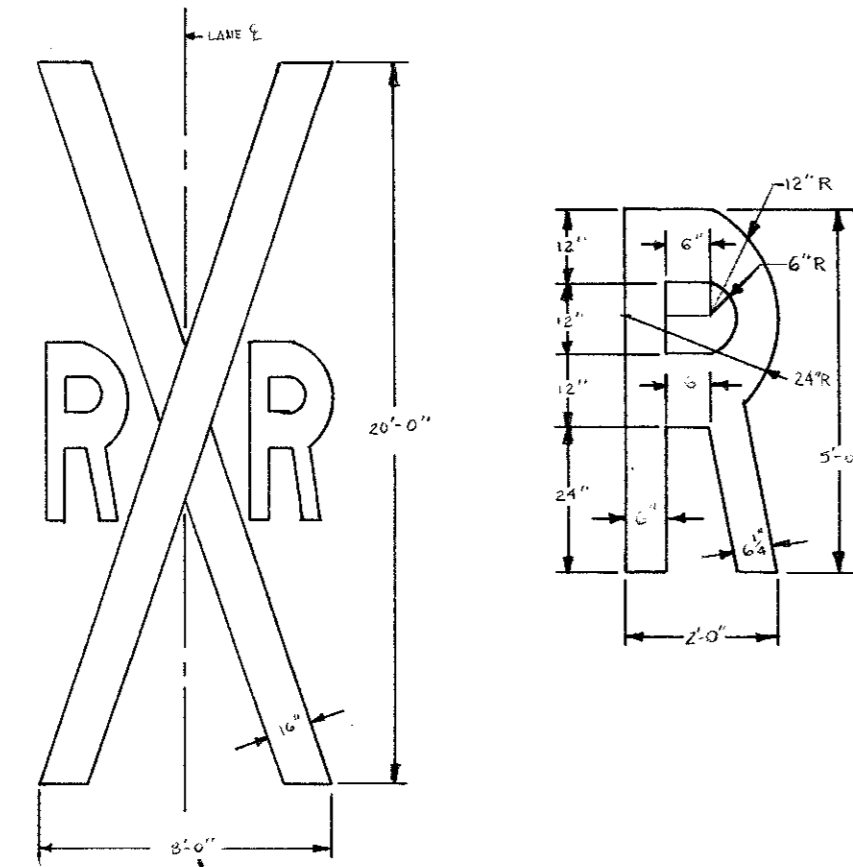


ELONGATED ARROWS FOR PAVEMENT MARKINGS

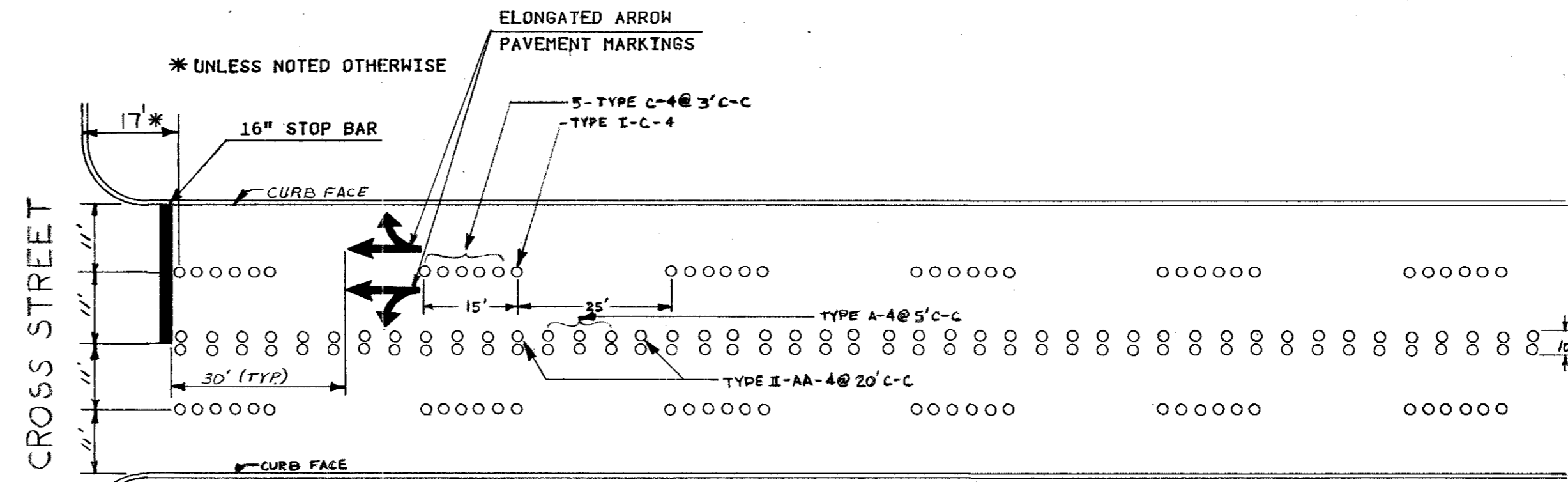


WIDTH MAY VARY ACCORDING TO LANE WIDTH

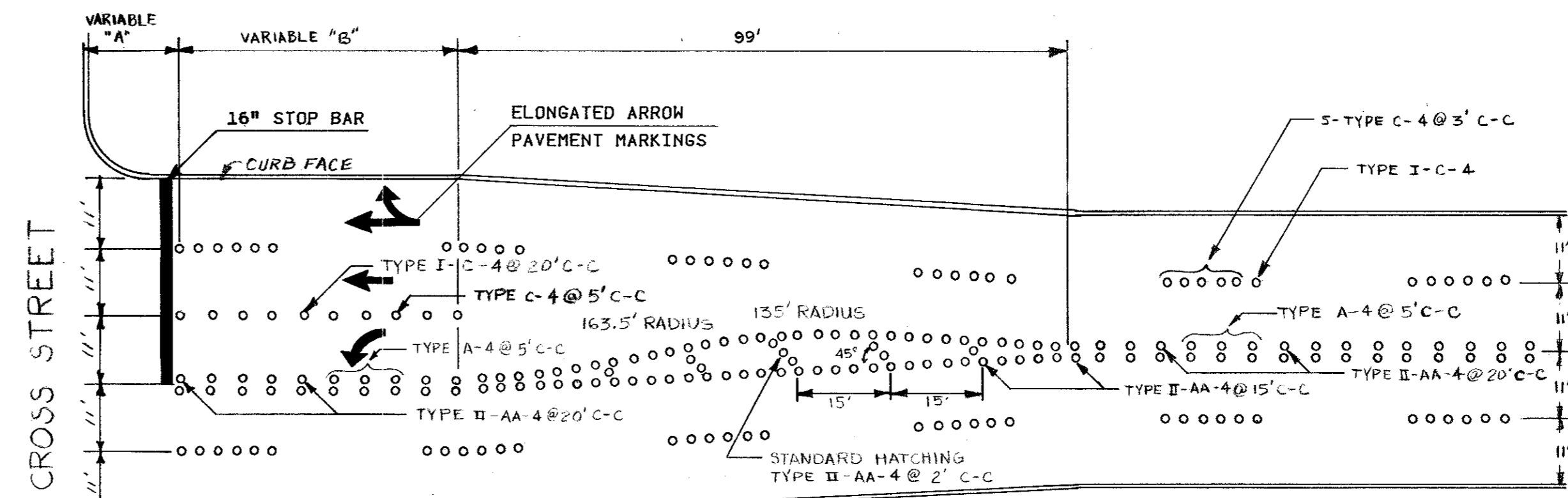
RAILROAD - HIGHWAY GRADE CROSSINGS FOR PAVEMENT MARKINGS

STOP BARS AND PAVEMENT MESSAGES

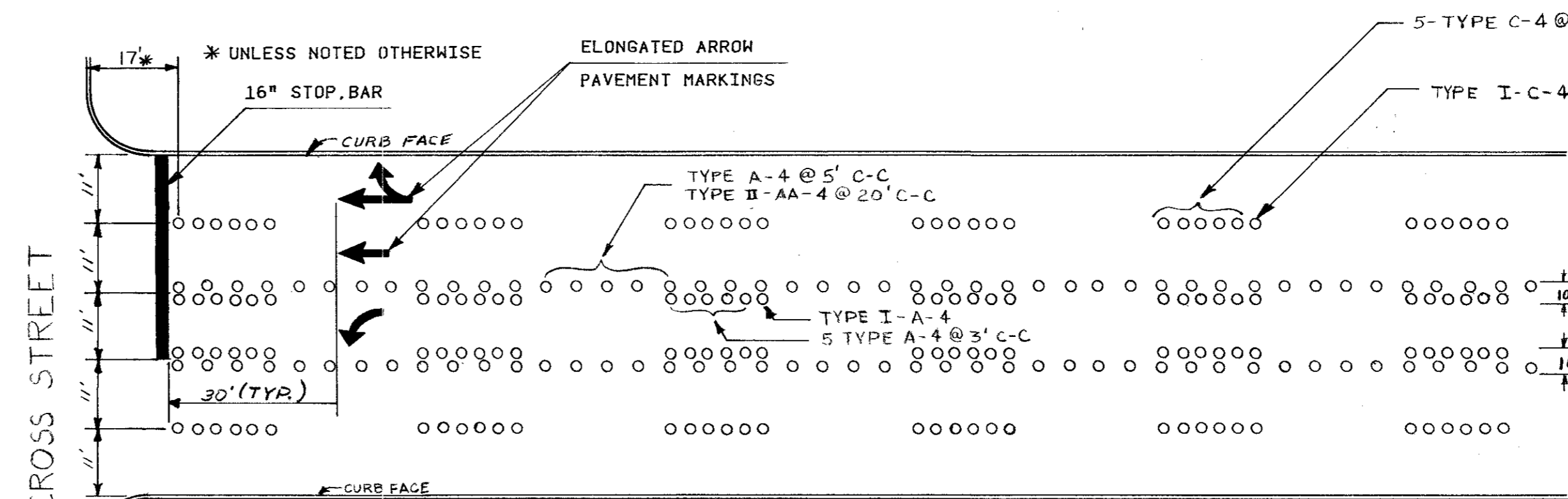
1. STOP BARS AND PAVEMENT MESSAGES SHALL BE ALKALYD THERMOPLASTIC OR APPROVED EQUAL EXCEPT WHERE THEY ARE LOCATED IN AREAS WHICH ARE TO BE MARKED WITH REFLECTORIZED