

CONSTRUCTION PLANS FOR: PAVING AND DRAINAGE IMPROVEMENTS 15841 ADDISON ROAD

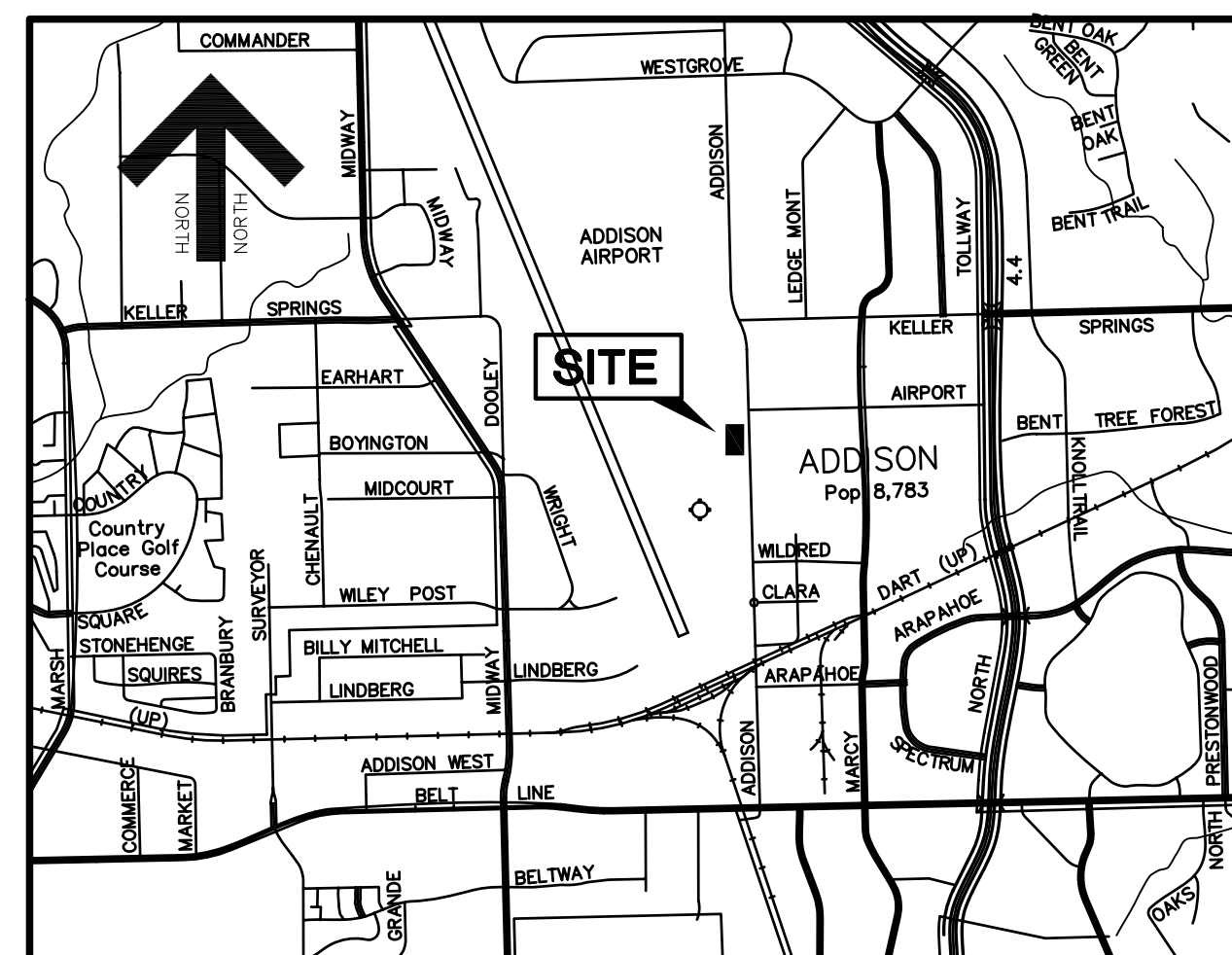
ADDISON AIRPORT
ADDISON, TEXAS

SEPTEMBER 2009

PW# 2008-21

SHEET INDEX

- COVERSHEET
- PLAT
- C1A SITE DIMENSIONAL CONTROL PLAN
- C1B SITE DIMENSIONAL CONTROL PLAN
- C2 DEMOLITION PLAN
- C3A GRADING, DRAINAGE AND PAVING PLAN
- C3B GRADING, DRAINAGE AND PAVING PLAN
- C3C PAVEMENT JOINT PATTERN PLAN
- C3D PAVEMENT JOINT PATTERN PLAN
- C4 STORM DRAIN PLAN
- C5 STORM DRAIN PROFILE AND DETAILS
- C6 FIRE SPRINKLER SERVICE PLAN
- C7 EROSION CONTROL PLAN



LOCATION MAP

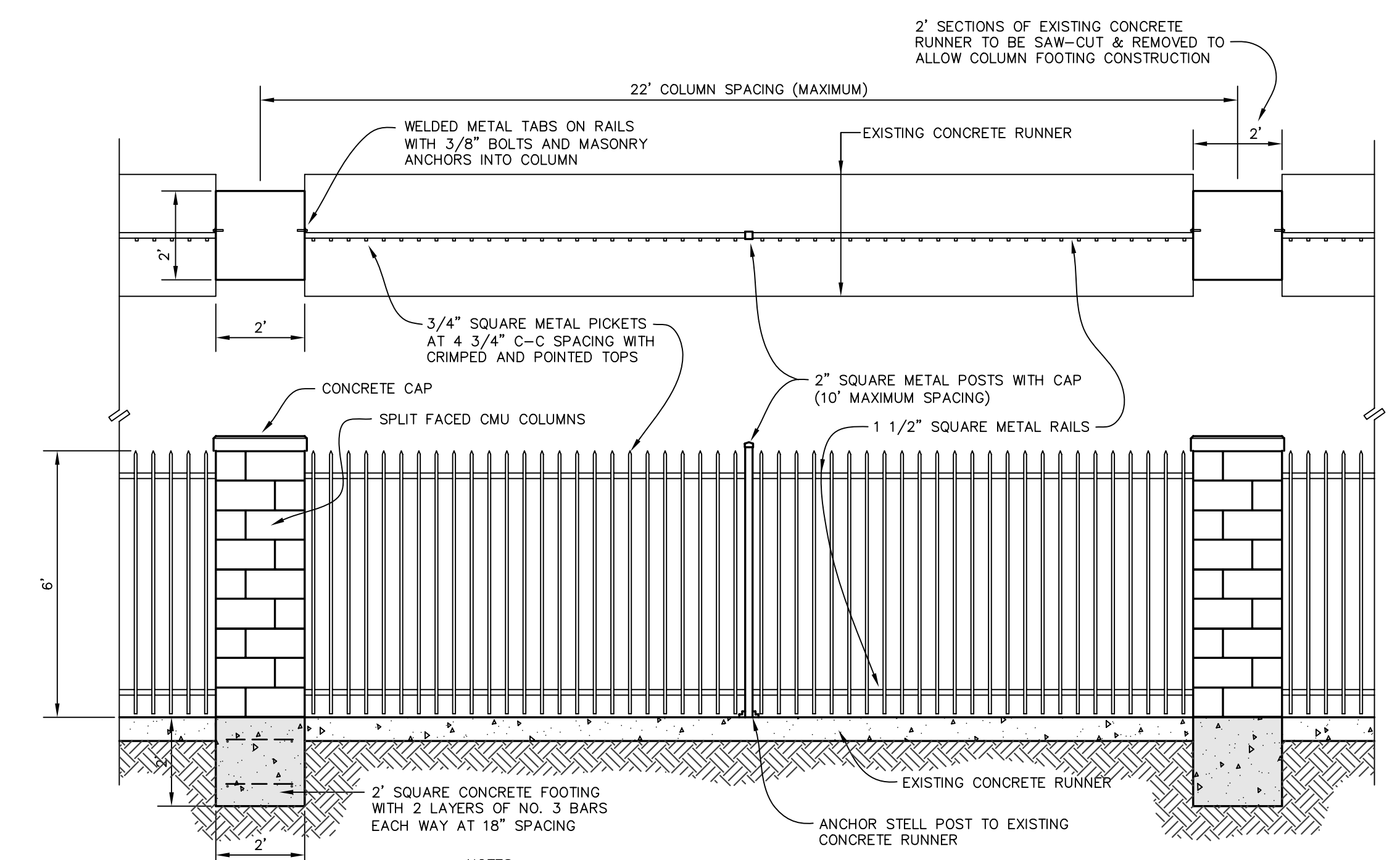
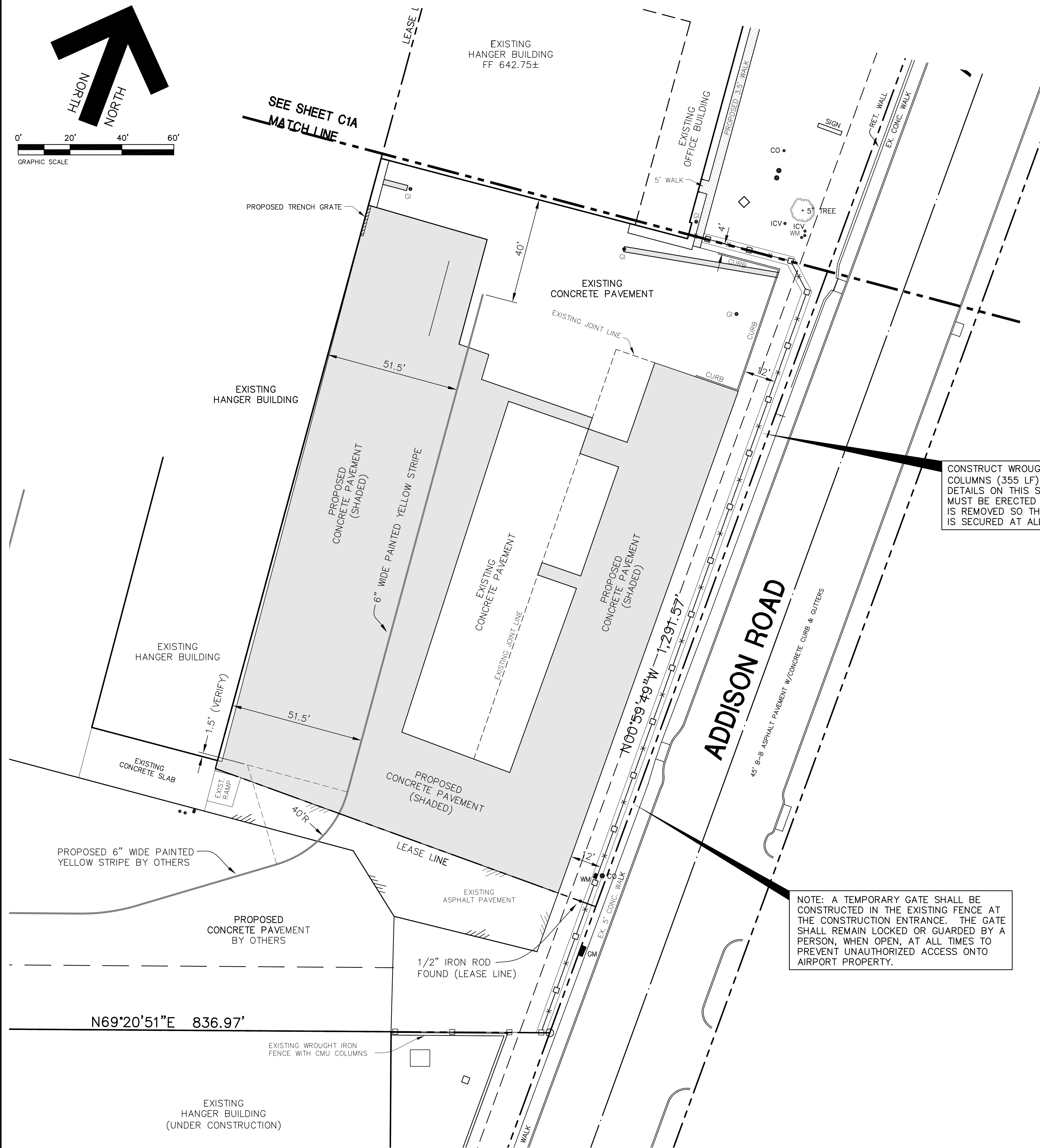
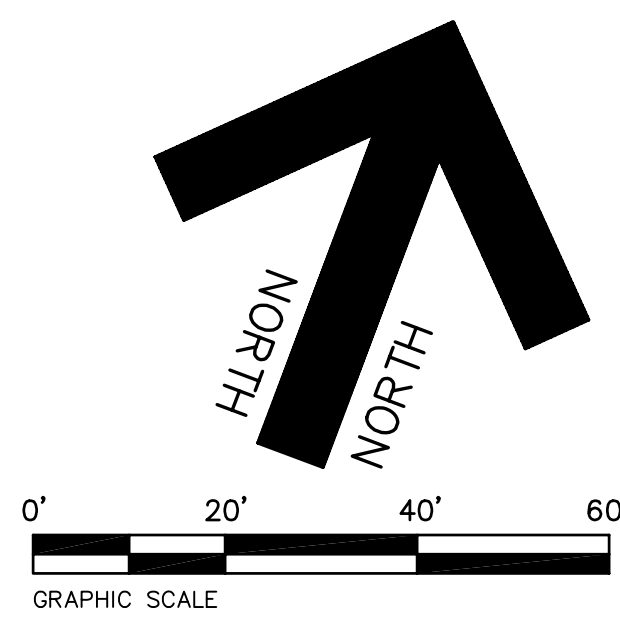
SCALE: 1"=2000'

"AS-BUILT"

THIS AS-BUILT DRAWING IS BASED ON INFORMATION OBTAINED FROM BUILDING CONTRACTORS DURING CONSTRUCTION AS PROVIDED TO JDJR ENGINEERS & CONSULTANTS, INC. JDJR ENGINEERS & CONSULTANTS, INC. DOES NOT GUARANTEE THE ACCURACY OR COMPLETENESS OF THESE RECORDS.

PREPARED FOR:
MSF PROPERTIES
TWO LEGACY TOWN CENTER
6900 N. DALLAS PARKWAY, SUITE 730
PLANO, TEXAS 75024
PHONE: (972) 403-0909
CONTACT: RICHARD FRENCH

JDJR	ENGINEERS AND CONSULTANTS, INC.
	TSBPE REGISTRATION NUMBER F-8527
ENGINEERS • SURVEYORS • LAND PLANNERS	
2500 Texas Drive Suite 100 Irving, Texas 75062	
Tel 972-252-5357 (JDJR) Fax 972-252-8958	



- NOTES:
1. FENCE PANELS TO BE WELDED CONSTRUCTION AND FINAL WIDTH TO BE FIELD ADJUSTED TO FIT BETWEEN COLUMNS.
 2. CMU COLOR AND STYLE TO MATCH EXISTING FENCE TO THE SOUTH OF THIS PROJECT.
 3. ALL METAL POSTS, RAILS AND PICKETS TO BE PRIMED AND PAINTED TO MATCH EXISTING FENCE TO THE SOUTH OF THIS PROJECT.

WROUGHT IRON FENCE DETAIL
NOT TO SCALE

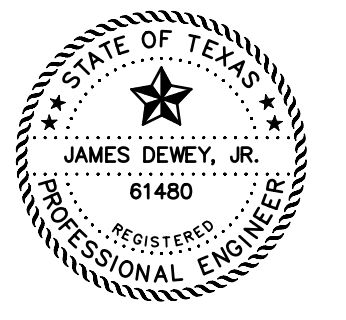
CONSTRUCT WROUGHT IRON FENCE WITH CMU COLUMNS (355 LF) AS SHOWN HERE AND PER DETAILS ON THIS SHEET. A TEMPORARY FENCE MUST BE ERECTED WHEN THE EXISTING FENCE IS REMOVED SO THAT THE AIRPORT PROPERTY IS SECURED AT ALL TIMES.

- NOTES:
1. ALL CONSTRUCTION TO BE DONE IN STRICT ACCORDANCE TO THESE PLANS AND ALL APPLICABLE MUNICIPAL CODES AND STANDARDS.
 2. SEE ARCHITECTURAL PLANS FOR ALL BUILDING DIMENSIONS AND DETAILS.
 3. SEE SHEET C-2 FOR ALL SITE DEMOLITION PLAN.
 4. SEE SHEET C-3 FOR ALL PAVING AND GRADING PLANS.
 5. SEE SHEET C-6 FOR UTILITY PLAN OF THE SITE.
 6. SEE SHEET C-7 FOR EROSION CONTROL PLAN OF THE SITE.

- AIRCRAFT ACCESS NOTES:
1. THE CONTRACTOR SHALL INSURE THAT THE PAVEMENT SURFACE WITHIN AN ACTIVE AIRCRAFT MOVEMENT AREA IS KEPT CLEAN FROM DIRT, MUD AND OTHER DEBRIS.
 2. AT ALL TIMES AIRCRAFT MOVEMENT SHALL HAVE THE RIGHT OF WAY OVER THE CONTRACTOR'S EQUIPMENT.

NOTE: A TEMPORARY GATE SHALL BE CONSTRUCTED IN THE EXISTING FENCE AT THE CONSTRUCTION ENTRANCE. THE GATE SHALL REMAIN LOCKED OR GUARDED BY A PERSON, WHEN OPEN, AT ALL TIMES TO PREVENT UNAUTHORIZED ACCESS ONTO AIRPORT PROPERTY.

REVISIONS:	
1/07/09	PER CITY REVIEW
8/11/09	REV FOR CLIENT
9/11/09	PER CITY REVIEW
9/23/09	PER CITY REVIEW
12/18/09	ADDED FENCE
3/10/10	REV. FENCE DTL.
4/13/10	ADD TRENCH GRATE
5/18/10	AS BUILT



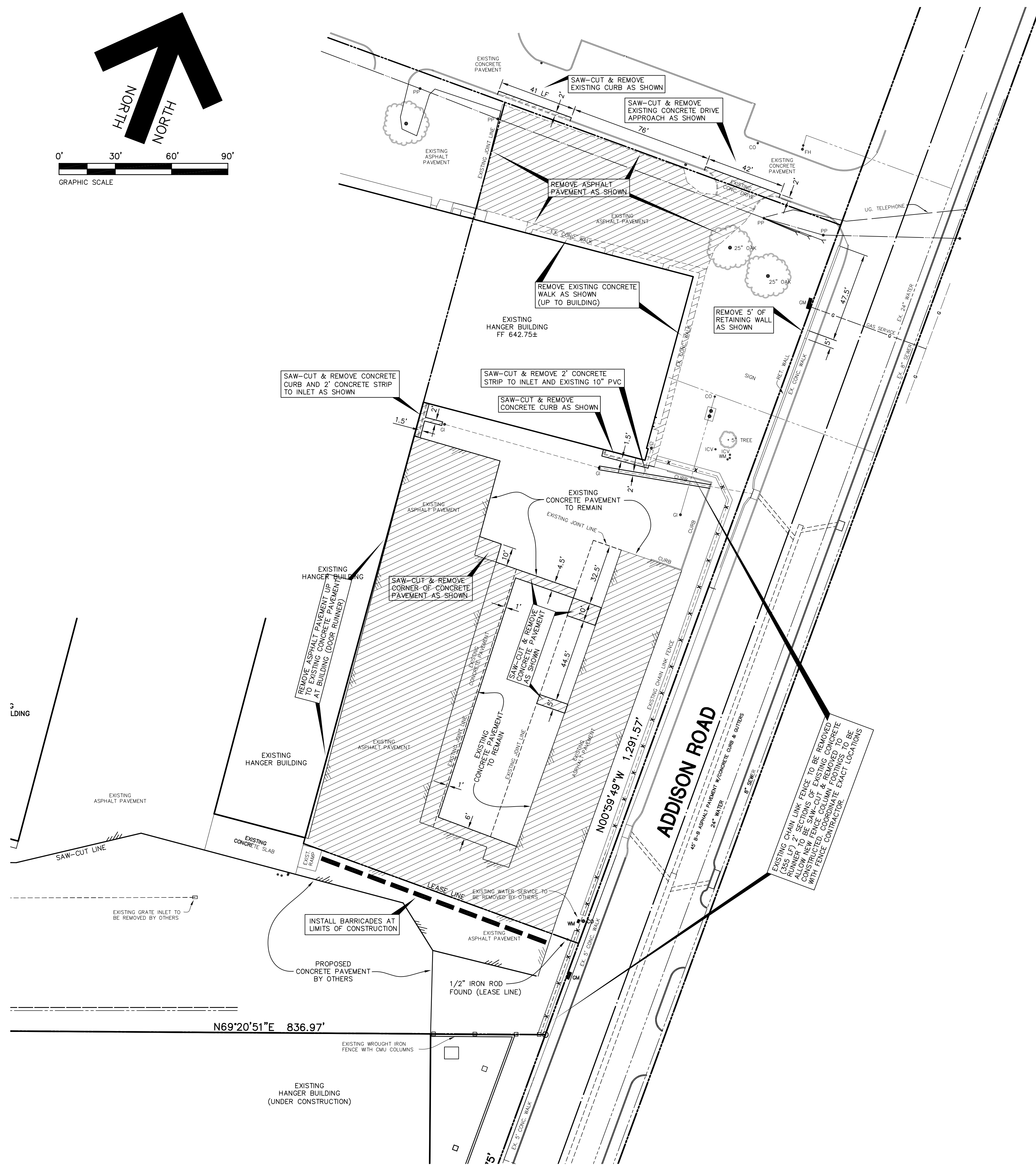
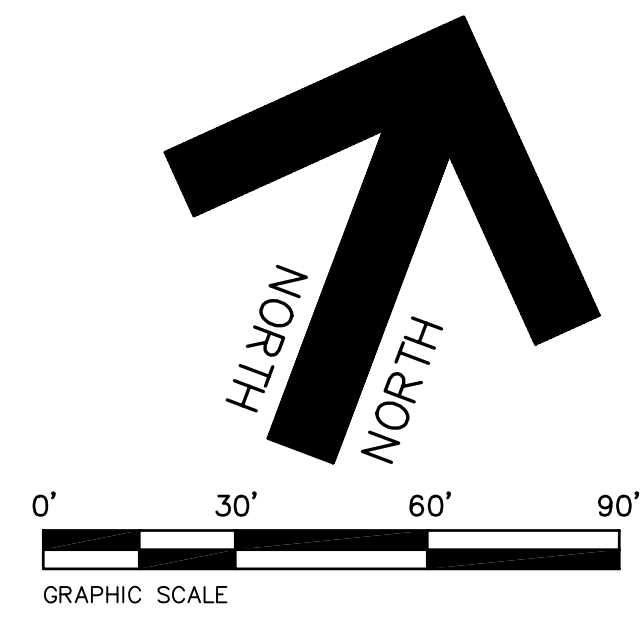
SHEET TITLE:
SITE DIMENSIONAL CONTROL PLAN
PAVING AND DRAINAGE IMPROVEMENTS
15841 ADDISON ROAD
ADDISON AIRPORT
ADDISON, TEXAS

