMES NO RESPONSIBILITY FOR THE ACCURACY OF SUCH RECORDS AND DOES NOT GUARANTEE THAT ALL UNDERGROUND UTILITIES ARE SHOWN OR ARE LOCATED PRECISELY AS INDICATED.
2. IT IS THE RESPONSIBILITY OF THE CONTRACTOR TO MAKE ARRANGEMENTS WITH THE OWNERS OF SUCH UNDERGROUND FACILITIES PRIOR TO WORKING IN THE AREA TO CONFIRM THEIR EXACT LOCATION AND TO DETERMINE WHETHER ANY ADDITIONAL FACILITIES OTHER THAN THOSE SHOWN ON THE PLANS MAY BE PRESENT.
3. THE CONTRACTOR SHALL PRESERVE AND PROTECT ALL UNDERGROUND FACILITIES FOUND.
4. NOTIFY JDJR ENGINEERS & CONSULTANTS IF ANY UNDERGROUND UTILITIES ARE NOT IN THE LOCATIONS INDICATED ON THESE PLANS (HORIZONTALLY AND VERTICALLY) OR CONFLICT WITH ANY PROPOSED IMPROVEMENTS ASSOCIATED WITH THESE PLANS.
5. ANY EXISTING UTILITY APPURTENANCES (MH, VALVES, METER BOXES, ETC.) TO BE ADJUSTED TO MATCH THE PROPOSED FINISHED GRADES AS INDICATED ON THESE PLANS. NOTIFY JDJR ENGINEERS & CONSULTANTS, INC. IF THERE IS A PROBLEM MAKING SAID ADJUSTMENTS.

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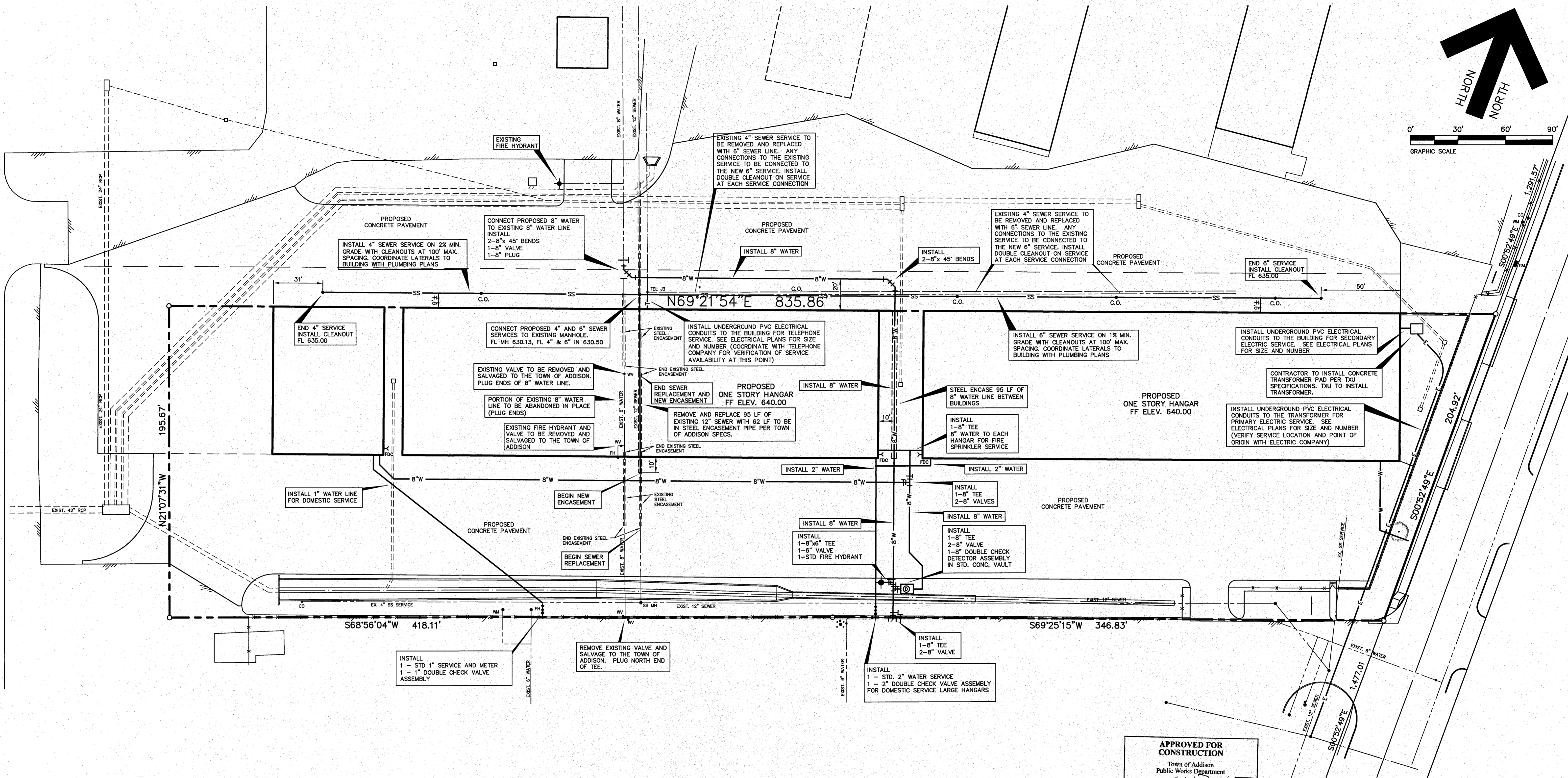
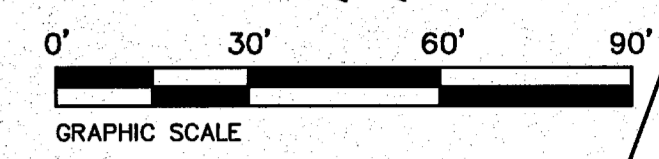
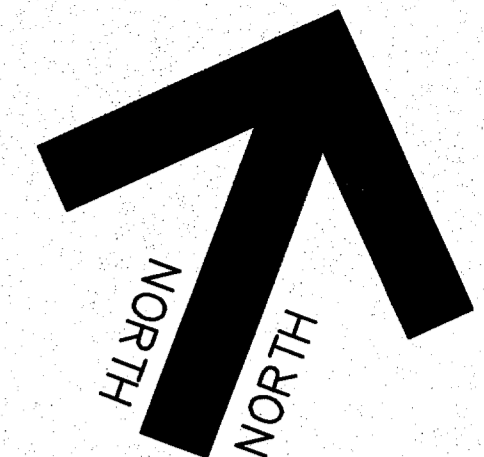
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BENCH MARK: SQUARE CUT FOUND C.L. OF CURB INLET, WEST SIDE OF ADDISON ROAD, 75' SOUTH OF THE C.L. OF AIRPORT PKWY.
 ELEVATION 642.51
 SITE TBM: X-CUT SET IN CONCRETE WALK ±38' NORTH OF THE NORTHEAST CORNER OF THIS TRACT.
 ELEVATION 642.86

