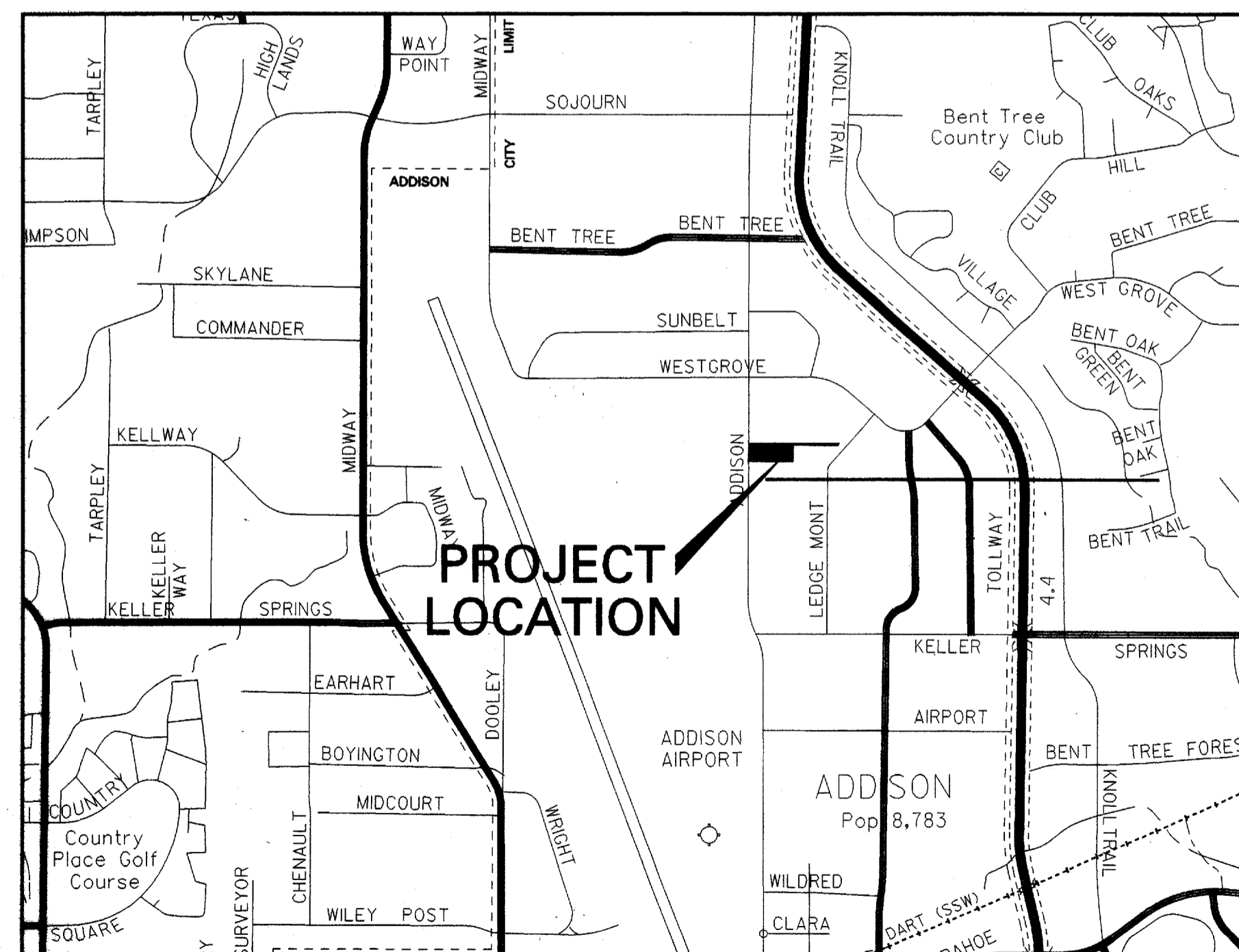


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CONSTRUCTION PLANS
FOR
PAVING, DRAINAGE & UTILITIES
ADDISON
OFFICE CONDOS
TOWN OF ADDISON, DALLAS COUNTY, TEXAS
MARCH 2006

FF6
16420 ADDISON RD.
ADDISON OFFICE CONDOS



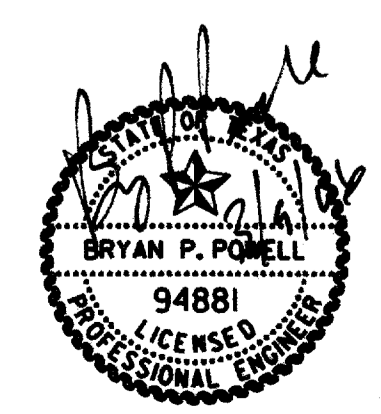
VICINITY MAP
NTS

SHEET INDEX

- 1 COVER SHEET
- 2 FINAL PLAT
- 3 GENERAL NOTES
- 4 DIMENSION CONTROL PLAN
- 5 GRADING PLAN
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- 15 HORIZONTAL INLET - TYPE 'H' DETAIL
- 16 DRAINAGE CALCULATIONS
- 17 PAVING DETAILS

RECORD DRAWING
THIS DRAWING HAS BEEN MODIFIED TO REFLECT CONSTRUCTION RECORDS PROVIDED TO THE ENGINEER

ENGINEER:



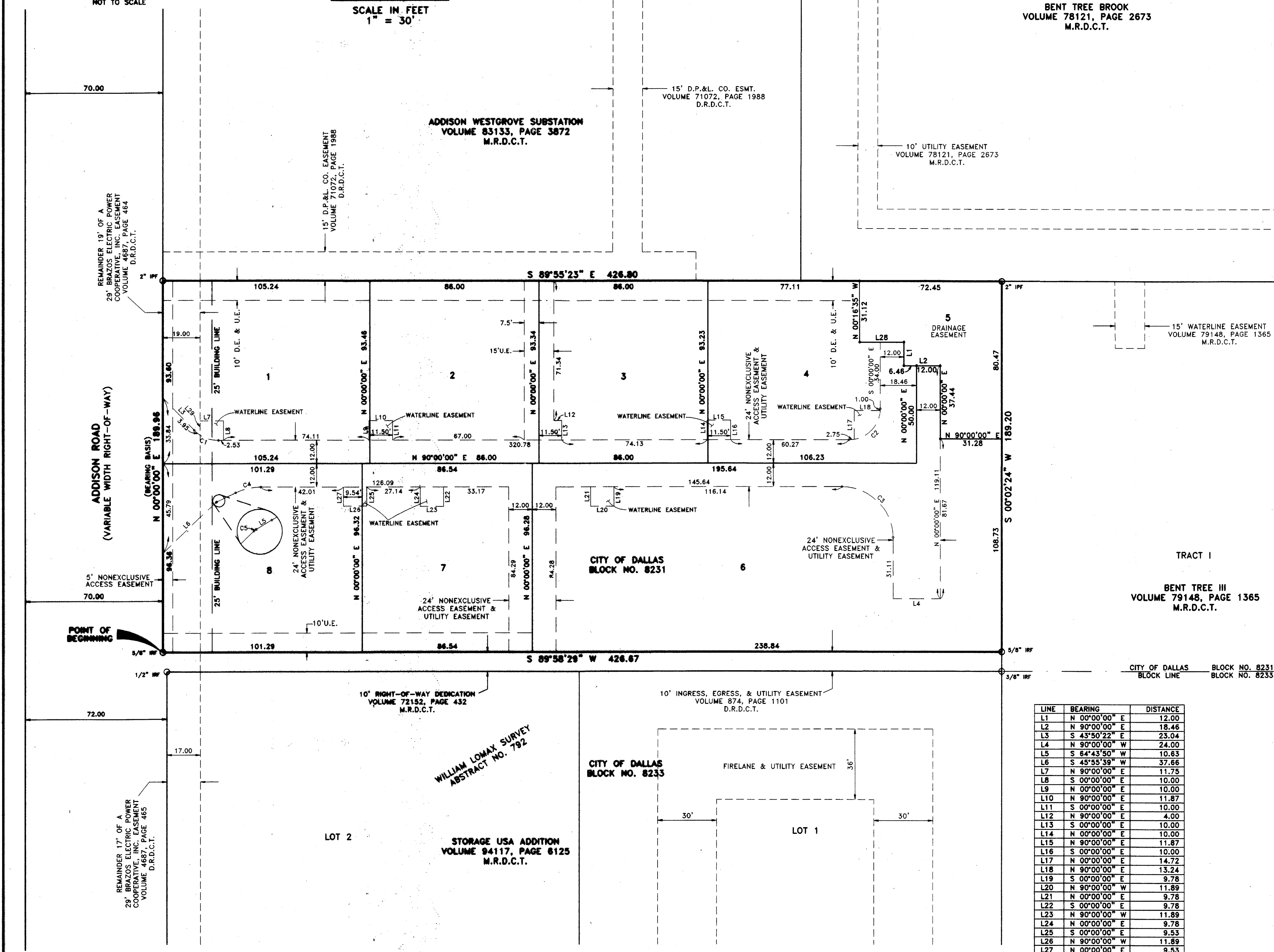
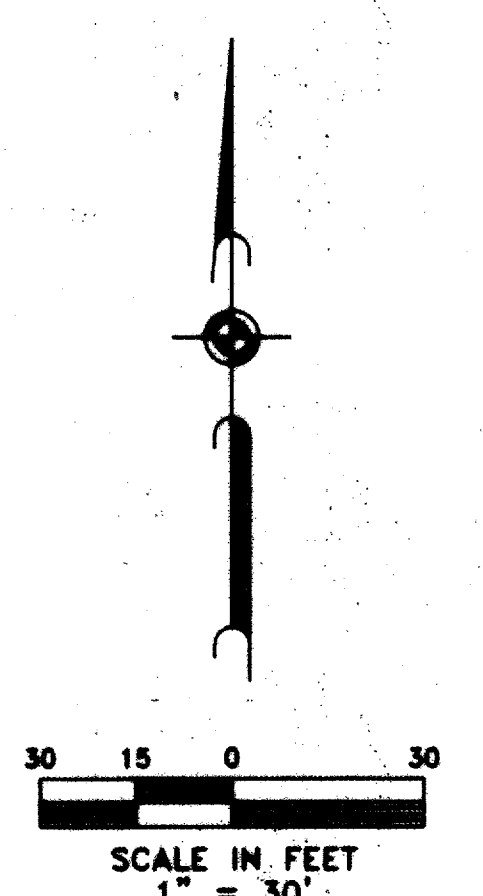
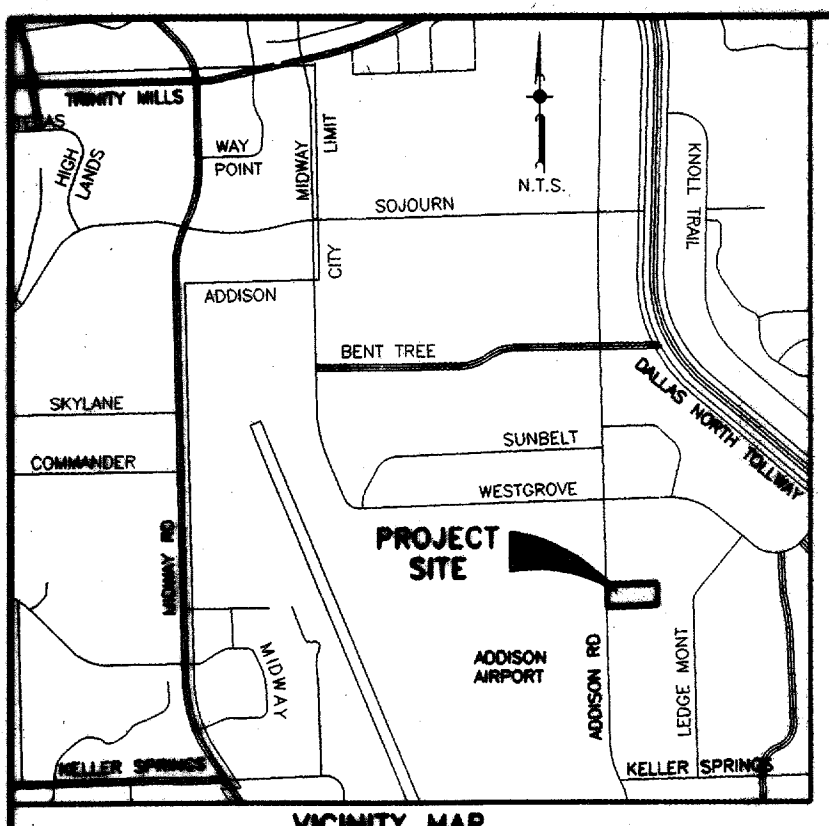
Carter = Burgess

CARTER & BURGESS, INC.
7950 ELMBROOK DRIVE, SUITE 250
DALLAS, TX. 75247-4951

FF-6

The seal appearing on this document was authorized by Bryan P. Powell, P.E. 94881, on March 07, 2006. Alteration of a sealed document without proper notification to the responsible engineer is an offense under the Texas Engineering Practice Act.

