

RAILROAD CROSSING IMPROVEMENTS AND TRACK REALIGNMENT FOR ADDISON ROAD

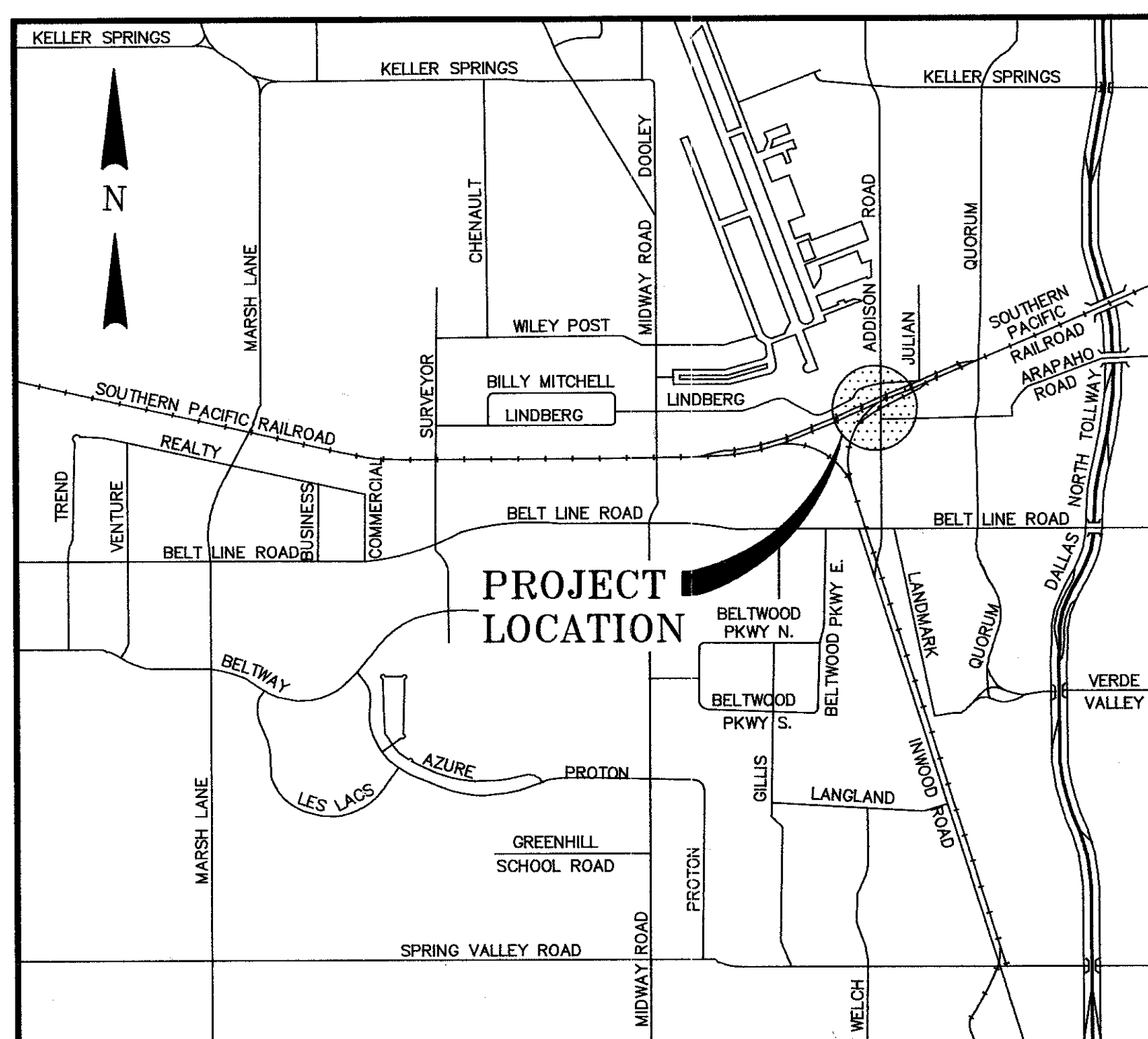
TOWN OF ADDISON

TOWN OF ADDISON

LYNN SPRUILL
MAYOR

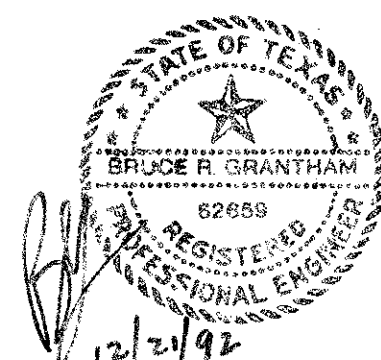
RON WHITEHEAD
CITY MANAGER

CITY COUNCIL
RICH BECKERT
MARY DOLAN
SUE HALPERN
STEVE MINSHEW
DICK WILKE



LOCATION MAP

N.T.S.



EH&A JOB NO. 14163

INDEX OF DRAWINGS

TITLE SHEET	1
QUANTITY SUMMARY/GENERAL NOTES	2
TRACK REALIGNMENT PLAN	3-4
TRACK REALIGNMENT PROFILES	5-7
ADDISON ROAD IMPROVEMENTS	8
CONSTRUCTION DETAILS	9
TRACK REALIGNMENT CROSS-SECTIONS	10-11
ADDISON STANDARD DETAILS	12-15

TOWN OF ADDISON ENGINEER JOHN BAUMGARTNER, P.E.

EH&A PROJECT MANAGER BRUCE GRANTHAM, P.E.



ENGINEER:
ESPEY, HUSTON & ASSOC., INC.
13800 MONTFORT DRIVE
SUITE 230
DALLAS, TEXAS 75240

Addison Rd RFR
 B2-14
 B2-14

SUMMARY OF QUANTITIES

ITEM NO.	DESCRIPTION	UNIT	QUANTITY
101	MOBILIZATION	L.S.	1
102	CLEARING AND GRUBBING	S.Y.	3000
103	UNCLASSIFIED EXCAVATION	C.Y.	1000
104	BORROW	C.Y.	500
105	REMOVE EXISTING ASPHALT PAVEMENT	S.Y.	150
106	REMOVE EXISTING ASPHALT PAVEMENT (4" DEPTH) BY MILLING	S.Y.	30
107	INSTALL NEW 4" ASPHALT PAVEMENT	S.Y.	170
108	SAWCUT EXISTING PAVEMENT (FULL DEPTH)	L.F.	122
109	SAWCUT EXISTING PAVEMENT (4" DEPTH)	L.F.	122
110	CONCRETE STREET HEADER AT RAILROAD	L.F.	122
111	8" REINFORCED CONCRETE PAVEMENT (EARLY HIGH-STRENGTH, MIN. 3600 PSI)	S.Y.	150
112	3" DIAMETER GALVANIZED CONDUIT	L.F.	15
113	SHIFT EXISTING RAILROAD TRACKAGE INCLUDING ALL RAILROAD APPURTENANCES	L.F.	1769
114	CONSTRUCT NEW 11525 WELDED TRACKAGE COMPLETE	L.F.	131
115	RESERVED		
116	RUBBER GRADE CROSSING (OMNI FULL-DEPTH HEAVY DUTY OR APPROVED EQUAL)	L.S.	1
117	RESERVED		
118	RAILROAD FLAGMAN	DAY	50
119	4" PERFORATED PVC UNDERDRAIN	L.F.	132
120	4" NON-PERFORATED PVC UNDERDRAIN W/FITTINGS	L.F.	31
121	2'-0" WIDE PETRO-MAT FABRIC (OR APPROVED EQUAL)	L.F.	122
122	PLACE TEMPORARY TYPE "D" COLD MIX LIMESTONE ROCK ASPHALT PAVEMENT (CLASS A)	C.Y.	20
123	BITUMINOUS BASE	S.Y.	65
124	TYPE I C-4 BUTTONS	EA.	300
125	TYPE C-4 BUTTONS	EA.	1205
126	TYPE II AA-4 BUTTONS	EA.	430
127	TYPE A-4 BUTTONS	EA.	1300
128	STOP BAR	L.F.	150
129	ELONGATED ARROWS	EA.	5

BUTTON AND PAVEMENT MARKING NOTES

- CONTRACTOR SHALL INSTALL NEW BUTTONS IN ACCORDANCE WITH THE DETAILS SHOWN ON SHEETS 2A AND 2B AT THE FOLLOWING LOCATIONS:
 - ADDISON ROAD FROM THE RAILROAD IMPROVEMENTS CONTAINED HEREIN TO AIRPORT PARKWAY;
 - FROM ADDISON ROAD WESTBOUND ON AIRPORT PARKWAY A DISTANCE OF 180 FEET;
 - FROM ADDISON ROAD WESTBOUND ON LINDBERG DRIVE A DISTANCE OF 1400 FEET;
- CONTRACTOR SHALL INSTALL PAVEMENT MARKINGS AS DIRECTED BY THE TOWN OF ADDISON. THESE MARKINGS SHALL BE INSTALLED WITH THE BUTTON LIMITS DESCRIBED IN NOTE 1.

SUGGESTED CONSTRUCTION SEQUENCING

UNLESS OTHERWISE APPROVED BY THE TOWN OF ADDISON, THE CONTRACTOR SHALL EXECUTE THE WORK IN ACCORDANCE WITH THE FOLLOWING SEQUENCING:

PHASE 1
 BETWEEN THE HOURS OF 9:00 P.M. FRIDAY AND 6:00 P.M. SUNDAY ON WEEKEND 1, THE CONTRACTOR SHALL CLOSE ADDISON ROAD AND REMOVE EXISTING RAILROAD TRACKAGE AND APPURTENANCES FROM STATION -0+97± TRACK 1 TO EXISTING TURNOUT T-5 AND INSTALL NEW WELDED TRACK WITH RUBBERIZED CROSSING. FILL REMAINING VOID AREA ON WEST SIDE OF ADDISON ROAD BETWEEN EXISTING PAVEMENT EDGE AND NEW CROSSING BALLAST WITH TEMPORARY COLD MIX ASPHALT (NO SEPARATE PAY).

PHASE 2
 BETWEEN THE HOURS OF 9:00 P.M. FRIDAY AND 6:00 P.M. SUNDAY ON WEEKEND 2, THE CONTRACTOR SHALL REMOVE ALL REMAINING AFFECTED TRACKAGE ON TRACK 1 AND INSTALL TURNOUT T-4 AND T-5.

PHASE 3
 BETWEEN THE HOURS OF 9:00 P.M. FRIDAY AND 6:00 P.M. SUNDAY ON WEEKEND 3, THE CONTRACTOR SHALL CLOSE ADDISON ROAD AND INSTALL NEW ADDISON ROAD PAVEMENT ON THE NORTH SIDE OF TRACK 1.

PHASE 4
 BETWEEN THE HOURS OF 9:00 P.M. FRIDAY AND 6:00 P.M. SUNDAY ON WEEKEND 4, THE CONTRACTOR SHALL CLOSE ADDISON ROAD AND INSTALL NEW ADDISON ROAD PAVEMENT ON THE SOUTH SIDE OF TRACK 1.

PHASE 5
 THE CONTRACTOR SHALL COMPLETE ALL REMAINING TRACK RELOCATION WORK AND COMPLETE PROJECT CLEANUP.

