

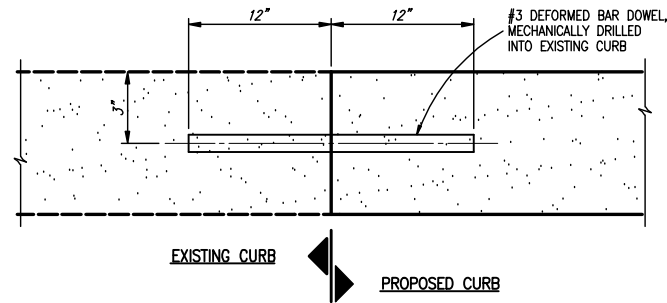
PLOTTED BY: BLOWE ON 5/14/2010

PLOT STYLE: 11x17.dwt

PLOT SCALE: 1:1,010

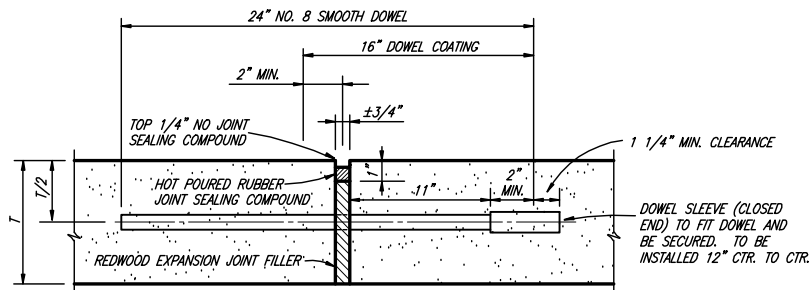
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REVISED: 5/16/10 - BLOWE



CURB DOWEL DETAIL

NOT TO SCALE

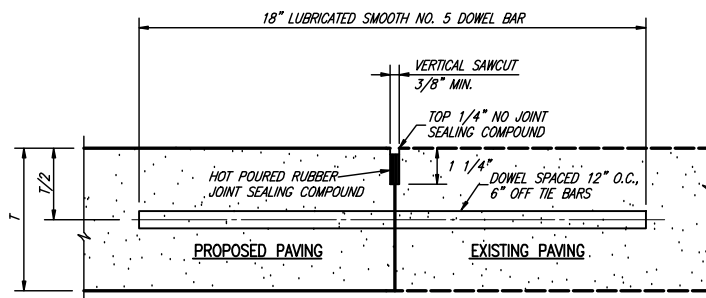


TRANSVERSE EXPANSION JOINT NOTES:

1. DOWELS AND REINFORCING BARS SHALL BE SUPPORTED BY AN APPROVED DEVICE.
2. TRANSVERSE EXPANSION JOINTS SHALL BE SPACED AT 400 FT. MAXIMUM AND AT DRIVEWAY CONNECTIONS.

TRANSVERSE EXPANSION JOINT

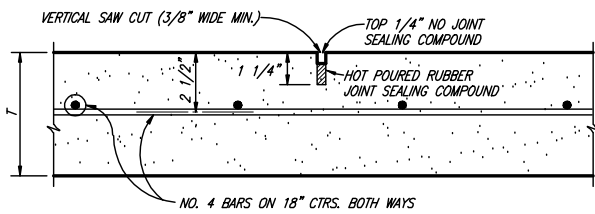
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NOTE:
DOWEL BARS SHALL BE DRILLED INTO PAVEMENT HORIZONTALLY BY USE OF A MECHANICAL OR HAND RIG. HORIZONTAL HAND DRILLING METHOD IS ACCEPTABLE. PUSHING DOWEL BARS INTO GREEN CONCRETE IS NOT ACCEPTABLE.

CONNECTION TO EXISTING PAVEMENT

NOT TO SCALE

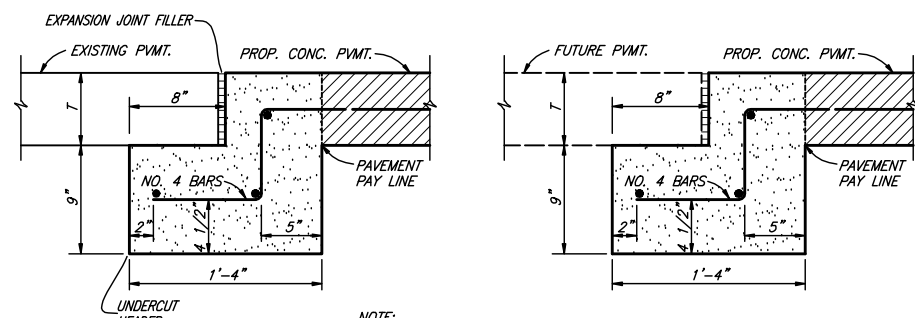


TRANSVERSE JOINTS SPACED 15 FT. C.-C. (MAX.)
LONGITUDINAL JOINTS SPACED 20 FT. C.-C. (MAX.)