CONSTRUCTION PLANS



LINE	BEARING	DISTANCE
L1	N 00°00'00" E	12.00
L2	N 90°00'00" E	18.46
L3	S 43°50'22" E	23.04
L4	N 90°00'00" W	24.00
L5	S 64°43'50" W	10.63
L6	S 45°55'59" W	37.66
L7	N 90°00'00" E	11.75
L8	S 00°00'00" E	10.00
L9	N 90°00'00" E	11.87
L10	N 90°00'00" E	11.87
L11	S 00°00'00" E	10.00
L12	N 90°00'00" E	4.00
L13	S 00°00'00" E	10.00
L14	N 00°00'00" E	10.00
L15	N 90°00'00" E	11.87
L16	S 00°00'00" E	10.00
L17	N 00°00'00" E	14.72
L18	N 90°00'00" E	13.24
L19	S 00°00'00" E	9.78
L20	N 90°00'00" W	11.89
L21	N 00°00'00" E	9.78
L22	S 00°00'00" E	9.78
L23	N 90°00'00" W	11.89
L24	N 00°00'00" E	9.78
L25	S 00°00'00" E	9.53
L26	N 90°00'00" W	11.89
L27	N 00°00'00" E	9.53
L28	N 90°00'00" E	22.50
L29	S 00°00'00" E	7.28

CURVE	RADIUS	DELTA ANGLE	CHORD BEARING	CHORD LENGTH	ARC LENGTH
C1	17.00	46°09'38"	N 66°55'11" W	13.33	13.70
C2	16.00	90°00'00"	S 45°00'00" W	22.63	25.13
C3	26.00	90°00'00"	S 45°00'00" E	36.77	40.84
C4	30.00	25°16'10"	N 77°21'55" E	13.12	13.23
C5	1.00	18°48'11"	N 55°19'44" E	0.33	0.33

FIELD NOTES

BEING A 1.857 ACRE TRACT OF LAND SITUATED IN THE WILLIAM LOMAX SURVEY, ABSTRACT NO. 792, IN THE TOWN OF ADDISON, DALLAS COUNTY, TEXAS, AND BEING ALL OF LOT 1 OF THE CITY OF DALLAS BLOCK NO. 8231, ACCORDING TO THE FINAL PLAT OF KEMP ENGINEERING CO. INC. SUBDIVISION, AN ADDITION TO THE CITY OF DALLAS, RECORDED IN VOLUME 72152, PAGE 432 OF THE MAP RECORDS OF DALLAS COUNTY, TEXAS (M.R.D.C.T.), SAID LOT 1 BEING DEDICATED TO 16420 ADDISON ROAD, LTD. BY THE DEED RECORDED IN VOLUME 2005002, PAGE 7425 OF THE DEED RECORDS OF DALLAS COUNTY, TEXAS (D.R.D.C.T.), SAID 1.857 ACRE TRACT BEING MORE PARTICULARLY DESCRIBED BY METES AND BOUNDS AS FOLLOWS:

BEGINNING AT A 5/8" IRON ROD FOUND FOR THE SOUTHWEST CORNER OF SAID LOT 1 OF SAID KEMP ENGINEERING CO. INC. SUBDIVISION, SAID CORNER BEING AT THE INTERSECTION OF THE EXISTING NORTH LINE OF A 10 FOOT WIDE RIGHT-OF-WAY DEDICATED BY SAID KEMP ENGINEERING CO. INC. SUBDIVISION AND THE EXISTING EAST RIGHT-OF-WAY LINE OF ADDISON ROAD, A VARIABLE WIDTH RIGHT-OF-WAY;

THENCE, N 00°00'00" E, ALONG SAID EXISTING EAST RIGHT-OF-WAY LINE OF ADDISON ROAD, A DISTANCE OF 198.96 FEET TO A 2" IRON PIPE FOUND FOR THE NORTHWEST CORNER OF SAID LOT 1 OF SAID KEMP ENGINEERING CO. INC. SUBDIVISION AND THE COMMON SOUTHWEST CORNER OF ADDISON WESTGROVE SUBSTATION, AN ADDITION TO THE CITY OF ADDISON, RECORDED IN VOLUME 83133, PAGE 3872, M.R.D.C.T.;

THENCE, S 89°55'23" E, DEPARTING SAID EXISTING EAST RIGHT-OF-WAY LINE OF ADDISON ROAD, A DISTANCE OF 426.80 FEET TO A 2" IRON PIPE FOUND FOR THE NORTHWEST CORNER OF SAID LOT 1 OF SAID KEMP ENGINEERING SUBDIVISION AND THE COMMON SOUTH LINES OF SAID ADDISON WESTGROVE SUBSTATION AND OF LOT 1, BLOCK 1 OF BENT TREE BROOK, AN ADDITION TO THE TOWN OF ADDISON, RECORDED IN VOLUME 78121, PAGE 2673, M.R.D.C.T.; A DISTANCE OF 426.80 FEET TO A 2" IRON PIPE FOUND FOR THE NORTHEAST CORNER OF SAID LOT 1 OF KEMP ENGINEERING CO. INC. SUBDIVISION AND THE COMMON NORTHWEST CORNER OF TRACT I OF BENT TREE III, AN ADDITION TO THE TOWN OF ADDISON, RECORDED IN VOLUME 79148, PAGE 1365, M.R.D.C.T.;

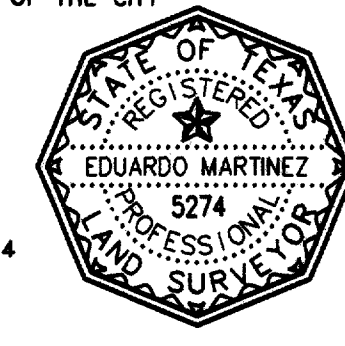
THENCE, S 00°02'24" W, DEPARTING SAID COMMON LINE, AND ALONG THE EAST LINE OF SAID LOT 1 OF SAID KEMP ENGINEERING CO. INC. SUBDIVISION AND THE COMMON WEST LINE OF SAID TRACT I, A DISTANCE OF 189.20 FEET TO A 5/8" IRON ROD FOUND FOR THE SOUTHWEST CORNER OF SAID LOT 1 OF SAID KEMP ENGINEERING CO. INC. SUBDIVISION AND THE COMMON NORTHEAST CORNER OF THE AFORESAID 10 FOOT WIDE RIGHT-OF-WAY DEDICATION BY SAID KEMP ENGINEERING CO. INC. SUBDIVISION;

THENCE, S 89°58'29" W, DEPARTING SAID COMMON LINE, AND ALONG THE SOUTH LINE OF SAID LOT 1 OF SAID KEMP ENGINEERING CO. INC. SUBDIVISION AND THE COMMON EXISTING NORTH RIGHT-OF-WAY LINE OF SAID 10 FOOT WIDE RIGHT-OF-WAY DEDICATION, A DISTANCE OF 426.67 FEET TO THE POINT OF BEGINNING, AND CONTAINING 1.857 ACRES OF LAND, MORE OR LESS.

SURVEYOR'S CERTIFICATE

I, EDUARDO MARTINEZ REGISTERED PROFESSIONAL LAND SURVEYOR IN THE STATE OF TEXAS, DO HEREBY CERTIFY THAT I HAVE PREPARED THIS PLAT FROM A ACTUAL ON THE GROUND SURVEY OF THE LAND, AND THE MONUMENTS SHOWN HEREON WERE FOUND AND/OR PLACED UNDER MY PERSONAL SUPERVISION IN ACCORDANCE WITH THE PLATTING RULES AND REGULATIONS OF THE CITY PLAN COMMISSION OF THE CITY OF LANCASTER.

DATED NOVEMBER 10, 2005



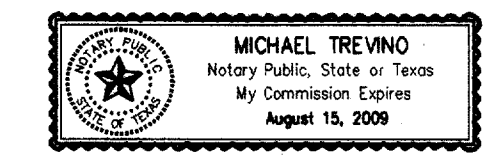
EDUARDO MARTINEZ
TEXAS REGISTERED PROFESSIONAL LAND SURVEYOR NO. 5274

STATE OF TEXAS
COUNTY OF DALLAS

BEFORE ME, THE UNDERSIGNED, A NOTARY PUBLIC IN AND FOR THE STATE OF TEXAS, ON THIS DAY PERSONALLY APPEARED EDUARDO MARTINEZ, KNOWN TO ME TO BE THE PERSON WHOSE NAME IS SUBSCRIBED FOR THE PURPOSE AND CONSIDERATION THEREIN EXPRESSED.

GIVEN UNDER MY HAND AND SEAL OF OFFICE, THIS 10th DAY OF NOVEMBER, 2005.

NOTARY PUBLIC, STATE OF TEXAS
COMMISSION EXPIRES: AUGUST 15, 2009



APPROVED _____ ATTEST _____

CHAIRMAN _____ SECRETARY _____
PLANNING & ZONING COMMISSION PLANNING & ZONING COMMISSION

DATE _____ DATE _____

EXECUTED PRO-FORMA

MAYOR _____ DATE 1-6-06

THE UNDERSIGNED, THE CITY SECRETARY OF THE TOWN OF ADDISON, TEXAS, HEREBY CERTIFIES THAT THE FOREGOING FINAL PLAT OF ADDISON OFFICE CONDOS, AN ADDITION TO THE TOWN OF ADDISON, WAS SUBMITTED TO THE PLANNING AND ZONING COMMISSION AND APPROVED BY IT ON THE 10th DAY OF NOVEMBER, 2005.

CITY SECRETARY _____

NOW, THEREFORE, KNOW ALL MEN BY THESE PRESENTS:

THAT 16420 ADDISON ROAD, LTD., DOES HEREBY ADOPT THIS PLAT DESIGNATING THE HEREIN ABOVE PROPERTY AS ADDISON OFFICE CONDOS, AN ADDITION TO THE TOWN OF ADDISON, TEXAS, AND SUBJECT TO THE CONDITIONS, RESTRICTIONS AND RESERVATIONS STATED HEREINAFTER, OWNER DEDICATES TO THE PUBLIC USE FOREVER THE STREETS AND ALLEYS SHOWN THEREON.

THE EASEMENT SHOWN ON THIS PLAT ARE HEREBY RESERVED FOR THE PURPOSES AS INDICATED, INCLUDING, BUT NOT LIMITED TO, THE INSTALLATION AND MAINTENANCE OF WATER, SANITARY SEWER, STORM SEWER, DRAINAGE, ELECTION, TELEPHONE, GAS AND CABLE TELEVISION. OWNER SHALL HAVE THE RIGHT TO USE THESE EASEMENTS, PROVIDED HOWEVER, THAT IT DOES NOT UNREASONABLY INTERFERE OR IMPEDE WITH THE PROVISION OF THE SERVICES TO OTHERS. SAID UTILITY EASEMENTS ARE HEREBY RESERVED BY MUTUAL USE AND ACCOMMODATION OF ALL PUBLIC UTILITIES USING OR DESIRING TO USE THE SAME. AN EXPRESS EASEMENT OF INGRESS AND EGRESS IS HEREBY EXPRESSLY GRANTED ON, OVER AND ACROSS ALL SUCH EASEMENTS FOR THE BENEFIT OF THE PROVIDER OF SERVICES FOR WHICH EASEMENTS ARE GRANTED.

ANY DRAINAGE AND FLOODWAY EASEMENT SHOWN HEREON IS HEREBY DEDICATED TO THE PUBLIC'S USE FOREVER, BUT INCLUDING THE FOLLOWING COVENANTS WITH REGARDS TO MAINTENANCE RESPONSIBILITIES. THE EXISTING CHANNELS OR CREEKS TRAVERSING THE DRAINAGE AND FLOODWAY EASEMENT WILL REMAIN AS AN OPEN CHANNEL, UNLESS REQUIRED TO BE ENCLOSED BY ORDINANCE, AT ALL TIMES AND SHALL BE MAINTAINED BY THE INDIVIDUAL OWNERS OF THE LOT OR LOTS THAT ARE TRAVERSED BY OR ADJACENT TO THE DRAINAGE AND FLOODWAY EASEMENT. THE TOWN WILL NOT BE RESPONSIBLE FOR THE MAINTENANCE AND OPERATION OF SAID CREEK OR CREEKS OR FOR ANY DAMAGE OR INJURY OF PRIVATE PROPERTY OR PERSON THAT RESULTS FROM THE FLOW OF WATER ALONG SAID CREEK, OR FOR THE CONTROL OF EROSION, NO OBSTRUCTION TO THE NATURAL FLOW OF WATER RUNOFF SHALL BE PERMITTED BY CONSTRUCTION OF ANY TYPE BUILDING, FENCE OR ANY OTHER STRUCTURE WITHIN THE DRAINAGE AND FLOODWAY EASEMENT. PROVIDED, HOWEVER, IT IS UNDERSTOOD THAT IN THE EVENT IT BECOMES NECESSARY FOR THE TOWN TO CHANNELIZE OR CONSIDER ERECTING ANY TYPE OF DRAINAGE STRUCTURE IN ORDER TO IMPROVE THE STORM DRAINAGE, THEN IN SUCH EVENT, THE TOWN SHALL HAVE THE RIGHT, BUT NOT THE OBLIGATION, TO ENTER UPON THE DRAINAGE AND FLOODWAY EASEMENT AT ANY POINT, OR POINTS, WITH ALL RIGHTS OF INGRESS AND EGRESS TO INVESTIGATE, SURVEY, ERECT, CONSTRUCT OR MAINTAIN ANY DRAINAGE FACILITY DEEMED NECESSARY BY THE TOWN FOR MAINTENANCE OR EFFICIENCY OF ITS RESPECTIVE SYSTEM OR SERVICE.

WATER MAIN AND SANITARY SEWER EASEMENTS SHALL ALSO INCLUDE ADDITIONAL AREA OF WORKING SPACE FOR CONSTRUCTION AND MAINTENANCE OF THE SYSTEMS. ADDITIONAL EASEMENT AREA IS ALSO CONVEYED FOR INSTALLATION AND MAINTENANCE OF MANHOLES, CLEAN OUTS, FIRE HYDRANTS, WATER SERVICE AND SEWER SERVICES FROM THE MAIN TO CURB OR PAVEMENT LINE, AND THE DESCRIPTIONS OF SUCH ADDITIONAL EASEMENTS HEREIN GRANTED SHALL BE DETERMINED BY THEIR LOCATIONS AS INSTALLED.

THIS PLAT IS APPROVED SUBJECT TO ALL PLATTING ORDINANCES, RULES, REGULATIONS AND RESOLUTIONS OF THE TOWN OF ADDISON, TEXAS.

WITNESS MY HAND THIS 10th DAY OF December, 2005.

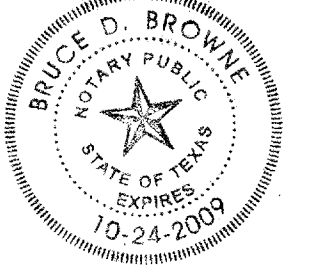
16420 ADDISON ROAD, LTD.
BY: 16420 ADDISON ROAD, LTD.
A LIMITED PARTNERSHIP
NAME: JAMES FEAGIN
TITLE: OWNER

STATE OF TEXAS
COUNTY OF DALLAS

BEFORE ME, THE UNDERSIGNED, A NOTARY PUBLIC IN AND FOR THE STATE OF TEXAS, ON THIS DAY PERSONALLY APPEARED JAMES FEAGIN, KNOWN TO ME TO BE THE PERSON WHOSE NAME IS SUBSCRIBED FOR THE PURPOSE AND CONSIDERATION THEREIN EXPRESSED.

GIVEN UNDER MY HAND AND SEAL OF OFFICE, THIS 10th DAY OF December, 2005.

NOTARY PUBLIC, STATE OF TEXAS
COMMISSION EXPIRES: 10-24-09



A
FINAL PLAT
OF
ADDISON OFFICE CONDOS
BEING A REPLAT OF
LOT 1, BLOCK 8231 OF
KEMP ENGINEERING CO. INC. SUBDIVISION
VOLUME 72152, PAGE 432
M.R.D.C.T.
OUT OF THE
WILLIAM LOMAX SURVEY, ABSTRACT NO. 792
IN THE
TOWN OF ADDISON, DALLAS COUNTY, TEXAS
1.857 ACRES / 8 LOTS
OWNER/DEVELOPER: 16420 ADDISON ROAD, LTD.
16660 N. DALLAS PARKWAY, SUITE 2900
DALLAS, TEXAS 75248
CONTACT: JAMES FEAGIN

ENGINEER/SURVEYOR: **Carter-Burgess**
CARTER & BURGESS, INC.
7500 ELMBROOK DRIVE, SUITE 290
DALLAS, TEXAS 75247-4961
(214) 638-0145

LEGEND

D.E.	DRAINAGE EASEMENT
U.E.	UTILITY EASEMENT
IRF	IRON ROD FOUND
IRPF	IRON PIPE FOUND
D.R.D.C.T.	DEED RECORDS OF DALLAS COUNTY, TEXAS
M.R.D.C.T.	MAP RECORDS OF DALLAS COUNTY, TEXAS

NOTES:

1.) BEARING BASIS IS THE EAST RIGHT-OF-WAY LINE OF ADDISON ROAD, A VARIABLE WIDTH R.O.W., ACCORDING TO THE PLAT RECORDED IN VOLUME 72152, PAGE 432 OF THE MAP RECORDS OF DALLAS COUNTY, TEXAS.

