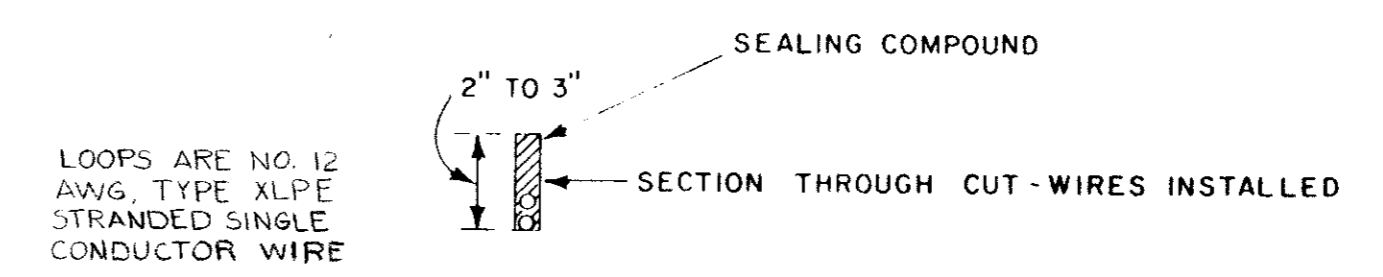
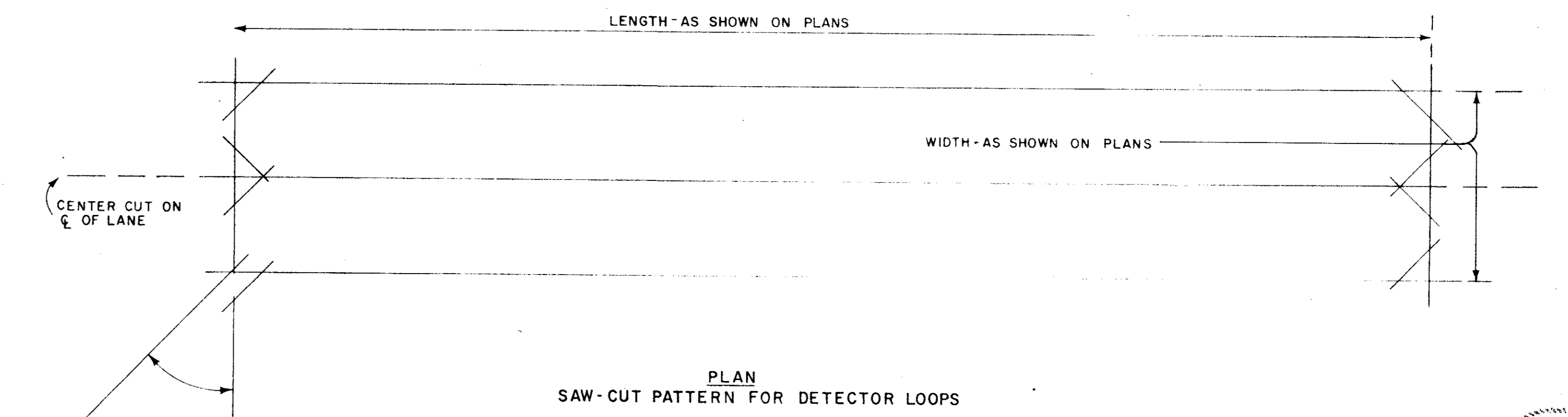
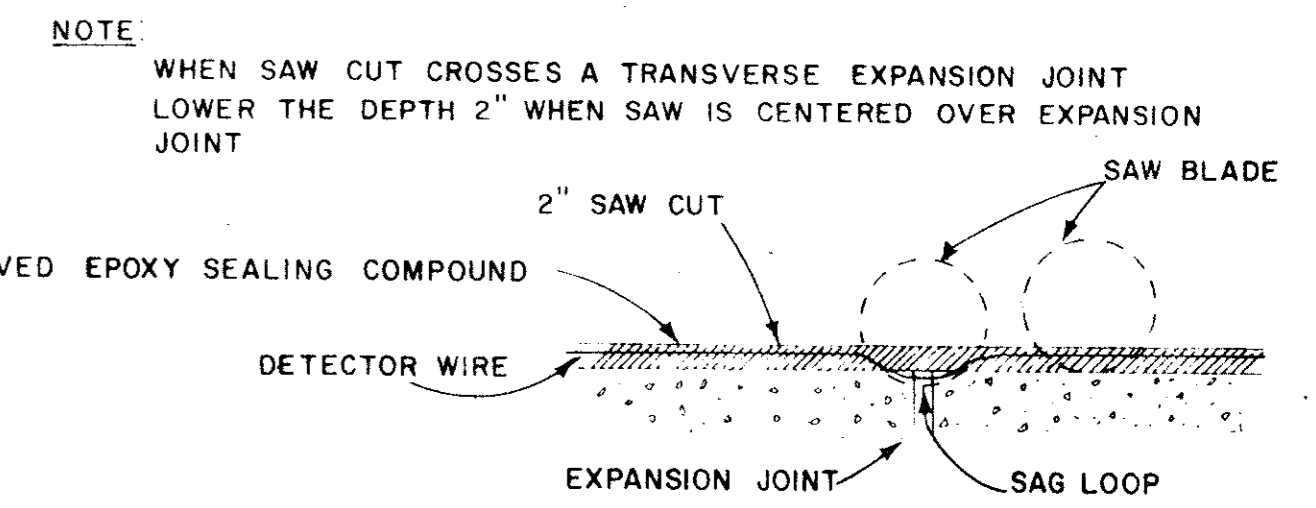
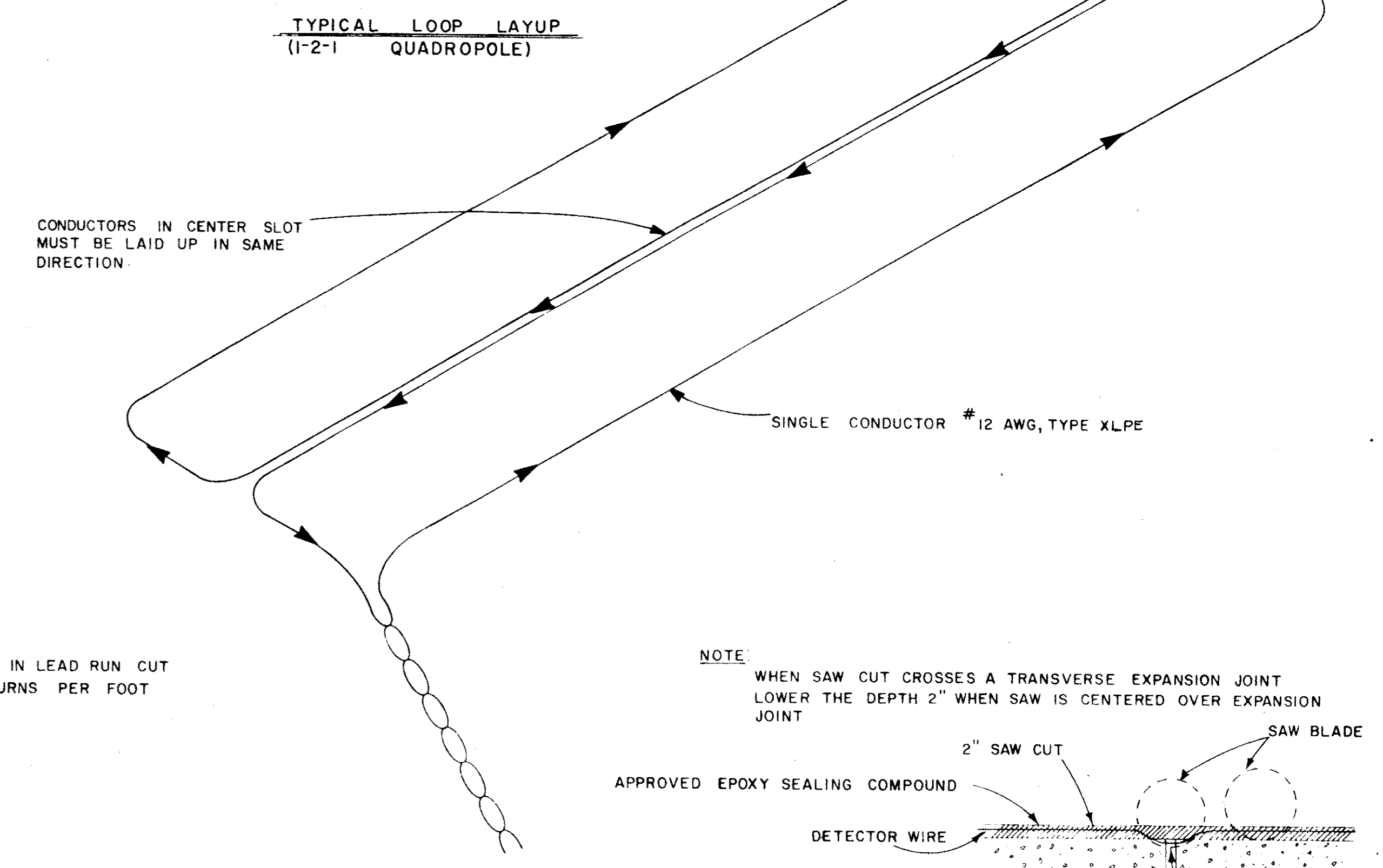
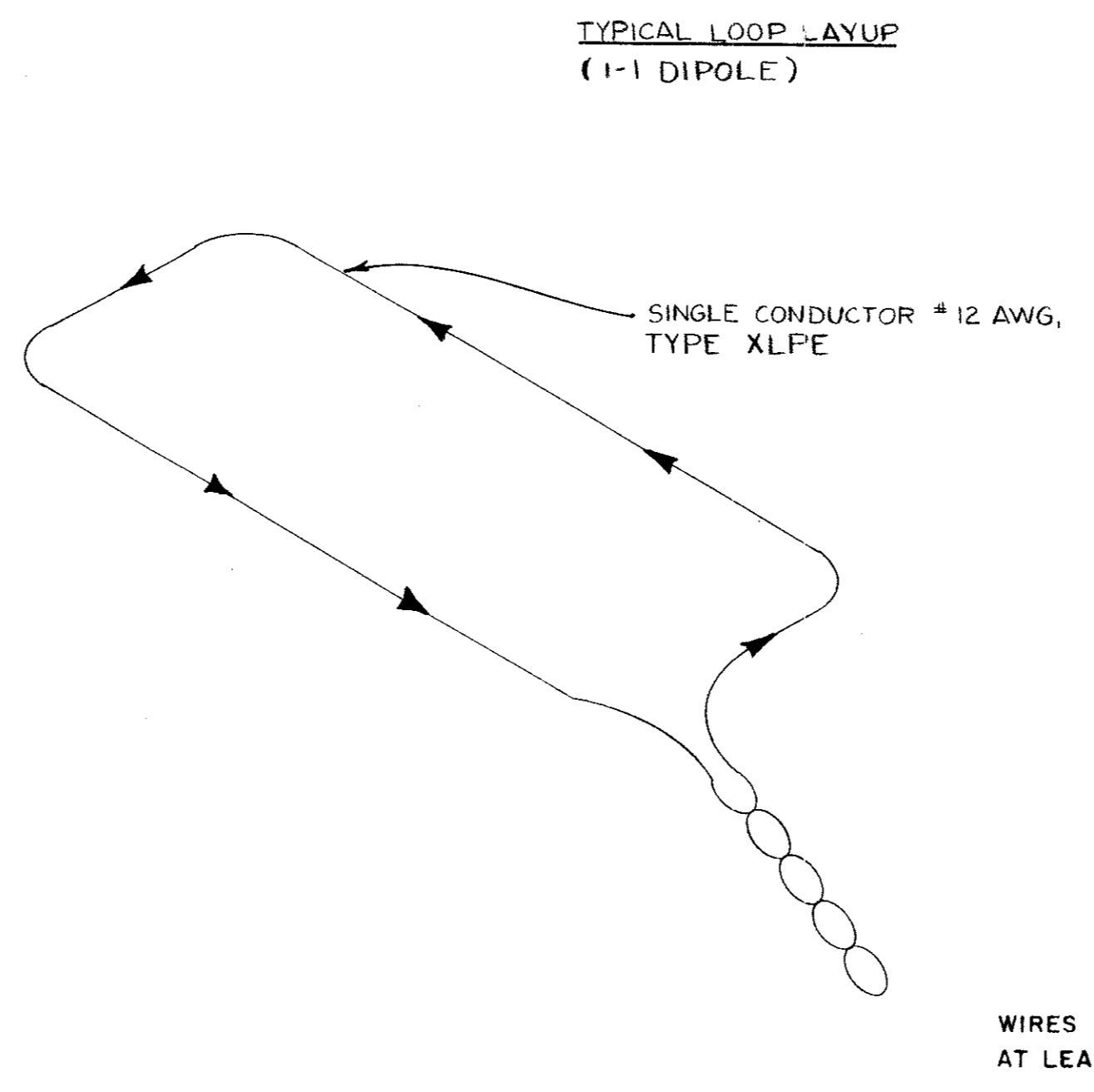
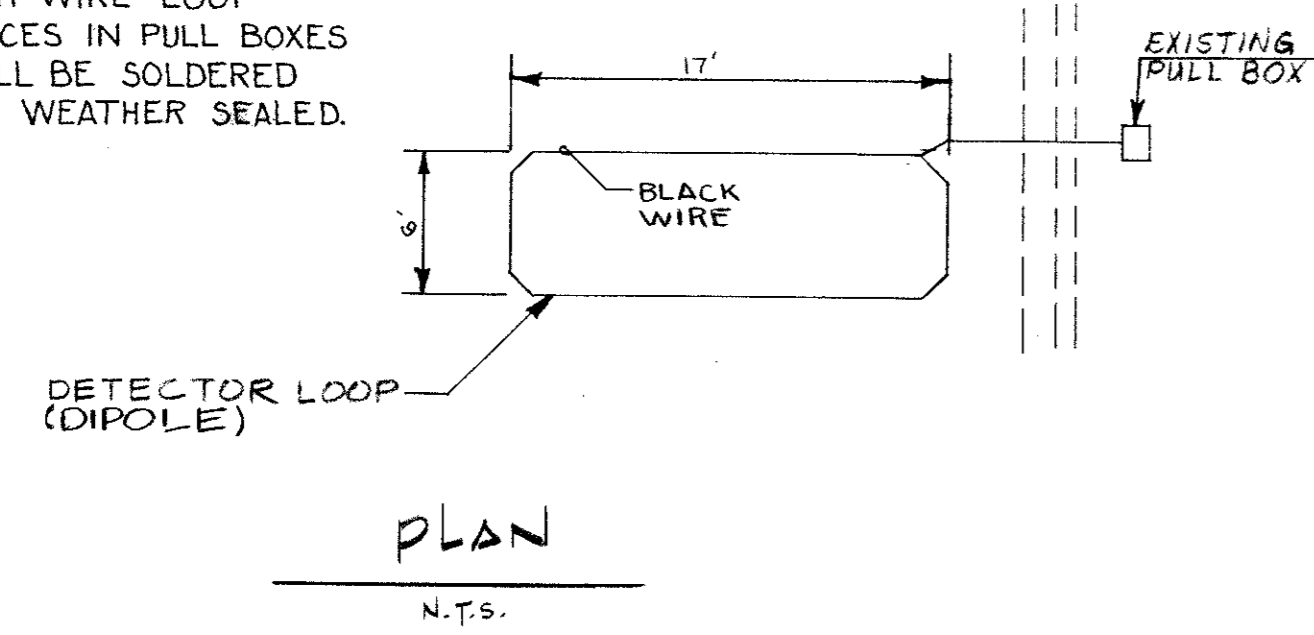
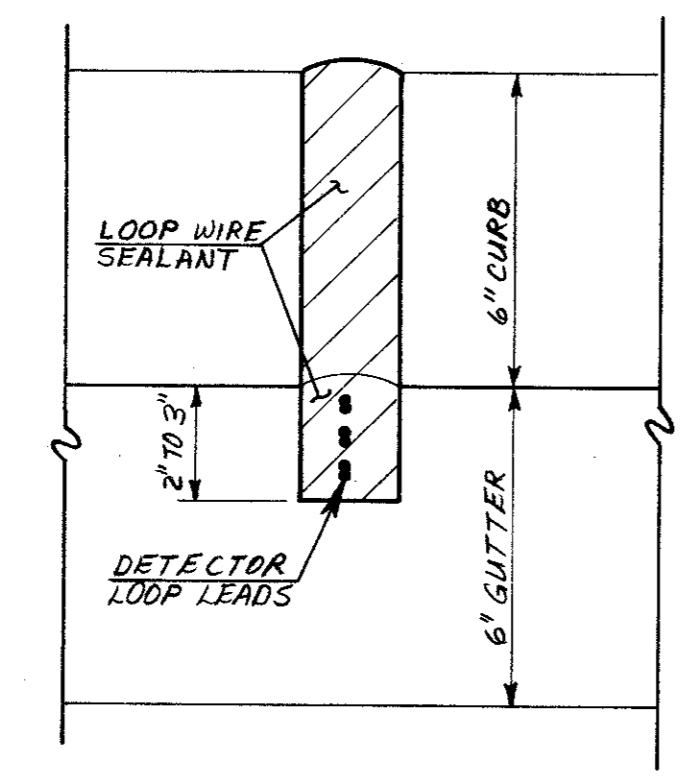