PREPARED BY:
JDJR ENGINEERS AND CONSULTANTS
TSBPE REGISTRATION NUMBER F-8527
ENGINEERS • SURVEYORS • LAND PLANNERS
2500 Texas Drive Suite 100 Irving, Texas 75062
Tel 972-252-5357 Fax 972-252-8958

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DATE: DEC. 2, 2008	DRAWN BY: SAS	SHEET NO.:
SCALE: 1" = 20'	CHECKED BY: JDJR	C1B OF 7

H:\JDF\2008\08\04\02\02-2 ADDISON AIRPORT\DWG\102-2-38 CIVIL_ADDITIONAL PAVING no sidewalk.dwg, 5/18/2010 8:45:42 AM, JDJR ENGINEERS & CONSULTANTS, INC., SCS



NOTES:

1. DEMOLITION CONTRACTOR TO BE FAMILIAR WITH ALL PROPOSED CONSTRUCTION PLANS BEFORE ANY SITE DEMOLITION COMMENCES.
2. DEMOLITION CONTRACTOR TO LOCATE ALL UNDERGROUND UTILITIES PRIOR TO ANY DEMOLITION WORK. CONTACT UTILITY COMPANIES AT LEAST 2 WORKING DAYS PRIOR TO ANY DEMOLITION WORK.
3. INSTALL BARRICADES AT LIMITS OF CONSTRUCTION AT THE SOUTH END OF PROJECT AS SHOWN ON PLAN. BARRICADES TO BE MULTI-BARRIER AIRPORT-RUNWAY BARRICADE AR-10x96 HDPE, AS MANUFACTURED BY OFF THE WALL PRODUCTS, DISTRIBUTED BY DALLAS LITE & BARRICADE, INC. (214) 748-5791, PART NO. P6000-101096.



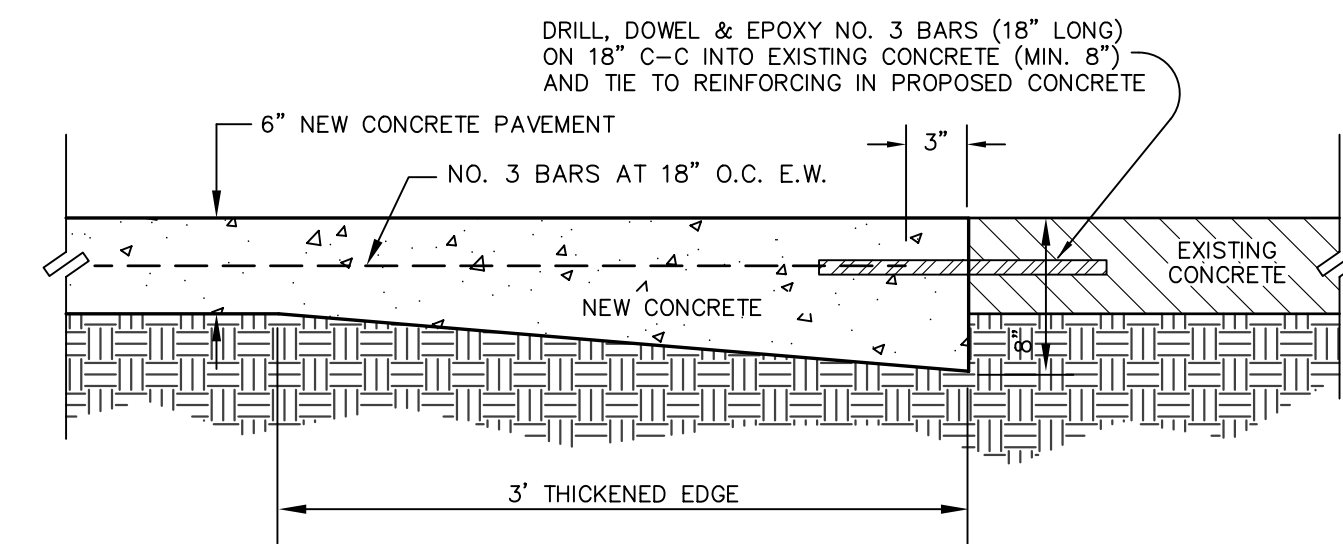
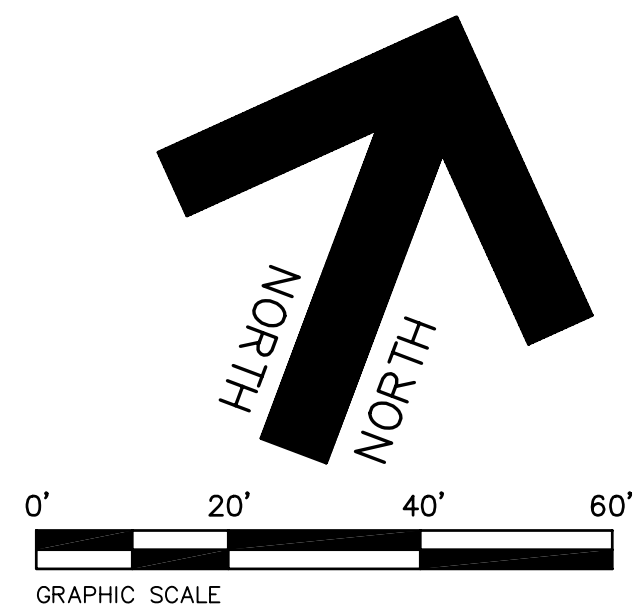
INDICATES EXISTING IMPROVEMENTS TO BE REMOVED

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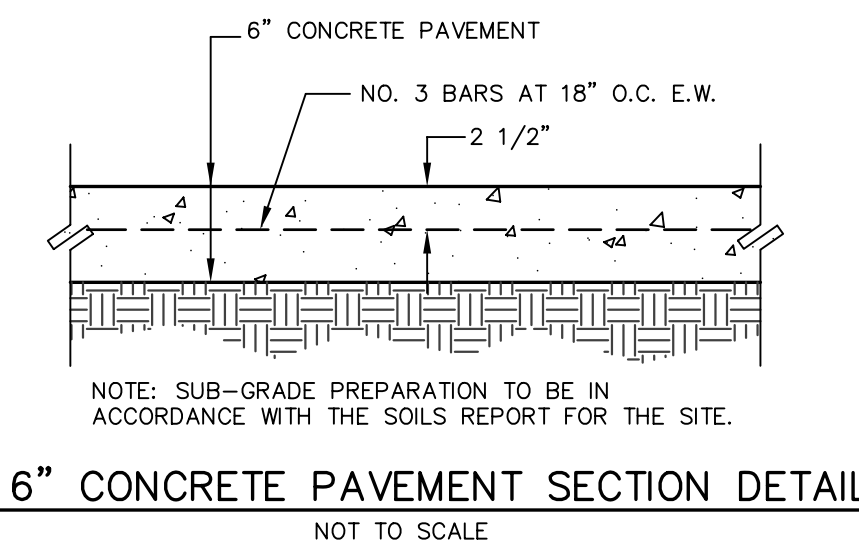
REVISIONS:	
1/07/09	PER CITY REVIEW
8/11/09	REV FOR CLIENT
9/11/09	PER CITY REVIEW
9/23/09	PER CITY REVIEW
12/18/09	FENCE REMOVAL
5/18/10	AS BUILT



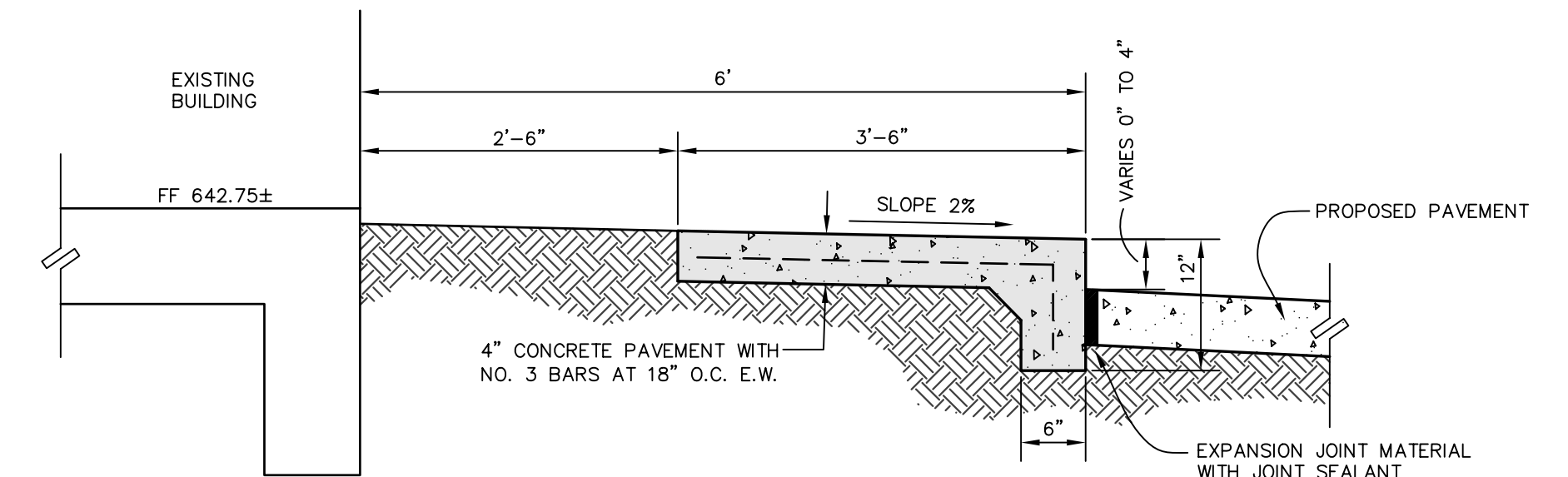
SHEET TITLE: DEMOLITION PLAN PAVING AND DRAINAGE IMPROVEMENTS 15841 ADDISON ROAD AIRPORT ADDISON, TEXAS		
PREPARED BY: JDJR ENGINEERS AND CONSULTANTS TSBPE REGISTRATION NUMBER F-8527		
ENGINEERS • SURVEYORS • LAND PLANNERS 2500 Texas Drive Suite 100 Irving, Texas 75062 Tel 972-252-5357 Fax 972-252-8958		
DATE: DEC. 2, 2008	DRAWN BY: SAS	SHEET NO.
SCALE: 1" = 30'	CHECKED BY: JDJR	C2 of 7



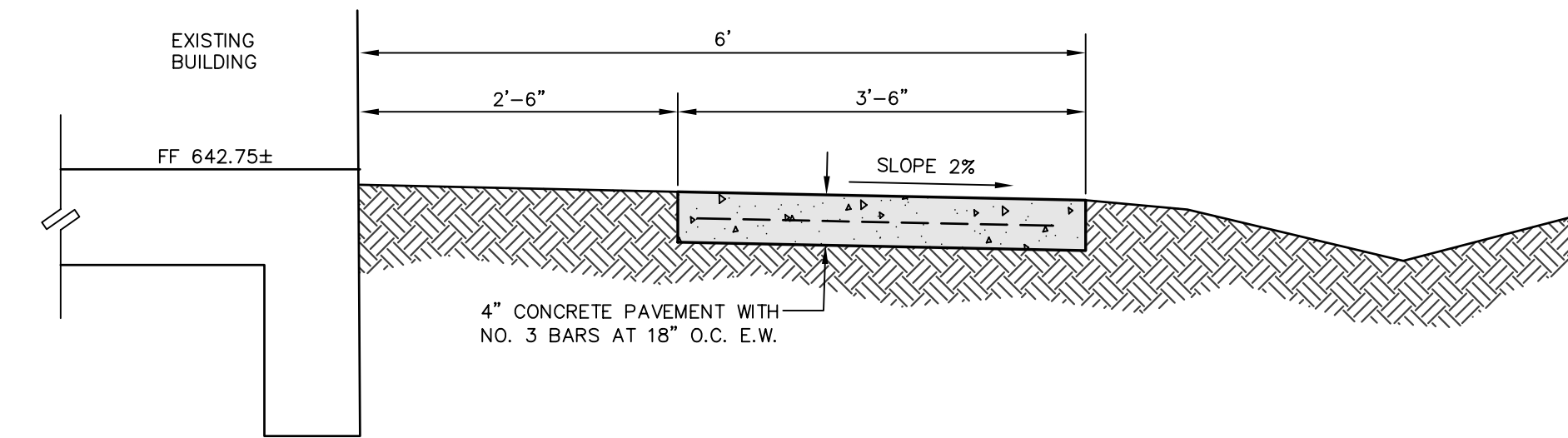
NEW CONCRETE TO EXISTING CONCRETE DETAIL
(AT DRIVE APPROACHES TO PARKING LOT)
NOT TO SCALE



6" CONCRETE PAVEMENT SECTION DETAIL
NOT TO SCALE



SECTION A-A SIDEWALK AT PARKING DETAIL
NOT TO SCALE



SECTION B-B SIDEWALK AT SWALE
NOT TO SCALE

GENERAL NOTES:

- ALL CONSTRUCTION SHALL BE DONE IN STRICT CONFORMANCE TO THESE PLANS AND ALL APPLICABLE MUNICIPAL CODES AND STANDARDS.
- SEE SHEET C-1 FOR ALL HORIZONTAL CONTROL DIMENSIONS.
- SEE SHEET C2 FOR SITE DEMOLITION PLAN.
- SEE SHEET C4 AND C5 FOR STORM DRAIN PLAN, PROFILE AND DETAILS.
- SEE SHEET C6 FOR UTILITY PLAN OF THE SITE.
- SEE SHEET C7 FOR EROSION CONTROL PLAN OF THE SITE.
- ALL SPOT ELEVATIONS ADJACENT TO CURBS ARE GUTTER ELEVATIONS UNLESS OTHERWISE SHOWN.
- ALL SITE PAVING TO BE DONE IN ACCORDANCE TO THE RECOMMENDATIONS AS OUTLINED IN THE SOILS REPORT FOR THIS SITE.
- ALL SITE GRADING AND SUBGRADE PREPARATION SHALL BE DONE IN ACCORDANCE TO THE RECOMMENDATIONS AS OUTLINED IN THE SOILS REPORT FOR THIS SITE.
- ALL LANDSCAPED AREAS TO BE UNIFORMLY GRADED AS SHOWN.

EXISTING UTILITIES NOTES:

- THE LOCATION OF ALL UNDERGROUND FACILITIES AS INDICATED ON THE PLANS ARE TAKEN FROM PUBLIC RECORDS. JDJR ENGINEERS & CONSULTANTS ASSUMES NO RESPONSIBILITY FOR THE ACCURACY OF SUCH RECORDS AND DOES NOT GUARANTEE THAT ALL UNDERGROUND UTILITIES ARE SHOWN OR ARE LOCATED PRECISELY AS INDICATED.
- IT IS THE RESPONSIBILITY OF THE CONTRACTOR TO MAKE ARRANGEMENTS WITH THE OWNERS OF SUCH UNDERGROUND FACILITIES PRIOR TO WORKING IN THE AREA TO CONFIRM THEIR EXACT LOCATION AND TO DETERMINE WHETHER ANY ADDITIONAL FACILITIES OTHER THAN THOSE SHOWN ON THE PLANS MAY BE PRESENT.
- THE CONTRACTOR SHALL PRESERVE AND PROTECT ALL UNDERGROUND FACILITIES FOUND.
- NOTIFY JDJR ENGINEERS & CONSULTANTS IF ANY UNDERGROUND UTILITIES ARE NOT IN THE LOCATIONS INDICATED ON THESE PLANS (HORIZONTALLY AND VERTICALLY) OR CONFLICT WITH ANY PROPOSED IMPROVEMENTS ASSOCIATED WITH THESE PLANS.

NOTE: THERE IS A POTENTIAL FOR A SURCHARGE IN THE STORMDRAIN SYSTEM DURING CERTAIN STORM EVENTS. THE SWALE TO THE EAST OF THE BUILDING AS SHOWN ON THIS SHEET MUST BE CONSTRUCTED AND MAINTAINED TO THE GRADES INDICATED ON THE PLAN, TO ENSURE EMERGENCY OVERFLOW PROTECTION FOR THE BUILDING. THE PARKING LOT TO THE NORTHWEST OF THE BUILDING AND THE SIDEWALK ONTO THE TAXIWAY AREA ALSO WILL SERVE AS EMERGENCY OVERFLOW POINTS SHOULD THE STORMDRAIN SYSTEM IN ADDISON ROAD BECOME INOPERATIVE.

DRAINAGE DATA							
AREA NO.	ACRES	C	T _c MIN	I ₂ IN/HR	Q ₂ CFS	I ₁₀₀ IN/HR	Q ₁₀₀ CFS
*A	1.11	0.95	10	5.04	5.31	8.82	9.30
B1	0.28	0.95	10	5.04	1.34	8.82	2.35
B2	0.28	0.95	10	5.04	1.34	8.82	2.35
B3	0.01	0.95	10	5.04	0.05	8.82	0.08
**C	0.03	0.95	10	5.04	0.14	8.82	0.25

* NOTE: AREA A1 CONFORMS TO THE APPROVED PLANS ON FILE AT THE CITY OF ADDISON FOR EHOAA - EXECUTIVE HANGAR OWNERSHIP ASSOCIATION OF ADDISON - CITY PLAN NO. FW# 2008-07
 ** NOTE: AREA C IS A PLANE WASH AREA AND DRAINS INTO THE SANITARY SEWER SERVICE (THROUGH A SAND TRAP)

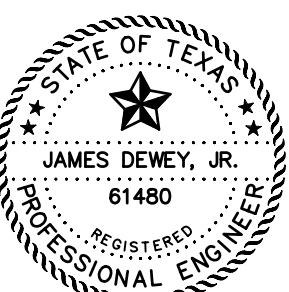
LEGEND

- 624 ——— EXISTING CONTOUR LINE
- + 480.25 ——— EXISTING SPOT ELEVATION
- 642.21 ——— PROPOSED SPOT ELEVATION
- 525 ——— PROPOSED CONTOUR LINE
- INVERT OR FLOWLINE
- DRAINAGE DIVIDE LINE
- PROPOSED CONCRETE PAVEMENT
- PROPOSED CONCRETE WALK

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BENCH MARK: SQUARE CUT FOUND C.L. OF CURB INLET, WEST SIDE OF ADDISON ROAD, 75' SOUTH OF THE C.L. OF AIRPORT PRKY.
 ELEVATION 642.51
 SITE TBM: X-CUT SET IN CONCRETE WALK ±38' NORTH OF THE NORTHEAST CORNER OF THIS TRACT.
 ELEVATION 642.86

REVISIONS:	
1/07/09	PER CITY REVIEW
8/11/09	REV FOR CLIENT
1/29/09	PER CITY REVIEW
9/11/09	PER CITY REVIEW
9/23/09	PER CITY REVIEW
5/18/10	AS BUILT



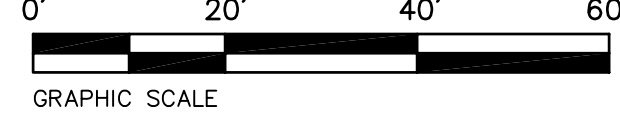
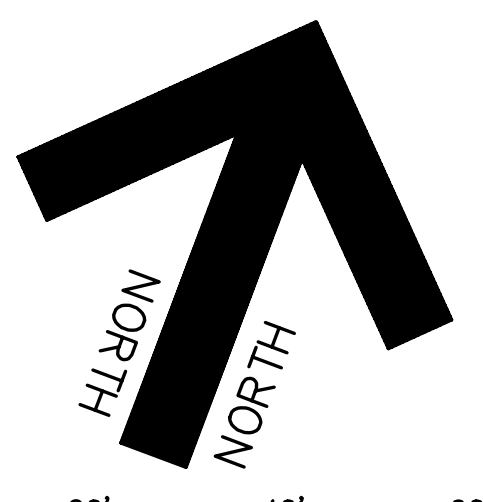
SHEET TITLE:
GRADING, DRAINAGE AND PAVING PLAN
 PAVING AND DRAINAGE IMPROVEMENTS
 15841 ADDISON ROAD
 ADDISON AIRPORT
 ADDISON, TEXAS

PREPARED BY:
JDJR ENGINEERS AND CONSULTANTS
 TSBP# REGISTRATION NUMBER F-8527
 ENGINEERS • SURVEYORS • LAND PLANNERS
 2500 Texas Drive Suite 100 Irving, Texas 75062
 Tel 972-252-5357 Fax 972-252-8958

DATE: DEC. 2, 2008 DRAWN BY: SAS SHEET NO.
 SCALE: 1" = 20' CHECKED BY: JDJR **C3A** of **7**



H:\JDJR\2008\04\02\152-2-38 CIVIL ADDITIONAL PAVING rev sidewalk.dwg, 5/18/2010 8:46:30 AM, JDJR ENGINEERS & CONSULTANTS, INC., JSL



REMOVE EXISTING SOIL WITHIN THE UNPAVED AREA NEXT TO THE HANGAR AND INSTALL 4 INCHES OF CONCRETE TO INSURE ADEQUATE DRAINAGE INTO 6" PIPE.