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DATE: 03/09/2006 10:44:26
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GENERAL NOTES:

SANITARY SEWER

1. SANITARY SEWER MAINLINE PIPING SHALL MEET THE EXTRA STRENGTH REQUIREMENTS OF ASTM SPECIFICATION D3034, OR SDR 35 PVC FOR INSTALLATIONS LESS THAN TEN FEET DEEP, FOR INSTALLATIONS GREATER THAN TEN FEET, SDR 26 PVC SHALL BE USED. PIPE SHALL HAVE THE BELL AND SPIGOT TYPE JOINTS, CONSISTING OF INTEGRAL WALL SECTION WITH FACTORY INSTALLED COMPRESSION RUBBER RING GASKET, SECURELY LOCKED IN BELL GROOVE TO PROVIDE POSITIVE SEAL UNDER ALL INSTALLATION CONDITIONS. PIPE SHALL BE LAID WITH THE BELL END ON THE UP STREAM SIDE.
2. FITTINGS SHALL BE IDENTICAL WITH THE BELL AND SPIGOT CONFIGURATION OF THE PIPE, AND OF THE SAME CLASS RATING.
3. MAINLINE EMBEDMENT SHALL BE NCTCOG CLASS "B+" WITH THE ADDITION OF A LAYER OF GEO-TEXTILE FABRIC PRIOR TO THE PLACEMENT OF THE SAND, OR AS INDICATED ON APPROVED, ENGINEERED PLANS. BACK FILL SHALL BE NATIVE MATERIAL COMPACTED IN 8" LOOSE LIFTS TO 95% STANDARD PROCTOR DENSITY, WITH ZERO TO PLUS THREE OPTIMUM MOISTURE, AND FREE OF ANY ROCKS OR CLODS LARGER THAN THREE INCHES IN DIAMETER.
4. MANHOLES ARE TO BE POURED IN PLACE WITH CLASS F (NCTCOG 7.4.5) MINIMUM CONCRETE MIX. MANHOLES SHALL BE WATERTIGHT. ASBESTOS CEMENT SLEEVES OR APPROVED EQUAL WITH RUBBER RING JOINT SHALL BE USED AT PIPE PENETRATIONS TO PROVIDE POSITIVE WATERTIGHT SEALS. CONNECTIONS AT EXISTING MANHOLES SHALL BE CORED.
5. THE CONTRACTOR OR PLUMBER SHALL MAKE ALL TAPS IN THE NEW OR EXISTING SEWER MAINS. WATER MUST BE PUMPED OUT OF THE TAPPING TRENCH THROUGHOUT THE OPERATION TO PREVENT INFLOW OF GROUNDWATER INTO THE SEWER SYSTEM. SERVICE TAPS SHALL BE MADE WITH STANDARD WYE FITTINGS SET AT 45 DEGREES OF VERTICAL. LATERAL LINES SHALL BE SDR 35 PVC LAID ON ONE QUARTER INCH PER FOOT MINIMUM GRADE TO THE PROPERTIE LINE. A DOUBLE CLEAN OUT SHALL BE PLACED AT THE PROPERTY LINE, AND SET AT FINISHED GRADE WITH AN 18"x18"x4" THICK CONCRETE LOCATOR PAD POURED IN ALL NON-PAVEMENT LOCATIONS. WYES TEES, AND BENDS ARE TO BE INCASED IN CLASS B CONCRETE 6 INCHES THICK. THE TOWN MUST INSPECT TAP PRIOR TO BACK FILL OPERATION. BACK FILL SHALL BE TWELVE INCHES OF SAND PLACED BY HAND SHOVEL ONLY. AFTER HAND OPERATION, THE REST OF THE TRENCH MAY BE MACHINE FILLED WITH SELECT FILL ABSENT OF ANY ROCKS OR CLODS LARGER THAN THREE INCHES IN DIAMETER. COMPACTION IN 6" LOOSE LIFTS.

STORM

1. ALL STORM SEWER PIPE SHALL BE CLASS III RCP

WATER

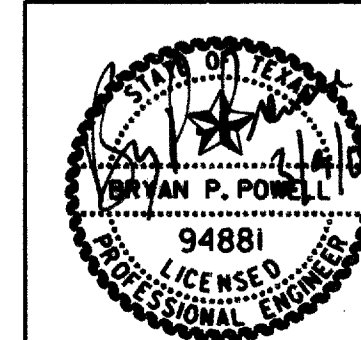
1. ALL MAINLINE, FIRE LINE, AND HYDRANT LEAD PIPING FROM 6-12 INCHES IN DIAMETER SHALL BE AWWA C909-98 MOLECULARLY ORIENTED PVC PRESSURE PIPE (CLASS 150MIN., CLASS 200 FOR FIRE LINES) WITH CAST IRON O.D., OR, WHEN PIPE PENETRATES METER VAULT WALLS IT SHALL BE DUCTILE IRON, PIPE JOINTS SHALL BE RUBBER RING AND INTEGRAL THICKENED BELL, ASSEMBLED WITH A FACTORY SUPPLIED LUBRICANT. CAST IRON FITTINGS SHALL BE MECHANICAL RESTRAINED JOINT (EBAA IRON "MEGA-LUG" OR EQUAL) TYPE, AND POLY-WRAPPED (8MIL) PRIOR TO BACK FILL, 4 INCH PIPE FOR FIRE LINE SERVICES SHALL BE AWWA C900-75, DR-14, CLASS 200 PVC.
2. EMBEDMENT SHALL COMPLY WITH NCTCOG CLASS "B+" EMBEDMENT OF CRUSHED STONE TO THE SPRING LINE OF THE PIPE, WITH SAND (12" MIN) OVER THE PIPE. A LAYER OF GEO-TEXTILE FABRIC SHALL BE PLACED ON TOP OF THE STONE PRIOR TO THE PLACEMENT OF THE SAND.
3. TRACER WIRE SHALL BE PLACED ON PIPE PRIOR TO EMBEDMENT. WIRE SHALL BE #12 PLASTIC COATED COPPER WIRE, TIED TO ALL VALVES AND FIRE HYDRANTS, AND EXTENDING TO SIX (6) INCHES ABOVE FINISHED GRADE ALONG THE OUTSIDE OF ALL VALVE STACKS AND HYDRANTS.
4. FINISH BACK FILL SHALL BE NATIVE SOIL FREE OF ALL ROCKS AND CLODS GREATER THAN THREE INCHES IN DIAMETER, COMPACTED TO 95% STANDARD PROCTOR DENSITY, IN 6 INCH MAXIMUM LOOSE LIFTS, WITH ZERO TO PLUS THREE OPTIMUM MOISTURE, TRENCHES UNDER PAVEMENT SHALL BE BACKFILLED WITH "FLOWABLE FILL" TO THE LEVEL INDICATED BY THE PAVEMENT THICKNESS, PENDING THE APPROVAL OF THE TOWN'S ENGINEERING DEPARTMENT. SUBMIT A "BATCH DESIGN" FOR ANY FLOWABLE FILL TO BE USED. (MINIMUM COMPRESSION STRENGTH 400PSI)
5. SERVICE TAPS MUST MEET TOWN OF ADDISON STANDARDS. SEE GENERAL REQUIREMENTS FOR WATER SERVICE FOR WATER SERVICE DETAILS. ALL TAPS MUST BE INSPECTED PRIOR TO BACK FILLING.

PAVEMENT

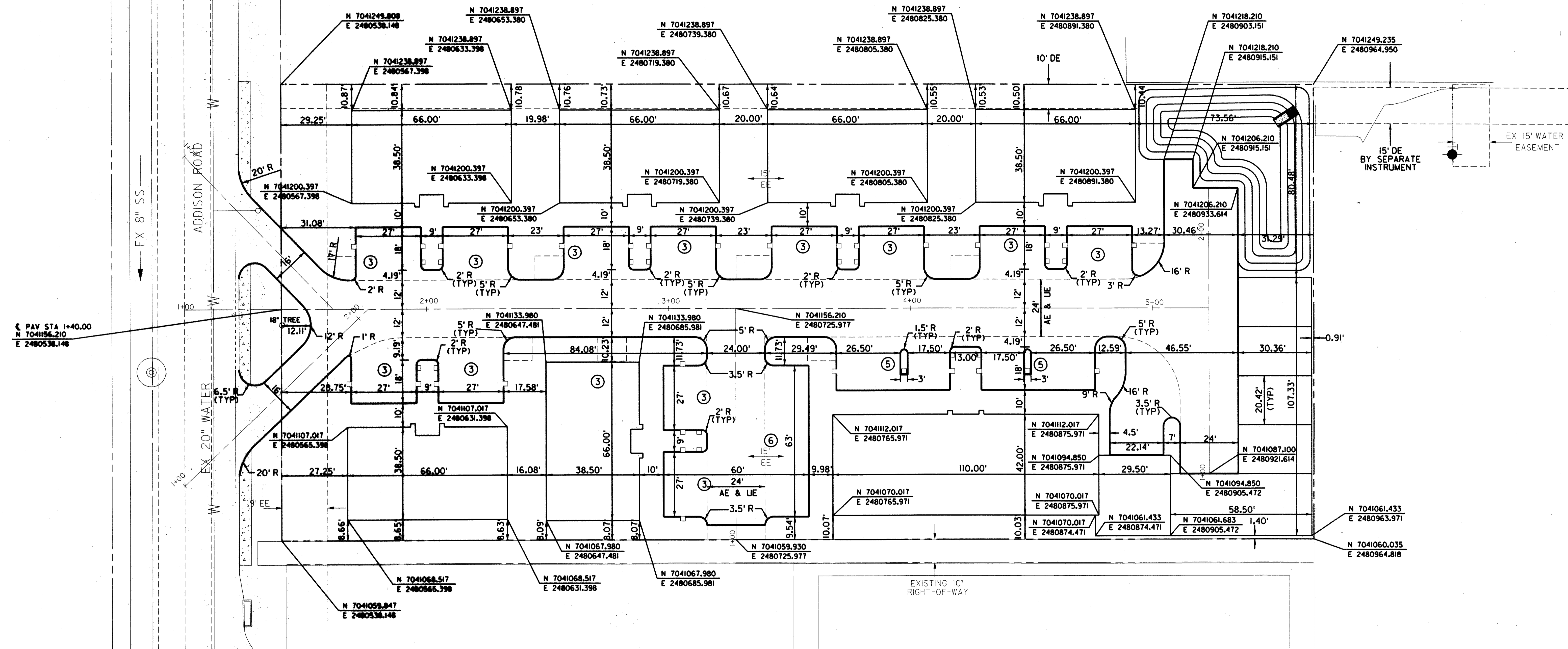
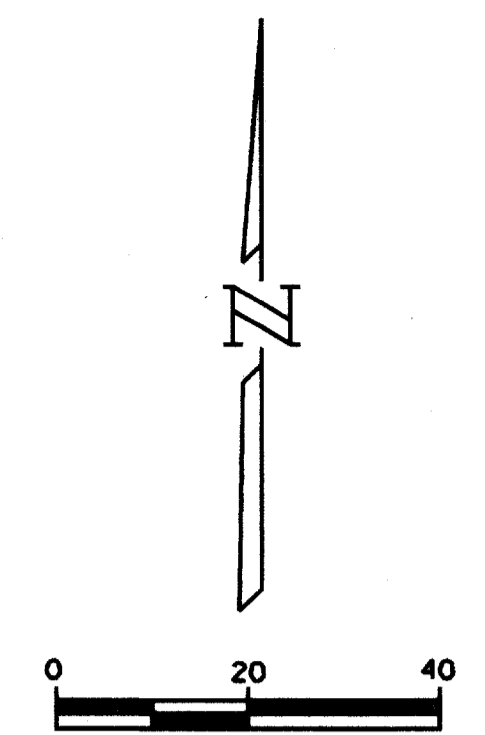
1. THE PAVEMENT SHALL BE 6" REINFORCED CONCRETE (3,600 PSI) WITH A 6" REINFORCED MONOLITHIC CURB.
2. THE PAVEMENT SHALL BE PLACED ON 6" LIME STABILIZED SUBGRADE.

RECORD DRAWING

THIS DRAWING HAS BEEN MODIFIED
TO REFLECT CONSTRUCTION RECORDS
PROVIDED TO THE ENGINEER



DATE	REVISION	MADE	CKD	APPD		
Carter-Burgess Carter & Burgess, Inc. 7950 Elmbrook Drive Dallas, Texas 75247-4961 (214) 638-0145 Metro (214) 263-2019 Fax (214) 638-0447						
GENERAL NOTES ADDISON OFFICE CONDOS						
TOWN OF ADDISON, DALLAS COUNTY, TEXAS						
DESIGN	DRAWN	CHECKED	SCALE	DATE	FILE	NO.
TIPTON		POWELL	NTS	MARCH'06	01-6628	3 OF 17



NOTE:
ALL DIMENSIONS ARE TO FACE OF CURB
AND OUTSIDE EDGE OF BUILDING

RECORD DRAWING

THIS DRAWING HAS BEEN MODIFIED
TO REFLECT CONSTRUCTION RECORDS
PROVIDED TO THE ENGINEER

DATE	REVISION	MADE	CKD	APPD

Carter-Burgess

Carter & Burgess, Inc. 7950 Elm Brook Drive Dallas, Texas 75247-4961
(214) 638-0145 Metro (214) 263-2019 Fax (214) 638-0447

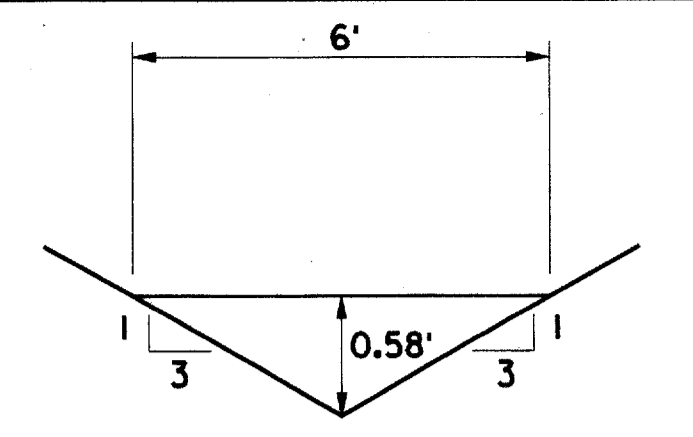
DIMENSION CONTROL PLAN ADDISON OFFICE CONDOS

TOWN OF ADDISON, DALLAS COUNTY, TEXAS

DESIGN	DRAWN	CHECKED	SCALE	DATE	FILE	NO.
TIPTON	POWELL		1" = 20'	MARCH'06	01-6628	4 OF 17



0100 = 1.83 CFS
 S = 0.77%
 V = 1.83 FPS
 d = 0.58 FT
 MIN 1'