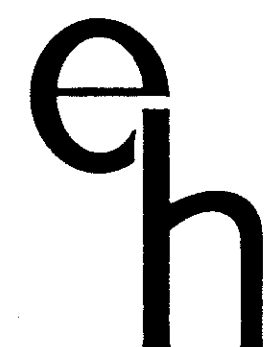
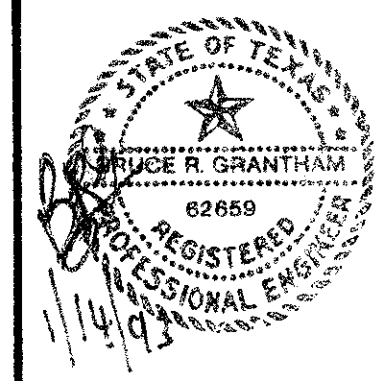
- GENERAL SEQUENCING
- THE MAIN TRACK (TRACK 1) SHALL REMAIN OPEN AT ALL TIMES EXCEPT AS NOTED BETWEEN THE HOURS OF 9:00 P.M. FRIDAY AND 6:00 P.M. SUNDAY ON WEEKENDS 1 THROUGH 4. NO TRAINS OPERATE ON THIS TRACK DURING THESE HOURS.
 - THE CONTRACTOR MAY PERFORM THE WORK OUTLINED IN PHASES 1 THROUGH 4 SIMULTANEOUSLY, PROVIDED THE TIME RESTRICTIONS OF NOTE 1 ARE MET.
 - THE WORK OUTLINED IN PHASE 5 SHOULD PROCEED ON AN ONGOING BASIS IN ACCORDANCE WITH THE RAILROAD TRAIN SCHEDULE AFTER THE WORK IN PHASE 1 IS COMPLETED. CONTRACTOR SHALL COORDINATE WITH THE RAILROAD ON ITS TRAIN SCHEDULE.
 - THE CONTRACTOR SHALL BE RESPONSIBLE FOR PUBLIC SAFETY DURING CONSTRUCTION AND WILL PROVIDE NECESSARY TRAFFIC BARRICADES, WARNING SIGNAGE AND TEMPORARY PAVEMENT MARKINGS TO PROTECT THE CONSTRUCTION SITE. CONSTRUCTION BARRICADES, SIGNAGE AND MARKINGS SHALL BE IN CONFORMANCE WITH THE TEXAS MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES. IN AREAS WHERE LONG TERM NIGHTTIME BARRICADES ARE USED, BARRICADES SHOULD INCLUDE HIGH INTENSITY REFLECTIVE SHEETING (NO SEPARATE PAY ITEM).
 - THE CONTRACTOR SHALL CONTACT THE RAILROAD TO COORDINATE THE SIGNAL RELOCATION WORK WHICH THE RAILROAD WILL PERFORM AND DRILL HOLES AND CUT RAIL AS REQUIRED BY THE RAILROAD (NO SEPARATE PAY ITEM).

GENERAL NOTES

- ALL ROADWAY CONSTRUCTION SHALL CONFORM TO THE NORTH CENTRAL TEXAS COUNCIL OF GOVERNMENTS (NCTCG) SPECIFICATIONS AND TOWN OF ADDISON STANDARDS AND SPECIFICATIONS, EXCEPT AS NOTED.
- ALL RAILROAD CONSTRUCTION SHALL CONFORM TO THE SOUTHERN PACIFIC TRANSPORTATION COMPANY SPECIFICATIONS FOR CONSTRUCTION OF INDUSTRY TRACK SYSTEM.
- ALL LOCATIONS OF UNDERGROUND UTILITY LINES SHOWN ON THE PLANS ARE APPROXIMATE. CONTRACTOR SHALL NOTIFY ALL APPLICABLE UTILITY COMPANIES 48 HOURS PRIOR TO CONSTRUCTION SO THAT UNDERGROUND LINES CAN BE MARKED.
- VERIFY LOCATIONS OF UNDERGROUND ELECTRIC, GAS, PETROLEUM, TELEPHONE FACILITIES AND RAILROAD SIGNAL CABLES PRIOR TO BEGINNING EXCAVATION. IF NECESSARY, DIG TEST DITCHES TO DETERMINE ACTUAL FIELD CONDITIONS. SEE PLAN VIEW FOR WARNING NOTES AND TELEPHONE NUMBERS.
- THE CONTRACTOR SHALL NOT PLACE FILL OR WASTE MATERIAL ON ANY PRIVATE PROPERTY WITHOUT A PRIOR WRITTEN AGREEMENT WITH THE PROPERTY OWNER. IF THE CONTRACTOR PLACES EXCESS MATERIAL IN AREAS WITHOUT WRITTEN PERMISSION, HE WILL BE RESPONSIBLE FOR ALL DAMAGE RESULTING FROM SUCH FILL AND HE SHALL REMOVE THE MATERIAL AT HIS OWN EXPENSE.
- NO TREES SHALL BE CUT.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR PUBLIC SAFETY DURING CONSTRUCTION AND WILL PROVIDE THE NECESSARY TRAFFIC BARRICADES AND WARNING SIGNAGE TO PROTECT THE CONSTRUCTION SITE. ALL BARRICADES, WARNING SIGNS, LIGHTS, DEVICES, ETC. FOR THE GUIDANCE AND PROTECTION OF TRAFFIC AND PEDESTRIANS MUST CONFORM TO THE 1980 TEXAS MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES, AS CURRENTLY AMENDED, BY THE TEXAS STATE DEPARTMENT OF HIGHWAYS AND PUBLIC TRANSPORTATION. (NO SEPARATE PAY ITEM)
- ALL CONCRETE SHALL HAVE A MINIMUM COMPRESSIVE STRENGTH OF 3,600 PSI AT 24 HOURS, UNLESS OTHERWISE NOTED.
- ALL REINFORCING STEEL AND DOWEL BARS IN PAVEMENT SHALL BE SUPPORTED AND MAINTAINED AT THE CORRECT CLEARANCES BY THE USE OF BAR CHAIRS OR OTHER APPROVED SUPPORT.
- THE CONTRACTOR IS RESPONSIBLE FOR KEEPING STREETS AND SIDEWALKS ADJACENT TO THE PROJECT FREE OF MUD AND DEBRIS FROM THE CONSTRUCTION.
- HAND FINISHING OF CONCRETE PAVEMENT SHALL NOT BE A SEPARATE PAY ITEM ON THIS PROJECT.
- THE CONTRACTOR SHALL MAINTAIN ADEQUATE SANITARY FACILITIES FOR USE BY THE WORKERS THROUGHOUT CONSTRUCTION.
- NO PERSON SHALL OPEN, TURN OFF, INTERFERE WITH, ATTACH ANY PIPE OR HOSE TO OR TAP ANY WATER MAIN BELONGING TO THE TOWN UNLESS DULY AUTHORIZED TO DO SO BY THE TOWN OF ADDISON WATER DEPARTMENT (450-2879). ARRANGEMENTS FOR CONSTRUCTION WATER SHALL BE MADE THROUGH THE TOWN OF ADDISON.
- ALL PAVING TO BE REMOVED SHALL BE SAWCUT (FULL DEPTH) TO A NEAT LINE AND THEN REMOVED.
- NO TRAFFIC SIGNS ARE TO BE RELOCATED OR REMOVED WITHOUT PRIOR APPROVAL BY THE TOWN.
- NO RAILROAD SIGNS, WARNING LIGHTS, SIGNAL WIRES OR OTHER SIGNAL EQUIPMENT ARE TO BE RELOCATED OR REMOVED WITHOUT PRIOR APPROVAL BY SOUTHERN PACIFIC TRANSPORTATION COMPANY.
- NO UTILITY MARKERS OR SIGNS ARE TO BE RELOCATED OR REMOVED WITHOUT PRIOR APPROVAL OF THE UTILITY COMPANY.
- THE CONTRACTOR SHALL OBTAIN ALL NECESSARY PERMITS AT LEAST 48 HOURS PRIOR TO START OF ANY CONSTRUCTION. NO FEES WILL BE REQUIRED BY THE TOWN OF ADDISON.
- ALL UTILITIES (i.e. VALVE BOXES, METER BOXES) LOCATED UNDER THE PROPOSED PAVEMENT SHALL BE RAISED OR LOWERED AS REQUIRED AND RESET TO MATCH PROPOSED ELEVATION (NO SEPARATE ITEM).
- CONTRACTOR SHALL INSURE POSITIVE SURFACE DRAINAGE RESULTS FROM ALL GRADING WORK.
- CONTRACTOR SHALL TAKE ALL NECESSARY PRECAUTIONS TO PROTECT EXISTING FACILITIES FROM DAMAGE. ANY DAMAGE TO EXISTING FACILITIES RESULTING FROM CONSTRUCTION WORK SHALL BE REPAIRED AT THE CONTRACTOR'S EXPENSE.
- THE CONTRACTOR SHALL NOT DISTURB OR COME WITHIN 15' OF ANY RAIL OR TRACK STRUCTURE WITHOUT PRIOR AUTHORIZATION IN WRITING FROM SOUTHERN PACIFIC TRANSPORTATION COMPANY. CONTACT SOUTHERN PACIFIC TRANSPORTATION COMPANY AT LEAST 48 HOURS PRIOR TO CONSTRUCTION.
- A RAILROAD FLAGMAN WILL BE REQUIRED WHENEVER WORK IS TO BE PERFORMED WITHIN 15' OF ANY TRACK.
- ALL SIGNAL RELOCATION WORK SHALL BE PERFORMED BY THE SOUTHERN PACIFIC TRANSPORTATION COMPANY (RAILROAD).
- CONTRACTOR SHALL EXCAVATE EXISTING SUBGRADE FROM RELOCATED TRACKS 2, 3, 4, AND 5 FOR USE IN BUILDING SUBGRADE FOR THE NEW TRACK ALIGNMENTS. EXISTING SUBGRADE SHALL BE EXCAVATED TO MATCH THE ELEVATION OF THE ADJACENT UNDISTURBED GROUND. ADDITIONAL SUBGRADE MATERIAL WHICH IS REQUIRED SHALL BE OBTAINED FROM A SOURCE APPROVED BY THE TOWN OF ADDISON AND HAVE A P.I. LESS THAN 15.
- CONTRACTOR SHALL OBTAIN A FIRE PERMIT FROM THE TOWN OF ADDISON FIRE DEPARTMENT. CONTACT 450-7201.

NO.	REVISION	BY	DATE

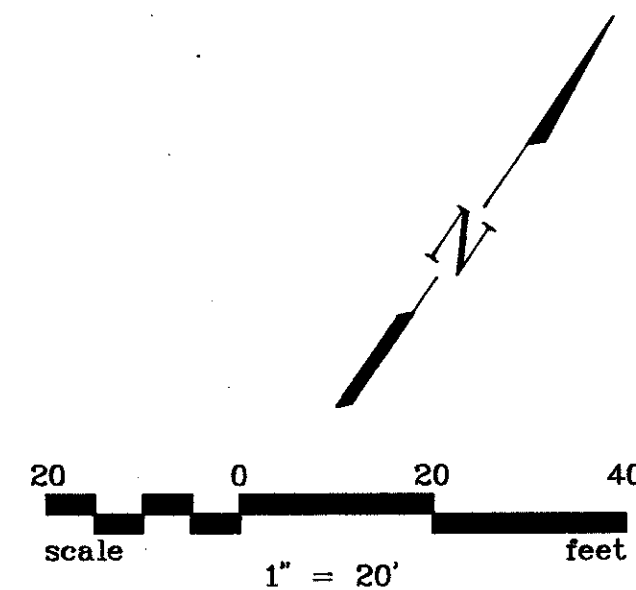
DESIGNED BY: J. WALDBAUER
 DRAWN BY: EH&A
 CHECKED BY: B. GRANTHAM
 SCALE: NO SCALE
 DATE: JULY 1992
 FILE: ADDR\QUANTITY



ESPEY, HUSTON & ASSOCIATES, INC.
 Engineering & Environmental Consultants
 13800 Montfort Drive Suite 230 Dallas, Texas 75240
 (214) 387-0771