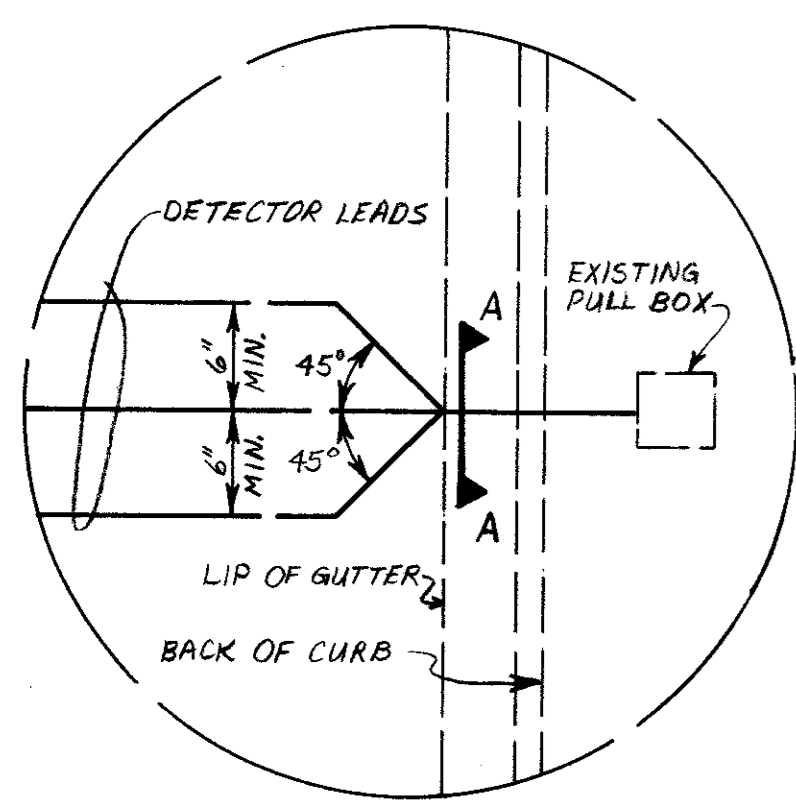


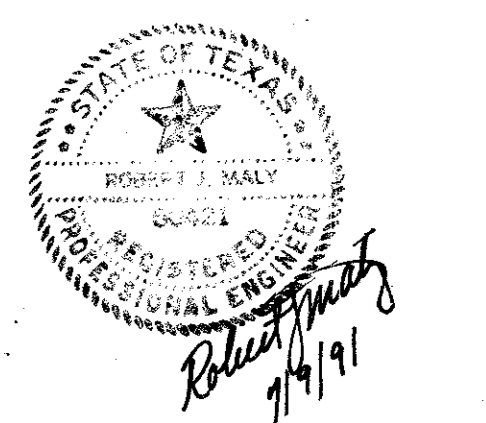
- NOTES:
1. CONTRACTOR TO PROVIDE INDIVIDUAL SAWED CHANNELS THRU CURB & GUTTER FOR EACH WIRE LOOP
 2. SPLICES IN PULL BOXES SHALL BE SOLDERED AND WEATHER SEALED.



1. INSTALLATION OF WIRE LOOPS IS TO BE MADE IN THE SHORTEST TIME PRACTICAL, NOT