PAINT. A SIXTEEN INCH TAPE WILL BE USED FOR A STOP BAR, NOT FOUR-FOUR INCH TAPES.
2. ALL STOP BARS AND PAVEMENT MESSAGES SHALL CONFORM TO THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES AS AMENDED IN THE 1978 VOLUME.



4 LANES UNDIVIDED



TYPICAL LEFT TURN

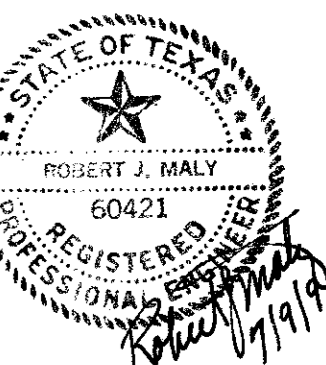


2 WAY LEFT TURN

INTERSECTIONS	TYPICAL LEFT TURN VARIABLES	
	A	B
ADDISON RD.-AIRPORT PKWY.	16'	NORTH 120'
ADDISON RD.-WESTGROVE DR.	13'	SOUTH 99', NORTH 125'
ADDISON RD.-SOJURN DR.	16'	66'

FOR OTHER LEFT TURN LANES SEE APPLICABLE PLAN SHEETS

NOTE: PAVEMENT MARKINGS ON ADDISON ROAD FROM LINDBERG DRIVE TO AIRPORT PARKWAY SHALL BE REFLECTORIZED PAINT. SEE PLAN SHEETS 9 THRU 14.



CITY OF ADDISON, TEXAS
ADDISON ROAD OVERLAY
PAVEMENT MARKING LAYOUT

Rady & Associates, Inc.
 Engineers • Architects • Planners
 910 Collier Street Fort Worth, Texas 76102 817/395-6511

DESIGN BY: RJM	REVISIONS	DATE: MAY 1991
DRAWN BY: CLR	BY:	JOB NO: 91009
CHECKED BY: WBR/RJM		SHEET NO. 2 OF 36