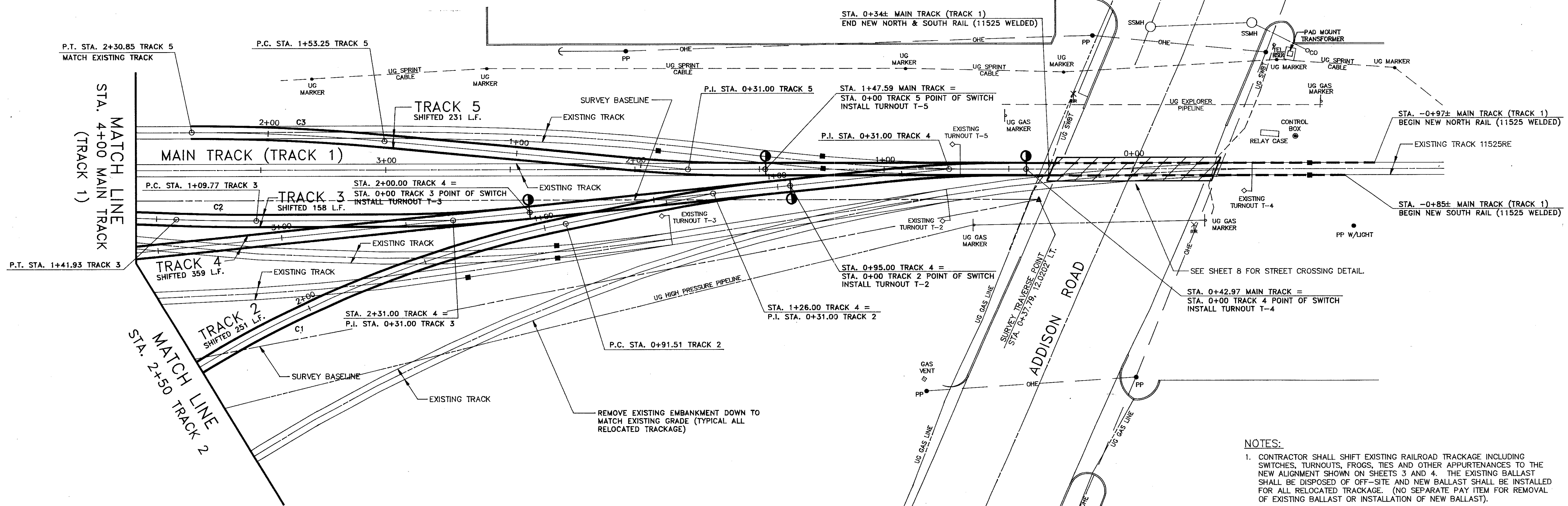
QUANTITY SUMMARY/GENERAL NOTES
 RAILROAD CROSSING IMPROVEMENTS AND
 TRACK REALIGNMENT FOR ADDISON ROAD
 for
 THE TOWN OF ADDISON

SHEET NO. 2
 OF 15 SHEETS
 JOB NO. 14163



LEGEND:

- EXISTING TRACKAGE
- SHIFTED TRACKAGE
- NEW TRACKAGE
- INSULATED JOINT (APPROXIMATE LOCATIONS ONLY)
- EXISTING POINT OF SWITCH
- POINT OF SWITCH FOR RELOCATED TURNOUT



BENCHMARK LIST (TOWN OF ADDISON)

FOUND "□" CUT ON SOUTHEAST CORNER OF CONCRETE WALK AT FRONT ENTRANCE TO 4805 ARAPAHO ROAD. ELEV. 630.66

FOUND "□" CUT 6± EAST AND 8± SOUTH OF GARAGE ENTRANCE FACING SOUTH AT PROFESSIONAL AUTOBODY & GLASS 15293 ADDISON ROAD. ELEV. 629.85

CURVE DATA

CURVE	RADIUS	ARC LENGTH	TANGENT	DELTA	DEGREE OF CURVE
C1	460.05'	402.25'	215.00'	50°05'49"	12°28'44"
C2	819.02'	32.16'	16.08'	02°15'00"	07°00'00"
C3	819.02'	77.60'	38.83'	05°25'44"	07°00'00"

!! WARNING !!

EXISTING UTILITIES IN OR NEAR CONSTRUCTION WORK AREA ARE SHOWN IN THEIR APPROXIMATE LOCATIONS. CONTRACTOR SHALL CONTACT THE FOLLOWING COMPANIES TO DETERMINE ACTUAL FIELD LOCATIONS PRIOR TO CONSTRUCTION.

TOWN OF ADDISON (WATER,SEWER)	450-2840
LONE STAR GAS	263-DIGG
SP TELECOM	1-800-283-4237
US SPRINT	1-800-521-0579
EXPLORER PIPELINE CO.	1-918-493-5100
CONOCO PIPELINE CO.	1-800-833-6012
SOUTHWESTERN BELL	234-7084
T.U. ELECTRIC	323-8907
STORER CABLE	647-5711

NOTE: PETROLEUM PIPELINE. DIGGING PERMIT REQUIRED. CONTACT:
ADDISON FIRE DEPARTMENT 450-7157

- NOTES:**
- CONTRACTOR SHALL SHIFT EXISTING RAILROAD TRACKAGE INCLUDING SWITCHES, TURNOUTS, FROGS, TIES AND OTHER APPURTENANCES TO THE NEW ALIGNMENT SHOWN ON SHEETS 3 AND 4. THE EXISTING BALLAST SHALL BE DISPOSED OF OFF-SITE AND NEW BALLAST SHALL BE INSTALLED FOR ALL RELOCATED TRACKAGE. (NO SEPARATE PAY ITEM FOR REMOVAL OF EXISTING BALLAST OR INSTALLATION OF NEW BALLAST).
 - RELOCATE COMPLETE TURNOUTS T-2, T-3, T-4 AND T-5 AS SHOWN ON THE PLANS.
 - CONTRACTOR SHALL REPLACE ALL SUBSTANDARD TIES, BALLAST AND OTHER TRACK APPURTENANCES AS REQUIRED BY THE SOUTHERN PACIFIC TRANSPORTATION COMPANY (NO SEPARATE PAY ITEM).
 - NEW TRACK AT ADDISON ROAD CROSSING SHALL BE 11525 WELDED RAIL INCLUDING NEW TIES, PLATES, BALLAST, UNDERDRAINS AND OTHER APPURTENANCES.
 - CONTRACTOR SHALL FIELD VERIFY ALL NEW TRACK AND EXISTING TRACK RELOCATION DIMENSIONS PRIOR TO STARTING CONSTRUCTION. THE ENGINEER SHALL BE CONTACTED IMMEDIATELY IF ANY DISCREPANCY IS FOUND.
 - CONTRACTOR SHALL DISPOSE OF ALL UNUSED RAILROAD TRACKAGE AND APPURTENANCES OFF-SITE (NO SEPARATE PAY ITEM).
 - ALL RAILROAD CONSTRUCTION SHALL BE PERFORMED IN ACCORDANCE WITH SOUTHERN PACIFIC TRANSPORTATION COMPANY STANDARDS. REFER TO TECHNICAL SPECIFICATIONS FOR DETAILS. ALL TRACKAGE WORK INCLUDING TURNOUTS AND APPURTENANCES IS SUBJECT TO APPROVAL BY THE RAILROAD.
 - ALL FILL AREAS SHALL BE COMPACTED IN MAXIMUM 8" LIFTS TO 95% STANDARD PROCTOR DENSITY AS DEFINED BY THE STANDARD COMPACTION TEST (ASTM D-698) AT A MOISTURE CONTENT WITHIN ±3% O.T.M.

NO.	REVISION	BY	DATE

DESIGNED BY: J. WALDBAUER
 DRAWN BY: EH&A
 CHECKED BY: B. GRANTHAM
 SCALE: 1" = 20'
 DATE: JULY 1992
 FILE: \ADRR\TRPLAN1

ESPEY, HUSTON & ASSOCIATES, INC.
 Engineering & Environmental Consultants
 13800 Montfort Drive Suite 230 Dallas, Texas 75240
 (214) 387-0771

TRACK REALIGNMENT PLAN

RAILROAD CROSSING IMPROVEMENTS AND TRACK REALIGNMENT FOR ADDISON ROAD

for THE TOWN OF ADDISON

SHEET NO. 3 OF 15 SHEETS
 JOB NO. 14163

SURVEY TRAVERSE POINT
STA. 7+27.06, 11.9171' LT.

SURVEY BASELINE

P.T. STA. 5+85.21 TRACK 4
MATCH EXISTING TRACK

STA. 3+08.70 TRACK 3
MATCH EXISTING TRACK

TRACK 3
SHIFTED 151 L.F.

P.C. STA. 1+91.92 TRACK 3

TRACK 4
SHIFTED 227 L.F.

P.T. STA. 2+18.46 TRACK 3

P.C. STA. 5+02.60 TRACK 4

EXISTING TRACK TRACK 8540

MAIN TRACK (TRACK 1)

EXISTING TRACK 110RE

EXISTING TRACK 85ASCE

EXISTING TRACK 8540

MATCH LINE
STA. 4+00 MAIN TRACK
(TRACK 1)

MATCH LINE
STA. 2+50 TRACK 2

SURVEY TRAVERSE POINT
STA. 3+50.57, 33.4342' RT.

SURVEY BASELINE

TRACK 2
SHIFTED 392 L.F.

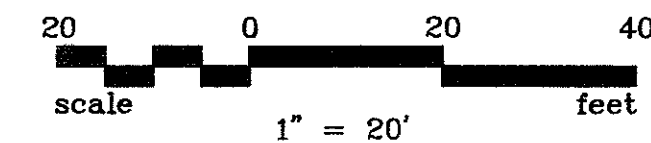
EXISTING TRACK 112-28RE

P.T. STA. 4+93.76 TRACK 2

STA. 6+42.24 TRACK 2
MATCH EXISTING TRACK

EXISTING TRACK 112-28RE

DBL. 4'X2X26'
BOX CULVERT



BENCHMARK LIST (TOWN OF ADDISON)

FOUND "□" CUT ON SOUTHEAST CORNER OF CONCRETE WALK AT FRONT ENTRANCE TO 4805 ARAPAHO ROAD. ELEV. 630.66

FOUND "□" CUT 6± EAST AND 8± SOUTH OF GARAGE ENTRANCE FACING SOUTH AT PROFESSIONAL AUTOBODY & GLASS 15293 ADDISON ROAD. ELEV. 629.85

CURVE DATA

CURVE	RADIUS	ARC LENGTH	TANGENT	DELTA	DEGREE OF CURVE
C1	460.05'	402.25'	215.00'	50°05'49"	12°28'44"
C4	819.02'	26.54'	13.27'	01°51'23"	07°00'00"
C5	819.02'	82.61'	41.34'	05°46'45"	07°00'00"

!! WARNING !!

EXISTING UTILITIES IN OR NEAR CONSTRUCTION WORK AREA ARE SHOWN IN THEIR APPROXIMATE LOCATIONS. CONTRACTOR SHALL CONTACT THE FOLLOWING COMPANIES TO DETERMINE ACTUAL FIELD LOCATIONS PRIOR TO CONSTRUCTION.

TOWN OF ADDISON (WATER,SEWER) 450-2840
LONE STAR GAS 263-DIGG
SP TELECOM 1-800-283-4237
US SPRINT 1-800-521-0579
EXPLORER PIPELINE CO. 1-918-493-5100
CONOCO PIPELINE CO. 1-800-833-6012
SOUTHWESTERN BELL 234-7084
T.U. ELECTRIC 323-8907
STORER CABLE 647-5711

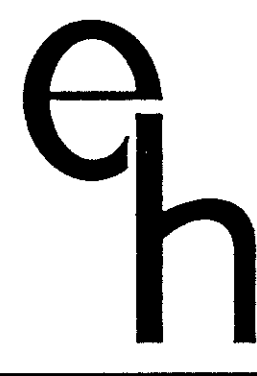
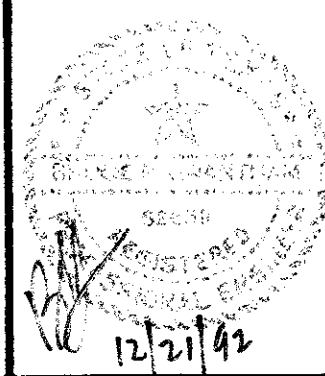
NOTE: PETROLEUM PIPELINE. DIGGING PERMIT REQUIRED. CONTACT:
ADDISON FIRE DEPARTMENT 450-7157

LEGEND:

- EXISTING TRACKAGE
- SHIFTED TRACKAGE
- NEW TRACKAGE
- INSULATED JOINT (APPROXIMATE LOCATIONS ONLY)
- EXISTING POINT OF SWITCH
- POINT OF SWITCH FOR RELOCATED TURNOUT