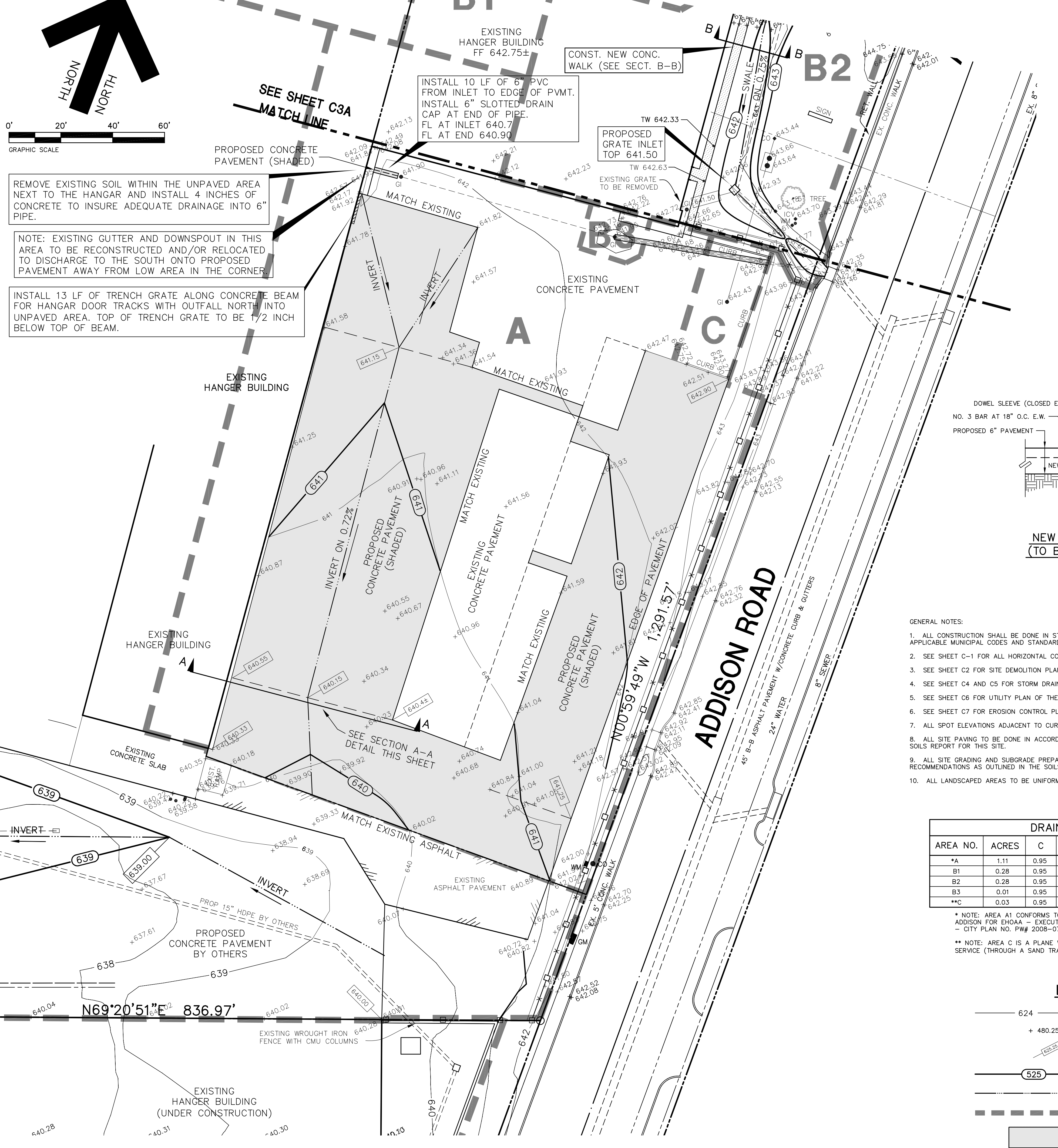
NOTE: EXISTING GUTTER AND DOWNSPOUT IN THIS AREA TO BE RECONSTRUCTED AND/OR RELOCATED TO DISCHARGE TO THE SOUTH ONTO PROPOSED PAVEMENT AWAY FROM LOW AREA IN THE CORNER.

INSTALL 13 LF OF TRENCH GRATE ALONG CONCRETE BEAM FOR HANGAR DOOR TRACKS WITH OUTFALL NORTH INTO UNPAVED AREA. TOP OF TRENCH GRATE TO BE 1/2 INCH BELOW TOP OF BEAM.

INSTALL 10 LF OF 6" PVC FROM INLET TO EDGE OF PVMT. INSTALL 6" SLOTTED DRAIN CAP AT END OF PIPE. FL AT INLET 640.70 FL AT END 640.90

CONST. NEW CONC. WALK (SEE SECT. B-B)

PROPOSED GRATE INLET TOP 641.50



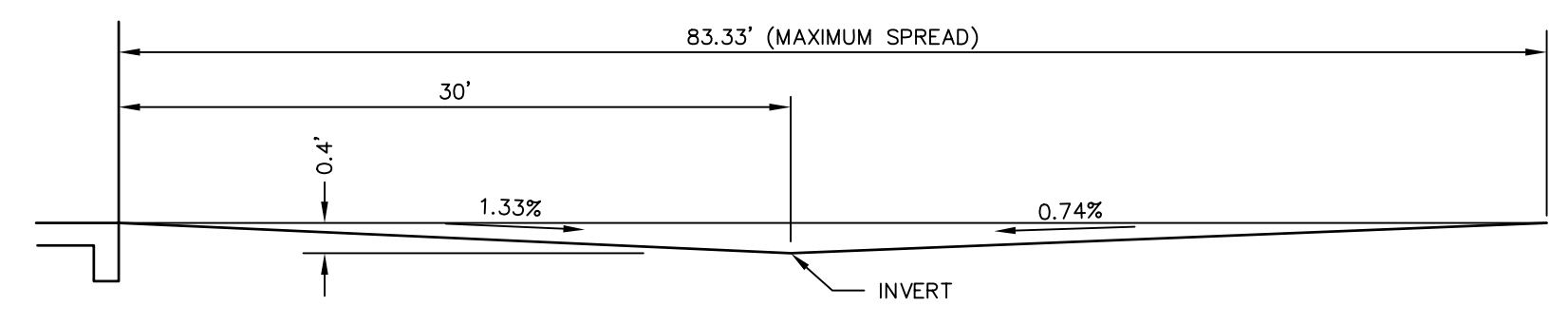
INVERT ANALYSIS
RATING CURVE COMPUTATION

Channel Bottom Slope (ft/ft) 0.0072
Manning's Roughness Coefficient (n-value) 0.013
Channel Left Side Slope (horizontal/vertical) 2:08 1.33%
Channel Right Side Slope (horizontal/vertical) 0.74%
Channel Bottom Width (ft) 0.0

Minimum Flow Depth (ft) 0.1
Maximum Flow Depth (ft) 0.5
Incremental Head (ft) 0.02

COMPUTATION RESULTS

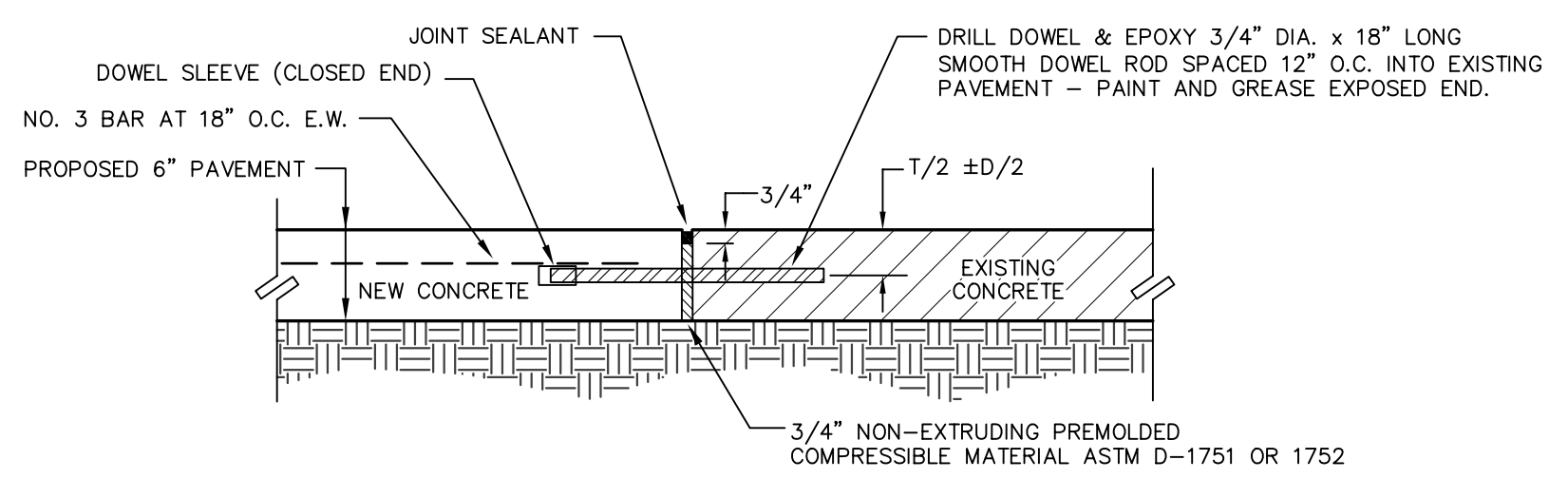
Flow Depth (ft)	Flow Rate (cfs)	Flow Velocity (fps)	Froude Number	Velocity Head (ft)	Energy Head (ft)	Flow Area (sq ft)	Top Width (ft)
0.1	1.17	1.32	1.039	0.027	0.127	0.89	17.6
0.2	1.9	1.49	1.071	0.035	0.155	1.27	21.1
0.4	2.86	1.65	1.098	0.042	0.162	1.73	24.6
0.6	4.07	1.8	1.123	0.051	0.211	2.26	28.1
0.8	5.57	1.95	1.145	0.059	0.239	2.85	31.6
1.0	7.37	2.09	1.165	0.068	0.268	3.52	35.1
1.2	9.49	2.23	1.184	0.077	0.297	4.26	38.6
1.4	11.97	2.36	1.201	0.087	0.327	5.06	42.1
1.6	14.81	2.49	1.217	0.097	0.357	5.94	45.6
1.8	18.04	2.62	1.232	0.107	0.387	6.89	49.1
2.0	21.67	2.74	1.247	0.117	0.417	7.91	52.6
2.2	25.73	2.86	1.26	0.127	0.447	8.99	56.1
2.4	30.24	2.98	1.273	0.138	0.478	10.15	59.6
2.6	35.21	3.1	1.285	0.149	0.509	11.38	63.1
2.8	40.66	3.21	1.297	0.16	0.54	12.67	66.6
3.0	46.61	3.32	1.308	0.171	0.571	14.04	70.1
3.2	53.08	3.43	1.319	0.183	0.603	15.48	73.6
3.4	60.08	3.54	1.329	0.194	0.634	16.98	77.1
3.6	67.63	3.64	1.339	0.206	0.666	18.56	80.6
3.8	75.75	3.75	1.348	0.218	0.699	20.21	84.1
4.0	84.45	3.85	1.357	0.231	0.731	21.92	87.6



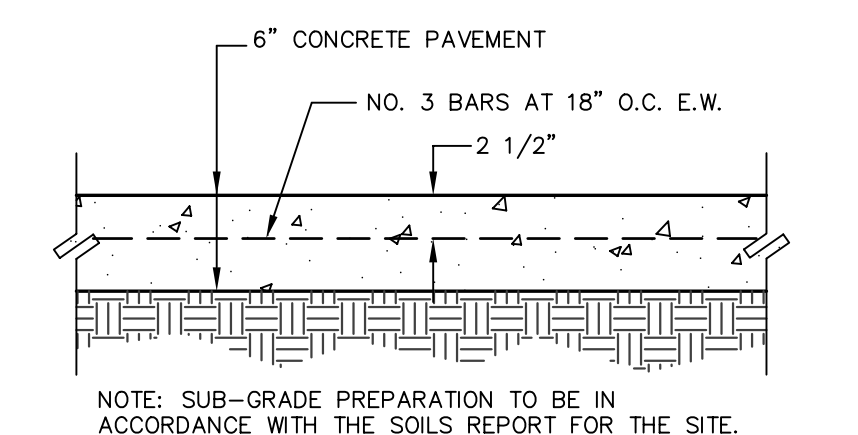
SECTION A-A TAXIWAY INVERT CAPACITY
NOT TO SCALE

D = <0.18' FOR Q2 OF 5.31
D = <0.22' FOR Q100 OF 9.30

CAPACITY AT 0.4' DEPTH



NEW CONCRETE TO EXISTING CONCRETE DETAIL
(TO BE USED ON ALL AIRCRAFT APRON PAVING)
NOT TO SCALE



6" CONCRETE PAVEMENT SECTION DETAIL
NOT TO SCALE

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DRAINAGE DATA

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** NOTE: AREA C IS A PLANE WASH AREA AND DRAINS INTO THE SANITARY SEWER SERVICE (THROUGH A SAND TRAP)

LEGEND

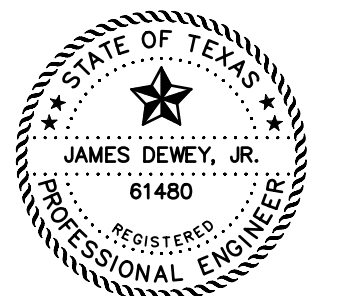
- 624 ——— EXISTING CONTOUR LINE
- + 480.25 ——— EXISTING SPOT ELEVATION
- (480.25) ——— PROPOSED SPOT ELEVATION
- 525 ——— PROPOSED CONTOUR LINE
- INVERT OR FLOWLINE
- DRAINAGE DIVIDE LINE
- ■ ■ ■ ■ PROPOSED CONCRETE PAVEMENT

"AS-BUILT"
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BENCH MARK: SQUARE CUT FOUND C.L. OF CURB INLET, WEST SIDE OF ADDISON ROAD, 75' SOUTH OF THE C.L. OF AIRPORT PRKY.
ELEVATION 642.51
SITE TBM: X-CUT SET IN CONCRETE WALK ±38' NORTH OF THE NORTHEAST CORNER OF THIS TRACT.
ELEVATION 642.86

REVISIONS:

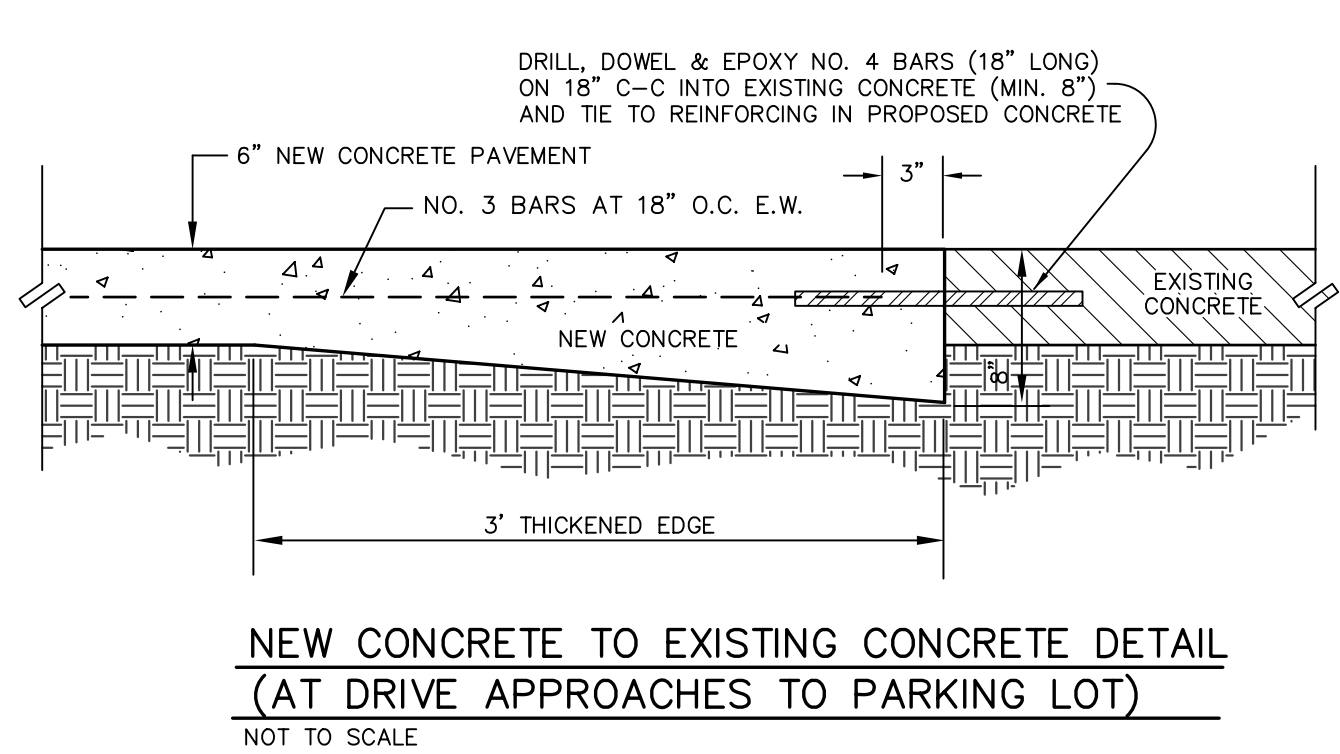
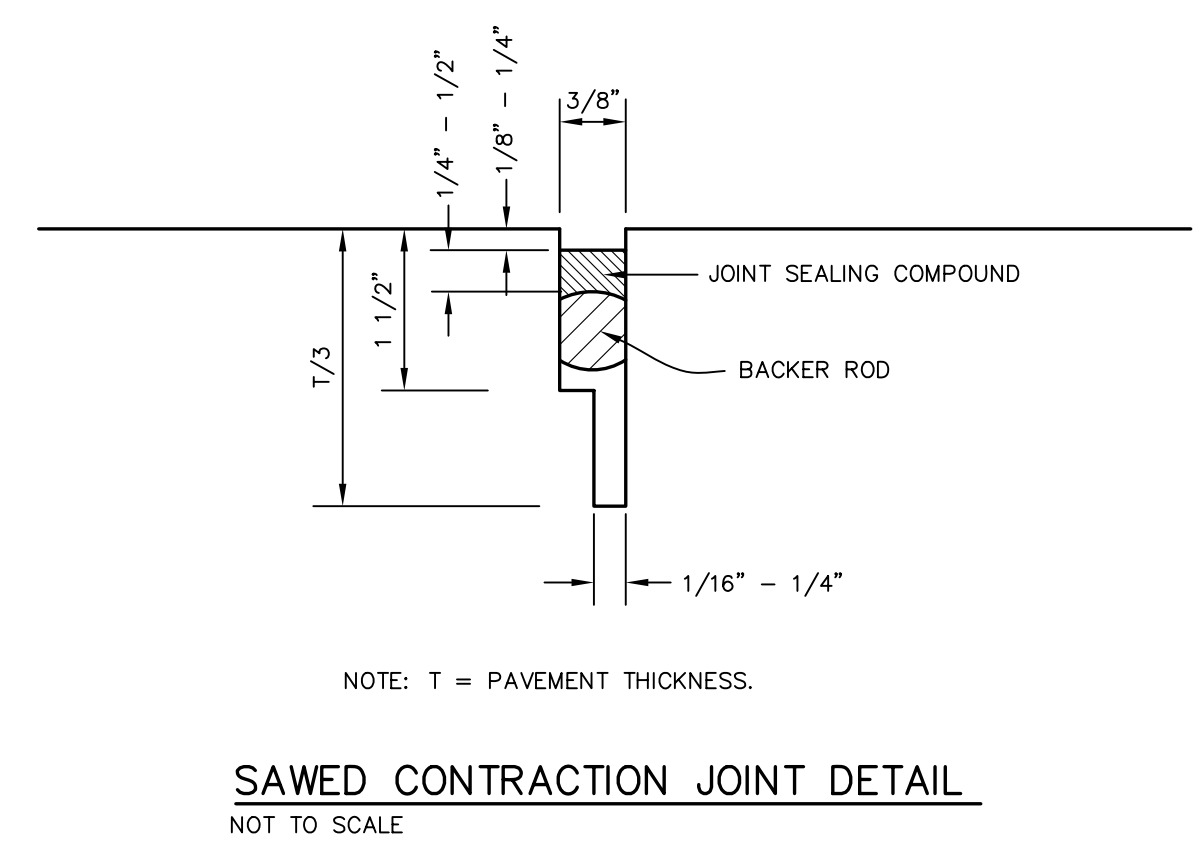
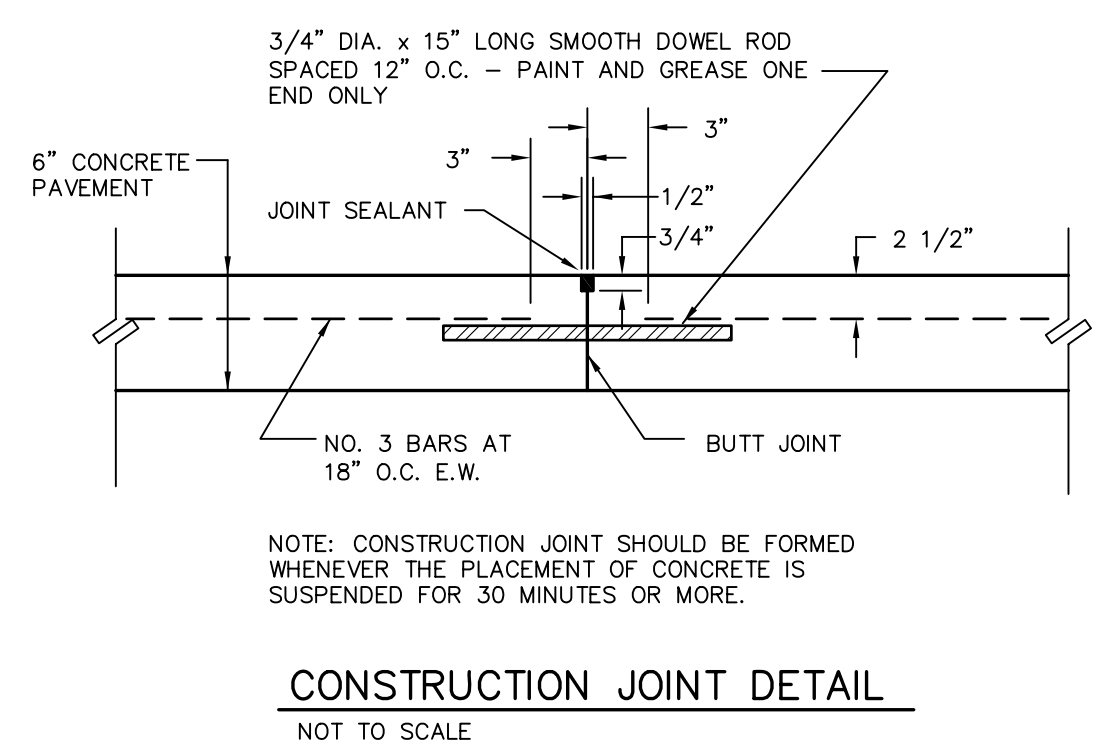
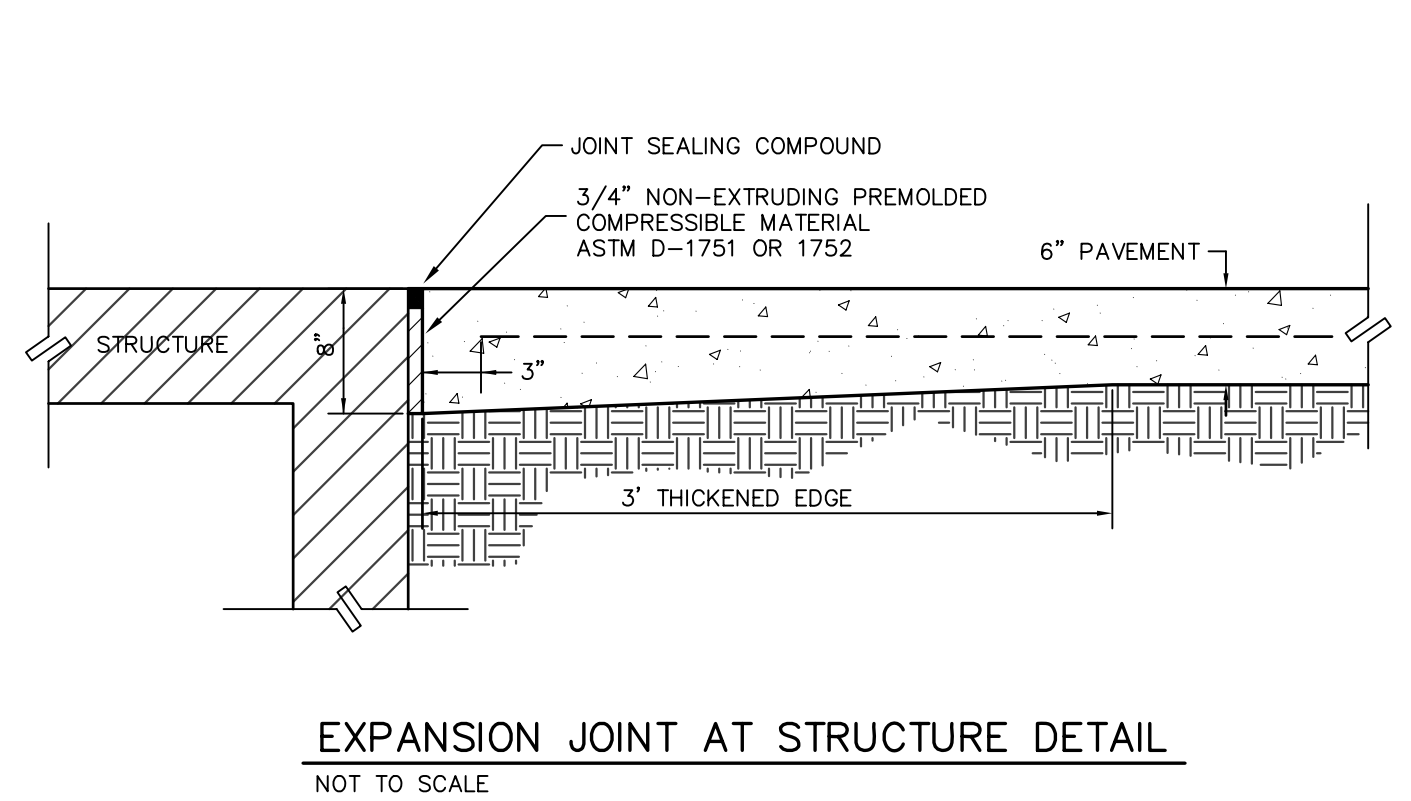
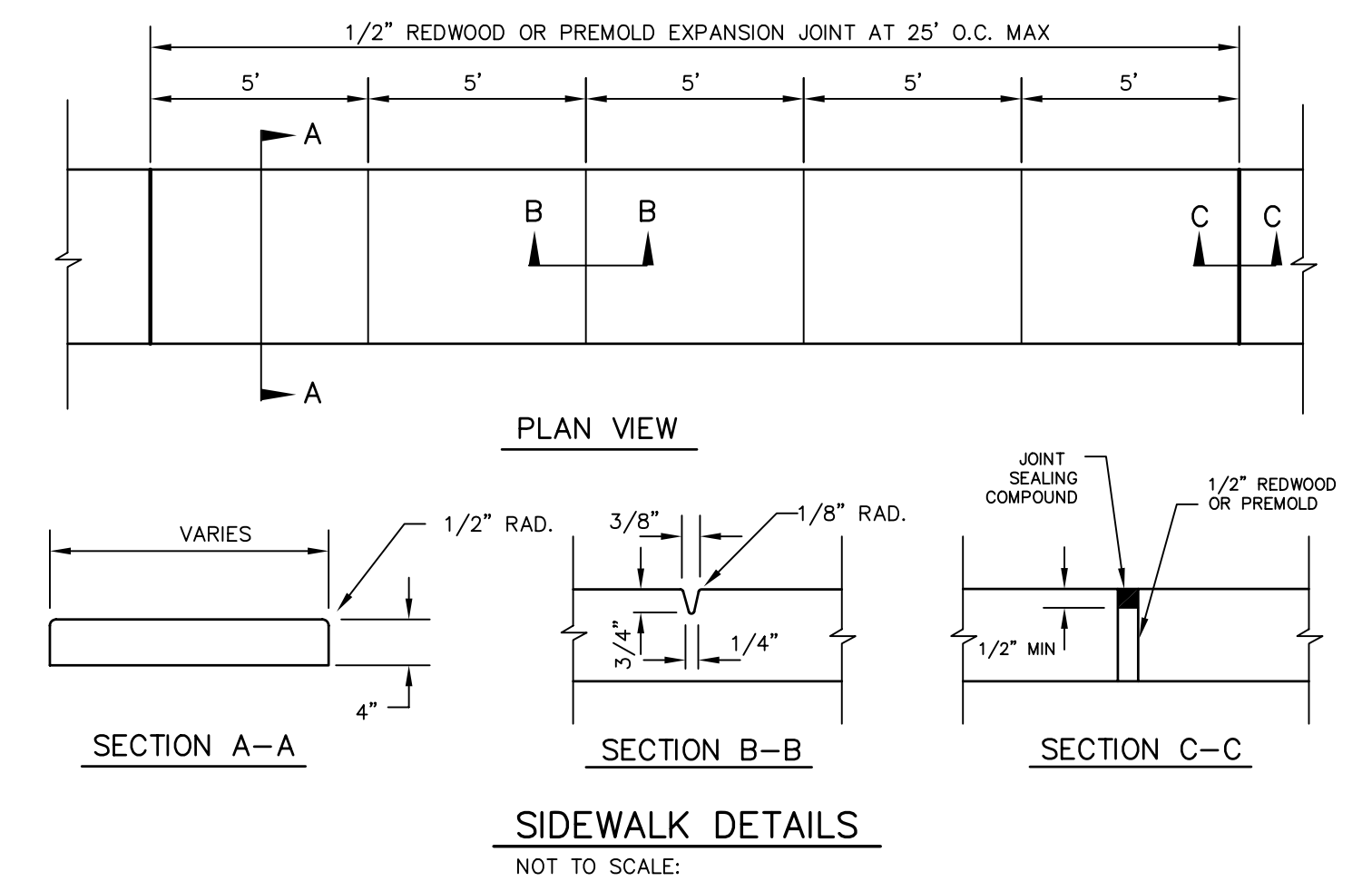
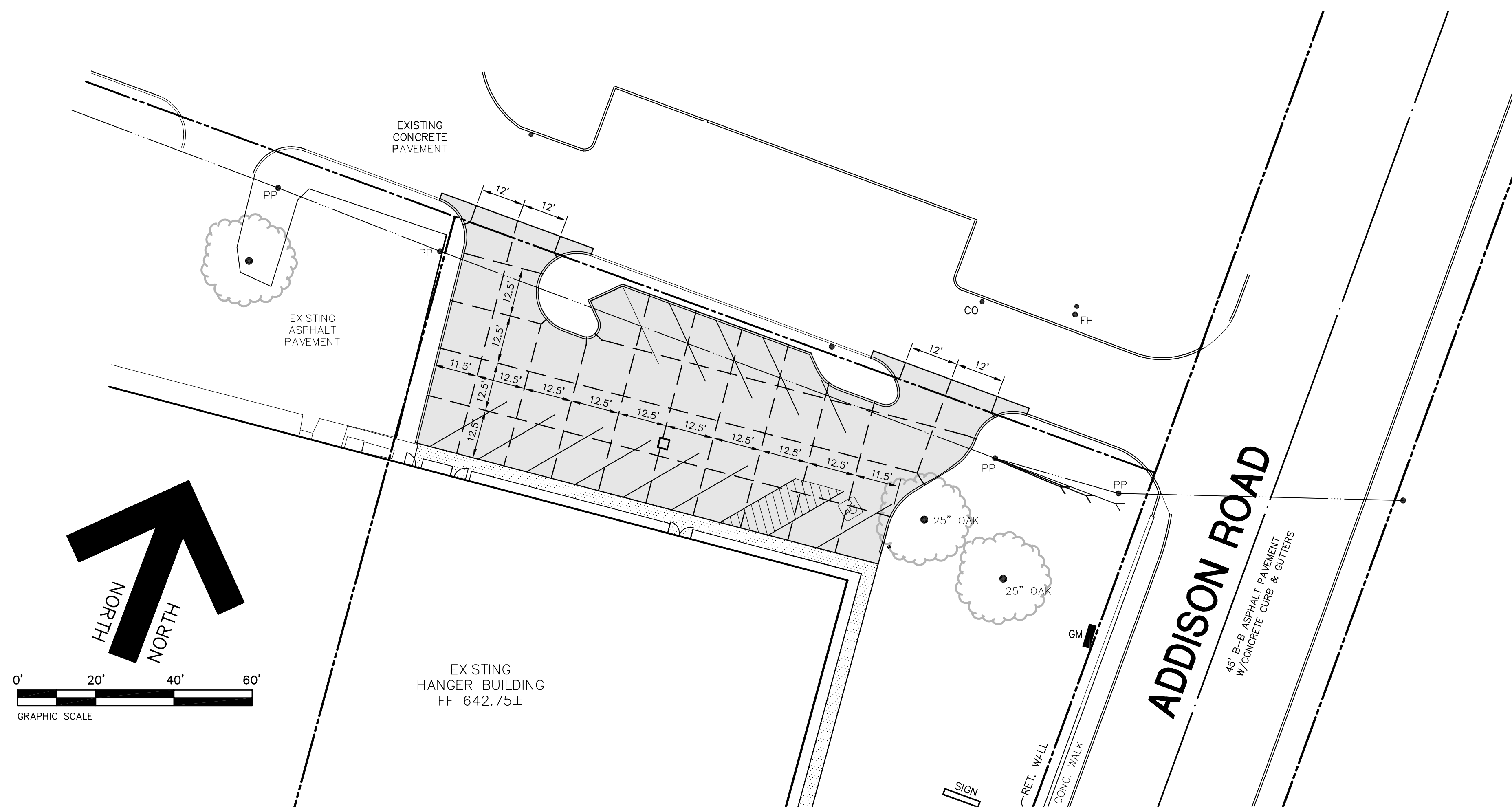
DATE	REVISION
1/07/09	PER CITY REVIEW
8/11/09	REV FOR CLIENT
9/11/09	PER CITY REVIEW
9/23/09	PER CITY REVIEW
4/13/10	ADD TRENCH GRATE
5/18/10	AS BUILT



GRADING, DRAINAGE AND PAVING PLAN
PAVING AND DRAINAGE IMPROVEMENTS
15841 ADDISON ROAD
ADDISON AIRPORT
ADDISON, TEXAS

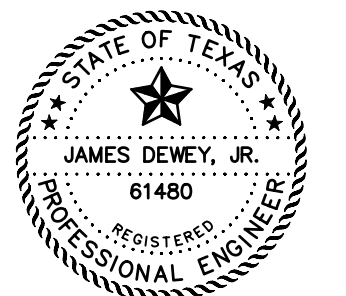
PREPARED BY:
JDJR ENGINEERS AND CONSULTANTS
TSBPE REGISTRATION NUMBER F-8527
ENGINEERS • SURVEYORS • LAND PLANNERS
2500 Texas Drive Suite 100 Irving, Texas 75062
Tel 972-252-5357 Fax 972-252-8958

DATE: DEC. 2, 2008 DRAWN BY: SAS SHEET NO.
SCALE: 1" = 20' CHECKED BY: JDJR **C3B** OF 7



"AS-BUILT"
 THIS AS-BUILT DRAWING IS BASED ON INFORMATION OBTAINED FROM BUILDING CONTRACTORS DURING CONSTRUCTION AS PROVIDED TO JDJR ENGINEERS & CONSULTANTS, INC.
 JDJR ENGINEERS & CONSULTANTS, INC. DOES NOT GUARANTEE THE ACCURACY OR COMPLETENESS OF THESE RECORDS.

REVISIONS:	
9/11/09	PER CITY REVIEW
9/23/09	PER CITY REVIEW
5/18/10	AS BUILT



LEGEND

- SAWED CONTRACTION JOINT
- ▒ PROPOSED CONCRETE PAVEMENT
- ▒ PROPOSED CONCRETE WALK

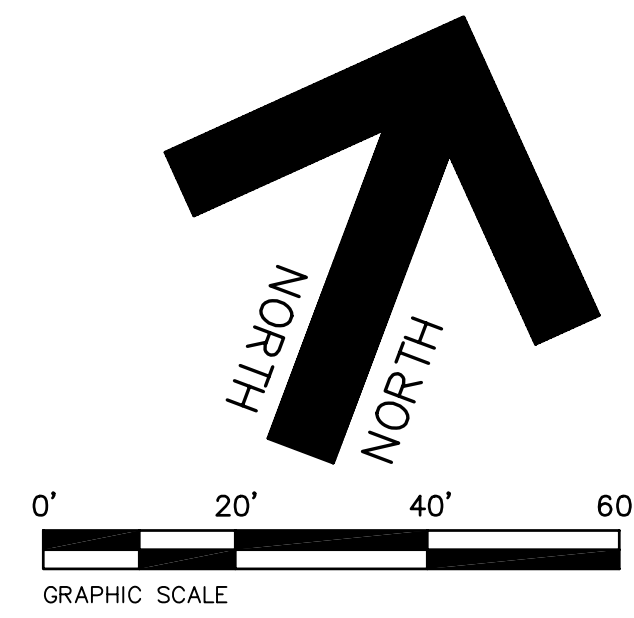
SHEET TITLE:
PAVEMENT JOINT PATTERN PLAN
 PAVING AND DRAINAGE IMPROVEMENTS
 15841 ADDISON ROAD
 ADDISON AIRPORT
 ADDISON, TEXAS

PREPARED BY:
JDJR ENGINEERS AND CONSULTANTS
 TSBPPE REGISTRATION NUMBER F-8527

ENGINEERS • SURVEYORS • LAND PLANNERS
 2500 Texas Drive Suite 100 Irving, Texas 75062
 Tel 972-252-5357 Fax 972-252-8958

DATE: DEC. 2, 2008 DRAWN BY: SAS SHEET NO.
 SCALE: 1" = 20' CHECKED BY: JDJR **C3C** OF **7**

H:\JDJR\2008\04\04\02\2-38 CIVIL ADDITIONAL PAVING no sidewalk.dwg, 5/18/2010 8:46:35 AM, JDJR ENGINEERS & CONSULTANTS, INC., SCS



EXISTING
HANGER BUILDING
FF 642.75±

EXISTING
HANGER BUILDING

EXISTING
HANGER BUILDING

EXISTING
CONCRETE SLAB

EXIST.
RAMP

EXISTING
ASPHALT PAVEMENT

EXISTING WROUGHT IRON
FENCE WITH CMU COLUMNS

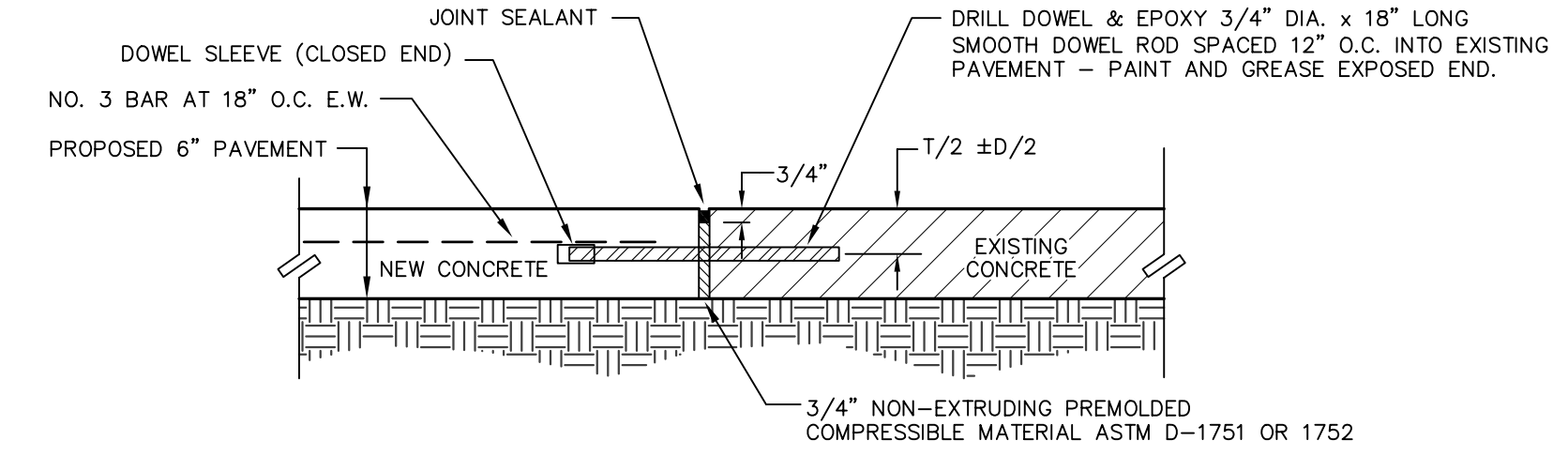
EXISTING
HANGER BUILDING
(UNDER CONSTRUCTION)

N69°20'51"E 836.97'

N00°59'49"W 1,291.57'