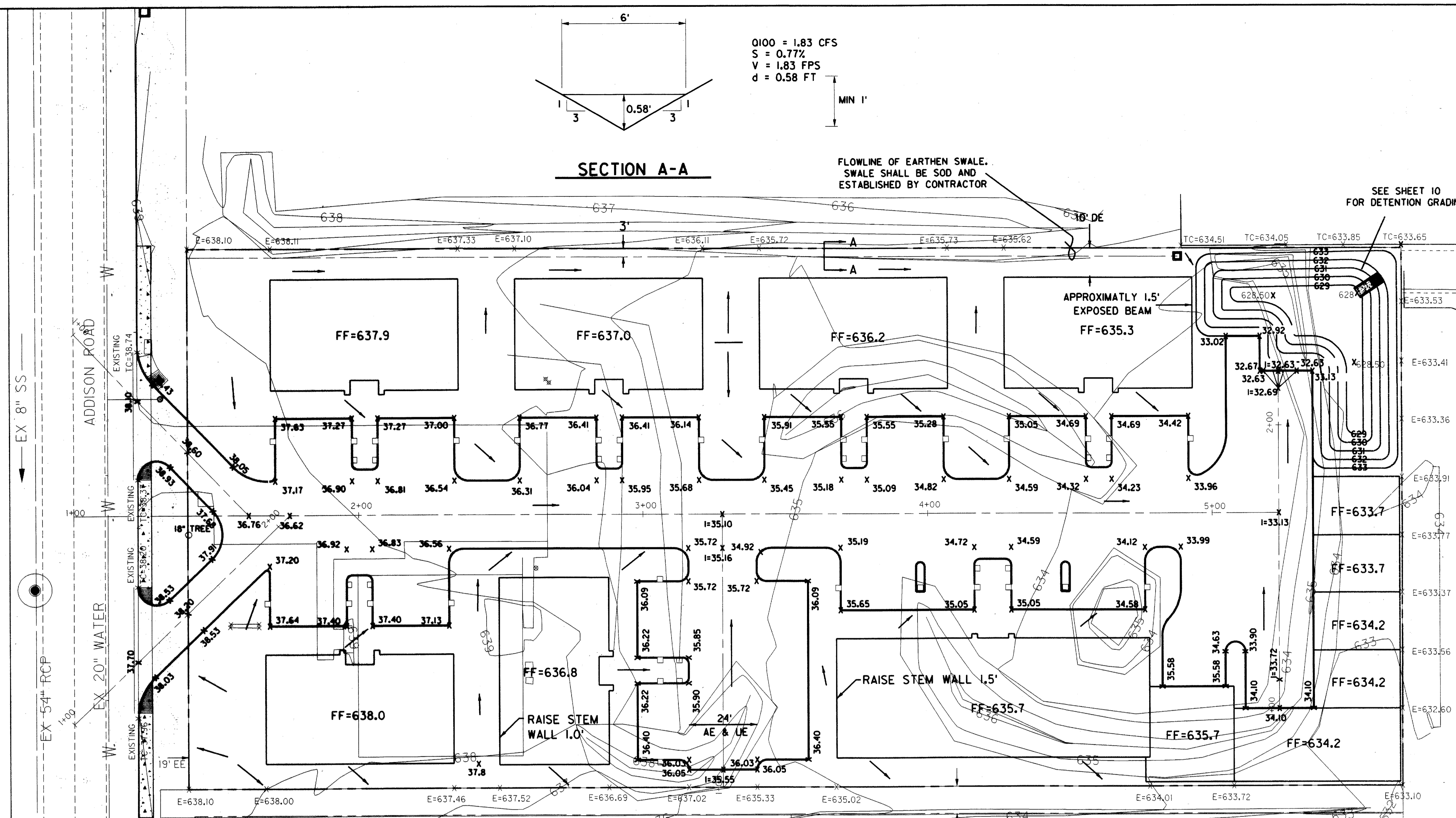
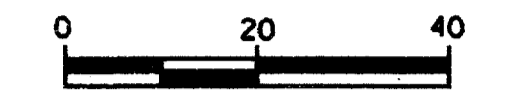


0100 = 1.83 CFS
 S = 0.77%
 V = 1.83 FPS
 d = 0.58 FT
 MIN 1'

SECTION A-A

FLOWLINE OF EARTHEN SWALE.
 SWALE SHALL BE SOD AND
 ESTABLISHED BY CONTRACTOR

SEE SHEET 10
FOR DETENTION GRADING



NOTES:

1. ALL GRADES ARE TOP OF PAVEMENT UNLESS NOTED OTHERWISE.
2. ALL DRIVES AND PARKING AREAS SHALL HAVE A 6 INCH CURB.
3. ARCHITECT AND/OR CONTRACTOR TO ENSURE THAT STEM WALLS ARE AT AN ELEVATION THAT WILL NOT ALLOW WATER TO PENETRATE BUILDING STRUCTURE WHERE FINISH FLOOR ELEVATIONS ARE LOWER THAN THE SURROUNDING GRADES.

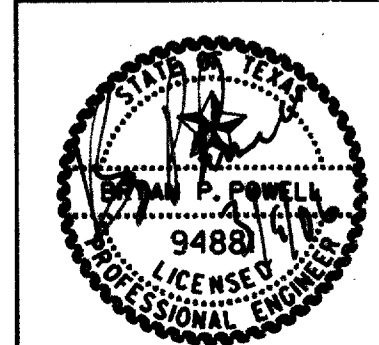
BENCHMARKS

- 1.) *** CUT ON INLET SW CORNER OF RATLIFF AND ADDISON ROAD. ELEV. 637.20
- 2.) *** CUT ON INLET EAST SIDE OF ADDISON ROAD, 20' 3/4 SOUTH OF THE SOUTHWEST CORNER OF SUBJECT TRACT. ELEV. 637.92

- LEGEND**
- x88.88 PROPOSED SPOT ELEVATIONS (TOP OF PAVEMENT)
 - PROPOSED FLOW DIRECTION
 - FF=888.8 PROPOSED FINISHED FLOOR
 - E=888.88 EXISTING GRADE
 - I=888.88 INVERT ELEVATION
 - EE ELECTRICAL EASEMENT
 - AE & UE ACCESS & UTILITY EASEMENT

RECORD DRAWING

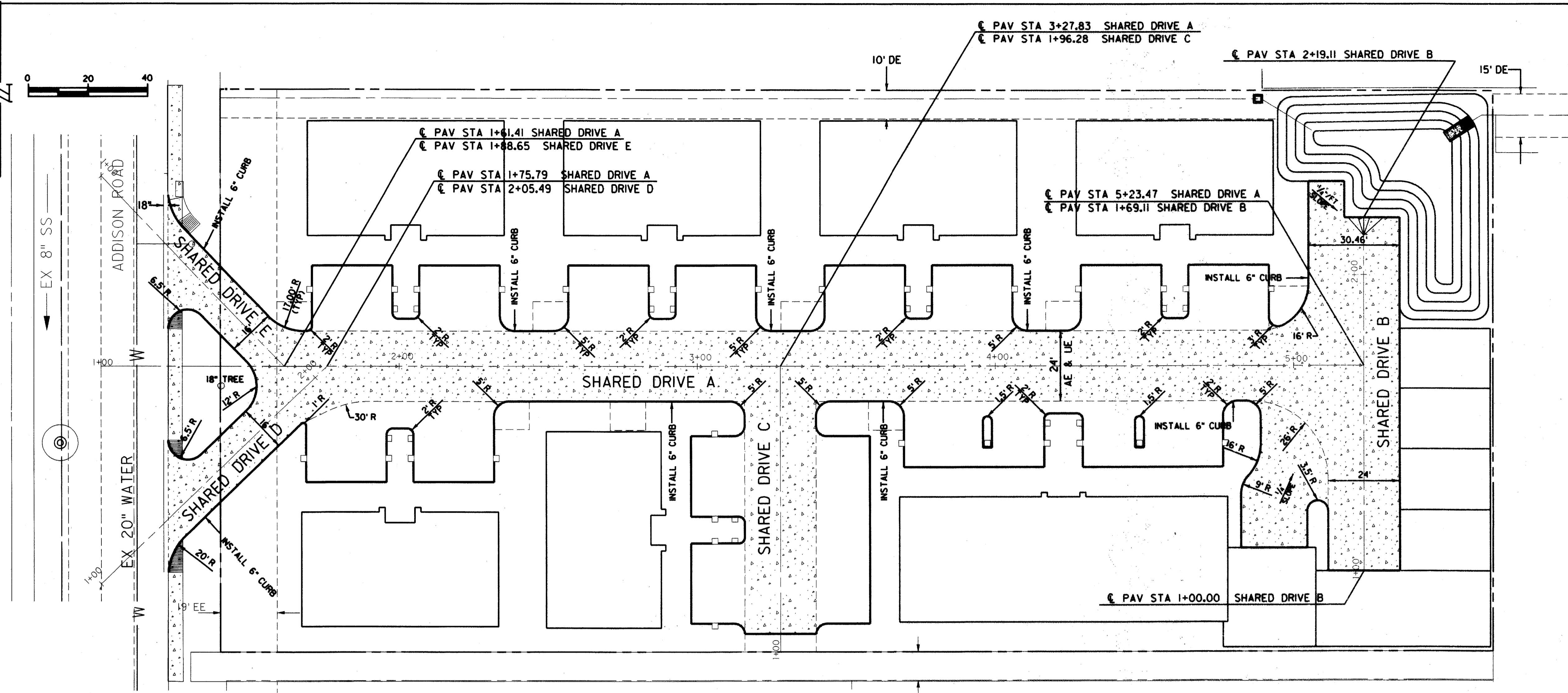
THIS DRAWING HAS BEEN MODIFIED TO REFLECT CONSTRUCTION RECORDS PROVIDED TO THE ENGINEER



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Carter-Burgess						
Carter & Burgess, Inc. 7950 Elmbrook Drive Dallas, Texas 75247-4961 (214) 638-0445 Metro (214) 263-2019 Fax (214) 638-0447						
GRADING PLAN						
ADDISON OFFICE CONDOS						
TOWN OF ADDISON, DALLAS COUNTY, TEXAS						
DESIGN	DRAWN	CHECKED	SCALE	DATE	FILE	NO.
TIPTON	SPONSEL	POWELL	1" = 20'	MARCH'06	01-6628	5 OF 17

- BENCHMARKS**
- 1.) "" CUT ON INLET SW CORNER OF RATLIFF AND ADDISON ROAD. ELEV. 637.20
 - 2.) "" CUT ON INLET EAST SIDE OF ADDISON ROAD, 20' ³⁵/₆₄ SOUTH OF THE SOUTHWEST CORNER OF SUBJECT TRACT. ELEV. 637.92

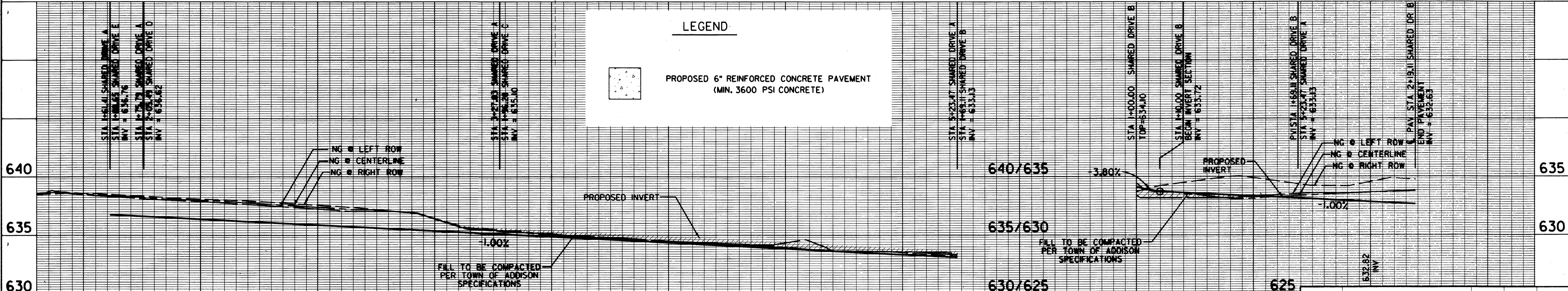
- NOTES:**
1. UNDERGROUND UTILITIES HAVE BEEN PLOTTED FROM FIELD OBSERVATIONS OF AVAILABLE RECORDS FURNISHED BY EACH UTILITY BUT ARE NOT NECESSARILY EXACT. VERIFY THE LOCATION OF EACH UTILITY IN ADVANCE OF TRENCHING OR BORING.
 2. ALL SIDEWALKS, BARRIER FREE RAMPS, HANDICAP SPACES, & PARKING SPACES BY OTHERS AND ARE NOT A PART OF THESE PLANS (SEE ARCHITECTURAL PLANS)




SHARED DRIVE A

SHARED DRIVE B

1+50 2+00 2+50 3+00 3+50 4+00 4+50 5+00 1+00 1+50 2+00 2+50

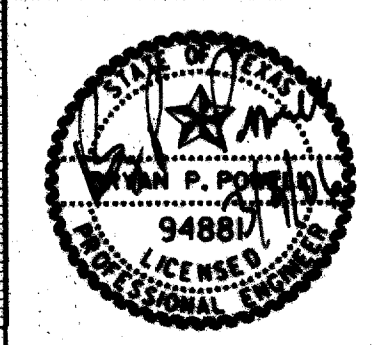


LEGEND


 PROPOSED 6" REINFORCED CONCRETE PAVEMENT (MIN. 3600 PSI CONCRETE)

RECORD DRAWING

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 (214) 638-0145 Metro (214) 263-2019 Fax (214) 638-0447