NO.	REVISION	BY	DATE

DESIGNED BY: J. WALDBAUER
DRAWN BY: FH&A
CHECKED BY: B. GRANTHAM
SCALE: 1" = 20'
DATE: JULY 1992
FILE: \ADDDR\TRPLAN2

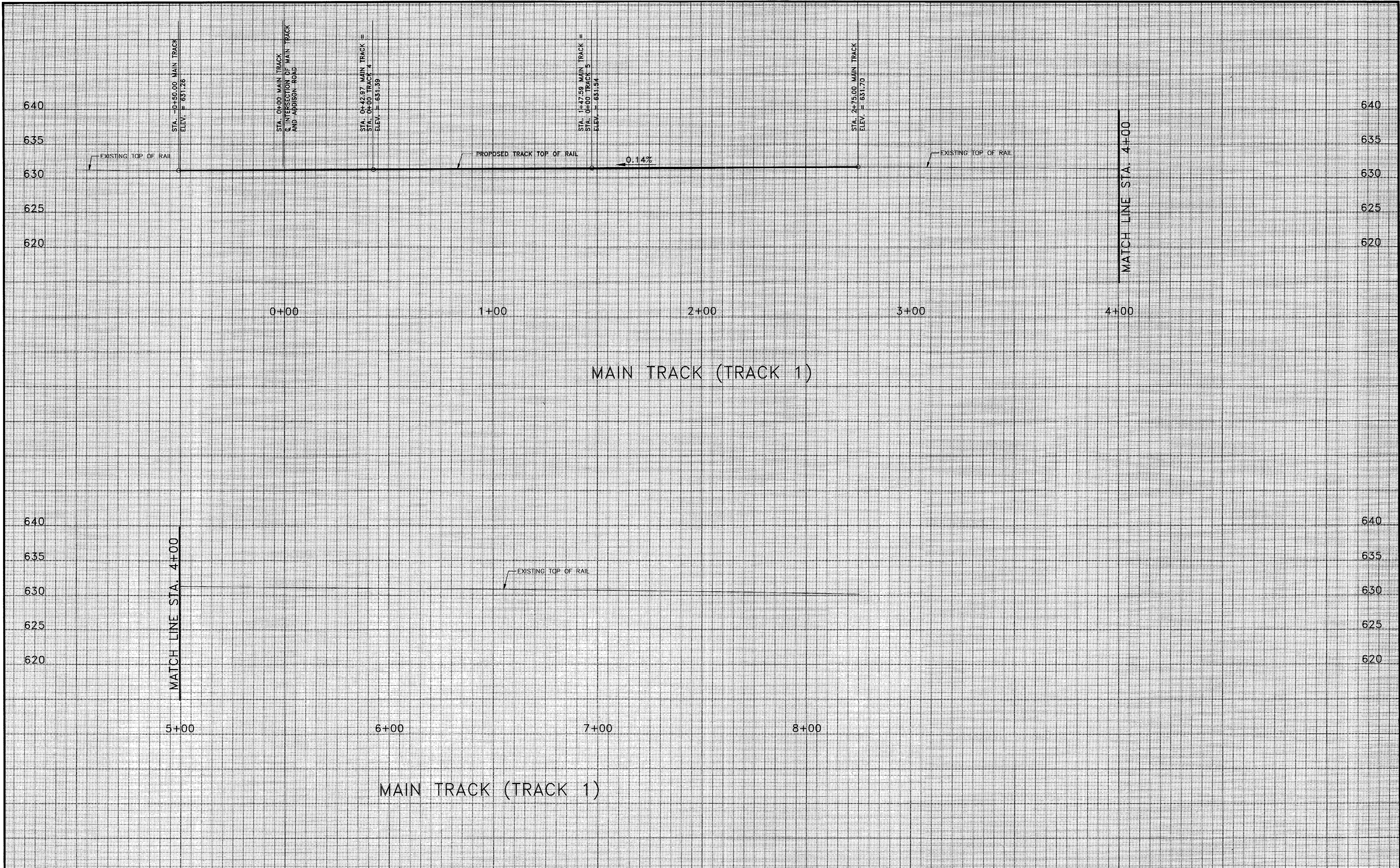


ESPEY, HUSTON & ASSOCIATES, INC.
Engineering & Environmental Consultants
13800 Montfort Drive Suite 230 Dallas, Texas 75240
(214) 387-0771

TRACK REALIGNMENT PLAN

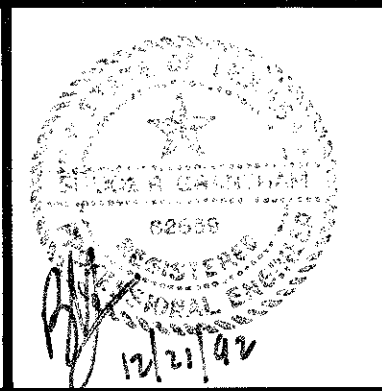
RAILROAD CROSSING IMPROVEMENTS AND
TRACK REALIGNMENT FOR ADDISON ROAD
for
THE TOWN OF ADDISON

SHEET NO. 4
OF 15 SHEETS
JOB NO. 14163



NO.	REVISION	BY	DATE

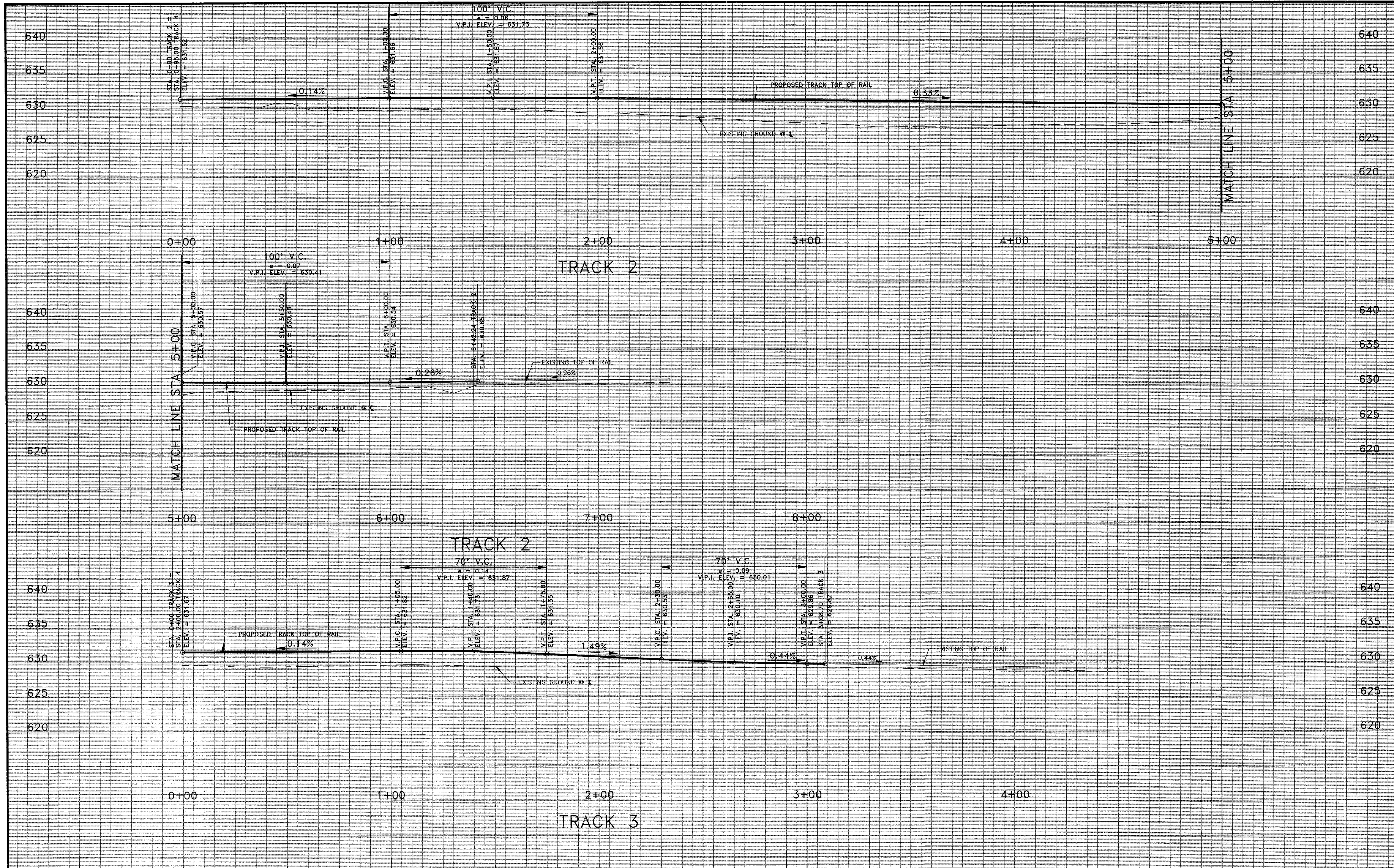
DESIGNED BY: J. WALDBAUER
 DRAWN BY: EH&A
 CHECKED BY: B. GRANTHAM
 SCALE: 1"=20'H; 1"=6'V
 DATE: JULY 1992
 FILE: \ADDRR\PROFILE



eh ESPEY, HUSTON & ASSOCIATES, INC.
 Engineering & Environmental Consultants
 17811 Waterview Parkway Dallas, Texas 75252
 (214) 669-9600

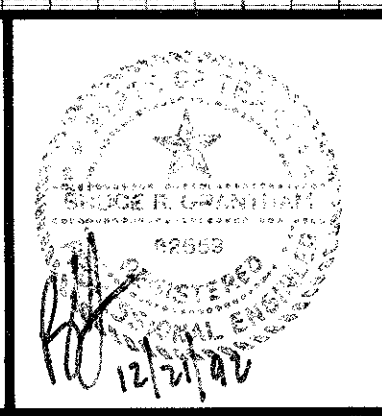
TRACK REALIGNMENT PROFILES
 RAILROAD CROSSING IMPROVEMENTS AND
 TRACK REALIGNMENT FOR ADDISON ROAD
 for
 THE TOWN OF ADDISON

SHEET NO. 5
 OF 15 SHEETS
 JOB NO. 14163



NO.	REVISION	BY	DATE

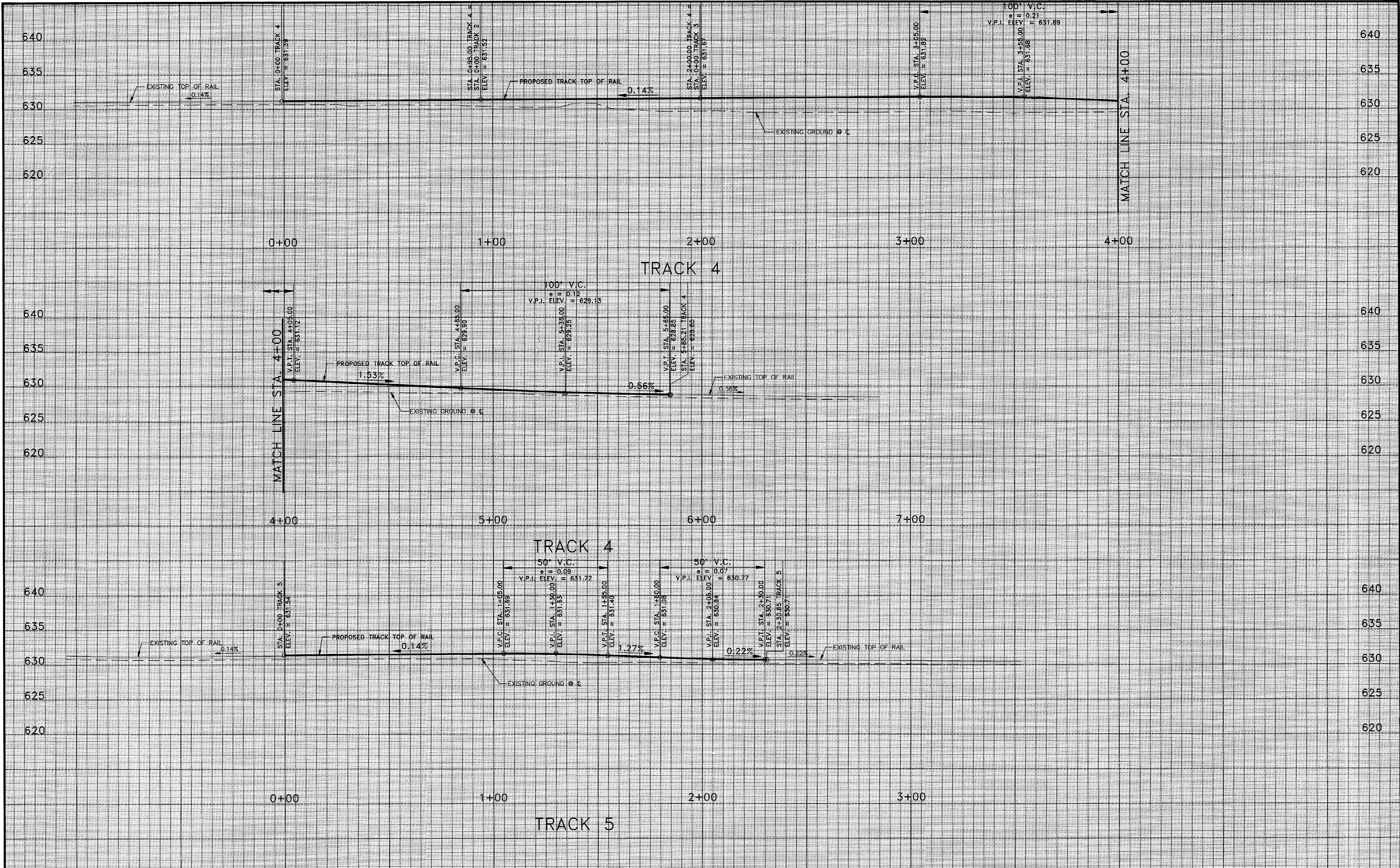
DESIGNED BY: J. WALDBAUER
 DRAWN BY: EH&A
 CHECKED BY: B. GRANTHAM
 SCALE: 1"=20'H; 1"=6'V
 DATE: JULY 1992
 FILE: \ADRR\PROFILE2