SAWED DUMMY JOINT

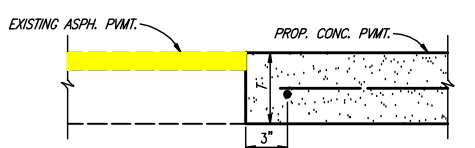
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- #### GENERAL NOTES
1. CONTRACTOR SHALL REMOVE AND PROPERLY DISPOSE THE EXISTING ASPHALT PAVEMENT, STRUCTURE, EXCAVATION, CONCRETE CURB & GUTTER, CONCRETE DRIVEWAYS AND CONCRETE SIDEWALK.
 2. ALL CONCRETE TO BE REMOVED SHALL BE WITH FULL DEPTH SAW CUT. IF JOINT IS WITHIN & OUTSIDE 5 FEET OF R.O.W. OR CURB RETURN, REMOVE CONCRETE AT JOINT. FINAL DETERMINATION TO BE MADE IN FIELD BY THE TOWN OF ADDISON.
 3. CONTRACTOR SHALL PROTECT ALL UNDERGROUND UTILITIES DAMAGE CAUSED BY THE CONTRACTOR SHALL BE REPAIRED AT THE CONTRACTORS EXPENSE.
 4. CONTRACTOR SHALL LAYOUT PROJECT BASED ON COORDINATES SHOWN IN THE PLANS. COORDINATES SHOWN AT
a) CURB RETURNS.
b) CENTER LINES OF PROPOSED ALIGNMENTS.
c) AT BEGINNING AND ENDING OF PROPOSED ALIGNMENTS.
d) AT FIELD SURVEY POINTS.
 5. CONCRETE PAVING SHALL BE 10 INCHES N.C.T.C.O.G. CLASS "A" MIN. SACKS/C.Y.-5 MINIMUM COMPRESSIVE STRENGTH AT 28 DAYS-3600 P.S.I. MAX. W/C RATIO-6.5 REINFORCING SHALL BE NO. 4 DEFORMED BARS ON 18 INCH CENTERS
 6. CONTRACTOR SHALL FURNISH AND PLACE SOLID SOD IN DISTURBED AREAS. (NO PAY ITEM)
 7. PAVEMENT BASE SHALL BE SCARIFIED TO A DEPTH OF 6 INCHES AND COMPACTED TO 95% MODIFIED PROCTOR.
 8. CONTRACTOR SHALL CONSTRUCT PROPOSED PAVEMENT 1/2 THE PROPOSED WIDTH AT A TIME.
 9. CONSTRUCTION JOINTS SHALL HAVE A WELL FORMED KEY WAY OR BE A LONGITUDINAL BUTT JOINT.



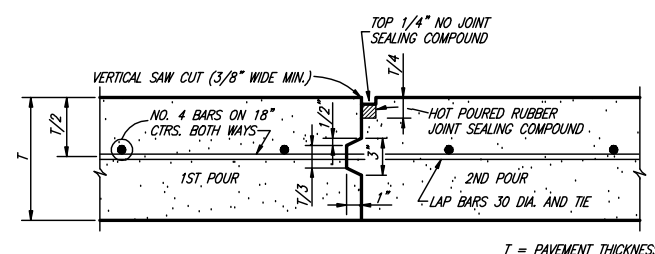
NOTE:
PAVEMENT BARS TO BE BENT DOWN INTO HEADER. PAVEMENT AND HEADER SHALL BE MONOLITHIC.