PAVING PLAN & PROFILE
ADDISON OFFICE CONDOS

SHARED DRIVE A & SHARED DRIVE B
 TOWN OF ADDISON, DALLAS COUNTY, TEXAS

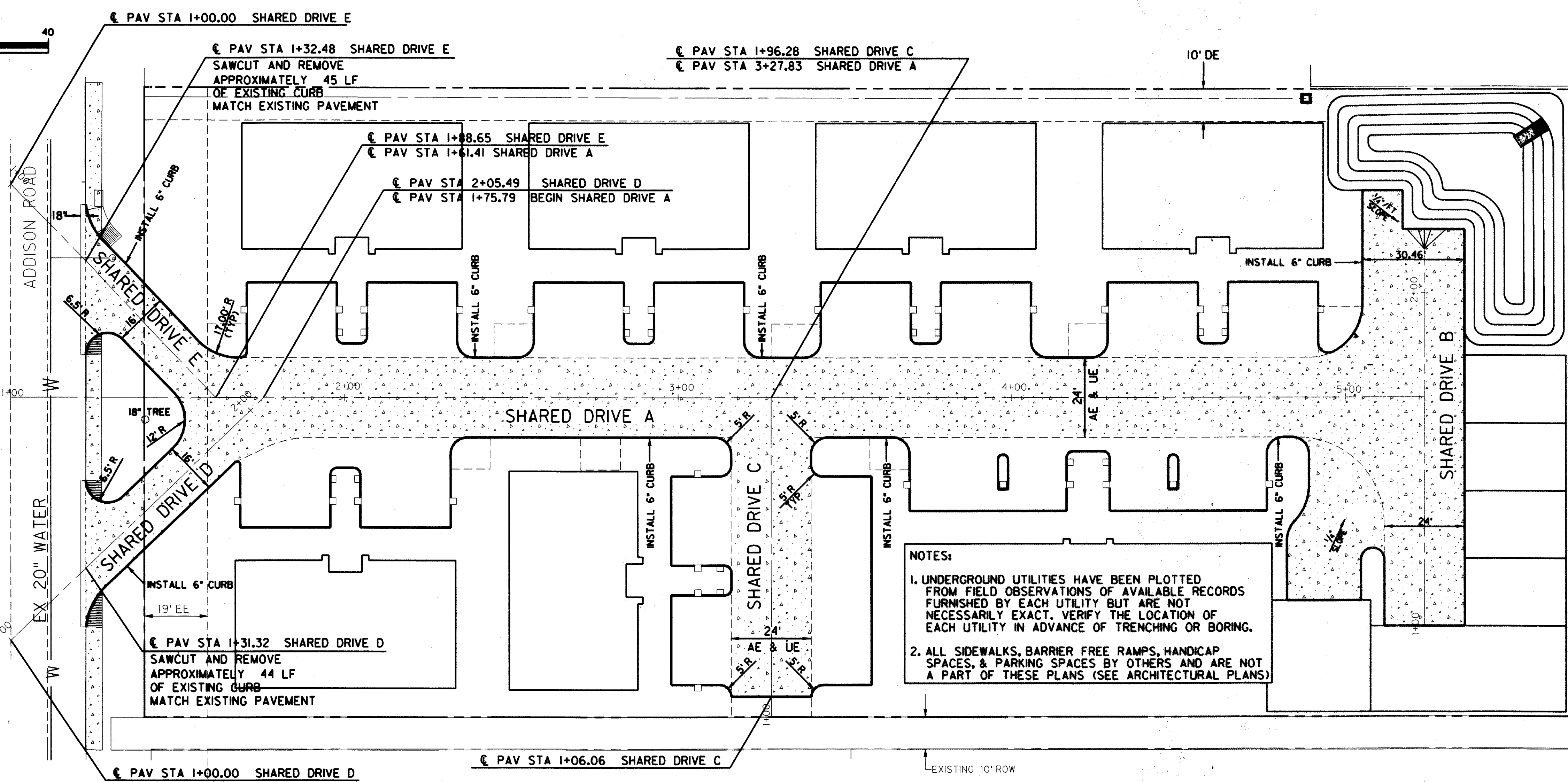
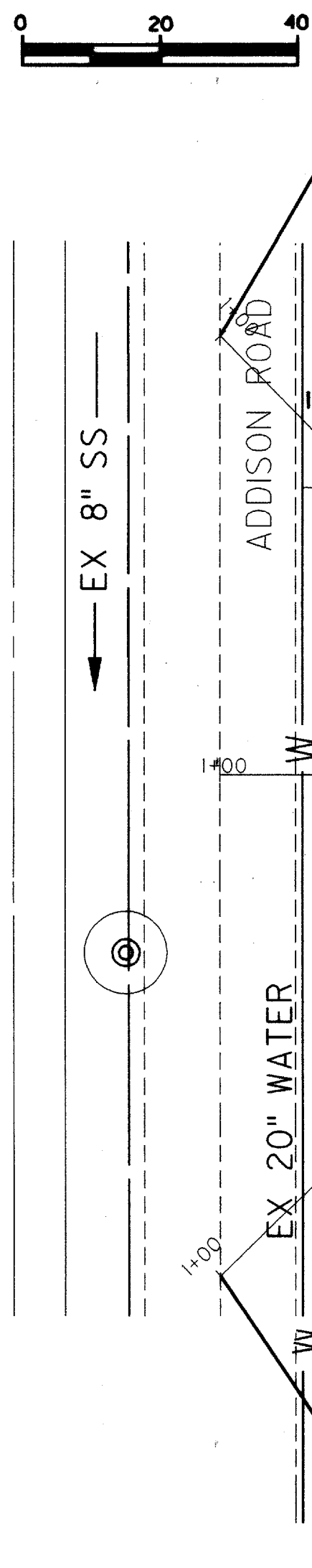
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TIPTON	SPONSEL	POWELL	1" = 20' H 1" = 4' V	MARCH'06	01-6628	6 OF 17

DATE PLOTTED: 03/07/06 11:00 AM
 PLOTTED BY: J. BURKHEAD
 SCALE: 1" = 20' H, 1" = 4' V
 SHEET: 6 OF 17

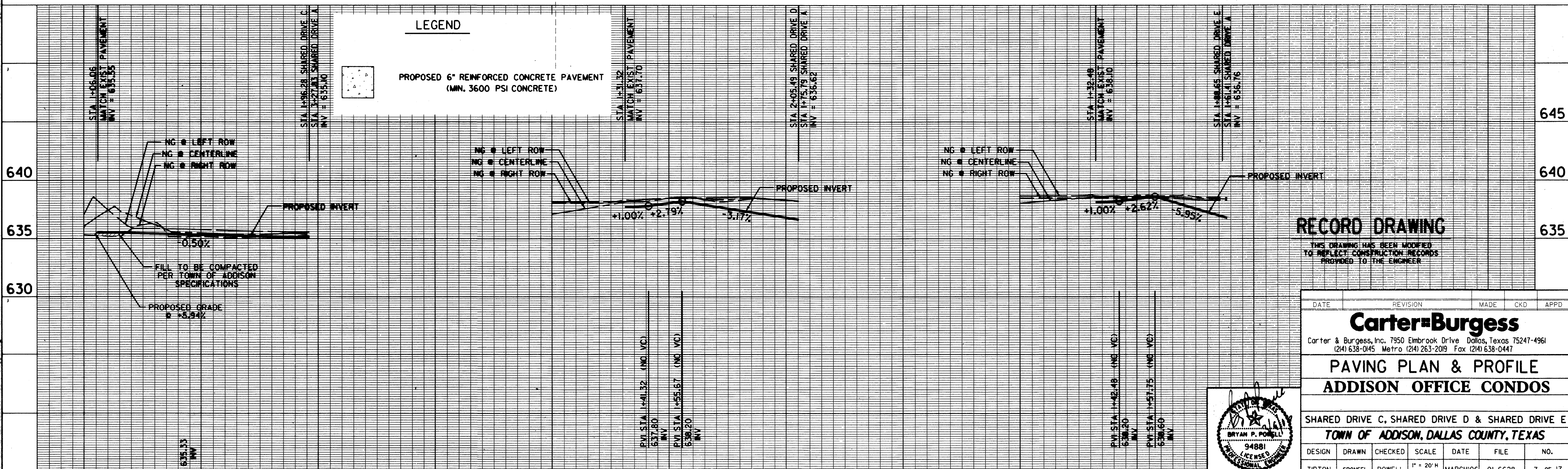
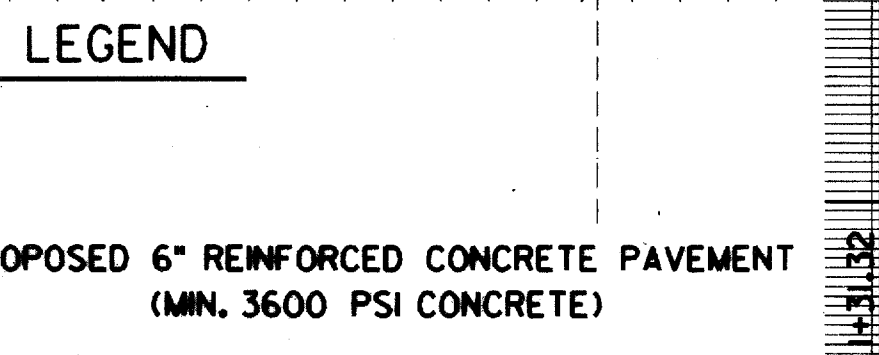
BENCHMARKS

1.) "" CUT ON INLET SW CORNER OF RATLIFF AND ADDISON ROAD.
ELEV. 637.20

2.) "" CUT ON INLET EAST SIDE OF ADDISON ROAD, 20'-3/4" SOUTH OF THE SOUTHWEST CORNER OF SUBJECT TRACT.
ELEV. 637.92

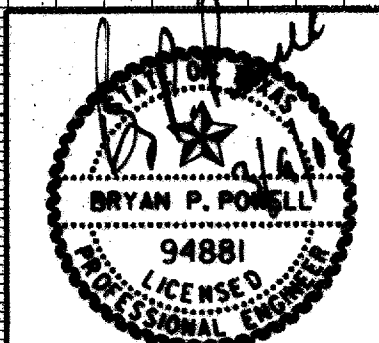


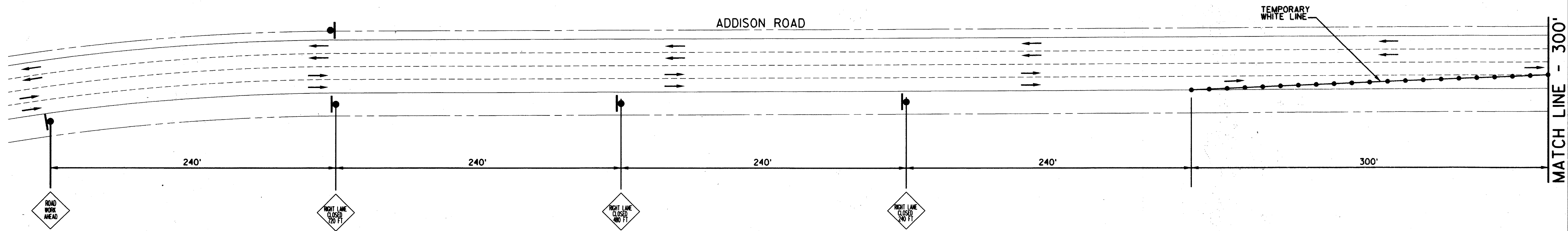
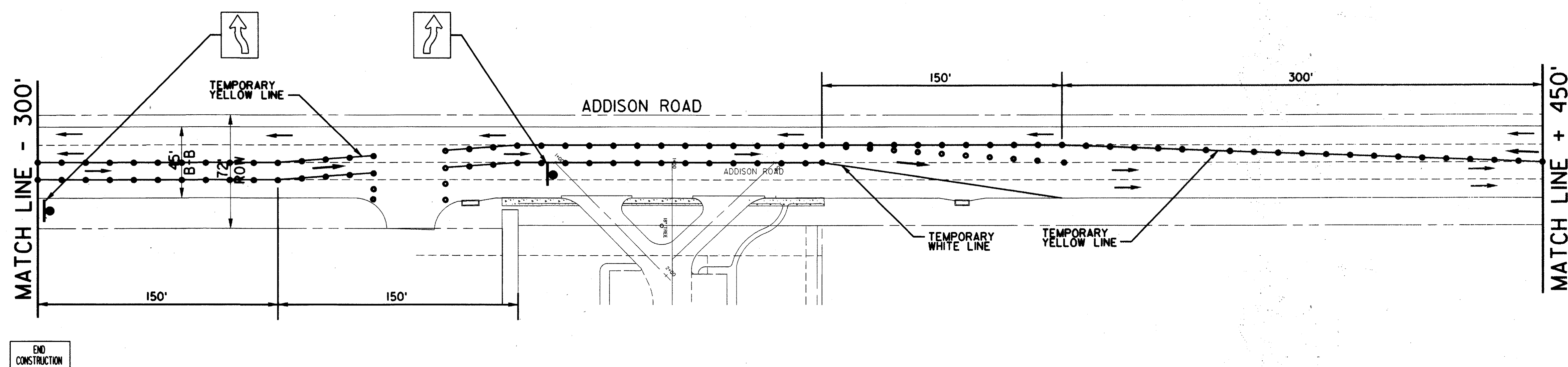
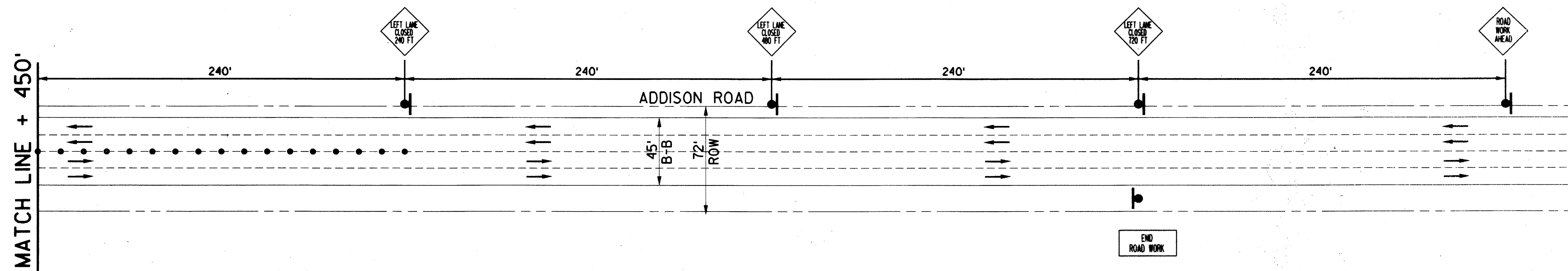
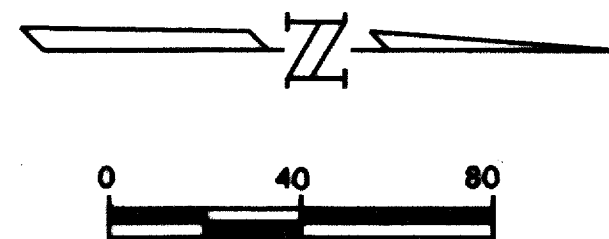
1+00 1+50 2+00 1+00 1+50 2+00 1+00 1+50 2+00



RECORD DRAWING
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PAVING PLAN & PROFILE ADDISON OFFICE CONDOS				
SHARED DRIVE C, SHARED DRIVE D & SHARED DRIVE E TOWN OF ADDISON, DALLAS COUNTY, TEXAS				
DESIGN	DRAWN	CHECKED	SCALE	DATE
TIPTON	SPONSEL	POWELL	1" = 20' H 1" = 4' V	MARCH '06
FILE	NO.	7 OF 17		
01-6628				





TRAFFIC CONTROL NOTES:

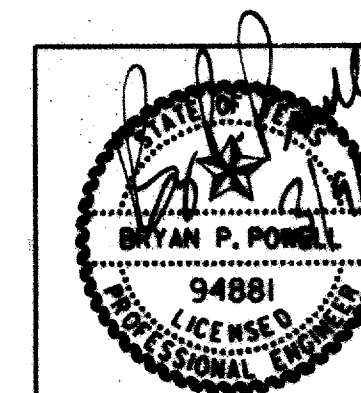
1. ALL SIGNAGE, BARRICADES, AND PAVEMENT MARKINGS SHALL BE IN ACCORDANCE WITH THE LATEST REVISION OF THE TEXAS MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.
2. LOCATIONS SHOWN FOR SIGNAGE AND PAVEMENT MARKINGS ARE APPROXIMATE; FINAL LOCATIONS MAY CHANGE DUE TO POST CONSTRUCTION CONDITIONS AND PRESENCE OF OTHER PHYSICAL FEATURES. FINAL LOCATION OF ALL TRAFFIC CONTROL DEVICES SHALL BE FIELD VERIFIED WITH CITY PRIOR TO INSTALLATION AT NO ADDITIONAL COST.
3. ALL PAVEMENT MARKINGS OTHER THAN BUTTONS SHALL BE THERMOPLASTIC UNLESS OTHERWISE NOTED.
4. ALL SIGNS SHALL BE ENGINEERING GRADE AND THE SIZES SHALL BE STANDARD UNLESS OTHERWISE NOTED.
5. NO TRAFFIC LANE OR SIDEWALK ALONG ADDISON ROAD IS TO BE CLOSED WITHOUT FIRST OBTAINING THE APPROPRIATE PERMIT FROM THE TOWN OF ADDISON. CLOSURE OF ANY TRAFFIC LANE MUST BE RESTRICTED TO THE HOURS OF 9:00 AM TO 3:30 PM WORKDAYS.