eh ESPEY, HUSTON & ASSOCIATES, INC.
 Engineering & Environmental Consultants
 17811 Waterview Parkway Dallas, Texas 75252
 (214) 669-9600

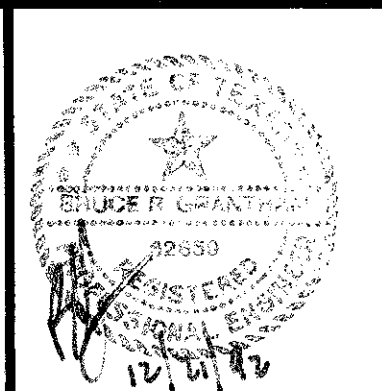
TRACK REALIGNMENT PROFILES
 RAILROAD CROSSING IMPROVEMENTS AND
 TRACK REALIGNMENT FOR ADDISON ROAD
 for
 THE TOWN OF ADDISON

SHEET NO. 6
 OF 15 SHEETS
 JOB NO. 14163



NO.	REVISION	BY	DATE

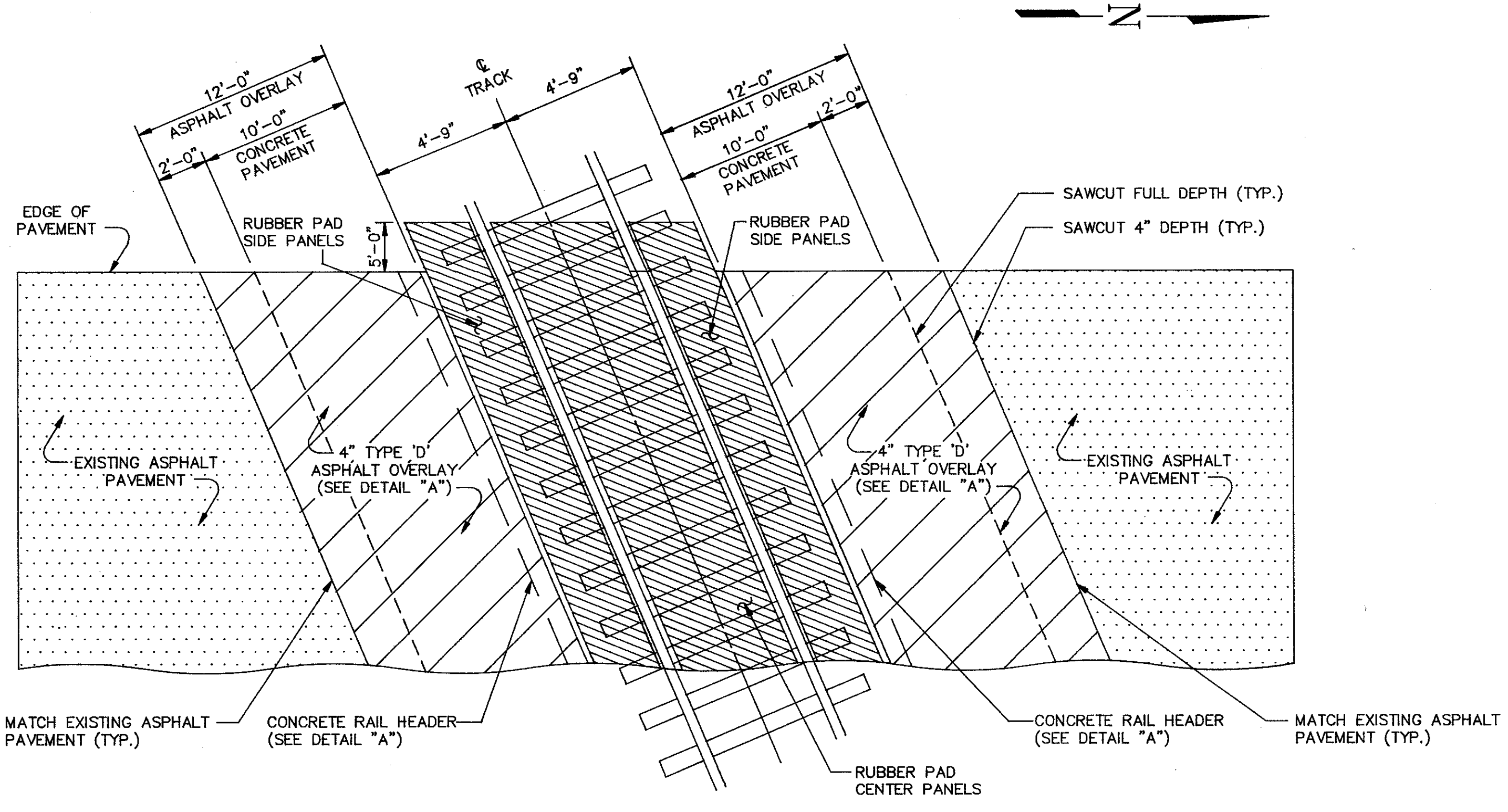
DESIGNED BY: J. WALDBAUER
 DRAWN BY: EH&A
 CHECKED BY: B. GRANTHAM
 SCALE: 1"=20'H, 1"=6'V
 DATE: JULY 1992
 FILE: \ADRR\PROFILES



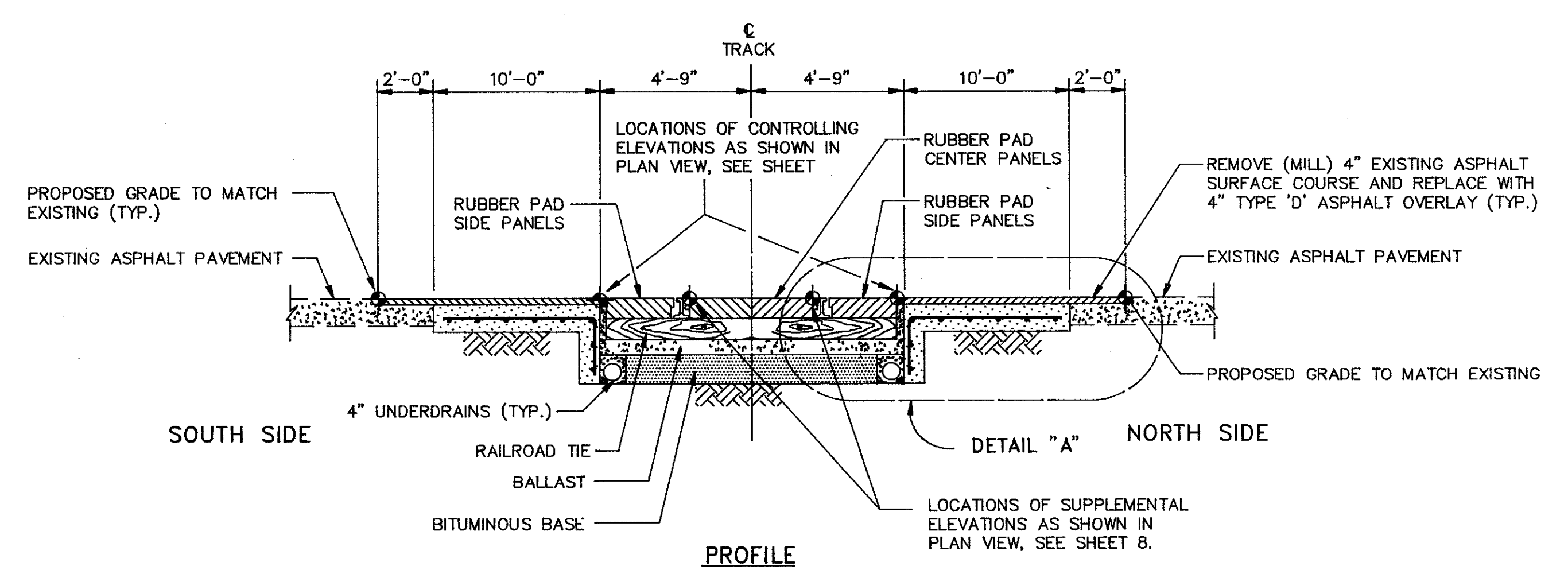
eh ESPEY, HUSTON & ASSOCIATES, INC.
 Engineering & Environmental Consultants
 17811 Waterview Parkway Dallas, Texas 75252
 (214) 669-9600

TRACK REALIGNMENT PROFILES
 RAILROAD CROSSING IMPROVEMENTS AND
 TRACK REALIGNMENT FOR ADDISON ROAD
 for
 THE TOWN OF ADDISON