STREET HEADER



EXISTING PAVEMENT DETAIL

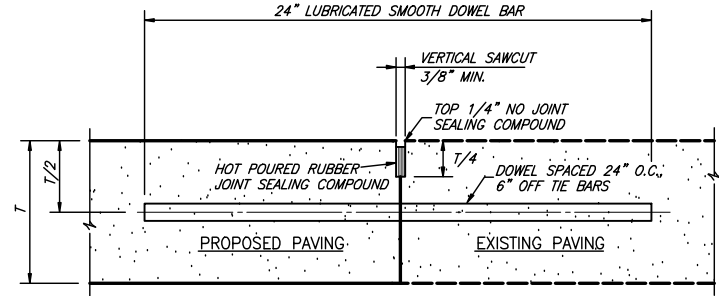
NO SCALE



NOTE:
CONTRACTOR SHALL PROTECT KEYWAY PRIOR TO SECOND POUR. IF LONGITUDINAL KEYWAY IS DAMAGED, CONTRACTOR SHALL REPAIR WITH THE USE OF LONGITUDINAL BUTT JOINT (DRILL DOWELS INTO FIRST POUR).

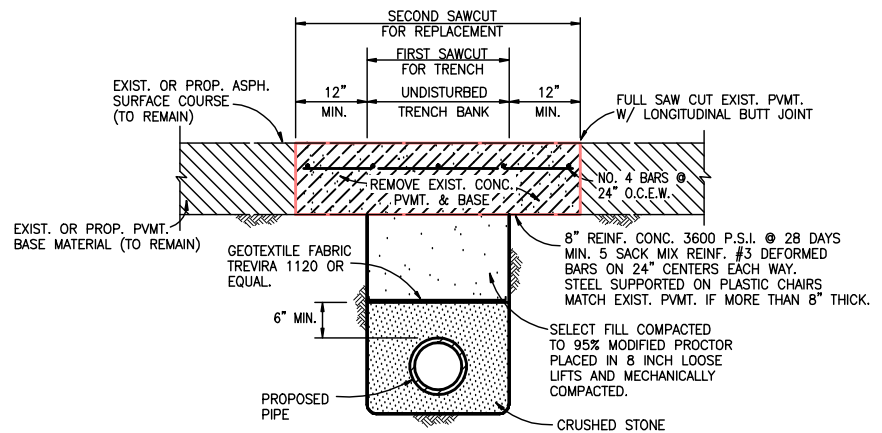
CONSTRUCTION JOINT

NO SCALE



- NOTE:
1. T-8" AND GREATER NO. 6 BAR, T-6" AND LESS NO. 5 BAR
 2. LONGITUDINAL BUTT CONSTRUCTION MAY BE UTILIZED IN PLACE OF LONGITUDINAL HINGED (KEYWAY) JOINT AT CONTRACTORS OPTION.
 3. DOWEL BARS SHALL BE DRILLED INTO PAVEMENT HORIZONTALLY BY USE OF A MECHANICAL RIG. HAND DRILLING NOT ACCEPTABLE. DAMAGE TO EXISTING PAVEMENT SHALL BE REMOVED BY CONTRACTOR AND JOINT CONSTRUCTED AT CONTRACTORS EXPENSE.
 4. DOWEL BAR SHOWN IS IN ADDITION TO TIE BARS (12" O.C.-6" OFF DOWELS).
 5. TIE BARS SHALL BE NO. 5 BAR DEFORMED. TIE BAR SHALL HAVE A LENGTH OF 24 INCHES. TIE BARS ARE REQUIRED TO BE DEFORMED.

LONGITUDINAL BUTT JOINT



CONCRETE STREET REPAIR

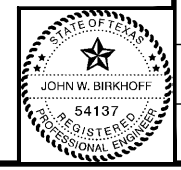
PIPE LINE CROSSING

This record drawing is a compilation of the sealed engineering drawing for this project; modified by addenda, change orders and information furnished by the contractor. The information shown on the record drawings that was provided by the contractor or others not associated with the design engineer cannot be verified for accuracy or completeness. This original sealed drawings are on file at the offices of Birkhoff, Hendricks & Carter, L.L.P.

BY J.W.B. DATE 05/04/2010

THESE DOCUMENTS ARE FOR BIDDING, CONSTRUCTION, AND PERMIT PURPOSES.

DATE: 10/31/06



TOWN OF ADDISON, TEXAS

ADDISON ROAD IMPROVEMENTS
BELT LINE ROAD TO ARAPAHO ROAD PHASE I
DETAILS

BIRKHOFF, HENDRICKS & CONWAY L. L. P.
CONSULTING ENGINEERS
Dallas, Texas

DESIGNED BY: J.W.B. PROJECT: 2002 102 SHEET NO. 24
DRAWN BY: M.W.C. DATE: SEPTEMBER 2006 OF 68 SHEETS