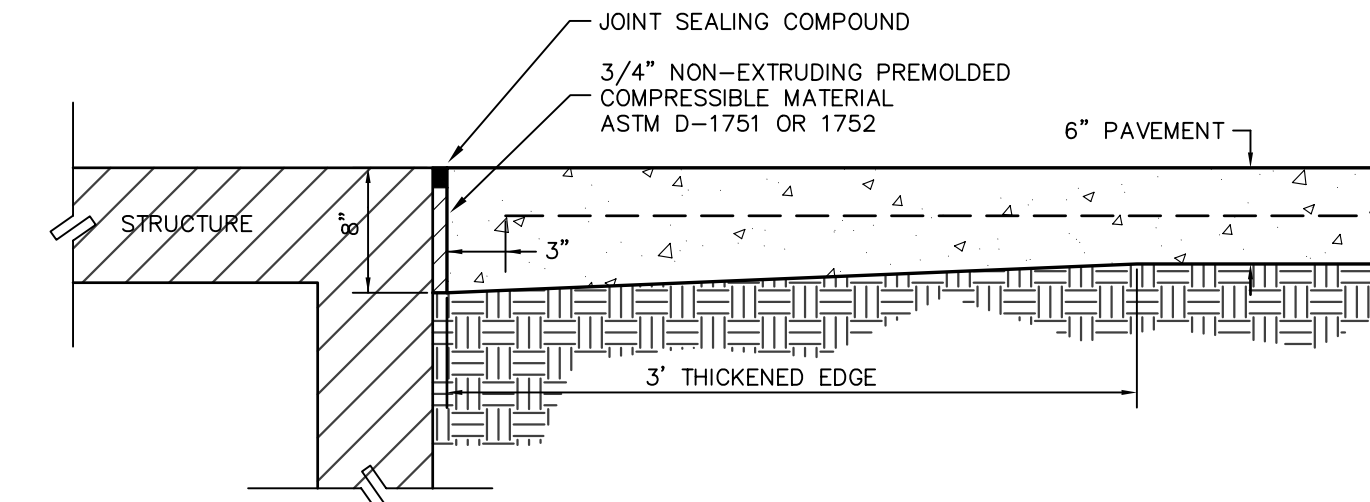
ADDISON ROAD

45' B-B ASPHALT PAVEMENT W/CONCRETE CURB & GUTTERS



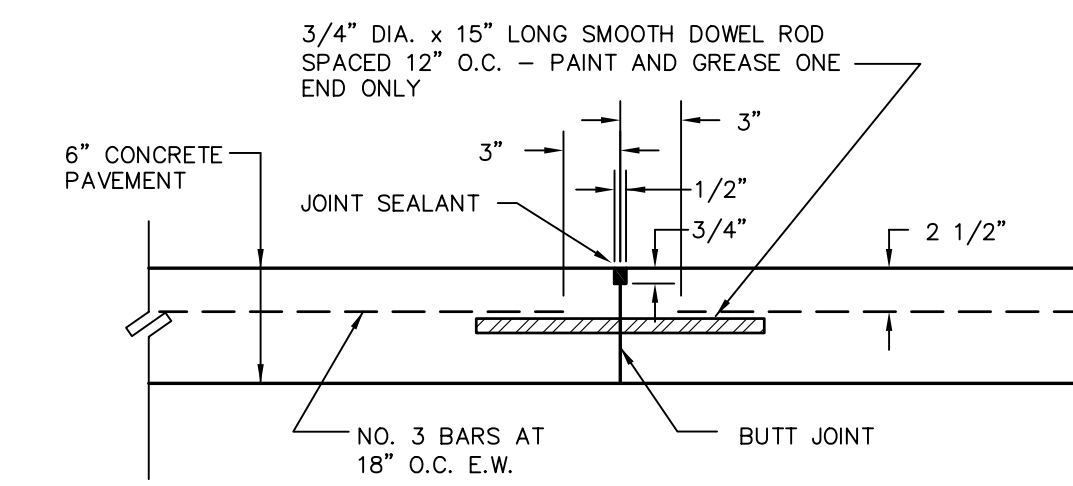
NEW CONCRETE TO EXISTING CONCRETE DETAIL
(TO BE USED ON ALL TAXI-WAY PAVING)

NOT TO SCALE



EXPANSION JOINT AT STRUCTURE DETAIL

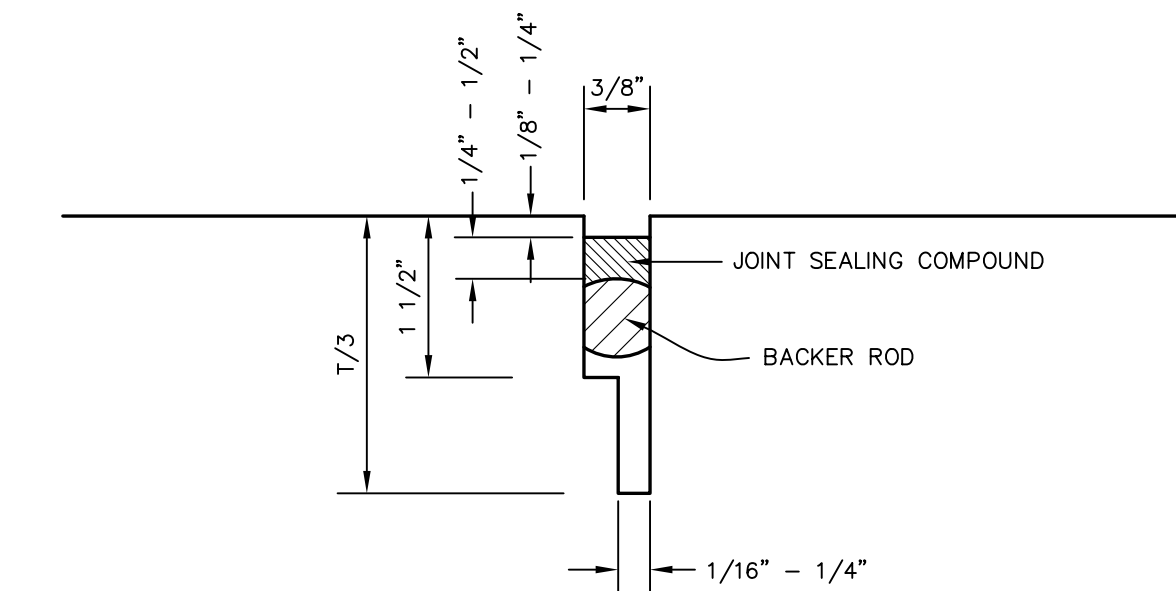
NOT TO SCALE



NOTE: CONSTRUCTION JOINT SHOULD BE FORMED
WHENEVER THE PLACEMENT OF CONCRETE IS
SUSPENDED FOR 30 MINUTES OR MORE.

CONSTRUCTION JOINT DETAIL

NOT TO SCALE



NOTE: T = PAVEMENT THICKNESS.

SAWED CONTRACTION JOINT DETAIL

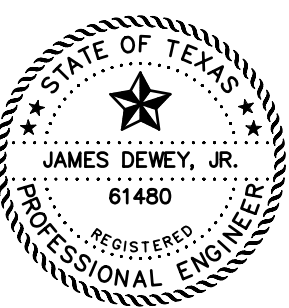
NOT TO SCALE

LEGEND

- SAWED CONTRACTION JOINT
- [Hatched Box] PROPOSED CONCRETE PAVEMENT
- [Dotted Box] PROPOSED CONCRETE WALK

"AS-BUILT"
THIS AS-BUILT DRAWING IS BASED ON INFORMATION
OBTAINED FROM BUILDING CONTRACTORS DURING
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CONSULTANTS, INC.
JDR ENGINEERS & CONSULTANTS, INC. DOES NOT
GUARANTEE THE ACCURACY OR COMPLETENESS OF
THESE RECORDS.

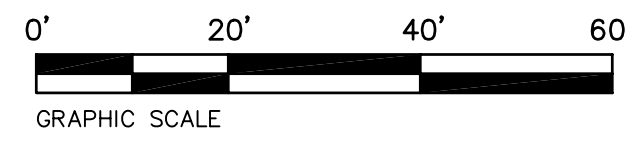
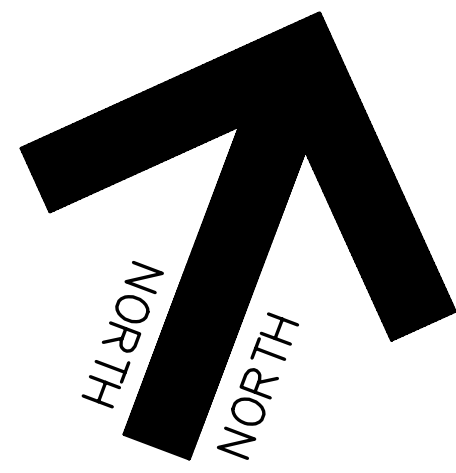
REVISIONS:	
9/11/09	PER CITY REVIEW
9/23/09	PER CITY REVIEW
5/18/10	AS BUILT



SHEET TITLE:
PAVEMENT JOINT PATTERN PLAN
PAVING AND DRAINAGE IMPROVEMENTS
15841 ADDISON ROAD
ADDISON AIRPORT
ADDISON, TEXAS

PREPARED BY:
JDR ENGINEERS AND CONSULTANTS
TSBPE REGISTRATION NUMBER F-8527
ENGINEERS • SURVEYORS • LAND PLANNERS
2500 Texas Drive Suite 100 Irving, Texas 75062
Tel 972-252-5357 Fax 972-252-8958

DATE: DEC. 2, 2008 DRAWN BY: SAS SHEET NO.
SCALE: 1" = 20' CHECKED BY: JDR **C3D** OF 7



GENERAL NOTES:

1. ALL CONSTRUCTION SHALL BE DONE IN STRICT CONFORMANCE TO THESE PLANS AND ALL APPLICABLE MUNICIPAL CODES AND STANDARDS.
2. SEE SHEET C1 FOR ALL HORIZONTAL CONTROL DIMENSIONS.
3. SEE SHEET C3 FOR GRADING PLAN AND DRAINAGE PLAN.
4. SEE SHEET C5 FOR STORM DRAIN PROFILE AND DETAILS.
5. SEE SHEET C7 FOR EROSION CONTROL PLAN OF THE SITE.
6. ANY EXISTING WATER SERVICE LINES OR IRRIGATION SYSTEM IN CONFLICT WITH PROPOSED CONSTRUCTION OR DAMAGED DURING CONSTRUCTION TO BE REPLACED/RELOCATED BY CONTRACTOR.
7. INSTALL SPLASH BLOCKS AT ANY DOWNSPOUTS IN PLANTER AREAS THAT DIRECT FLOW ONTO SIDEWALK.

EXISTING UTILITIES NOTES:

1. THE LOCATION OF ALL UNDERGROUND FACILITIES AS INDICATED ON THE PLANS ARE TAKEN FROM PUBLIC RECORDS. JDJR ENGINEERS & CONSULTANTS ASSUMES NO RESPONSIBILITY FOR THE ACCURACY OF SUCH RECORDS AND DOES NOT GUARANTEE THAT ALL UNDERGROUND UTILITIES ARE SHOWN OR ARE LOCATED PRECISELY AS INDICATED.
2. IT IS THE RESPONSIBILITY OF THE CONTRACTOR TO MAKE ARRANGEMENTS WITH THE OWNERS OF SUCH UNDERGROUND FACILITIES PRIOR TO WORKING IN THE AREA TO CONFIRM THEIR EXACT LOCATION AND TO DETERMINE WHETHER ANY ADDITIONAL FACILITIES OTHER THAN THOSE SHOWN ON THE PLANS MAY BE PRESENT.
3. THE CONTRACTOR SHALL PRESERVE AND PROTECT ALL UNDERGROUND FACILITIES FOUND.
4. NOTIFY JDJR ENGINEERS & CONSULTANTS IF ANY UNDERGROUND UTILITIES ARE NOT IN THE LOCATIONS INDICATED ON THESE PLANS (HORIZONTALLY AND VERTICALLY) OR CONFLICT WITH ANY PROPOSED IMPROVEMENTS ASSOCIATED WITH THESE PLANS.

NOTE: THERE IS A POTENTIAL FOR A SURCHARGE IN THE STORMDRAIN SYSTEM DURING CERTAIN STORM EVENTS. THE SWALE TO THE EAST OF THE BUILDING AS SHOWN ON SHEET C3B MUST BE CONSTRUCTED AND MAINTAINED TO THE GRADES INDICATED ON THE PLAN, TO ENSURE EMERGENCY OVERFLOW PROTECTION FOR THE BUILDING. THE PARKING LOT TO THE NORTHWEST OF THE BUILDING AND THE SIDEWALK ONTO THE TAXIWAY AREA ALSO WILL SERVE AS EMERGENCY OVERFLOW POINTS SHOULD THE STORMDRAIN SYSTEM IN ADDISON ROAD BECOME INOPERATIVE.

REMOVE EXISTING SOIL WITHIN THE UNPAVED AREA NEXT TO THE HANGAR AND INSTALL 4 INCHES OF CONCRETE TO INSURE ADEQUATE DRAINAGE INTO 6" PIPE.

NOTE: EXISTING GUTTER AND DOWNSPOUT IN THIS AREA TO BE RECONSTRUCTED AND/OR RELOCATED TO DISCHARGE TO THE SOUTH ONTO PROPOSED PAVEMENT AWAY FROM LOW AREA IN THE CORNER.

INSTALL 13 LF OF TRENCH GRATE (4 - 1 METER LONG CHANNELS) ALONG CONCRETE BEAM FOR HANGAR DOOR TRACKS WITH OUTFALL NORTH INTO UNPAVED AREA. TOP OF TRENCH GRATE TO BE 1/2 INCH BELOW TOP OF BEAM. TRENCH GRATE TO BE ACO DRAIN KLASSIKDRAIN K100S WITH SLOTTED GALVANIZED STEEL GRATE (CLASS E).

EXISTING HANGER BUILDING

EXISTING CONCRETE PAVEMENT

PROPOSED CONCRETE PAVEMENT

EXISTING HANGER BUILDING
FF 642.75±

ADDISON ROAD

NOTE: VERIFY EXACT LOCATION AND DEPTH OF EXISTING SEWER SERVICES IN AREA PRIOR TO ANY CONSTRUCTION. NOTIFY JDJR ENGINEERS & CONSULTANTS, INC. IF A CONFLICT IS EVIDENT.

INSTALL 10 LF OF 6" PVC FROM INLET TO EDGE OF PVMT. INSTALL 6" SLOTTED DRAIN CAP AT END OF PIPE. FL AT INLET 640.7 FL AT END 640.90

STA. 2+10, END 12" RCP. INSTALL PRE-CAST GRATE INLET. TOP 641.90

INSTALL 62 LF OF 12" CL IV RCP

STA. 1+59 INSTALL 12"x 45" BEND

STA. 1+31 INSTALL 12"x 45" BEND

INSTALL 101 LF OF 12" PVC

STA. 0+47, END 15" RCP. INSTALL PRE-CAST GRATE INLET BEGIN 12" PVC NORTH TOP OF GRATE 641.50

EXISTING SAND TRAP TO BE SERVICED

STA. 0+05
INSTALL 15"x10" 60" WYE

EXISTING GRATE INLET TO BE REMOVED (OUTLET PIPE TO BE CAPPED)

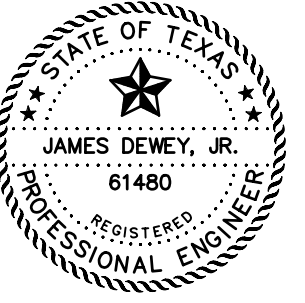
INSTALL 45 LF OF 15" PVC

STA. 0+00
BEGIN 15" PVC AT BACK OF EXISTING CURB INLET F.L. EX. INLET 639.20

INSTALL 10"x 45" BEND AT PROPOSED WYE AND 78 LF OF 10" PVC ON 0.13% GRADE TO EXISTING GRATE INLET

EXISTING GRATE INLET AND PIPE TO BE CLEANED OUT

REVISIONS:	
1/07/09	PER CITY REVIEW
1/29/09	PER CITY REVIEW
8/11/09	REV FOR CLIENT
9/11/09	PER CITY REVIEW
9/23/09	PER CITY REVIEW
12/18/09	10" PVC DRAIN
4/13/10	ADD TRENCH GRATE
5/18/10	AS BUILT



"AS-BUILT"
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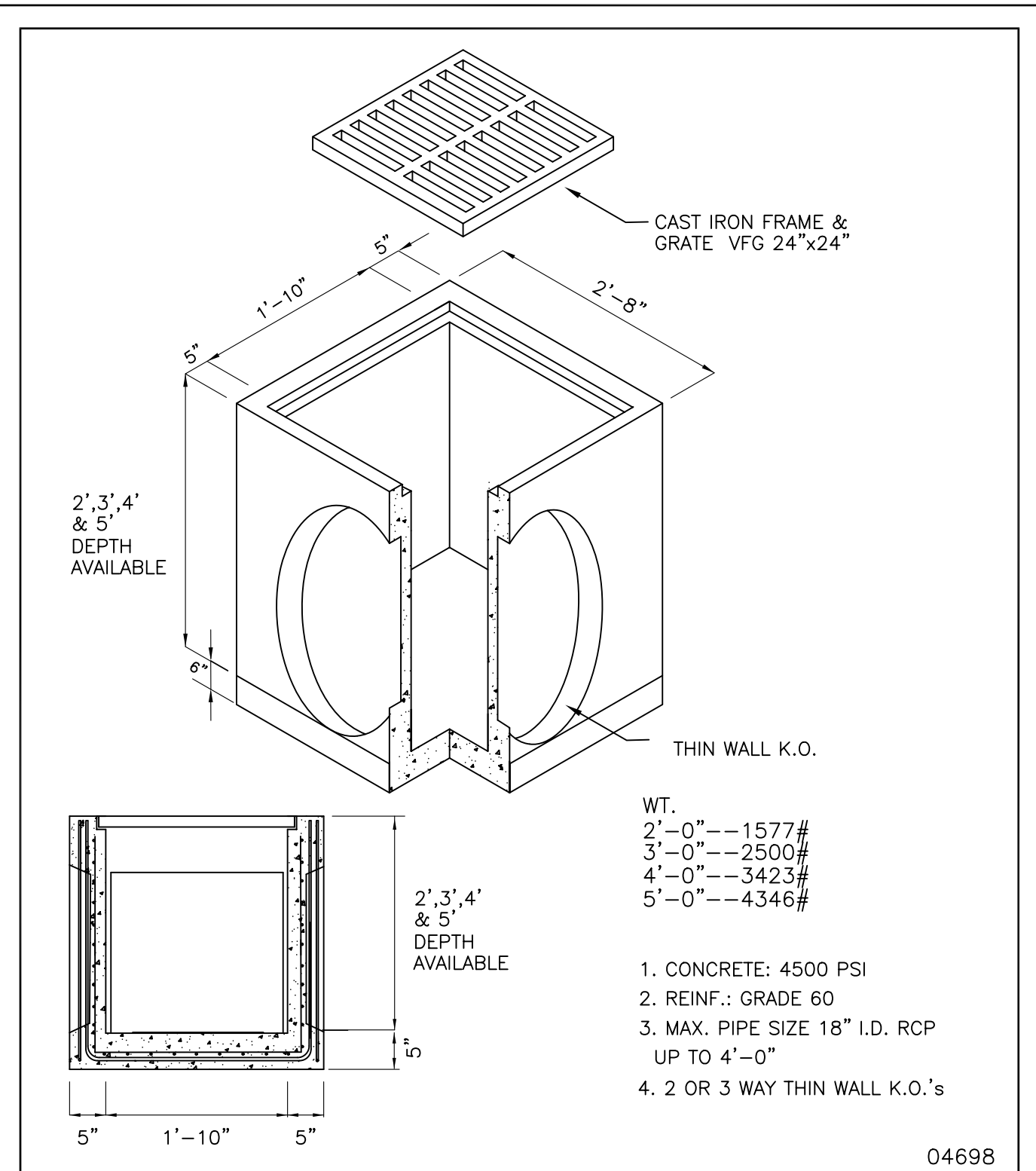
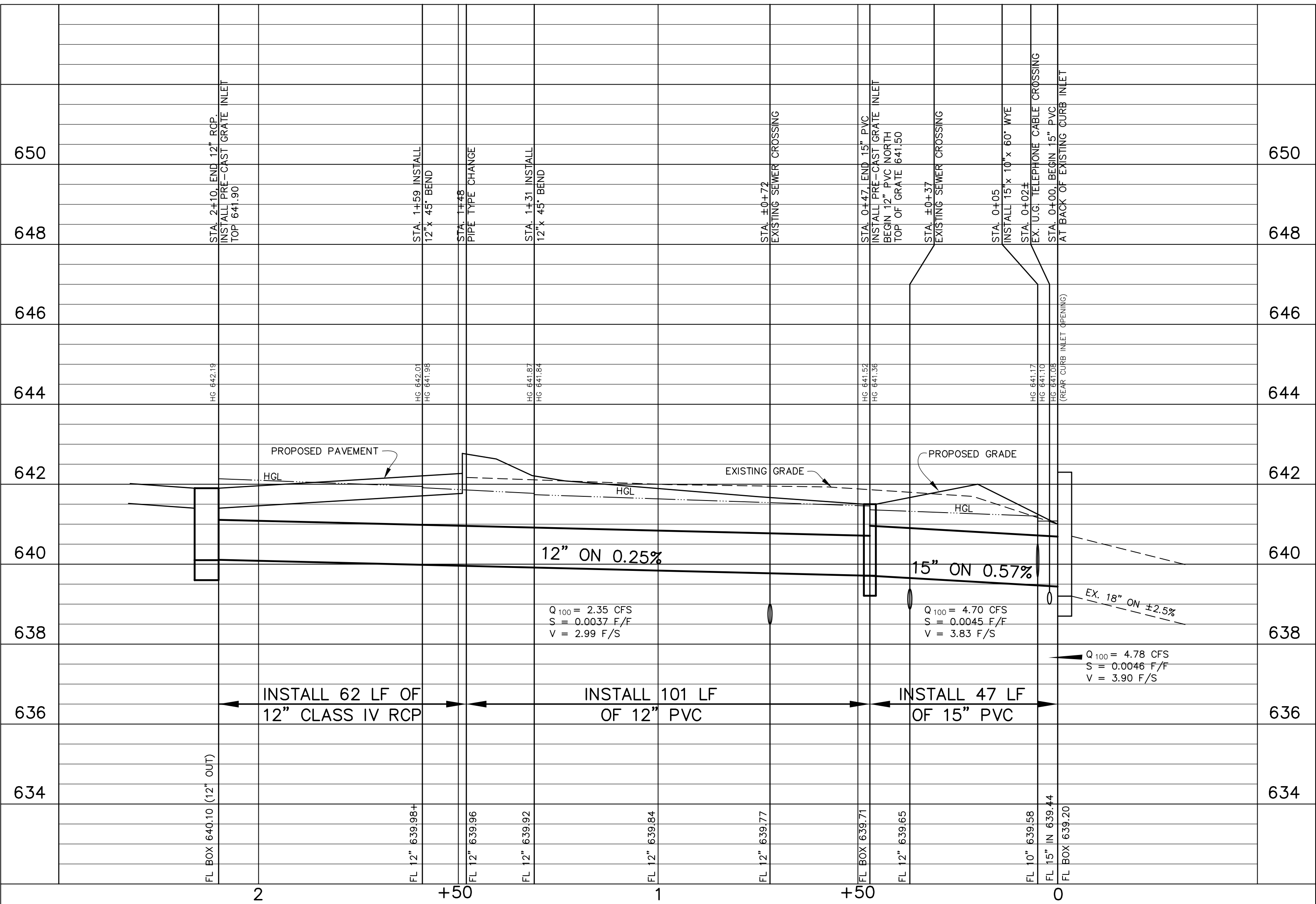
BENCH MARK: SQUARE CUT FOUND C.L. OF CURB INLET, WEST SIDE OF ADDISON ROAD, 75' SOUTH OF THE C.L. OF AIRPORT PRKY.
ELEVATION 642.51
SITE TBM: X-CUT SET IN CONCRETE WALK ±38' NORTH OF THE NORTHEAST CORNER OF THIS TRACT.
ELEVATION 642.86