SHEET NO. 7
 OF 15 SHEETS
 JOB NO. 14163



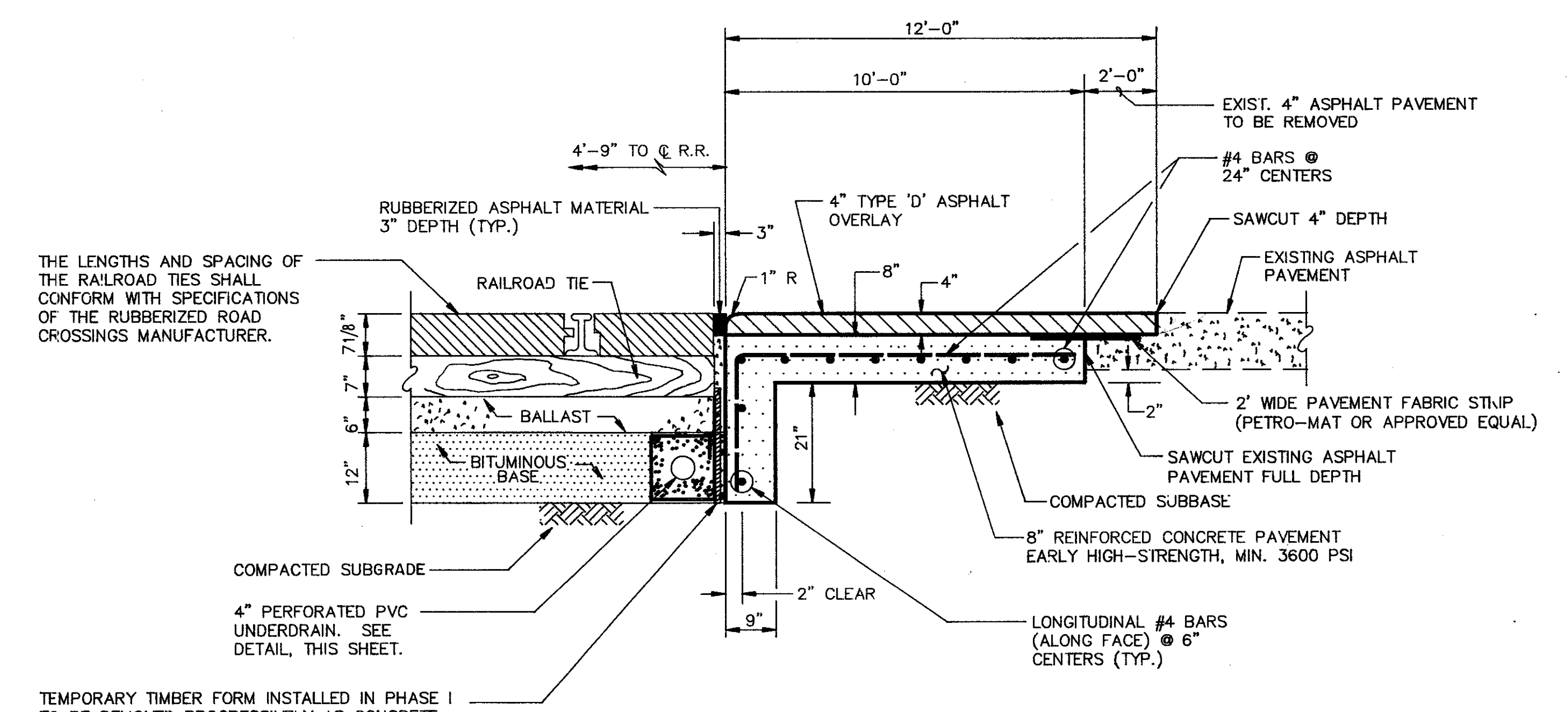
PLAN



PROFILE

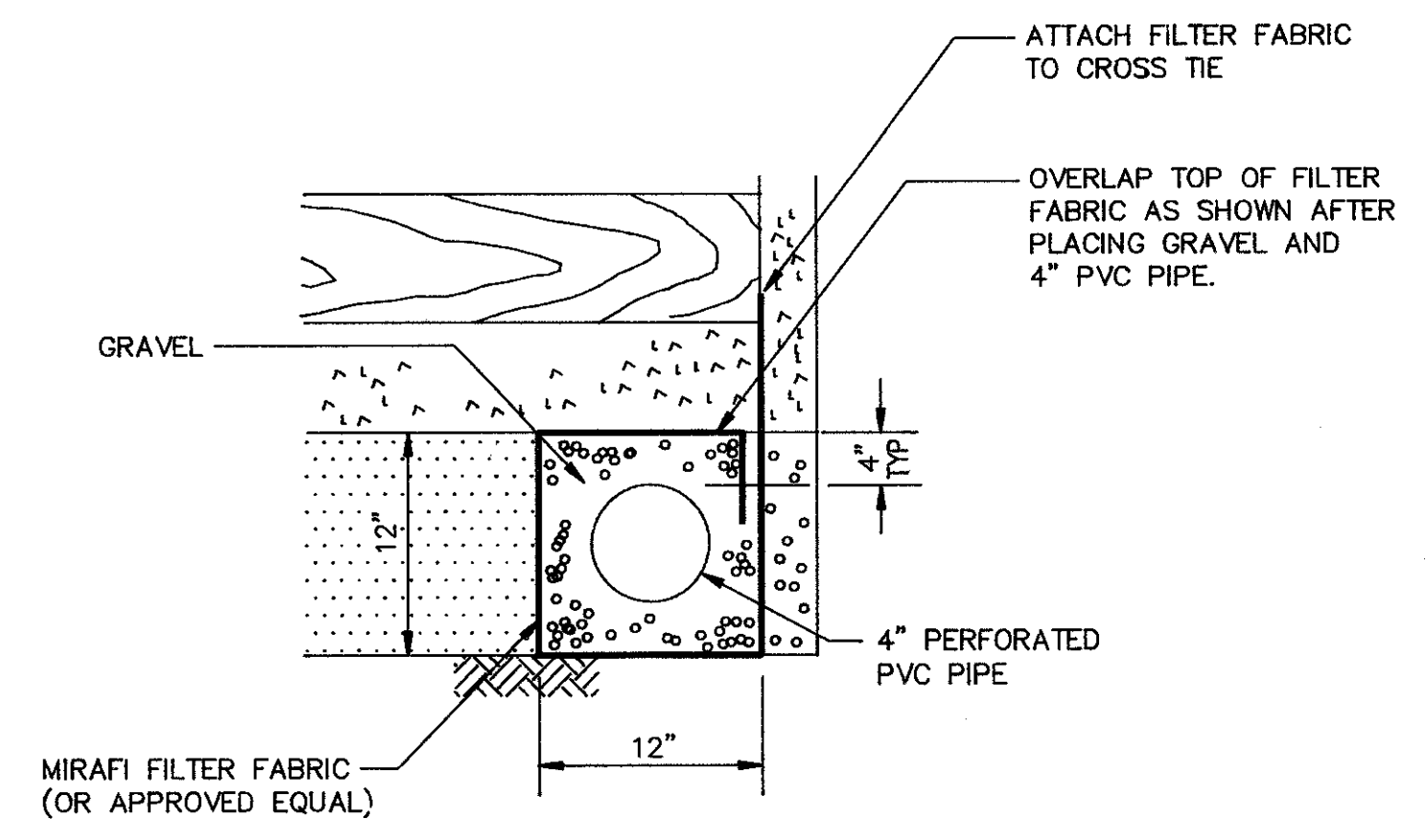
RAILROAD HEADER AND CROSSING DETAIL
NOT TO SCALE

NOTE: RUBBER CROSSING PADS SHALL BE OMNI FULL-DEPTH HEAVY DUTY (PANELIZED) OR APPROVED EQUAL.



DETAIL "A"
NOT TO SCALE

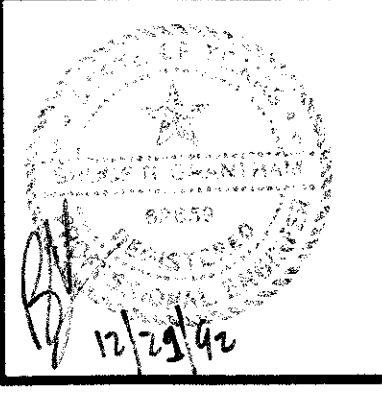
- NOTES:
1. SYMMETRICAL ABOUT RR C/L FOR NORTH AND SOUTH SIDES.
 2. CONCRETE SHALL BE EARLY HIGH-STRENGTH CONCRETE FOR MIN. 3600 PSI TO ALLOW FOR TRAFFIC LOADS AFTER ONE DAY.



UNDERDRAIN DETAIL
NOT TO SCALE

NO.	REVISION	BY	DATE

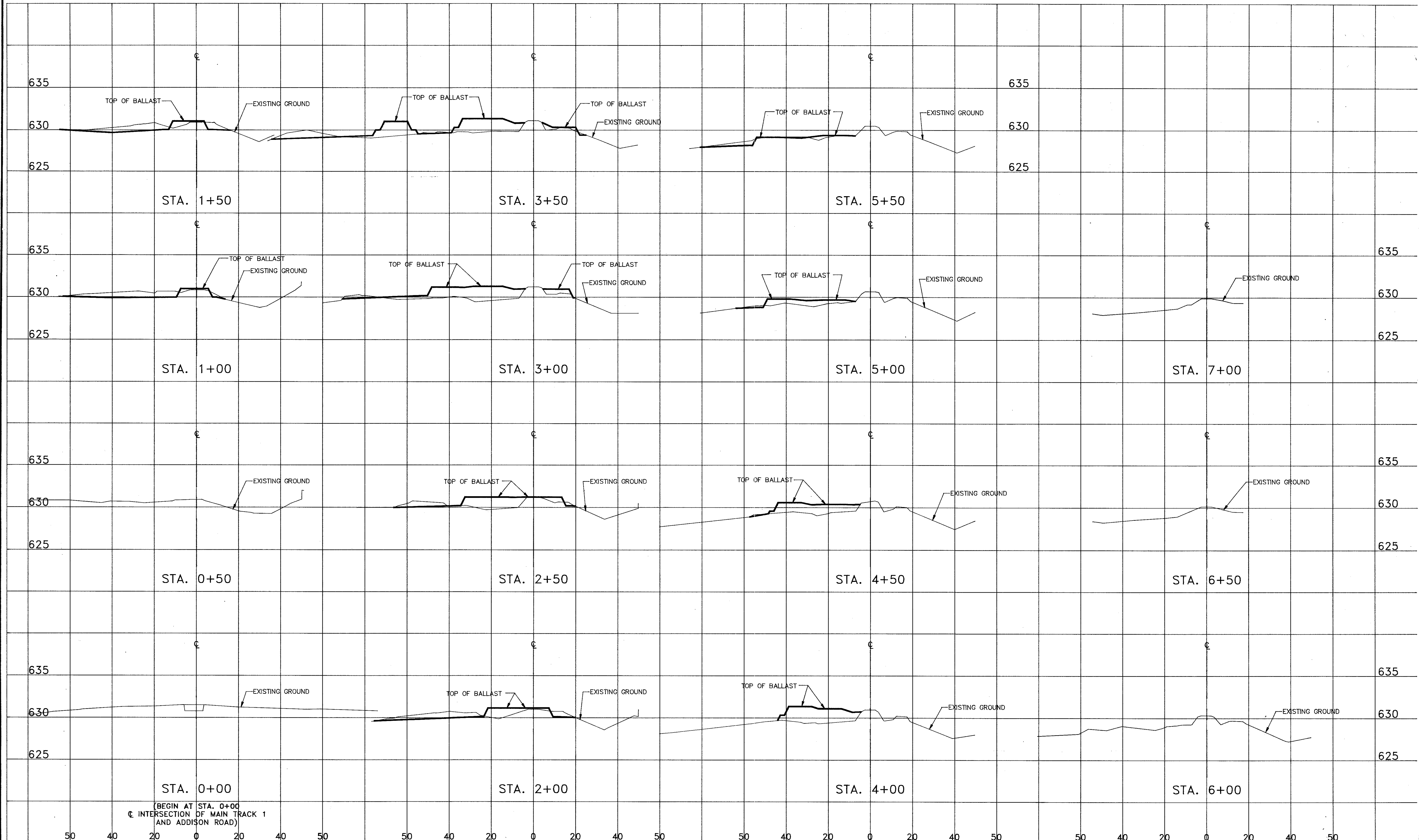
DESIGNED BY: J. WALDBAUER
 DRAWN BY: EH&A
 CHECKED BY: B. GRANTHAM
 SCALE: NOT TO SCALE
 DATE: JULY 1992
 FILE: \ADRR\DETAILS



eh ESPEY, HUSTON & ASSOCIATES, INC.
 Engineering & Environmental Consultants
 13800 Montfort Drive Suite 230 Dallas, Texas 75240
 (214) 387-0771

CONSTRUCTION DETAILS
 RAILROAD CROSSING IMPROVEMENTS AND
 TRACK REALIGNMENT FOR ADDISON ROAD
 for
 THE TOWN OF ADDISON

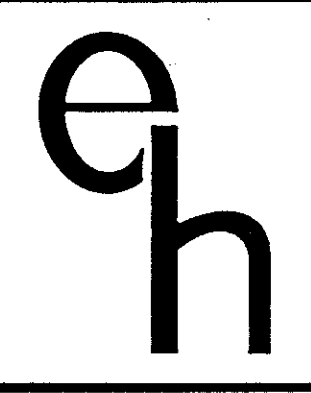
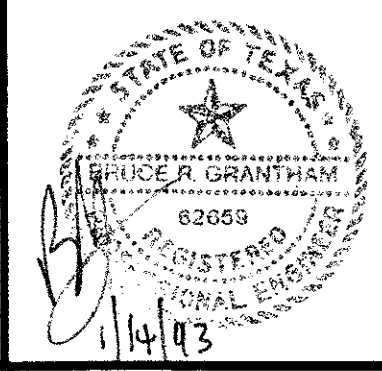
SHEET NO. 9
 OF 15 SHEETS
 JOB NO. 14163