TO EXCEED A 4 HR. MAX. AND SCHEDULED DURING OFF PEAK HOURS TO MINIMIZE DELAY TO VEHICLE TRAFFIC.
2. THE PAVEMENT CUT IS TO BE CUT WITH A CONCRETE SAW TO NEAT LINES AND LOOSE MATERIAL REMOVED. THE CUT SHOULD BE CLEAN AND DRY WHEN THE SEALING COMPOUND IS PLACED.
3. THE LEAD-IN WIRES ARE TO BE TWISTED A MINIMUM OF TWO TURNS PER FOOT AND REMAIN UNDISTURBED AFTER THE LOOP HAS BEEN TUNED.
4. EACH LOOP IS TO BE RETURNED TO CONTROLLER VIA ONE PAIR OF UNSPLICED SHIELDED LEAD-IN WIRES. MULTIPLE, TWISTED LEADS TO MORE THAN ONE LOOP IN SINGLE LEAD RUN SAW SLOT ARE NOT PERMISSIBLE.



VEHICLE LOOP DETECTOR LAYOUT



NOT TO SCALE

CITY OF ADDISON, TEXAS
ADDISON ROAD OVERLAY

TRAFFIC SIGNAL
DETAILS

Rady & Associates, Inc.
Engineers • Architects • Planners
910 Collier Street Fort Worth, Texas 76102 817/395-6511

DESIGN BY: RJM	REVISIONS BY:	DATE: MAY 1991
DRAWN BY: CLR		JOB NO: 91009
CHECKED BY: WBR/RJM		SHEET NO. 4 OF 36