LEGEND

- WARNING SIGN (FREE STANDING)
- TRAFFIC CONES
- TRAFFIC FLOW DIRECTION
- CONSTRUCTION AREA

RECORD DRAWING

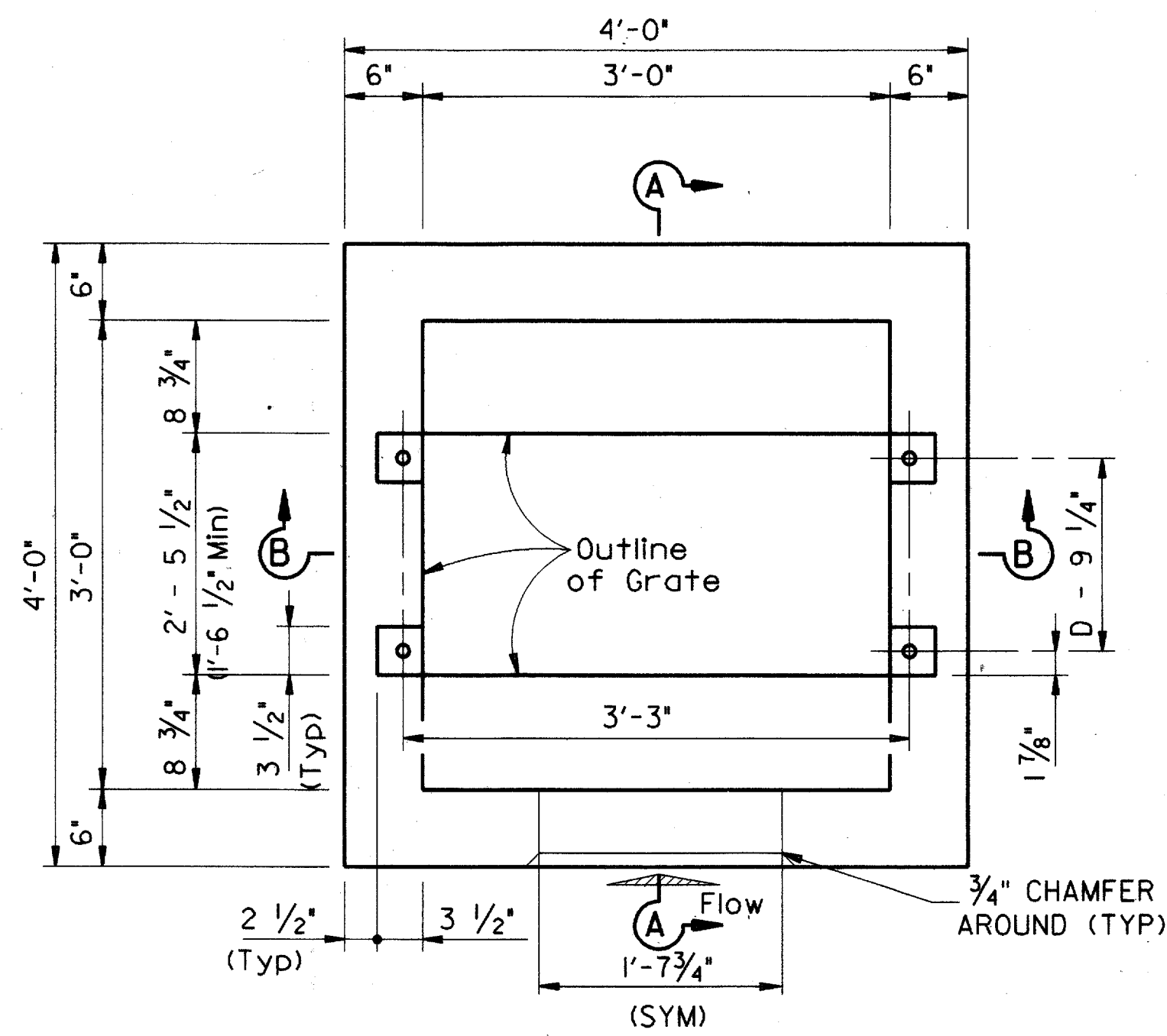
THIS DRAWING HAS BEEN MODIFIED TO REFLECT CONSTRUCTION RECORDS PROVIDED TO THE ENGINEER



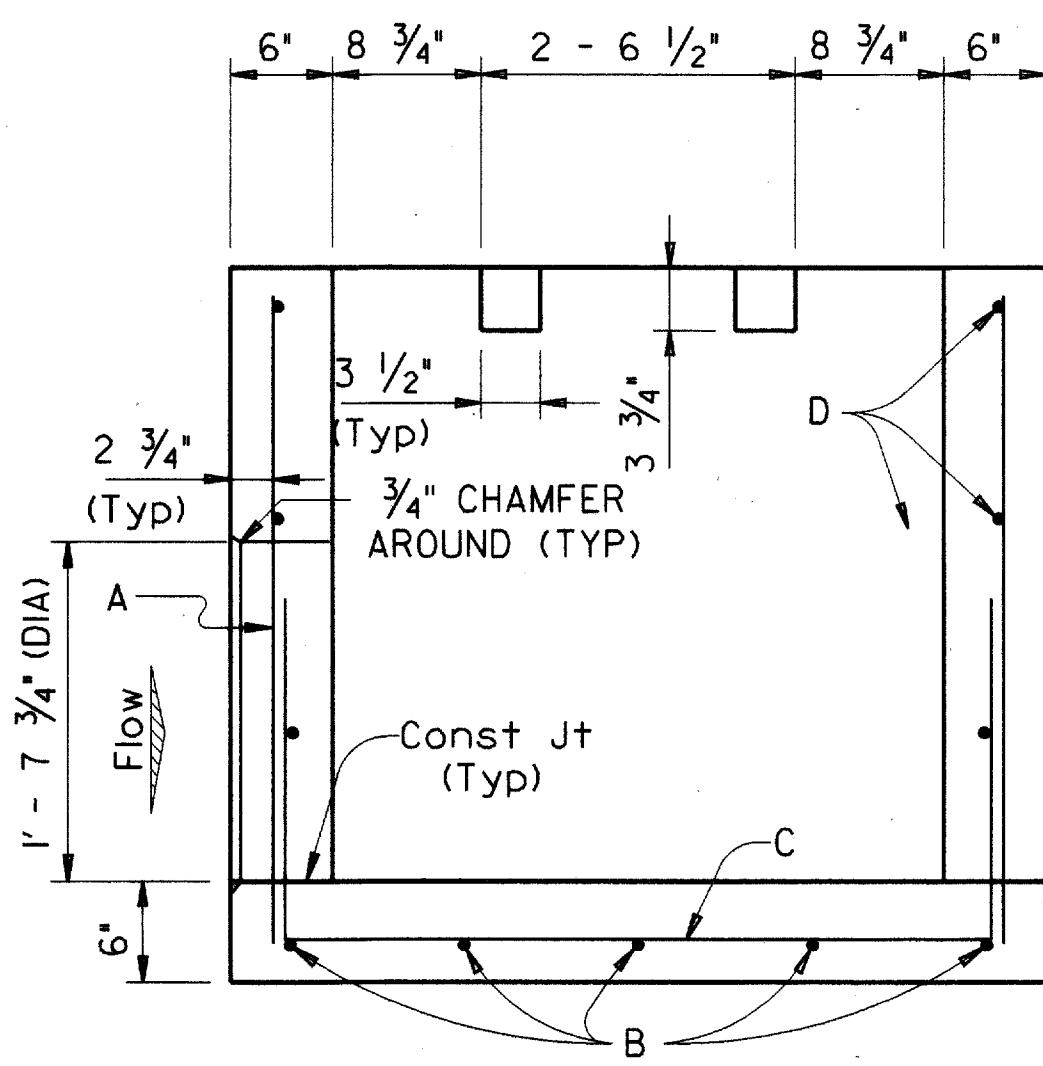
DATE	REVISION	MADE	CKD	APPD		
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TRAFFIC CONTROL PLAN						
ADDISON OFFICE CONDOS						
TOWN OF ADDISON, DALLAS COUNTY, TEXAS						
DESIGN	DRAWN	CHECKED	SCALE	DATE	FILE	NO.
RYAN	SPONSEL	POWELL	1" = 40'	MARCH'06	01-6628	13 OF 17