(BEGIN AT STA. 0+00
 C INTERSECTION OF MAIN TRACK 1
 AND ADDISON ROAD)

NO.	REVISION	BY	DATE

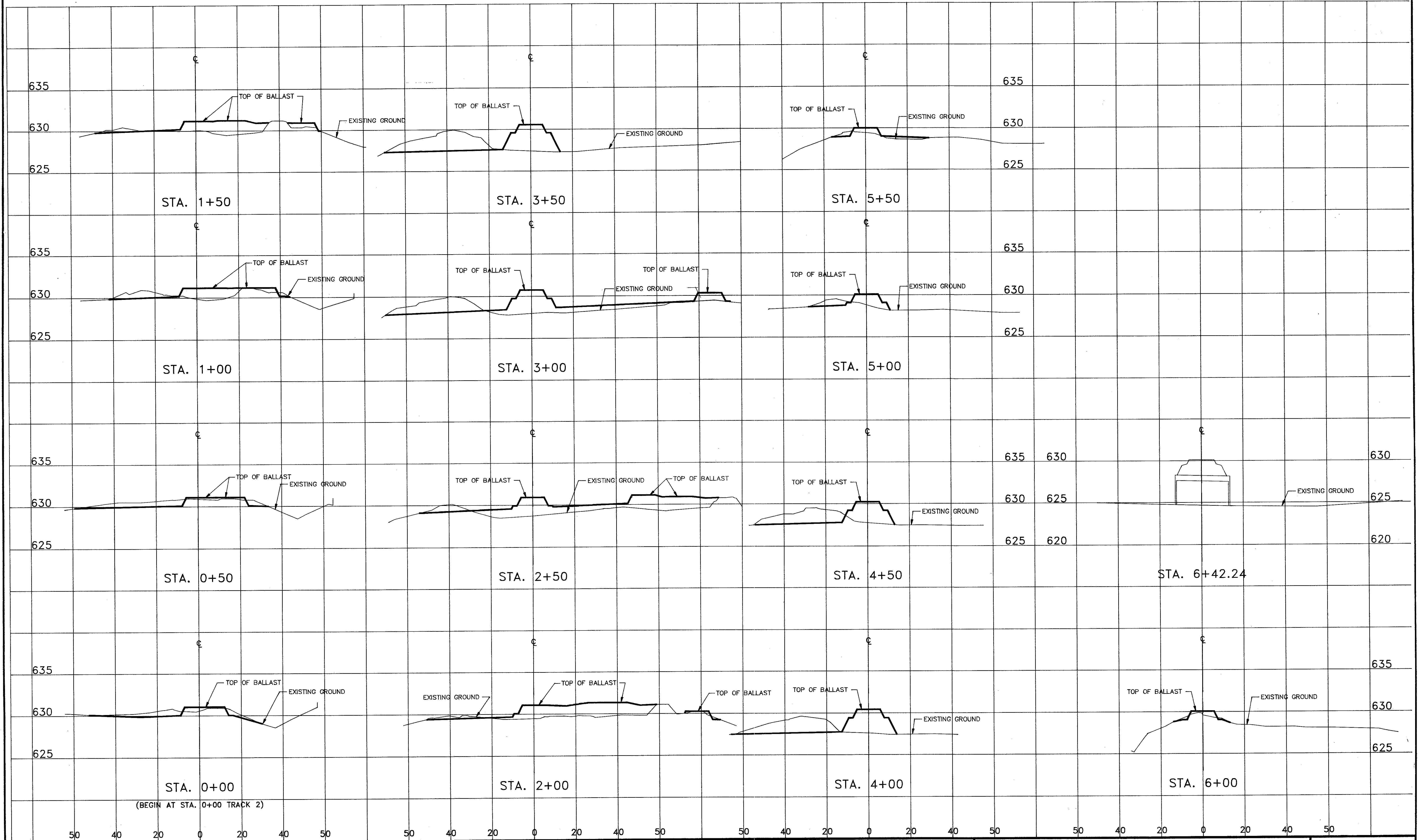
DESIGNED BY: J. WALDBAUER
 DRAWN BY: EH&A
 CHECKED BY: B. GRANTHAM
 SCALE: 1"=20'H; 1"=5'V
 DATE: JULY 1992
 FILE: \ADDR\XSECI



ESPEY, HUSTON & ASSOCIATES, INC.
 Engineering & Environmental Consultants
 13800 Montfort Drive Suite 230 Dallas, Texas 75240
 (214) 387-0771

TRACK REALIGNMENT CROSS-SECTIONS
 RAILROAD CROSSING IMPROVEMENTS AND
 TRACK REALIGNMENT FOR ADDISON ROAD
 for
 THE TOWN OF ADDISON

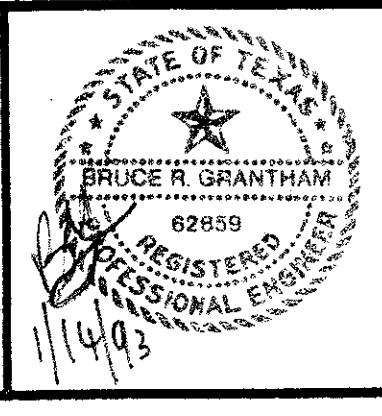
SHEET NO. 10
 OF 15 SHEETS
 JOB NO. 14163



(BEGIN AT STA. 0+00 TRACK 2)

NO.	REVISION	BY	DATE

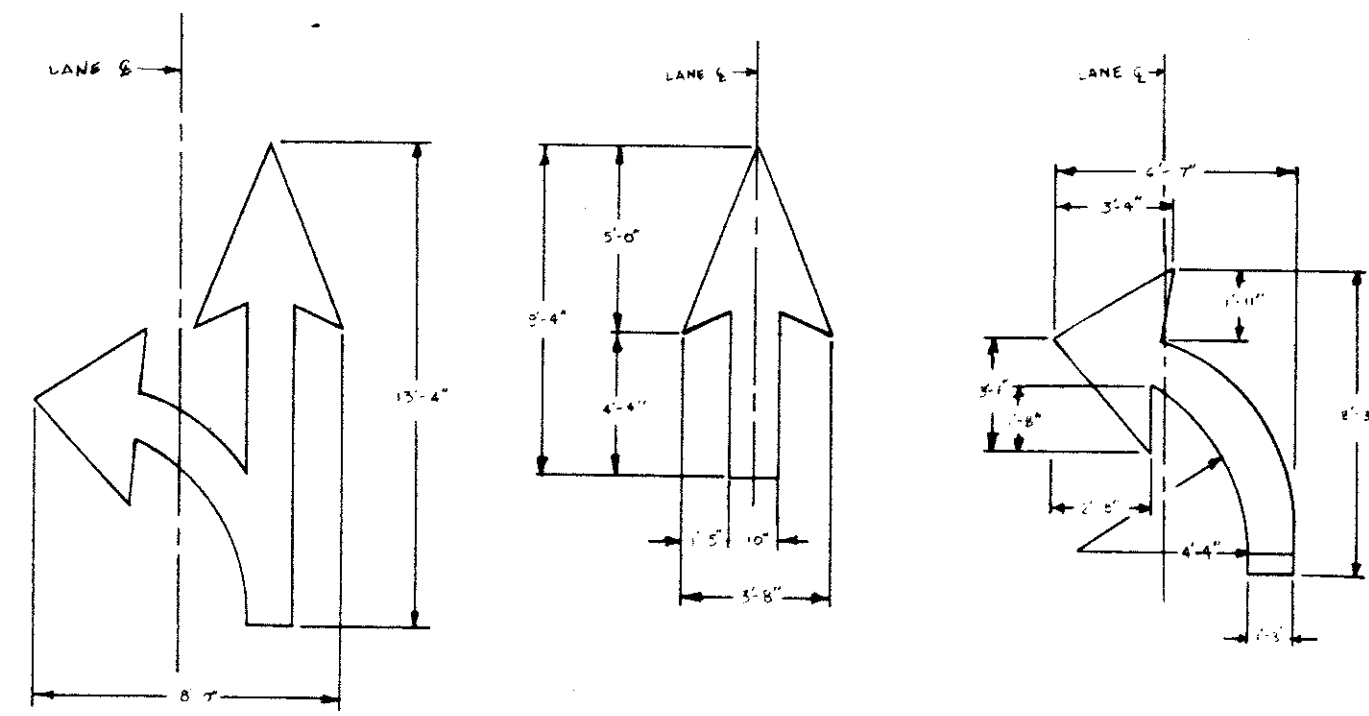
DESIGNED BY: J. WALDBAUER
 DRAWN BY: FH&A
 CHECKED BY: B. GRANTHAM
 SCALE: 1"=20'H: 1"=5'V
 DATE: JULY 1992
 FILE: \ADRR\XSEC2



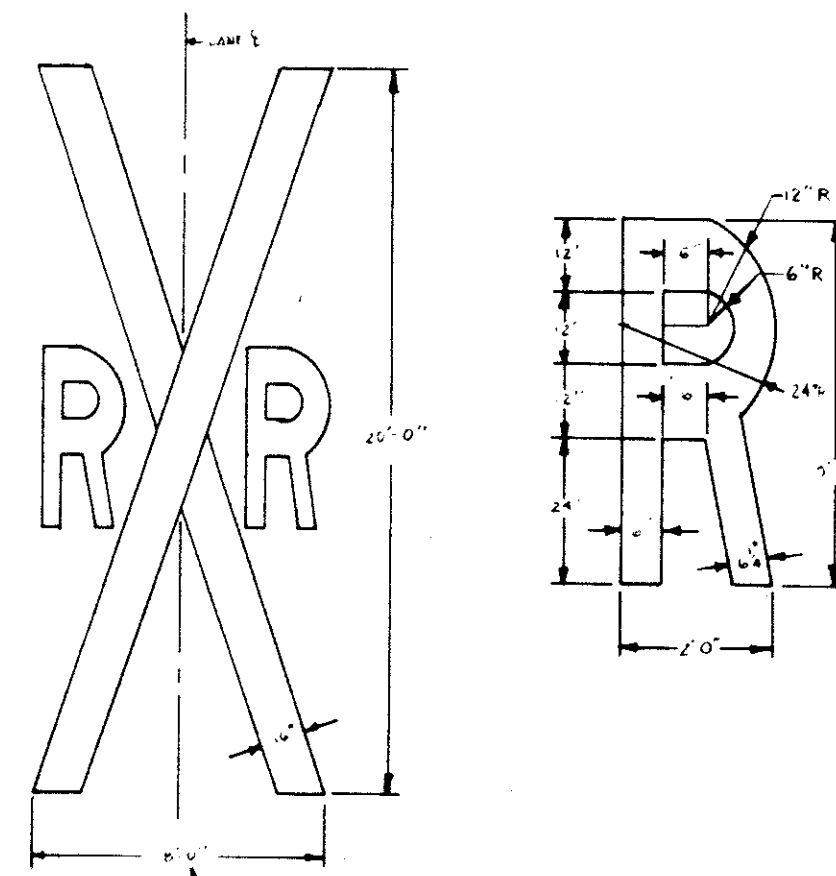
eh ESPEY, HUSTON & ASSOCIATES, INC.
 Engineering & Environmental Consultants
 13800 Montfort Drive Suite 230 Dallas, Texas 75240
 (214) 387-0771

TRACK REALIGNMENT CROSS-SECTIONS
 RAILROAD CROSSING IMPROVEMENTS AND
 TRACK REALIGNMENT FOR ADDISON ROAD
 for
 THE TOWN OF ADDISON