SHEET TITLE:
STORM DRAIN PLAN
PAVING AND DRAINAGE IMPROVEMENTS
15841 ADDISON ROAD
AIRPORT
ADDISON, TEXAS

PREPARED BY:
JDJR ENGINEERS AND CONSULTANTS
TSBPE REGISTRATION NUMBER F-8527
ENGINEERS • SURVEYORS • LAND PLANNERS
2500 Texas Drive Suite 100 Irving, Texas 75062
Tel 972-252-5357 Fax 972-252-8958

DATE: DEC. 2, 2008 DRAWN BY: SAS SHEET NO.
SCALE: 1" = 20' CHECKED BY: JDJR **C4** OF **7**

H:\D\Ferris\2008\04\02\02-2-08 CIVIL ADDITIONAL PAVING no sidewalk.dwg, 5/19/2010 8:51:16 AM, JDIR ENGINEERS & CONSULTANTS, INC. -SLS



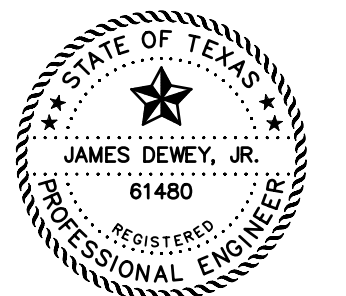
A CATCH BASIN NO. 24

American Industrial Pre-Cast Products, Inc.
 P.O. BOX 365 ALVARADO, TEXAS 76009, METRO: 817-477-5286
 501 EAST BAXTER SEGUIN, TEXAS 78155 210-401-0555

04698

NOTE: THERE IS A POTENTIAL FOR A SURCHARGE IN THE STORMDRAIN SYSTEM DURING CERTAIN STORM EVENTS. THE SWALE TO THE EAST OF THE BUILDING AS SHOWN ON SHEET C3B MUST BE CONSTRUCTED AND MAINTAINED TO THE GRADES INDICATED ON THE PLAN, TO ENSURE EMERGENCY OVERFLOW PROTECTION FOR THE BUILDING, THE PARKING LOT TO THE NORTHWEST OF THE BUILDING AND THE SIDEWALK ON THE TAXIWAY AREA ALSO WILL SERVE AS EMERGENCY OVERFLOW POINTS SHOULD THE STORMDRAIN SYSTEM IN ADDISON ROAD BECOME INOPERATIVE.

REVISIONS:	
1/07/09	PER CITY REVIEW
1/07/09	PER CITY REVIEW
8/11/09	REV FOR CLIENT
9/11/09	PER CITY REVIEW
12/8/09	FIELD ADJUSTMENT
5/18/10	AS BUILT



"AS-BUILT"

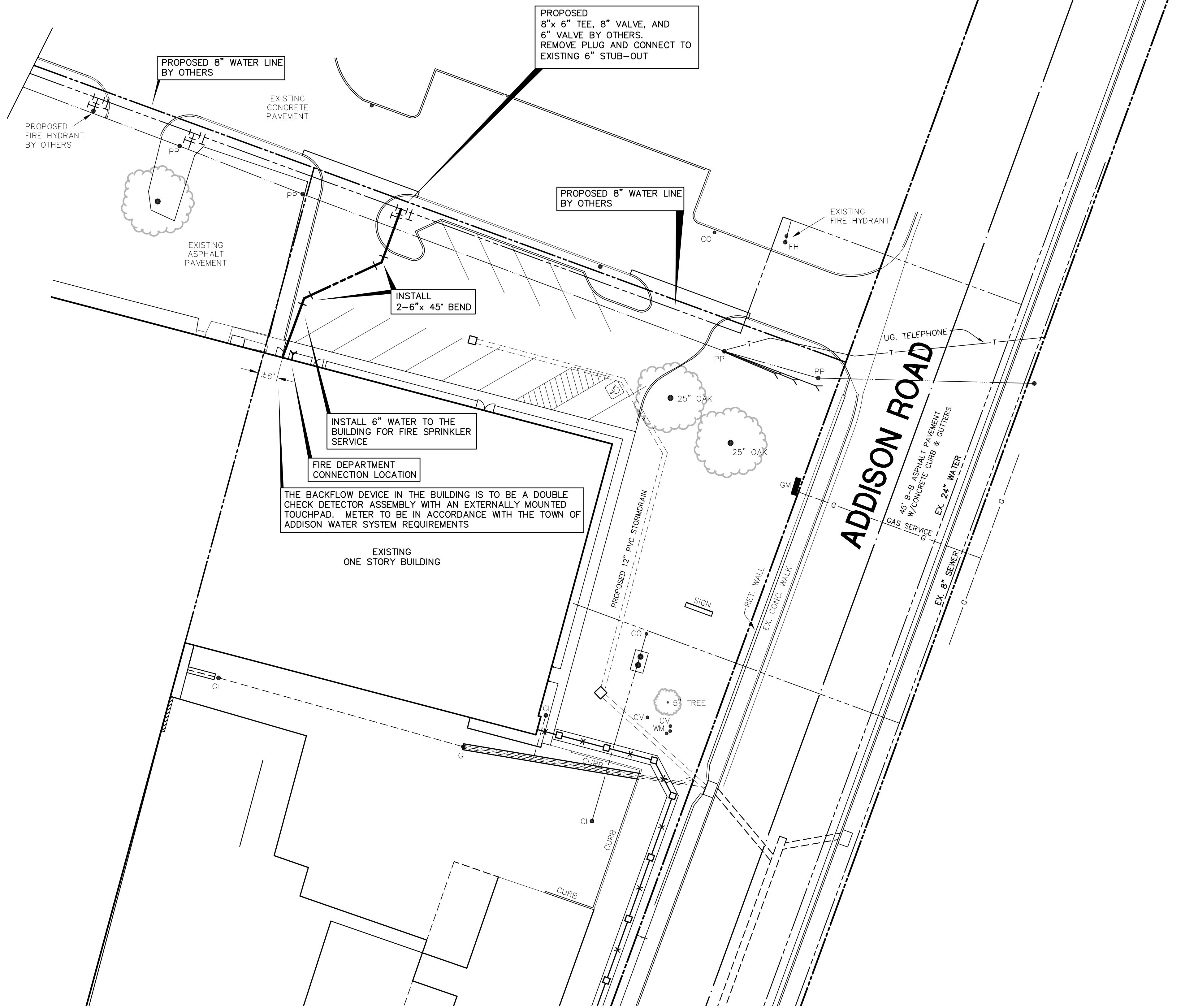
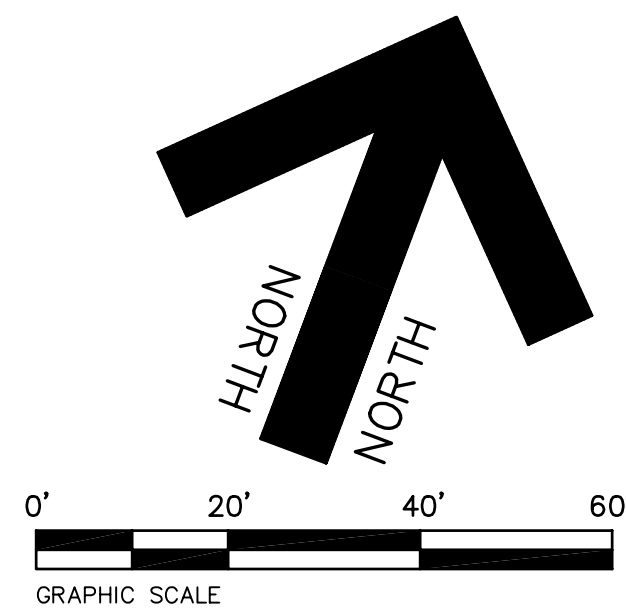
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SHEET TITLE:
STORM DRAIN PROFILE AND DETAILS
 PAVING AND DRAINAGE IMPROVEMENTS
 15841 ADDISON ROAD
 ADDISON AIRPORT
 ADDISON, TEXAS

PREPARED BY:
JDIR ENGINEERS AND CONSULTANTS
 TSBP REGISTRATION NUMBER F-8527

ENGINEERS • SURVEYORS • LAND PLANNERS
 2500 Texas Drive Suite 100 Irving, Texas 75062
 Tel 972-252-5357 Fax 972-252-8958

DATE: DEC. 2, 2008	DRAWN BY: SAS	SHEET NO.
SCALE: 1"=20'H, 1"=2'V	CHECKED BY: JDIR	05 OF 7



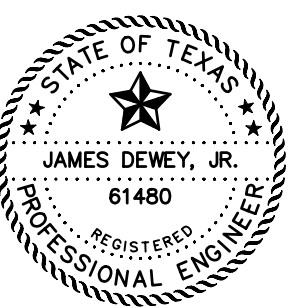
GENERAL NOTES:

1. ALL CONSTRUCTION SHALL BE DONE IN STRICT CONFORMANCE TO THESE PLANS AND ALL APPLICABLE MUNICIPAL CODES AND STANDARDS.
2. THE CONTRACTOR SHALL MAKE APPLICATION FOR SERVICES, OBTAIN ALL PERMITS, AND PAY ALL CHARGES, FEES, AND CONNECTION COSTS REQUIRED FOR EACH UTILITY SERVICE. (THESE COSTS AND FEES SHALL NOT BE INCLUDED IN THE BASE BID).
3. SEE PLUMBING PLANS FOR EXACT LOCATIONS AND DETAILS OF SERVICES INTO BUILDING.
4. CONTRACTOR TO VERIFY THE LOCATION AND DEPTH OF ALL EXISTING UTILITIES PRIOR TO CONSTRUCTION.
5. ANY EXISTING WATER SERVICE LINES OR IRRIGATION SYSTEM IN CONFLICT WITH PROPOSED CONSTRUCTION OR DAMAGED DURING CONSTRUCTION TO BE REPLACED BY CONTRACTOR.
6. ALL WATER MAINS SHALL BE AWWA C909 PVC WATER PIPE.
7. ALL WATER MAINS SHALL HAVE A MINIMUM COVER AS FOLLOWS: 48" BELOW FINISHED STREET GRADE OR 60" DEPTH IN UNIMPROVED AREAS OR AS REQUIRED TO CLEAR OTHER UTILITIES.
8. CONTRACTOR SHALL COORDINATE ALL TAPS TO EXISTING WATER MAINS WITH CITY INSPECTOR TO MINIMIZE DISRUPTION TO WATER SERVICE.
9. CONTRACTOR SHALL COORDINATE EXCAVATION AROUND POWER POLES AND GUY-WIRES WITH POWER COMPANY.
10. THE BACKFLOW DEVICE IN THE BUILDING IS TO BE A DOUBLE CHECK DETECTOR ASSEMBLY WITH AN EXTERNALLY MOUNTED TOUCHPAD. METER TO BE IN ACCORDANCE WITH THE TOWN OF ADDISON WATER SYSTEM REQUIREMENTS

EXISTING UTILITIES NOTES:

1. THE LOCATION OF ALL UNDERGROUND FACILITIES AS INDICATED ON THE PLANS ARE TAKEN FROM PUBLIC RECORDS. JDJR ENGINEERS & CONSULTANTS ASSUMES NO RESPONSIBILITY FOR THE ACCURACY OF SUCH RECORDS AND DOES NOT GUARANTEE THAT ALL UNDERGROUND UTILITIES ARE SHOWN OR ARE LOCATED PRECISELY AS INDICATED.
2. IT IS THE RESPONSIBILITY OF THE CONTRACTOR TO MAKE ARRANGEMENTS WITH THE OWNERS OF SUCH UNDERGROUND FACILITIES PRIOR TO WORKING IN THE AREA TO CONFIRM THEIR EXACT LOCATION AND TO DETERMINE WHETHER ANY ADDITIONAL FACILITIES OTHER THAN THOSE SHOWN ON THE PLANS MAY BE PRESENT.
3. THE CONTRACTOR SHALL PRESERVE AND PROTECT ALL UNDERGROUND FACILITIES FOUND.
4. NOTIFY JDJR ENGINEERS & CONSULTANTS IF ANY UNDERGROUND UTILITIES ARE NOT IN THE LOCATIONS INDICATED ON THESE PLANS (HORIZONTALLY AND VERTICALLY) OR CONFLICT WITH ANY PROPOSED IMPROVEMENTS ASSOCIATED WITH THESE PLANS.
5. ANY EXISTING UTILITY APPURTENANCES (MH, VALVES, METER BOXES, ETC.) TO BE ADJUSTED TO MATCH THE PROPOSED FINISHED GRADES AS INDICATED ON THESE PLANS. NOTIFY JDJR ENGINEERS & CONSULTANTS, INC. IF THERE IS A PROBLEM MAKING SAID ADJUSTMENTS.

REVISIONS:	
1/07/09	PER CITY REVIEW
1/29/09	PER CITY REVIEW
8/11/09	REV FOR CLIENT
9/11/09	PER CITY REVIEW
5/18/10	AS BUILT



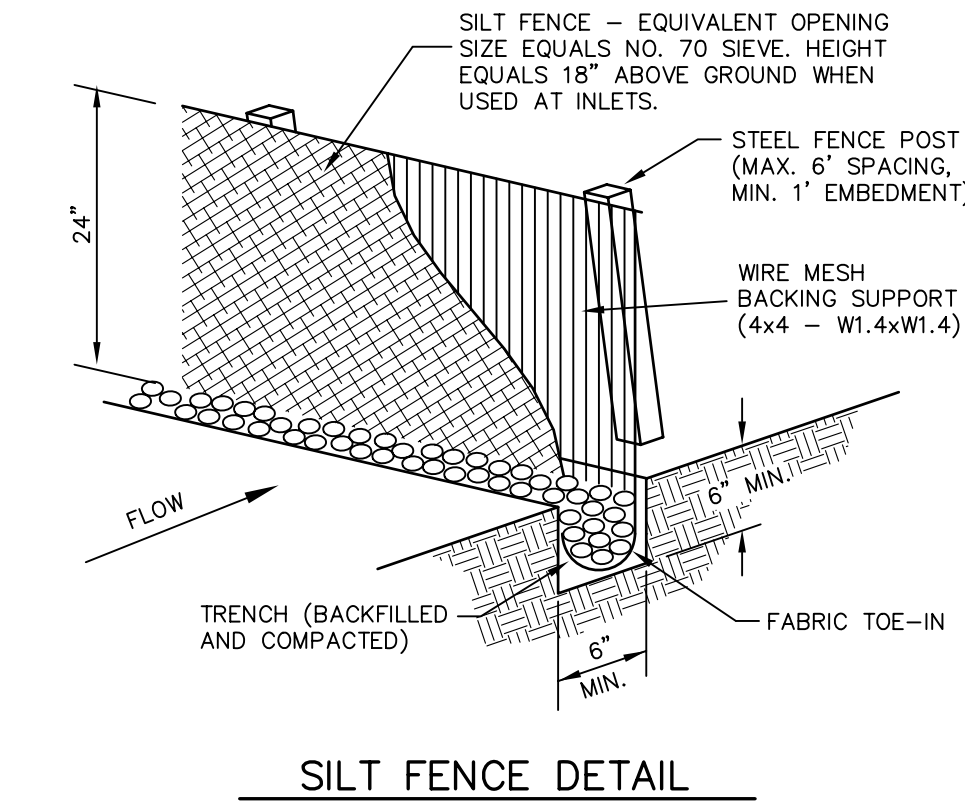
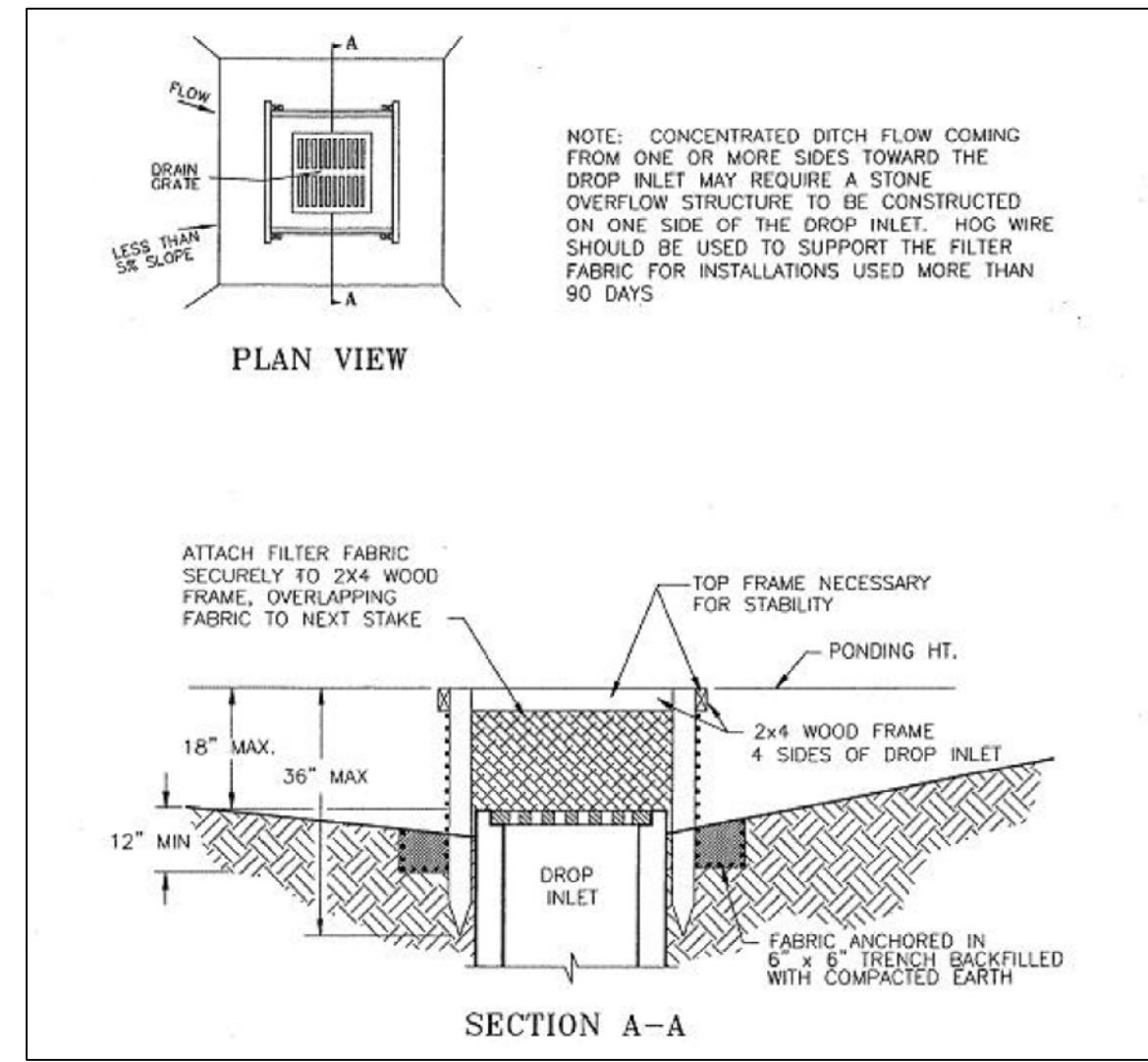
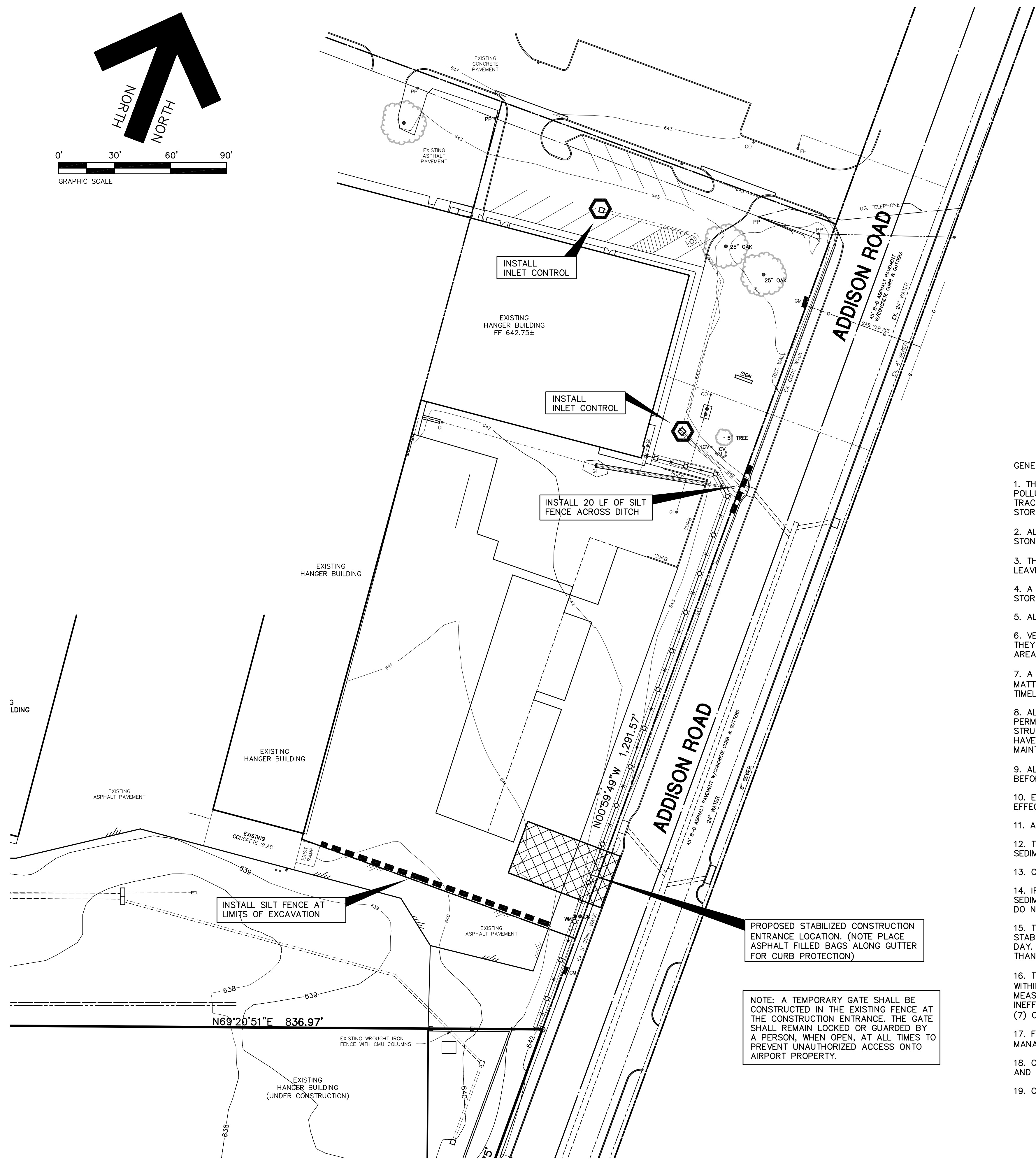
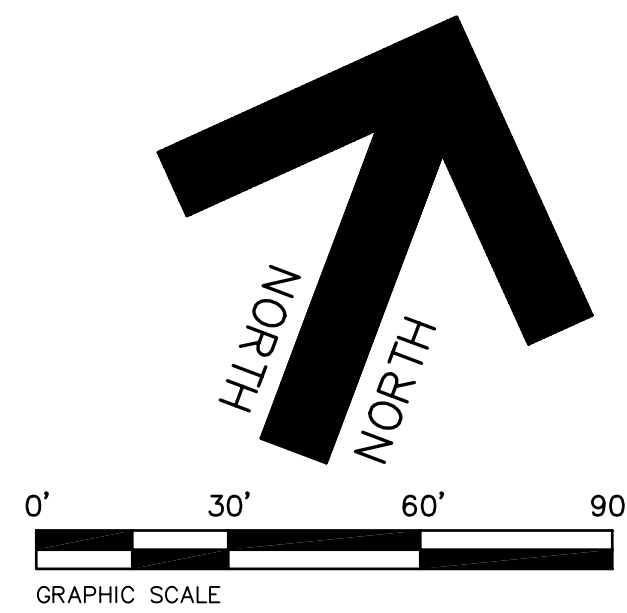
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 JDJR ENGINEERS & CONSULTANTS, INC. DOES NOT GUARANTEE THE ACCURACY OR COMPLETENESS OF THESE RECORDS.