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LEVELS DISPLAYED

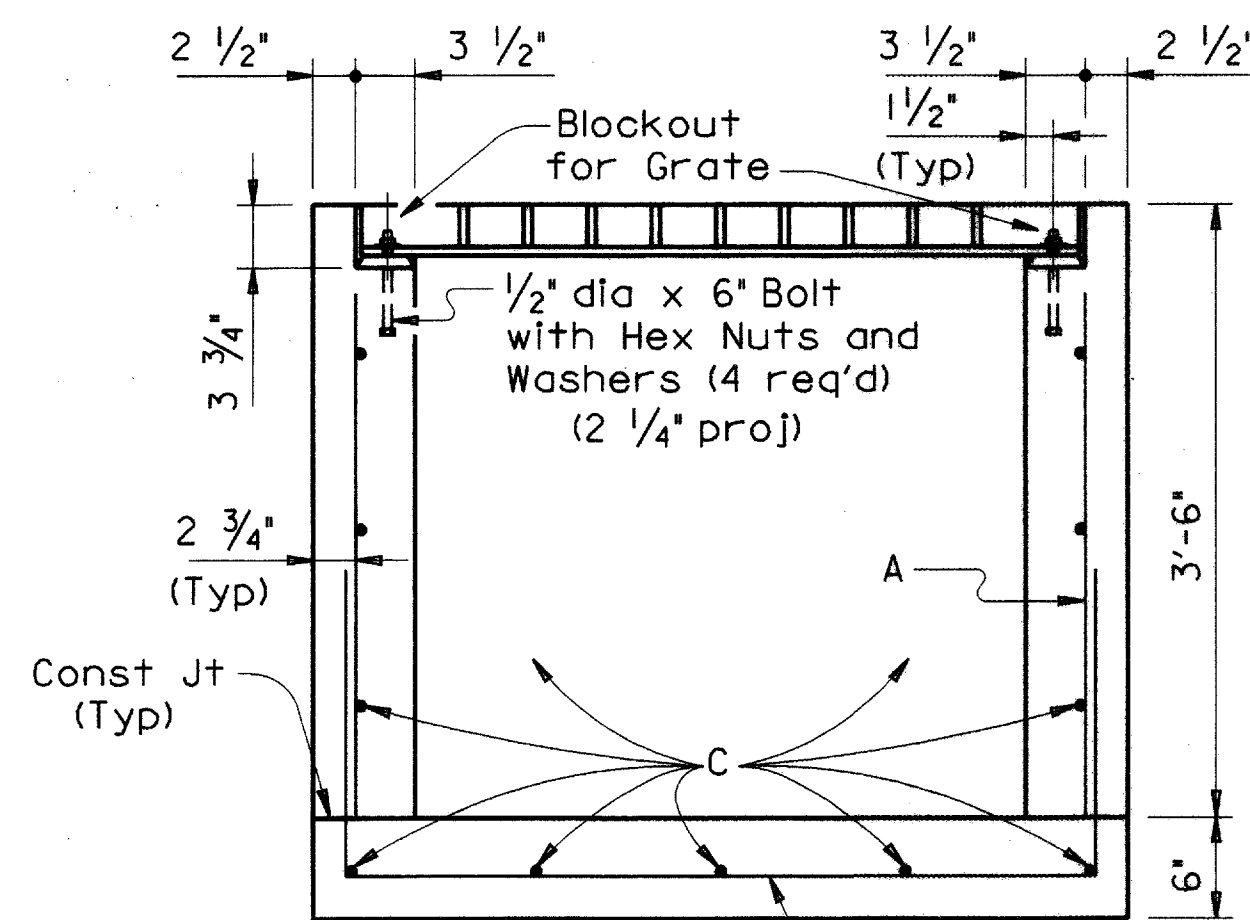


PLAN

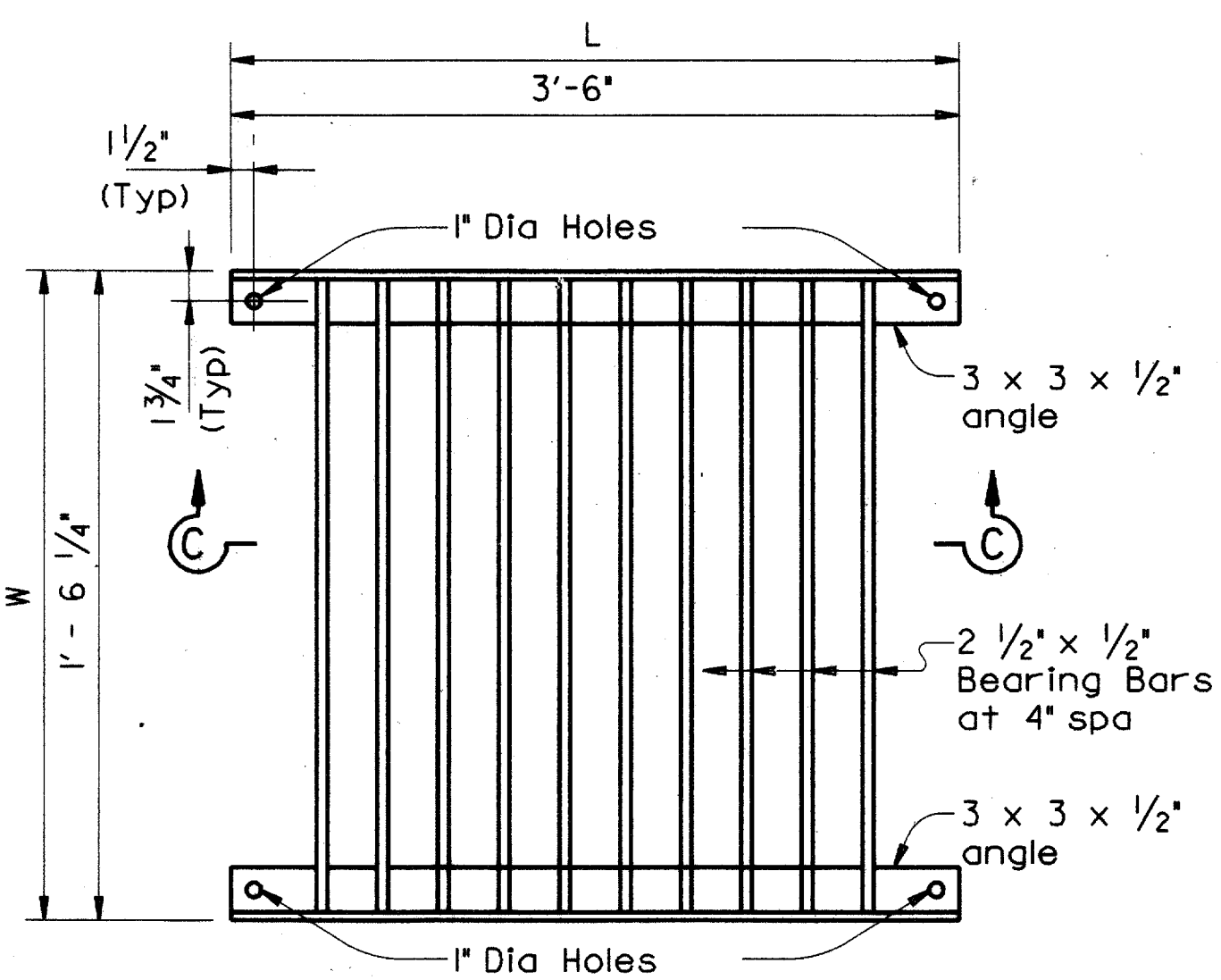


SECTION A-A

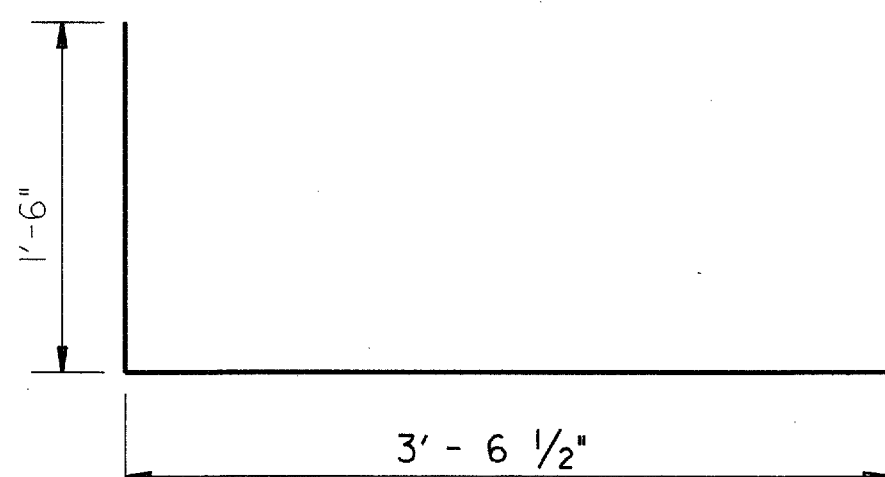
Space Bars A, B, C and D at 12" Max



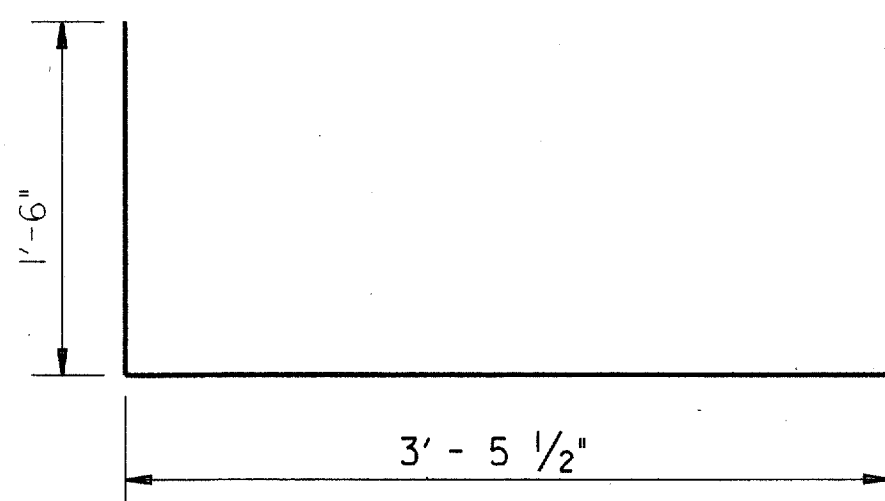
SECTION B-B



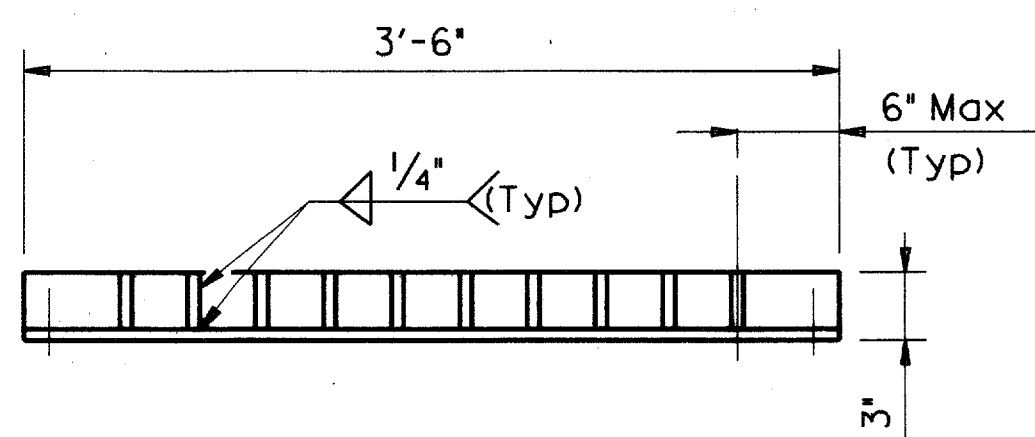
PLAN ~ GRATE



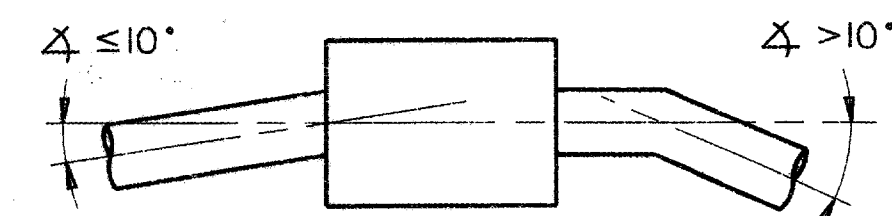
BARS B



BARS C

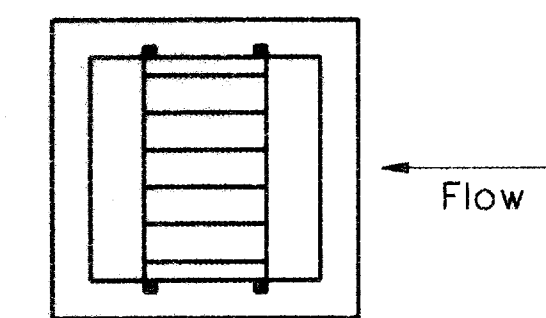


SECTION C-C



PIPE CONNECTION DETAIL

Connecting pipes should enter within 10° of normal to inlet wall. If necessary, pipe elbow or curved approach alignment should be used to stay within this limit.



GENERAL NOTES:

Quantities shown hereon are for Contractor's information only. Unless otherwise shown in the plans, payment will be made for each inlet of the Type specified.

Alternate design drawings bearing the seal of a registered professional engineer will be acceptable for precast construction of the inlets.

Shop drawings will not be required.

The Contractor may with the approval of the Engineer furnish inlets of equivalent structural design.

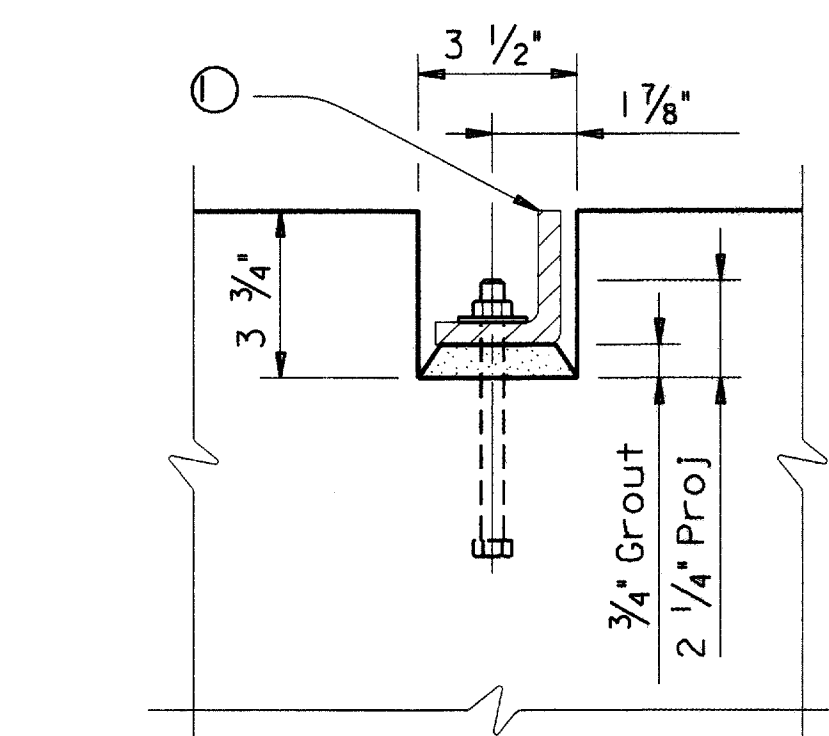
In areas of conflict between reinforcing steel, blockouts, pipes, anchor belts or other reinforcing steel, the reinforcement shall be bent or adjusted to clear as directed by the Engineer.

Structural Steel for grates shall conform to the requirements of ASTM Designation A-36 or AISI Designation M1010-M1020.

All reinforcing steel shall be #4 unless otherwise noted.

The pipe diameter, 'D', to be used in determining horizontal dimensions of Type 'H' inlet, shall be the largest pipe entering or exiting the inlet which would control that particular wall dimension. For vertical dimension, use largest 'D' or 1'-0" above highest pipe soffit as a minimum dimension.

All steel components except reinforcing, shall be galvanized after fabrication. Galvanizing damaged during transport or construction shall be repaired in accordance with the specifications.



GRATE MOUNTING DETAIL

Interior angle with 4 ~ 1/2" dia x 6" Bolts, Hex Nuts and Washers

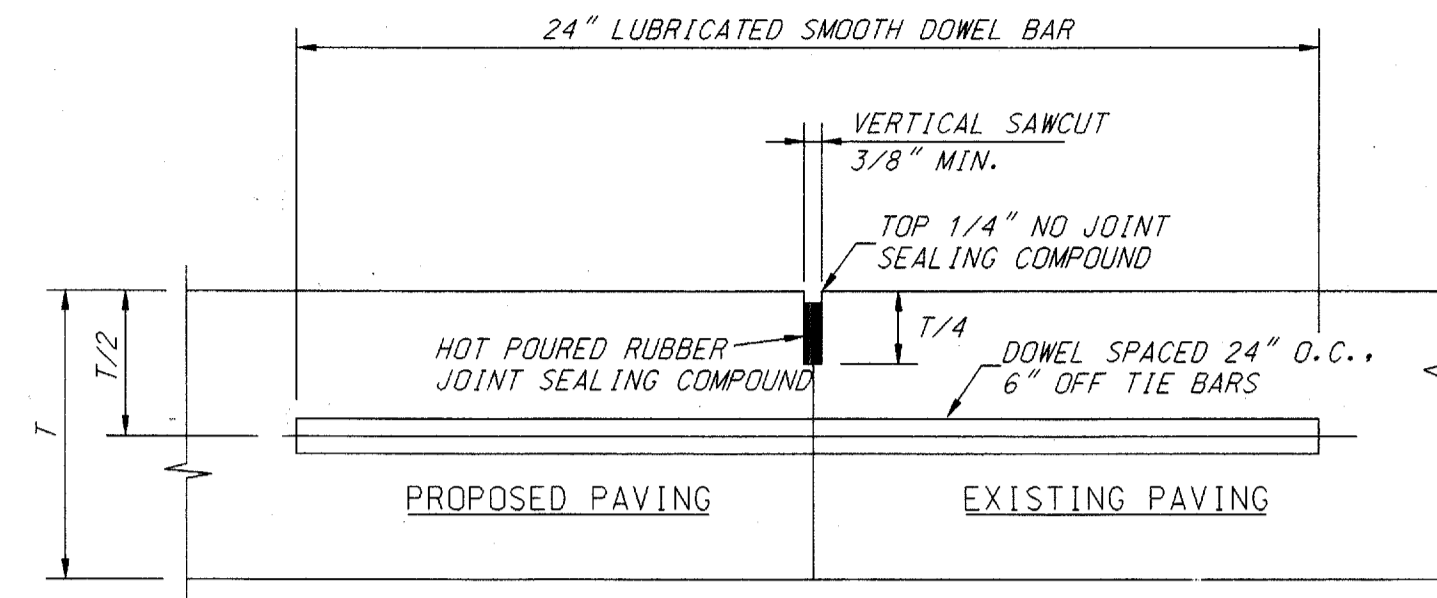
Texas Department of Transportation
Bridge Division