SHEET TITLE:
UTILITY SERVICES PLAN
 HANGER ADDITION PLANS FOR EXECUTIVE HANGER OWNERS ASSOCIATION OF ADDISON AIRPORT ADDISON, TEXAS

PREPARED BY:
JDJR ENGINEERS AND CONSULTANTS
 ENGINEERS • SURVEYORS • LAND PLANNERS
 2500 Texas Drive Suite 100 Irving, Texas 75062
 Tel 972-262-5357 Fax 972-262-6956

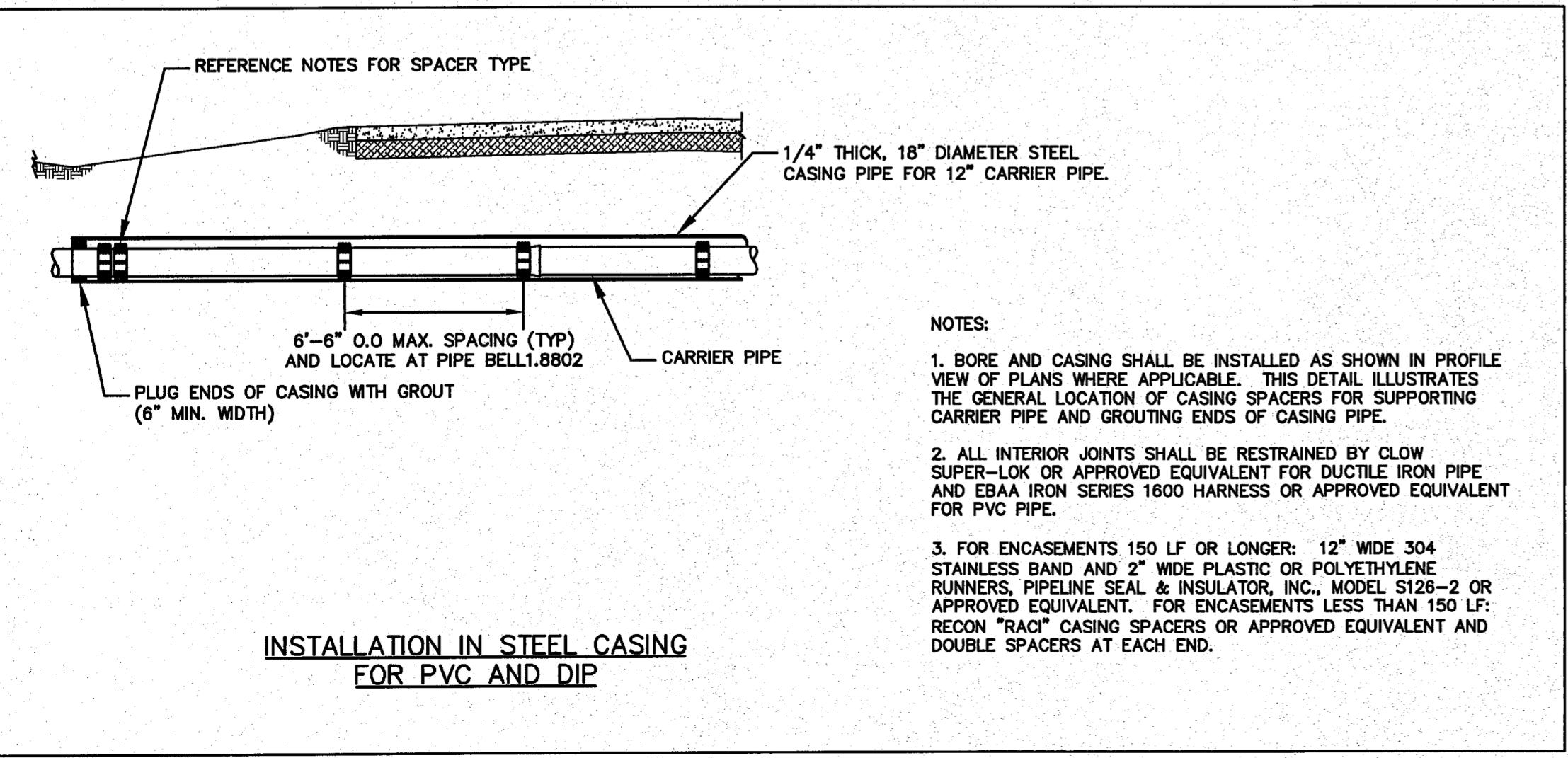
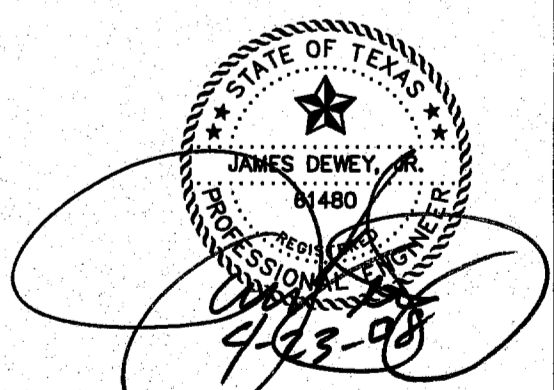
DATE: APR. 8, 2008 DRAWN BY: SAS SHEET NO.
 SCALE: 1" = 30' CHECKED BY: JDJR **C10** OF 10



APPROVED FOR CONSTRUCTION
Town of Addison
Public Works Department
APPROVED BY: *CLAY BARRETT*
DATE: 9-24-08