BENCH MARK: SQUARE CUT FOUND C.L. OF CURB INLET, WEST SIDE OF ADDISON ROAD, 75' SOUTH OF THE C.L. OF AIRPORT PRKY.
 ELEVATION 642.51
 SITE TBM: X-CUT SET IN CONCRETE WALK ±38' NORTH OF THE NORTHEAST CORNER OF THIS TRACT.
 ELEVATION 642.86

SHEET TITLE:
FIRE SPRINKLER SERVICE PLAN
 PAVING AND DRAINAGE IMPROVEMENTS
 15841 ADDISON ROAD
 AIRPORT
 ADDISON, TEXAS

PREPARED BY:
JDJR ENGINEERS AND CONSULTANTS
 TSBP REGISTRATION NUMBER F-8527
 ENGINEERS • SURVEYORS • LAND PLANNERS
 2500 Texas Drive Suite 100 Irving, Texas 75062
 Tel 972-252-5357 Fax 972-252-8958

DATE: DEC. 2, 2008 DRAWN BY: SAS SHEET NO.
 SCALE: 1" = 20' CHECKED BY: JDJR **C6** OF **7**



- SILT FENCE NOTES:**
1. STEEL POSTS WHICH SUPPORT THE SILT FENCE SHALL BE INSTALLED ON A SLIGHT ANGLE TOWARD THE ANTICIPATED RUNOFF SOURCE. POST MUST BE EMBEDDED A MINIMUM OF ONE FOOT.
 2. THE TOE OF THE SILT FENCE SHALL BE TRENCHED IN WITH A SPADE OR MECHANICAL TRENCHER, SO THAT THE DOWNSLOPE FACE OF THE TRENCH IS FLAT AND PERPENDICULAR TO THE LINE OF FLOW. WHERE FENCE CANNOT BE TRENCHED IN (AS IN PAVEMENT), WEIGHT FABRIC FLAP WITH WASHED GRAVEL ON UPHILL SIDE TO PREVENT FLOW UNDER FENCE.
 3. THE TRENCH MUST BE A MIN. OF 6" DEEP AND 6" WIDE TO ALLOW FOR THE SILT FENCE FABRIC TO BE LAID IN THE GROUND AND BACKFILLED WITH COMPACTED MATERIAL.
 4. SILT FENCE SHOULD BE SECURELY FASTENED TO EACH STEEL SUPPORT POST OR WOVEN WIRE, WHICH IS IN TURN ATTACHED TO THE STEEL FENCE POST. THERE SHALL BE A 6" OVERLAP, SECURELY FASTENED WHERE ENDS OF FABRIC MEET.
 5. INSPECTION SHALL BE MADE WEEKLY OR AFTER EACH RAINFALL. REPAIR OR REPLACEMENT SHALL BE MADE PROMPTLY AS NEEDED.
 6. SILT FENCE SHALL BE REMOVED WHEN THE SITE IS COMPLETELY STABILIZED SO AS NOT TO BLOCK OR EMPEDE STORM FLOW OR DRAINAGE.
 7. ACCUMULATED SILT SHALL BE REMOVED WHEN IT REACHES A DEPTH OF 6". THE SILT SHALL BE DISPOSED OF IN AN APPROVED SITE AND IN SUCH A MANNER AS TO NOT CONTRIBUTE TO ADDITIONAL SILTATION.

- NOTES:**
1. SILT FENCE SHOULD BE CONSTRUCTED SURROUNDING INLET AS INLETS ARE CONSTRUCTED AND MAINTAINED UNTIL AREAS DRAINING TO THE INLET ARE ESTABLISHED WITH VEGETATION OR ARE PAVED.
 2. FOR INLETS IN PAVED AREAS, 10" DIAMETER STRAW WATTLES SHOULD BE INSTALLED IN A CONTINUOUS RING AROUND THE INLET AFTER PAVEMENT IS POURED. STRAW WATTLES AS DISTRIBUTED BY: HANEY GEO COMPONENTS, P.O. BOX 535547, GRAND PRAIRIE, TX. PHONE: 469-523-0444, OR APPROVED EQUAL.

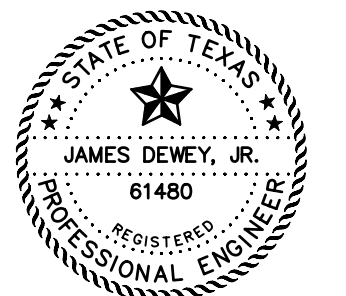
INLET CONTROL DETAIL

- GENERAL NOTES:**
1. THE GENERAL CONTRACTOR AND OWNER RESPONSIBLE FOR PREVENTING SEDIMENT AND OTHER POLLUTANTS FROM LEAVING THE SITE. CARE SHALL BE EXERCISED TO PREVENT THE FLOW OR OFF-SITE TRACKING OF SEDIMENT AND OTHER POLLUTANTS TO ADJACENT PAVED DRIVEWAYS, ROADWAYS, INLETS AND STORM DRAIN SYSTEM.
 2. ALL LOCATIONS USED AS AN EXIT MUST HAVE ROCK STABILIZATION 50' MINIMUM LENGTH, 3" DIAMETER STONE OVER GEOTEXTILE FABRIC.
 3. THE STABILIZED CONSTRUCTION ENTRY/EXIT SHALL BE USED AS A WHEEL WASH AREA FOR ALL TRUCKS LEAVING THE SITE.
 4. A BERM OR OTHER SPILL PROTECTION MEASURE SHALL BE CONSTRUCTED FOR ANY TEMPORARY FUEL STORAGE TANKS ON SITE DURING CONSTRUCTION.
 5. ALL TRASH SHALL BE CONTAINED IN AN ENCLOSURE UNTIL PROPER DISPOSAL AT OFF-SITE FACILITIES.
 6. VEHICLE PARKING AREAS, STAGING AREAS, STOCKPILES, SPOILS, ETC. SHALL BE LOCATED SUCH THAT THEY WILL NOT ADVERSELY AFFECT STORM WATER QUALITY. OTHERWISE, COVERING OR ENCIROLING THE AREAS WITH PROTECTIVE MEASURES SHALL BE NECESSARY.
 7. A DENSITY OF TEMPORARY OR PERMANENT GROUND COVER (I.E., VEGETATION, EROSION CONTROL, MATTING, ETC.) SUFFICIENT TO PREVENT EROSION SHALL BE ESTABLISHED ON ALL SWALES AND SLOPES IN A TIMELY MANNER IN ORDER TO PREVENT EROSION PROBLEMS FROM DEVELOPING IN THESE AREAS.
 8. ALL SURFACE AREAS DISTURBED WITHIN OR ADJACENT TO THE CONSTRUCTION LIMITS MUST BE PERMANENTLY STABILIZED. STABILIZATION IS OBTAINED WHEN THE SITE IS COVERED WITH IMPERVIOUS STRUCTURES, PAVING OR A UNIFORM PERENNIAL VEGETATIVE COVER. THE PERENNIAL VEGETATION MUST HAVE A COVERAGE DENSITY OF AT LEAST 70 PERCENT. STABILIZATION IS REQUIRED BEFORE TERMINATING MAINTENANCE AND REMOVAL OF EROSION CONTROL MEASURES.
 9. ALL PERIMETER EROSION CONTROL MEASURES AND A ROCK STABILIZED ENTRY/EXIT MUST BE IN PLACE BEFORE STARTING SOIL DISTURBING ACTIVITIES.
 10. EROSION CONTROL MEASURES THAT PROVE TO BE INEFFECTIVE SHALL BE REPLACED WITH MORE EFFECTIVE MEASURES OR ADDITIONAL MEASURES.
 11. A MAINTENANCE PROGRAM FOR ALL PROPOSED EROSION CONTROL MEASURES SHALL BE ESTABLISHED.
 12. TO PREVENT ENTRY OF SEDIMENT INTO PROPOSED STORM SEWERS DURING CONSTRUCTION, INSTALL PIPE SEDIMENT FILTER OR SEDIMENT BARRIER AT THE END OF EACH WORK DAY.
 13. CONTRACTOR TO CONSTRUCT A PIT OR WASH BASIN ON-SITE FOR WASH-OUT OF CONCRETE TRUCKS.
 14. IF PUMPS ARE USED TO REMOVE WATER FROM PONDED AREAS, FILTER THE DISCHARGE TO REMOVE SEDIMENT AND OTHER POLLUTANTS BEFORE THE WATER LEAVES THE SITE OR ENTERS STORM DRAIN SYSTEM. DO NOT BYPASS SILT BARRIERS OR INLET SEDIMENT FILTERS WITH THE DISCHARGE.
 15. TO PREVENT DAMAGE TO VEGETATION IN DOWNSTREAM WATER COURSES, LIMIT ANY PROPOSED LIME STABILIZATION OPERATIONS TO THAT WHICH CAN BE MIXED AND COMPACTED BY THE END OF EACH WORK DAY. A SILT FENCE IS NOT EFFECTIVE IN FILTERING LIME SINCE THE GRAIN SIZE IS SIGNIFICANTLY SMALLER THAN THE OPENING IN THE FABRIC.
 16. THE CONTRACTOR(S) SHALL INSPECT EROSION CONTROL MEASURES AT LEAST ONCE EACH WEEK AND WITHIN 24 HOURS AFTER A STORM EVENT OF 1/2 INCH OR GREATER. REPAIR OR REPLACE DAMAGED MEASURES AS NECESSARY TO RETAIN SEDIMENT ON SITE. EROSION CONTROL MEASURES THAT PROVE TO BE INEFFECTIVE SHALL BE REPLACED WITH MORE EFFECTIVE MEASURES OR ADDITIONAL MEASURES WITHIN SEVEN (7) CALENDAR DAYS.
 17. FOR ALTERNATIVE STABILIZATION AND EROSION CONTROL MEASURES, REFER TO THE CONSTRUCTION BEST MANAGEMENT PRACTICES (BMP) MANUAL PUBLISHED BY NORTH CENTRAL TEXAS COUNCIL OF GOVERNMENTS.
 18. CONTRACTOR TO CHECK AREAS ADJACENT TO PROPERTY DAILY FOR CONSTRUCTION WASTE MATERIALS AND DEBRIS THAT HAVE BLOWN OR WASHED OFF-SITE AND REMOVE IMMEDIATELY.
 19. CONTAIN ALL RUNOFF FROM MATERIALS USED IN THE SUBGRADE STABILIZATION PROCESS.

LEGEND

- 545 — EXISTING CONTOUR LINE
- PROPOSED SILT FENCE
- ⬡ INLET CONTROL SILT FENCE CONSTRUCTED CONTINUOUSLY AROUND INLET.
- ▨ PROPOSED STABILIZED CONSTRUCTION ENTRANCE

REVISIONS:	
1/07/09	PER CITY REVIEW
8/11/09	REV FOR CLIENT
9/11/09	PER CITY REVIEW
9/23/09	PER CITY REVIEW
5/18/10	AS BUILT



SHEET TITLE:
EROSION CONTROL PLAN
 PAVING AND DRAINAGE IMPROVEMENTS
 15841 ADDISON ROAD
 ADDISON AIRPORT
 ADDISON, TEXAS

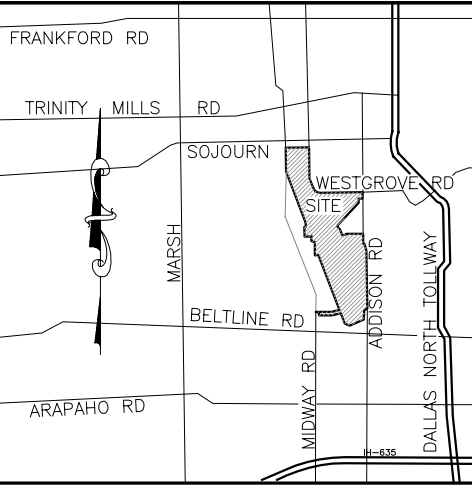
PREPARED BY:
JDJR ENGINEERS AND CONSULTANTS
 TSBPPE REGISTRATION NUMBER F-8527

ENGINEERS • SURVEYORS • LAND PLANNERS
 2500 Texas Drive Suite 100 Irving, Texas 75062
 Tel 972-252-5357 Fax 972-252-8958

DATE: DEC. 2, 2008 DRAWN BY: SAS SHEET NO.
 SCALE: 1" = 30' CHECKED BY: JDJR **C7** OF **7**

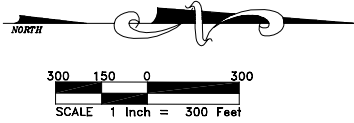
"AS-BUILT"
 THIS AS-BUILT DRAWING IS BASED ON INFORMATION OBTAINED FROM BUILDING CONTRACTORS DURING CONSTRUCTION AS PROVIDED TO JDJR ENGINEERS & CONSULTANTS, INC.
 JDJR ENGINEERS & CONSULTANTS, INC. DOES NOT GUARANTEE THE ACCURACY OR COMPLETENESS OF THESE RECORDS.

H:\JD\Projects\2008\02410242-Addison Airport\02410242-2-08 CIVIL ADDITIONAL PAVING rev.sldraw.dwg, 5/18/2010 8:52:34 AM, JDJR ENGINEERS & CONSULTANTS, INC. - SLS

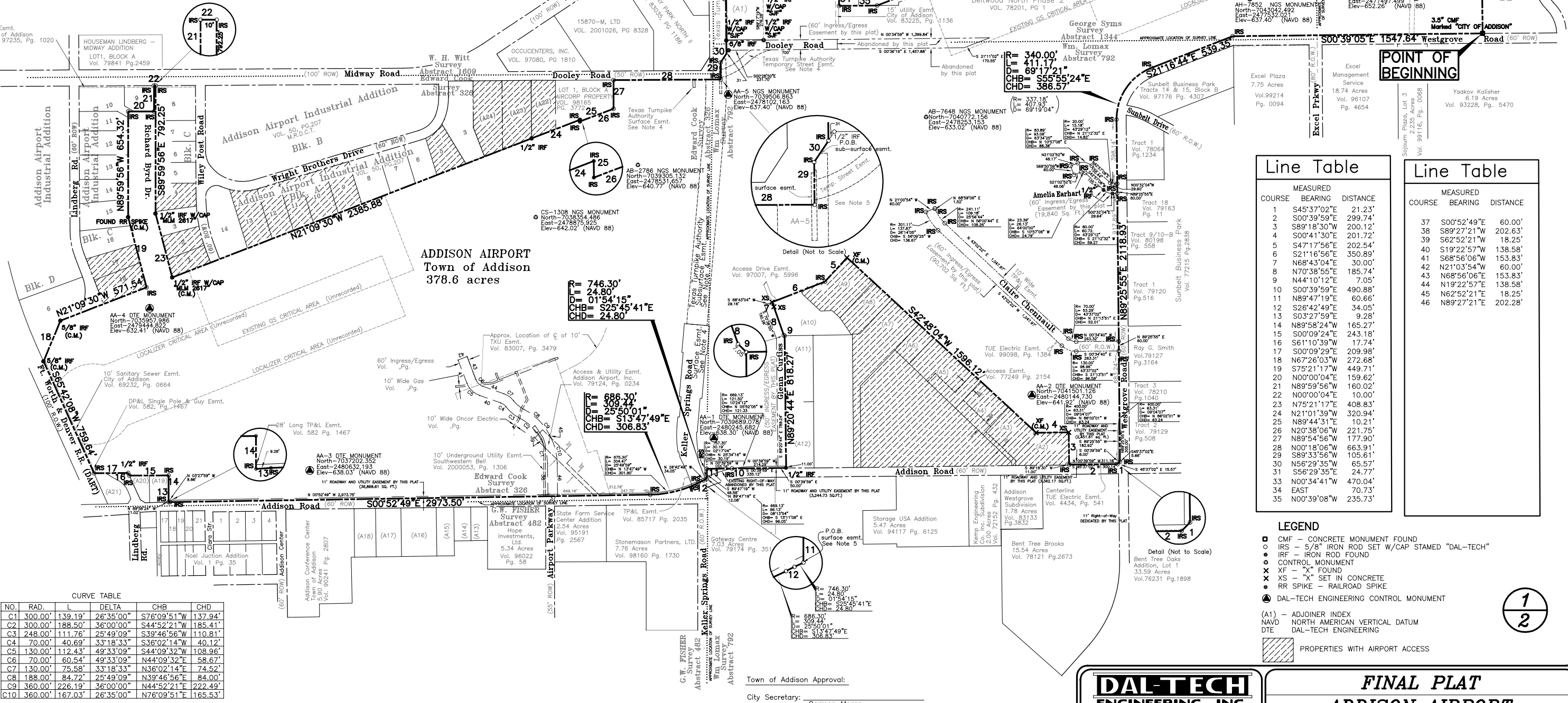


GENERAL NOTES:

- Bearings are referenced to the Texas State Plane coordinate system (NAD83).
- All objects shown hereon as open circles are 5/8-inch iron rods set with a plastic cap that is stamped "DAL-TECH" unless otherwise noted.
- Hatched areas currently have access to the airport property.
- All references to surface and sub-surface easements to the Texas Turnpike Authority are found in Interlocal agreement by and between The Town of Addison and Texas Turnpike Authority (Dallas Tollway and Addison Airport Toll Tunnel) dated March 10, 1997, as recorded in the D.R.D.C.T. Volume 97084, Page 02628.
- Texas Power & Light Easement recorded in Volume 5021, Page 357 of the D.R.D.C.T., may affect subject tract, but was not locatable by description in the field.