HORIZONTAL INLET
TYPE H

IL-H

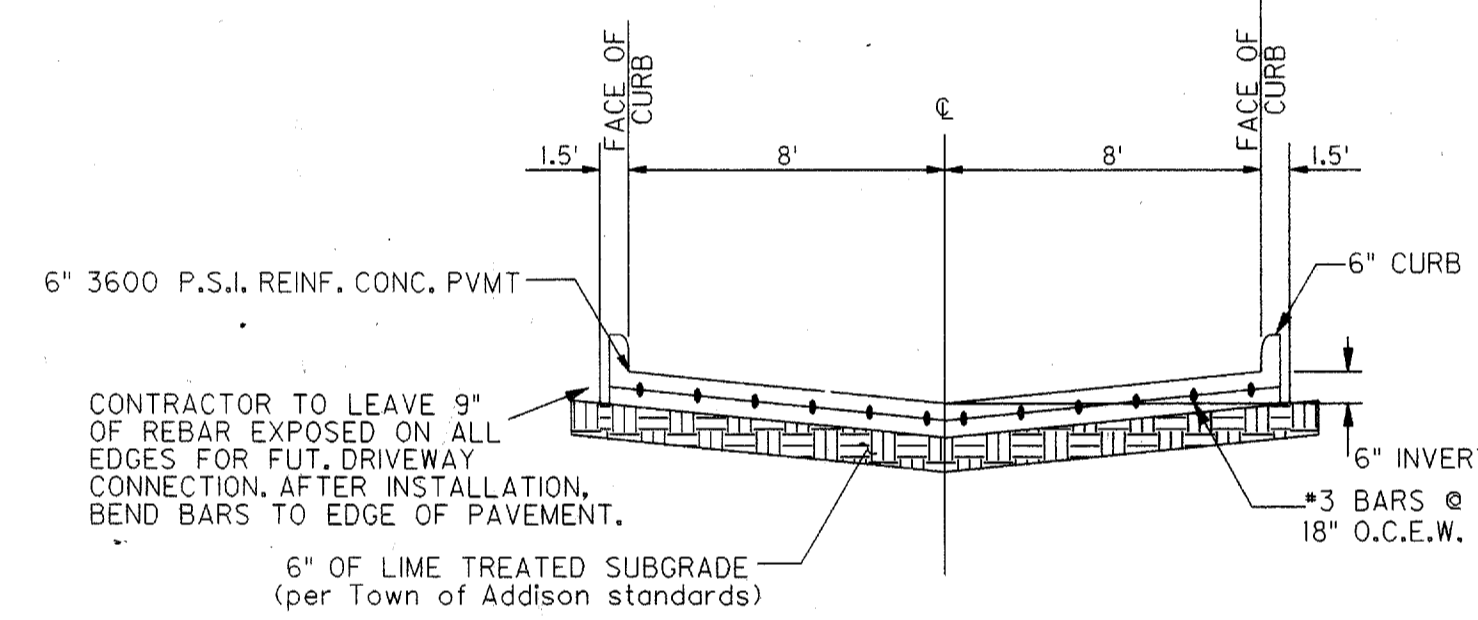
FILE: il-hstde.dgn	DN: TxDOT	CK: TER	DW: MCB	CK: TER/GAF
© TxDOT September 2000	DISTRICT	FEDERAL AID PROJECT		SHEET
REVISIONS				15 OF 17
	COUNTY	CONTROL SECT	JOB	HIGHWAY

USER: rjp1010 JOB: 04-00000000 PLOTTED Thu Mar 09 10:50:00 2006
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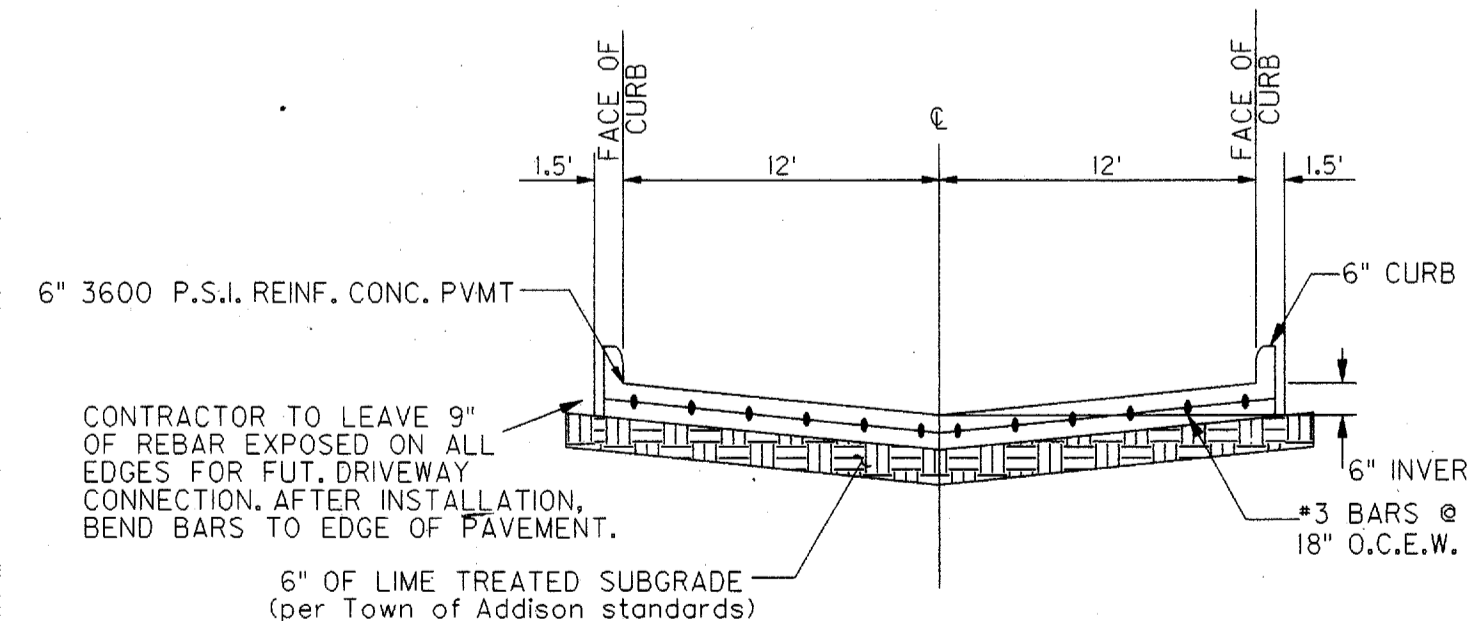


- NOTE:
1. T-8" AND GREATER NO. 6 BAR, T-6" AND LESS NO. 5 BAR
 2. LONGITUDINAL BUTT CONSTRUCTION MAY BE UTILIZED IN PLACE OF LONGITUDINAL HINGED (KEYWAY) JOINT AT CONTRACTOR'S OPTION.
 3. DOWEL BARS SHALL BE DRILLED INTO PAVEMENT HORIZONTALLY BY USE OF A MECHANICAL RIG. HAND DRILLING NOT ACCEPTABLE. DAMAGE TO EXISTING PAVEMENT SHALL BE REMOVED BY CONTRACTOR AND JOINT CONSTRUCTED AT CONTRACTOR'S EXPENSE.
 4. DOWEL BAR SHOWN IS IN ADDITION TO TIE BARS (12" O.C. - 6" OFF DOWELS).
 5. TIE BARS SHALL BE NO. 5 BAR DEFORMED. TIE BAR SHALL HAVE A LENGTH OF 24 INCHES.

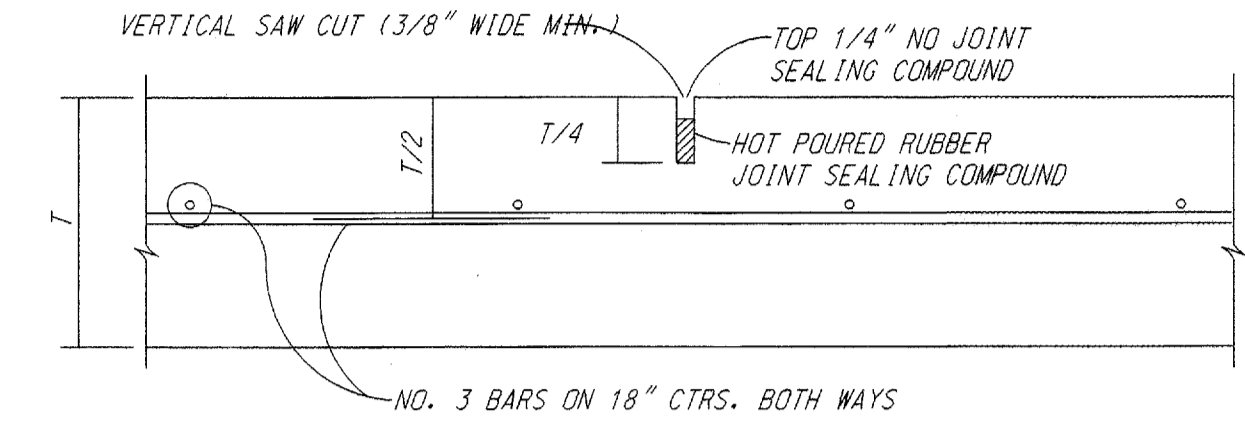
LONGITUDINAL BUTT JOINT



TYPICAL SECTION
SHARED DRIVE D & E
N.T.S.

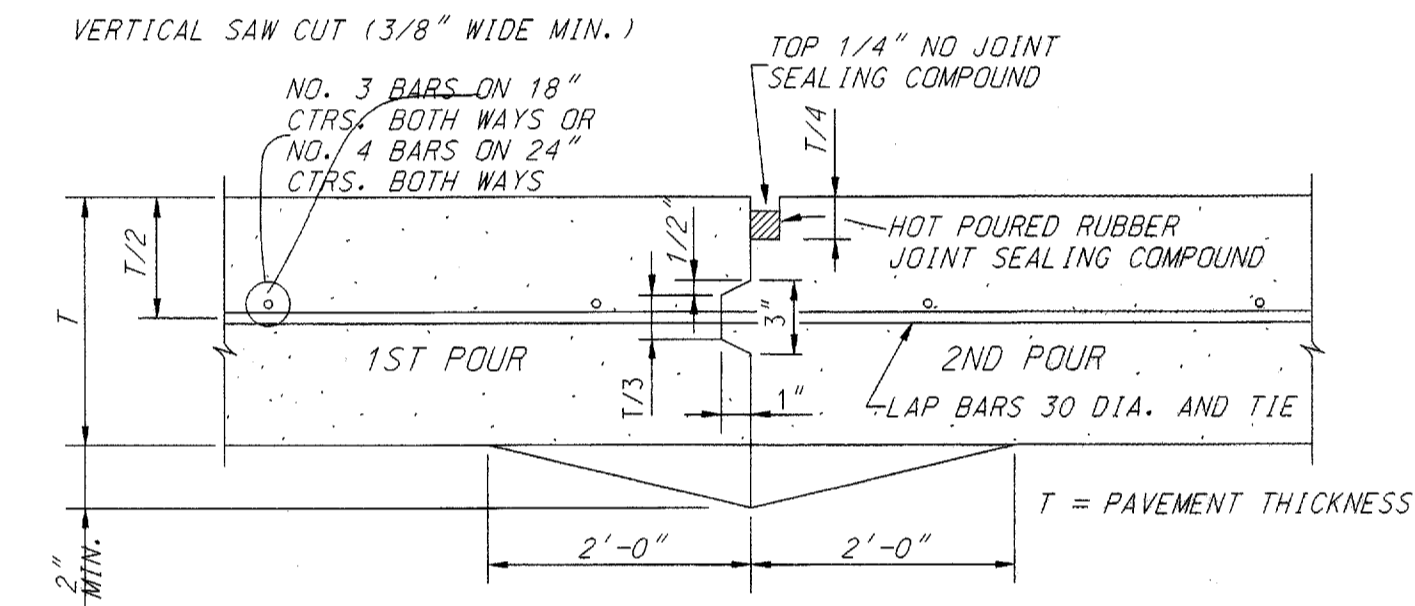


TYPICAL SECTION
SHARED DRIVE A, B, & C
N.T.S.



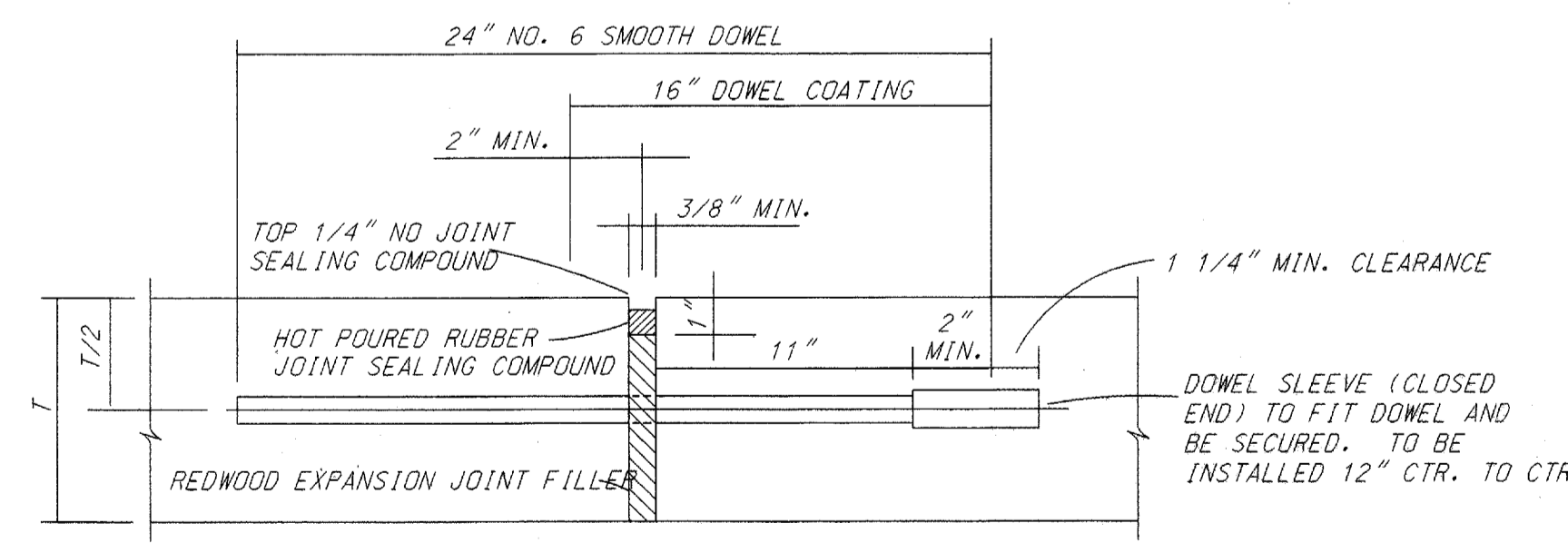
TRANSVERSE JOINTS SPACED 15 FT. C.-C. (MAX.)
LONGITUDINAL JOINTS SPACED 20 FT. C.-C. (MAX.)

SAWED DUMMY JOINT



- CONSTRUCTION JOINT NOTES:
1. CONTRACTOR SHALL PROTECT KEYWAY PRIOR TO SECOND POUR. IF LONGITUDINAL KEYWAY IS DAMAGED, CONTRACTOR SHALL REPAIR WITH THE USE OF LONGITUDINAL BUTT JOINT (DRILL DOWELS INTO FIRST POUR).
 2. THICKENED EDGES ARE REQUIRED FOR FUTURE WIDENING ONLY.

CONSTRUCTION JOINT

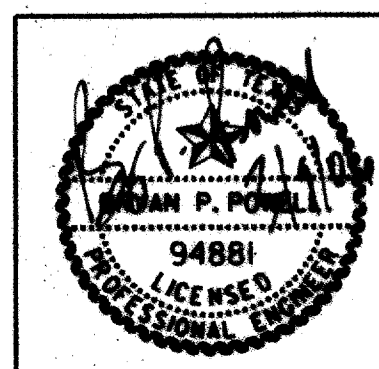


- TRANSVERSE EXPANSION JOINT NOTES:
1. DOWELS AND REINFORCING BARS SHALL BE SUPPORTED BY AN APPROVED DEVICE.
 2. TRANSVERSE EXPANSION JOINTS SHALL BE SPACED AT 60 FT. MAXIMUM AND AT ALL INTERSECTIONS.

TRANSVERSE EXPANSION JOINT

RECORD DRAWING

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PAVING DETAILS				
ADDISON OFFICE CONDOS				
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DESIGN	DRAWN	CHECKED	SCALE	DATE
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FILE	NO.			
01-6628	17 OF 17			