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REVISIONS:	
6/04/08	CITY COMMENTS
7/25/08	CITY COMMENTS
8/01/08	CITY COMMENTS
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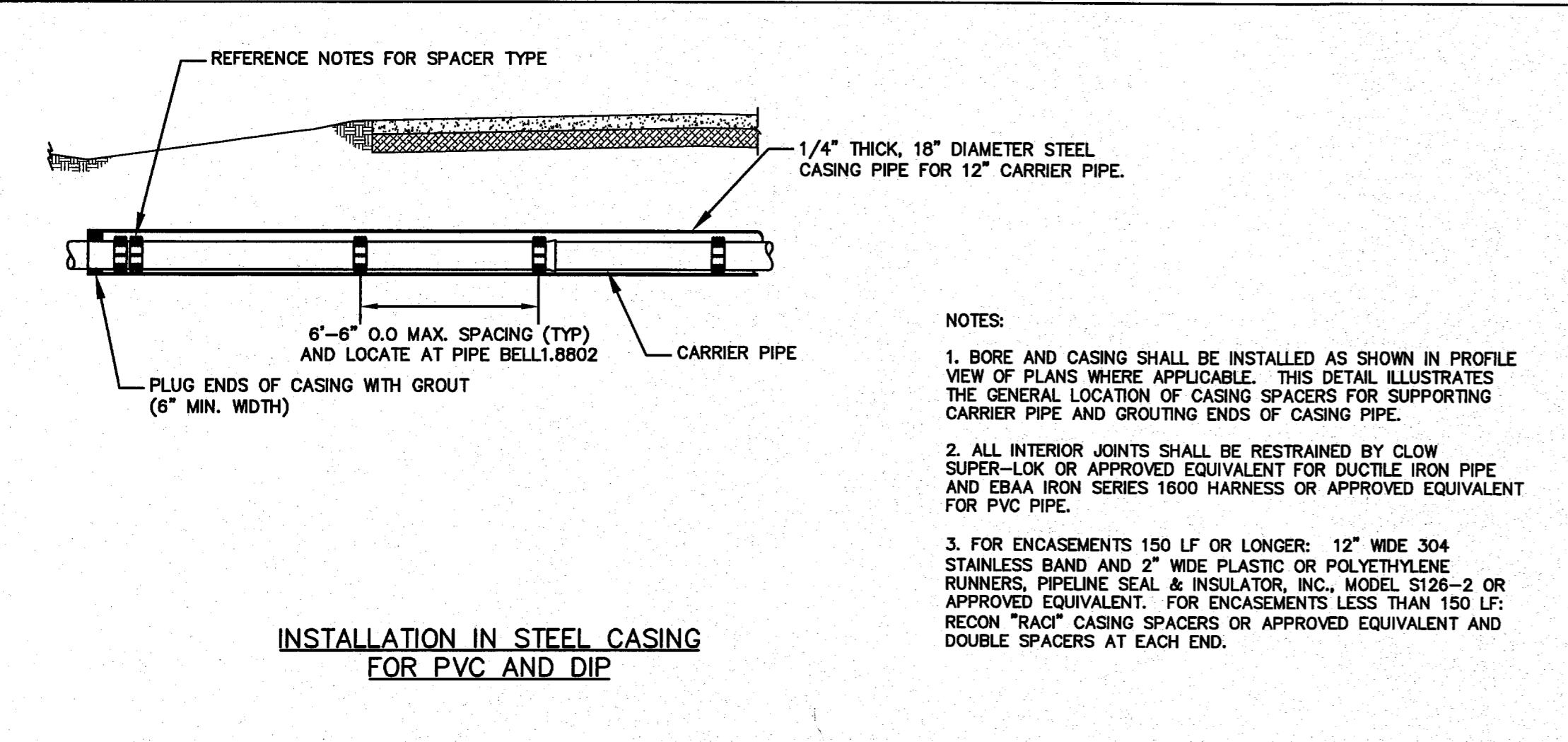
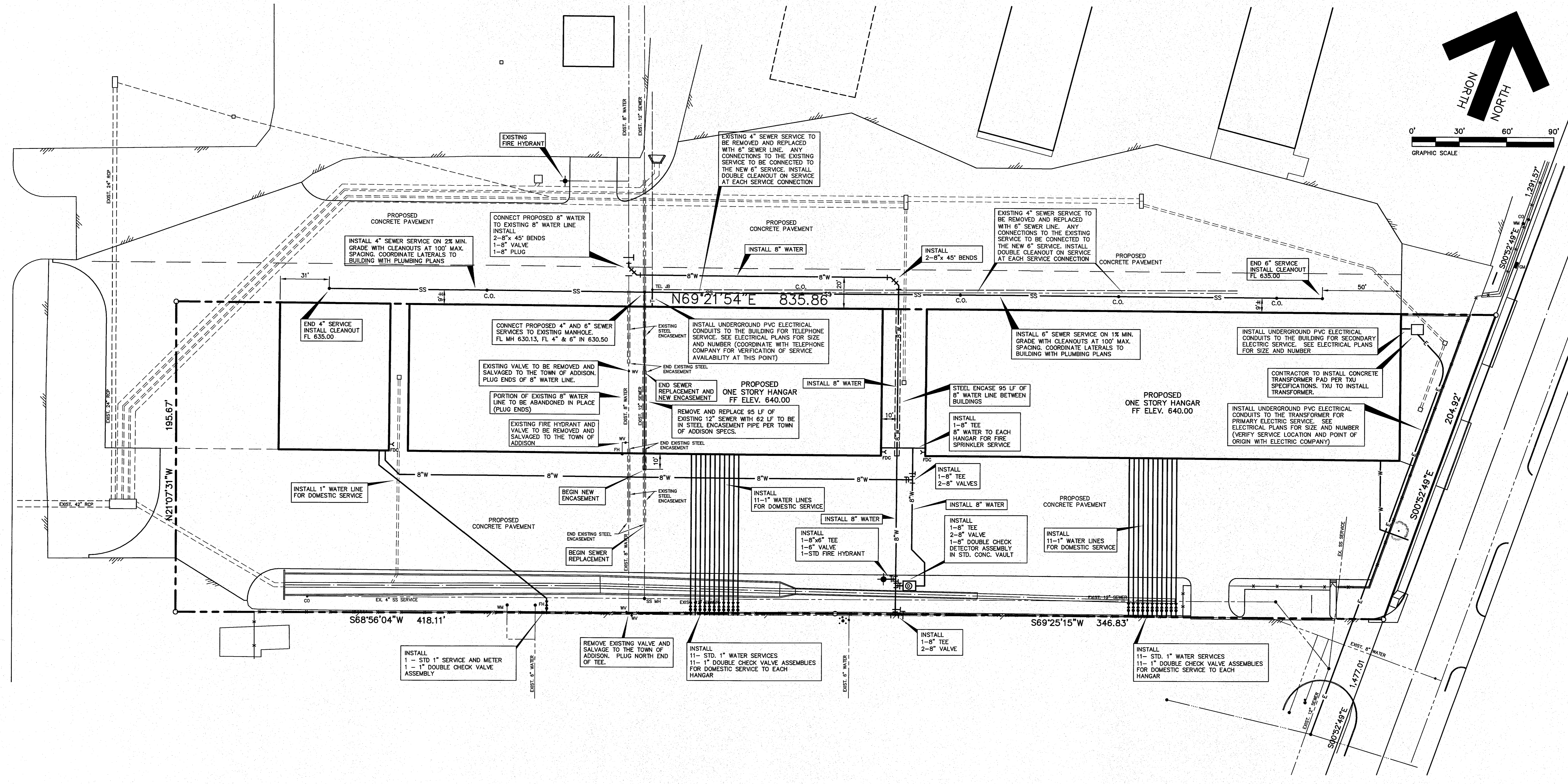
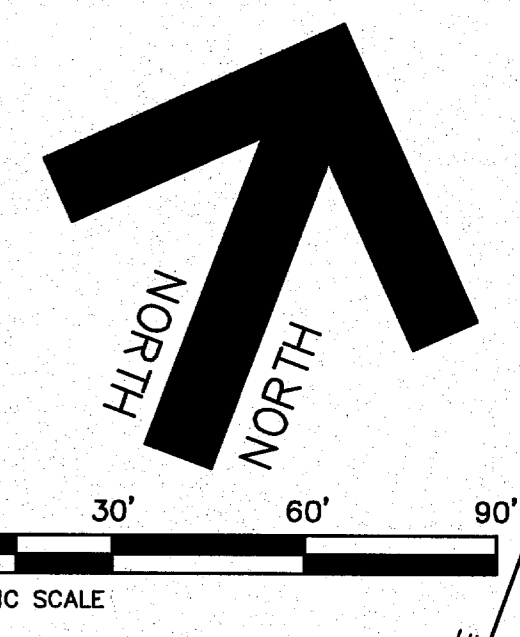
SHEET TITLE:
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HANGER ADDITION PLANS FOR
EXECUTIVE HANGER OWNERS ASSOCIATION OF ADDISON
AIRPORT
ADDISON, TEXAS

PREPARED BY:
JDJR ENGINEERS AND CONSULTANTS

ENGINEERS • SURVEYORS • LAND PLANNERS
2500 Texas Drive Suite 100 Irving, Texas, 75062
Tel 972-252-5367 Fax 972-252-9858

DATE: APR. 8, 2008 DRAWN BY: SAS
SCALE: 1" = 30' CHECKED BY: JDJR

SHEET NO. **C10** OF **10**

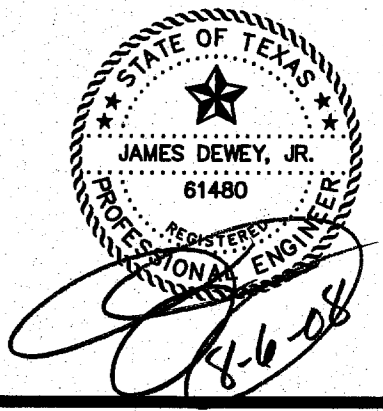


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 2500 Texas Drive Suite 100 • Irving, Texas 75082
 Tel: 972-252-5357 Fax: 972-252-8668

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C10 of 10

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