LOCATION MAP



POINT OF BEGINNING

Line Table

COURSE	BEARING	DISTANCE
1	S45°37'02"E	21.23'
2	S00°39'59"E	299.74'
3	S89°18'30"W	200.12'
4	S00°41'30"E	201.72'
5	S47°17'56"E	202.54'
6	S21°16'44"E	350.89'
7	N88°43'04"E	30.00'
8	N70°38'55"E	185.74'
9	N44°10'12"E	7.05'
10	S00°39'59"E	490.88'
11	N89°47'19"E	60.66'
12	S26°42'48"E	34.05'
13	S03°27'59"E	9.28'
14	N89°58'24"W	165.27'
15	S00°09'24"E	243.18'
16	S61°10'39"W	17.74'
17	S00°09'29"W	209.98'
18	N67°26'03"W	272.68'
19	S75°2'17"W	449.71'
20	N00°00'04"E	159.62'
21	N89°59'56"W	160.02'
22	N00°00'04"E	10.00'
23	N75°21'17"E	408.83'
24	N21°01'39"W	320.94'
25	N89°44'31"W	10.21'
26	N20°38'06"W	221.75'
27	N89°54'56"W	177.90'
28	N00°18'06"W	663.91'
29	S89°33'56"W	105.61'
30	N56°29'35"W	65.57'
31	S56°29'35"E	24.77'
33	N00°34'41"W	470.04'
34	EAST	70.73'
35	N00°39'08"W	235.73'

Line Table

COURSE	BEARING	DISTANCE
37	S00°52'49"E	60.00'
38	S89°27'21"W	202.63'
39	S62°52'21"W	18.25'
40	S19°22'57"W	138.58'
41	S68°56'06"W	153.83'
42	N21°03'54"W	60.00'
43	N68°56'06"E	153.83'
44	N19°22'57"E	138.58'
45	N62°52'21"E	18.25'
46	N89°27'21"E	202.28'

- LEGEND**
- CMF - CONCRETE MONUMENT FOUND
 - IRS - 5/8" IRON ROD SET W/CAP STAMPED "DAL-TECH"
 - IRF - IRON ROD FOUND
 - CONTROL MONUMENT
 - ✕ XF - "X" FOUND
 - ✕ XS - "X" SET IN CONCRETE
 - RR SPIKE - RAILROAD SPIKE
 - DAL-TECH ENGINEERING CONTROL MONUMENT
 - (A1) - ADJOINER INDEX
 - NAVD - NORTH AMERICAN VERTICAL DATUM
 - DTE - DAL-TECH ENGINEERING

CURVE TABLE

NO.	RAD.	L	DELTA	CHB	CHD
C1	300.00'	139.19'	26°35'00"	S76°09'51"W	137.94'
C2	300.00'	188.50'	36°00'00"	S44°52'21"W	185.41'
C3	248.00'	111.76'	25°49'09"	S39°46'56"W	110.81'
C4	70.00'	40.69'	33°18'33"	S36°02'14"W	40.12'
C5	130.00'	112.43'	49°33'09"	S44°09'32"W	108.96'
C6	70.00'	60.54'	49°33'09"	N44°09'32"E	58.67'
C7	130.00'	75.58'	33°18'33"	N36°02'14"E	74.52'
C8	188.00'	84.72'	25°49'09"	N39°46'56"E	84.00'
C9	360.00'	226.19'	36°00'00"	N44°52'21"E	222.49'
C10	360.00'	167.03'	26°35'00"	N76°09'51"E	165.53'

DAL-TECH ENGINEERING, INC.
CONSULTING CIVIL ENGINEERS / SURVEYORS
CONSTRUCTION MANAGERS
17311 DALLAS PARKWAY SUITE 200
DALLAS, TEXAS 75248
(972) 250-2127 (972) 250-4774

FINAL PLAT ADDISON AIRPORT TOWN OF ADDISON, TEXAS

DRAWN	JOB NO.	CONTRACT NO.	SCALE	DATE
DTE	0216	-	1"=300'	DEC. 2004

Town of Addison Approval:
City Secretary: Carmen Moran
Scott Wheeler: Mayor (Town of Addison)

1
2

Adjoiner Index

Table with 4 columns: Index, Owner, Vol., Page. Lists various land parcels and their owners, including Texas Turnpike Authority, Blakely Airport Addition, Van-Hoff Subdivision, etc.

Parent Tracts

Owner: City of Addison
Address: 15333 Addison Road City of Addison, Texas
Legal Description: Being all the certain tract of land 373.332 acres, more or less, out of the Edward Cook Survey Abstract 326, the William Lomar Survey Abstract 792, the George Syms Survey Abstract 1344, the William Rowe Survey Abstract 1257, Lot 1, Block A of Coil Addition, Vol. 2002003, Pg. 103, Dallas County, Texas, and part of Lot 1 and Lot 2 of Block "A" of The Carroll Estates Addition, Volume 10 Page 473, Dallas County, Texas

Control Monument

(NAD 83, TEXAS NORTH CENTRAL - 4202)

Table with 4 columns: North, East, Elevation, Desc. Lists monument coordinates and descriptions for various points.

PROPERTY DESCRIPTION

Being a tract of land out of the E. Cook Survey, Abstract No. 326, the William Lomax Survey, Abstract No. 792, the George Syms Survey, Abstract 1344, and the William Rowe Survey, Abstract 1257, and being all of a tract of land described in deed to the City of Addison recorded in Volume 77010, Page 1391 of the Deed Records of Dallas County, Texas (D.R.D.C.T.), all of a tract of land described in judgment to the City of Addison recorded in Volume 91079, Page 2687, D.R.D.C.T., LOT 7, Block B of Beltwood North Addition, Phase II, an Addition to the City of Dallas, Texas, according to the map thereof recorded in Volume 78201, Page 0001, D.R.D.C.T., as described in deed to the City of Addison recorded in Volume 82022, Page 1211, D.R.D.C.T., a tract of land described in deed to the Town of Addison recorded in Volume 98015, Page 2143, D.R.D.C.T., a tract of land described in deed to the Town of Addison recorded in Volume 97217, Page 1350, D.R.D.C.T., Lot 1, Block 1, of Kincaid Addition, an Addition to the City of Addison, Dallas County, Texas, according to the Plat thereof recorded in Volume 84087, Page 48, D.R.D.C.T., a tract of land described in deed to the Town of Addison recorded in Volume 99249, Page 5436, D.R.D.C.T., a tract of land described in deed to the Town of Addison recorded in Volume 99078, Page 1254, D.R.D.C.T., a tract of land described in Deed to the City of Addison recorded in Volume 5143, Page 162, D.R.D.C.T., and a tract of land described in Deed to the City of Addison recorded in Volume 81150, Page 1985, D.R.D.C.T., said tracts to be known as the Addison Airport tract and being more particularly described as follows:

Beginning at a concrete monument found with a 3 1/2 inch disk stamped "City of Addison" lying in the west right-of-way line of Westgrove Road (a 60 foot wide right-of-way) said point being the most northerly northeast corner of said Addison Airport tract and the southeast corner of Lot 1, Block A, Academy Addition, an addition to the City of Carrollton, Dallas County, Texas according to the Plat thereof recorded in Volume 79186, Page 1, D.R.D.C.T.;

THENCE South 00 degrees 39 minutes 05 seconds East, along the west right-of-way line of said Westgrove Road, a distance of 1547.64 feet to a 5/8 inch iron rod set with cap stamped "DAL-TECH";

THENCE South 21 degrees 16 minutes 44 seconds East, continuing along said west right-of-way line of Westgrove Road, a distance of 539.35 feet to a 5/8 inch iron rod set with cap stamped "DAL-TECH";

THENCE along a curve to the left of said Westgrove Road having a radius of 340.00 feet, a central angle of 69 degrees 17 minutes 21 seconds and an arc length of 411.17 feet, being subtended by a chord of South 55 degrees 55 minutes 24 seconds East for a distance of 386.57 feet to a 5/8 inch iron rod set with cap stamped "DAL-TECH";

THENCE North 89 degrees 25 minutes 55 seconds East, along the southerly right-of-way line of Westgrove Road for a distance of 2,118.93 feet to a 5/8 inch iron rod set with cap stamped "DAL-TECH" at the corner clip of said southerly right-of-way line of Westgrove Road and the west right-of-way line of Addison Road (a 60 foot wide right-of-way);

THENCE South 45 degrees 37 minutes 02 seconds East, along said corner clip, a distance of 21.23 feet to a 5/8 inch iron rod set with cap stamped "DAL-TECH";

THENCE South 00 degrees 39 minutes 59 seconds East, along said west right-of-way line of Addison Road, a distance of 299.74 feet to a 5/8 inch iron rod set with cap stamped "DAL-TECH";

THENCE South 89 degrees 18 minutes 30 seconds West, departing said west right-of-way line of Addison Road, a distance of 200.12 feet to a chiseled "X" set in concrete;

THENCE South 00 degrees 41 minutes 30 seconds East, a distance of 201.72 feet to a found "X";

THENCE South 42 degrees 48 minutes 04 seconds West, a distance of 1,596.12 feet to a found "X";

THENCE South 47 degrees 17 minutes 56 seconds East, a distance of 202.54 feet to a 5/8 inch iron rod set with cap stamped "DAL-TECH";

THENCE South 21 degrees 16 minutes 56 seconds East, a distance of 350.89 feet to a chiseled "X" set in concrete;

THENCE North 68 degrees 43 minutes 04 seconds East, a distance of 30.00 feet to a chiseled cross set in concrete;

THENCE North 70 degrees 38 minutes 55 seconds East, a distance of 185.74 feet to a 5/8 inch iron rod set with cap stamped "DAL-TECH";

THENCE North 44 degrees 10 minutes 12 seconds East, a distance of 7.05 feet to a 5/8 inch iron rod set with cap stamped "DAL-TECH", said point is the common corner between the aforesaid Addison Airport tract and the southwest corner of a tract of land described in deed to O.J. Broughton and E.E. Ericson recorded in Volume 4350, Page 491 of D.R.D.C.T.;

THENCE North 89 degrees 20 minutes 44 seconds East, along the common line of said Addison Airport tract and the south line of said O.J. Broughton tract for a distance of 818.27 feet to a 5/8 inch iron rod set with cap stamped "DAL-TECH" in the west right-of-way line of aforesaid Addison Road;

THENCE South 00 degrees 39 minutes 59 seconds East, a distance of 490.88 feet to a 5/8 inch iron rod set with cap stamped "DAL-TECH" in the apparent common survey line between the aforesaid William Lomax Survey, Abstract No. 792 and the aforesaid E. Cook Survey, Abstract No. 326;

THENCE North 89 degrees 47 minutes 19 seconds East, a distance of 60.66 feet to a 1/2 inch iron rod set with cap stamped "DAL-TECH" in the aforesaid west right-of-way line of Addison Road;

THENCE along a curve to the left in the said west right-of-way line of Addison Road, having a radius of 746.30 feet, a central angle of 01 degree 54 minutes 15 seconds and an arc length of 24.80 feet, being subtended by a chord of South 25 degrees 45 minutes 41 seconds East for a distance of 24.80 feet to a 5/8 inch iron rod set with cap stamped "DAL-TECH";

THENCE South 26 degrees 42 minutes 49 seconds East, along said west right-of-way line of Addison Road, a distance of 34.05 feet to a 5/8 inch iron rod set with cap stamped "DAL-TECH";

THENCE along a curve to the right in the said west right-of-way line of Addison Road having a radius of 686.30 feet, a central angle of 25 degrees 50 minutes 01 second and an arc length of 309.44 feet, being subtended by a chord of South 13 degrees 47 minutes 49 seconds East for a distance of 306.83 feet to a 5/8 inch iron rod set with cap stamped "DAL-TECH";

THENCE South 00 degrees 52 minutes 49 seconds East, along said west right-of-way line of Addison Road, a distance of 2,973.50 feet to a 5/8 inch iron rod set with cap stamped "DAL-TECH";

THENCE South 03 degrees 27 minutes 59 seconds East, along said west right-of-way line of Addison Road, a distance of 9.28 feet to a 5/8 inch iron rod set with cap stamped "DAL-TECH";

THENCE North 89 degrees 58 minutes 24 seconds West, departing said west right-of-way line of Addison Road, a distance of 165.27 feet to a 5/8 inch iron rod set with cap stamped "DAL-TECH";

THENCE South 00 degrees 09 minutes 24 seconds East, a distance of 243.18 feet to a 5/8 inch iron rod set with cap stamped "DAL-TECH";

THENCE South 61 degrees 10 minutes 39 seconds West, a distance of 17.74 feet to a 1/2 inch iron rod found;

THENCE South 00 degrees 09 minutes 29 seconds East, a distance of 209.98 feet to a 5/8 inch iron rod set with cap stamped "DAL-TECH" in the north right-of-way line of the Fort Worth and Denver Railroad spur;

THENCE South 65 degrees 42 minutes 08 seconds West, along said north right-of-way line of said Fort Worth and Denver Railroad spur, a distance of 759.64 feet to a 5/8 inch iron rod found at the most easterly corner of the Addison Airport Industrial District, an Addition to the City of Addison according to the plat thereof recorded in Volume 50, Page 207, D.R.D.C.T.;

THENCE North 67 degrees 26 minutes 03 seconds West, along the said easterly line of Addison Airport Industrial District addition, a distance of 272.68 feet to a 5/8 inch iron rod found;

THENCE North 21 degrees 09 minutes 30 seconds West, along the said easterly line of Addison Airport Industrial District addition, a distance of 571.54 feet to a 5/8 inch iron rod set with cap stamped "DAL-TECH";

THENCE South 75 degrees 21 minutes 17 seconds West, a distance of 449.71 feet to a found railroad spike;

THENCE North 89 degrees 59 minutes 56 seconds West, a distance of 654.32 feet to a 5/8 inch iron rod set with cap stamped "DAL-TECH";

THENCE North 00 degrees 00 minutes 04 seconds East, a distance of 159.62 feet to a 5/8 inch iron rod set with cap stamped "DAL-TECH";

THENCE North 89 degrees 59 minutes 56 seconds West, a distance of 160.02 feet to a 5/8 inch iron rod set with cap stamped "DAL-TECH" in the east right-of-way line of Midway Road (a 100 foot wide right-of-way);

THENCE North 00 degrees 00 minutes 04 seconds East, along said east right-of-way line of Midway Road, a distance of 10.00 feet to a 5/8 inch iron rod set with cap stamped "DAL-TECH";

THENCE South 89 degrees 59 minutes 56 seconds East, a distance of 792.25 feet to a 1/2 inch iron rod found with cap stamped "MLM #2617";

THENCE North 75 degrees 21 minutes 17 seconds East, a distance of 408.83 feet to a 1/2 inch iron rod found with cap stamped "MLM #2617";

THENCE North 21 degrees 09 minutes 30 seconds West, along the aforesaid easterly line of said Addison Airport Industrial District addition, a distance of 2,385.88 feet record to a 1/2 inch iron rod found;

THENCE North 21 degrees 01 minute 39 seconds West, a distance of 320.94 feet to a 5/8 inch iron rod set with cap stamped "DAL-TECH";

THENCE North 89 degrees 44 minutes 31 seconds East, a distance of 10.21 feet to a 5/8 inch iron rod set with cap stamped "DAL-TECH";

THENCE North 20 degrees 38 minutes 06 seconds West, a distance of 221.75 feet to a 5/8 inch iron rod set with cap stamped "DAL-TECH";

THENCE North 89 degrees 54 minutes 56 seconds West, a distance of 177.90 feet to a 5/8 inch iron rod set with cap stamped "DAL-TECH" in the east right-of-way line of Dooley Road (a 50 foot wide right-of-way)

THENCE North 00 degrees 18 minutes 06 seconds West, along the said east right-of-way line of Dooley Road, a distance of 663.91 feet to a 5/8 inch iron rod set with cap stamped "DAL-TECH" for the apparent intersection of said east right-of-way line of Dooley Road and the north right-of-way line of the old Keller Springs Road (a 50 foot wide right-of-way);

THENCE South 89 degrees 33 minutes 56 seconds West, with the said north line of Keller Springs Road, a distance of 105.61 feet to a 5/8 inch iron rod set with cap stamped "DAL-TECH";

THENCE North 56 degrees 29 minutes 35 seconds West, a distance of 65.57 feet to a 5/8 inch iron rod set with cap stamped "DAL-TECH" in the aforesaid east right-of-way line of Dooley Road (a 60 foot wide right-of-way);

THENCE South 89 degrees 19 minutes 27 seconds West, a distance of 361.02 feet to a 5/8 inch iron rod found;

THENCE North 00 degrees 28 minutes 50 seconds West, a distance of 231.53 feet to a 1/2 inch iron rod found;

THENCE North 89 degrees 17 minutes 19 seconds East, a distance of 1.24 feet to a 1/2 inch iron rod found with cap stamped SJ&F

THENCE North 00 degrees 34 minutes 41 seconds West, a distance of 470.04 feet to a 5/8 inch iron rod set with cap stamped "DAL-TECH" in the east line of Beltwood North Phase 2, an addition to the City of Dallas, Texas, according to the Plat thereof recorded in Volume 78201, Page 1, D.R.D.C.T.;

THENCE East, a distance of 70.73 feet along said Beltwood North Phase 2 addition to a 5/8 inch iron rod set with cap stamped "DAL-TECH";

THENCE North 00 degrees 39 minutes 08 seconds West, along said Beltwood North Phase 2 addition, a distance of 235.73 feet to a 5/8 inch iron rod set with cap stamped "DAL-TECH" for the most southerly corner of Lot 7 of said Beltwood North Phase 2 addition;

THENCE North 21 degrees 08 minutes 57 seconds West, passing the north line of said Beltwood North Phase 2 addition at a called distance of 576.04 feet and continuing on for a total distance of 2,125.91 feet to a 5/8 inch iron rod set with cap stamped "DAL-TECH" for the northeast corner of Beltwood North Addition, according to the plat thereof recorded in Volume 78201, Page 1, D.R.D.C.T., same being the southeast corner of Lot 1R-1, Block A, Coil Addition, according to the plat thereof recorded in Volume 2002003, Page 103, D.R.D.C.T.;

THENCE South 89 degrees 35 minutes 51 seconds West, along the common line between the north line of said Beltwood Addition and the south line of said Coil Addition, a distance of 214.48 feet to a 1/2 inch iron rod found in the east right-of-way line of Midway Road (a 100 foot wide right-of-way);

THENCE North 00 degrees 21 minutes 08 seconds West, along said east right-of-way line of Midway Road, passing a 1/2 inch iron rod found for the North corner of said Lot 1R-1, Block A, at a distance of 564.89 feet and continuing for a total distance of 1,756.20 feet to a 1/2 inch iron rod found with cap stamped "SURVCON";

THENCE North 89 degrees 24 minutes 55 seconds East, a distance of 1,165.10 feet to the POINT OF BEGINNING and CONTAINING 16,492,399 square foot or; 378.6 acres of land, more or less.

I, Alan Moore, a Registered Professional Land Surveyor in the State of Texas, do hereby certify that I prepared this Plat from an actual survey of the land under my supervision in accordance with the "Minimum Standards of Practice" approved by the Texas Board of Professional Land Surveyors.

Alan Moore, RPLS No. 5537



Town of Addison Approval: Carmen Moran
City Secretary:
Scott Wheeler Mayor (Town of Addison)

DAL-TECH ENGINEERING, INC. CONSULTING CIVIL ENGINEERS / SURVEYORS CONSTRUCTION MANAGERS 17311 DALLAS PARKWAY SUITE 200 DALLAS, TEXAS 75248 (972) 250-2727 (972) 250-4774

FINAL PLAT ADDISON AIRPORT TOWN OF ADDISON, TEXAS DRAWN JOB NO. CONTRACT NO. SCALE DATE DTE 0216 - 1"=300' DEC. 2004

This plat is approved subject to all platting ordinances, rules, regulations and resolutions of the Town of Addison, Texas.