SHEET NO. 11
 OF 15 SHEETS
 JOB NO. 14163



ELONGATED ARROWS FOR PAVEMENT MARKINGS

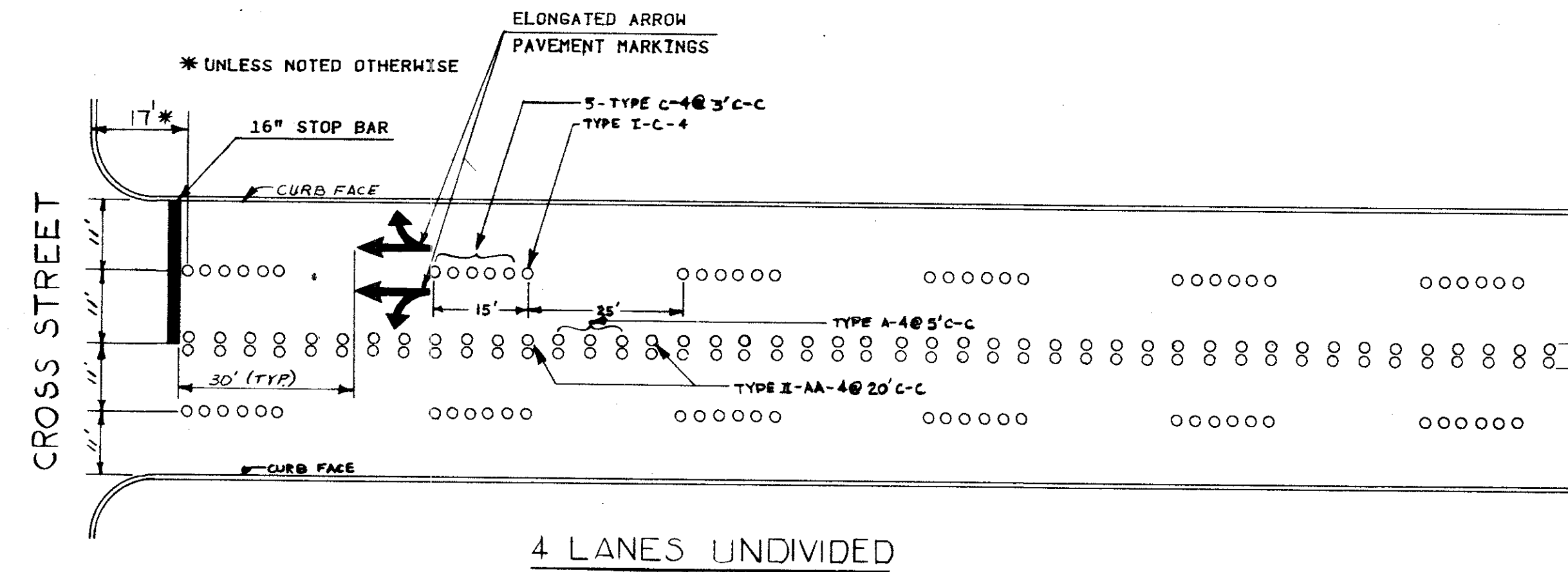


WIDTH MAY VARY ACCORDING TO LANE WIDTH

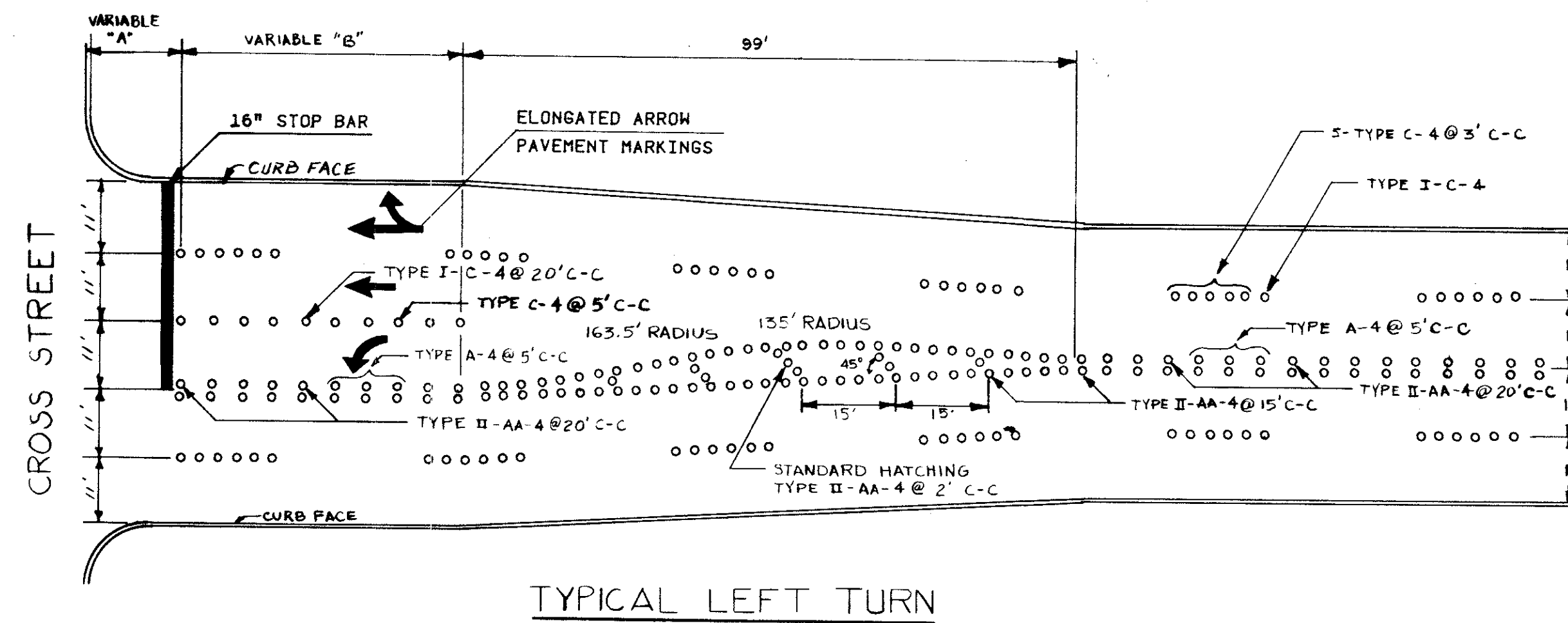
RAILROAD - HIGHWAY GRADE CROSSING FOR PAVEMENT MARKINGS

STOP BARS AND PAVEMENT MESSAGES

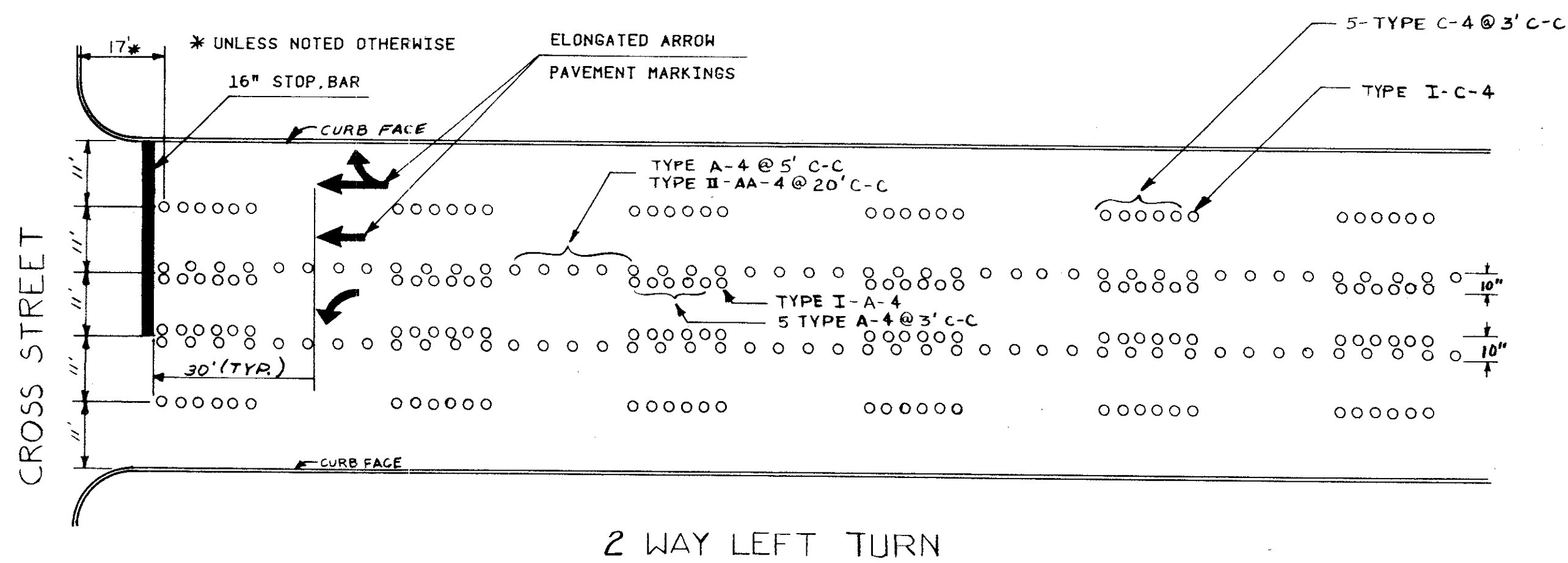
1. STOP BARS AND PAVEMENT MESSAGES SHALL BE ALKALYD THERMOPLASTIC OR APPROVED EQUAL EXCEPT WHERE THEY ARE LOCATED IN AREAS WHICH ARE TO BE MARKED WITH REFLECTORIZED PAINT. A SIXTEEN INCH TAPE WILL BE USED FOR A STOP BAR, NOT FOUR-FOUR INCH TAPES.
2. ALL STOP BARS AND PAVEMENT MESSAGES SHALL CONFORM TO THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES AS AMENDED IN THE 1978 VOLUME.



4 LANES UNDIVIDED



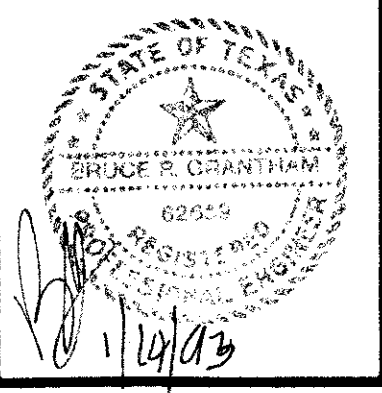
TYPICAL LEFT TURN



2 WAY LEFT TURN

NO.	REVISION	BY	DATE

DESIGNED BY: _____
 DRAWN BY: _____
 CHECKED BY: _____
 SCALE: _____
 DATE: _____



ESPEY, HUSTON & ASSOCIATES, INC.
 Engineering & Environmental Consultants
 13800 Montfort Drive Suite 230 Dallas, Texas 75240
 (214) 387-0771

PAVEMENT MARKING LAYOUT
 RAILROAD CROSSING IMPROVEMENTS AND
 TRACK REALIGNMENT FOR ADDISON ROAD
 for
 THE TOWN OF ADDISON

SHEET NO. 12
 OF 15 SHEETS
 JOB NO. 14163

ROADWAY MARKERS SPECIFICATIONS

COLOR OF MARKERS :

- A : YELLOW BODY - AMBER REFLECTOR
- C : WHITE BODY - CRYSTAL REFLECTOR
- R : RED BODY - RED REFLECTOR

REFLECTIVE FACES :

- I : ONE FACE REFLECTORIZED
- II : BOTH FACES REFLECTORIZED

SIZES AND KINDS OF MARKERS :

- 4 : 4 INCH LANE MARKER
- 6 : 6 INCH CHANNEL MARKER OR JIGGLE BAR
- 8 : 8 INCH CHANNEL MARKER

CHANNEL MARKERS :

- 6 or 8 INCHES (ROUND)

JIGGLE BARS :

- 6 INCHES (SQUARE)

EXAMPLES OF ROADWAY MARKERS :

- TYPE II-CR-4 : 4 INCH REFLECTORIZED LANE MARKER, ONE FACE REFLECTS CRYSTAL, ONE FACE REFLECTS RED LIGHT.
- TYPE A-4 : 4 INCH NON-REFLECTIVE YELLOW LANE MARKER
- TYPE I-A-6 : 6 INCH REFLECTORIZED CHANNEL MARKER, ONLY ONE FACE REFLECTS AMBER LIGHT

EPOXY ADHESIVE

TYPE II and II-M :

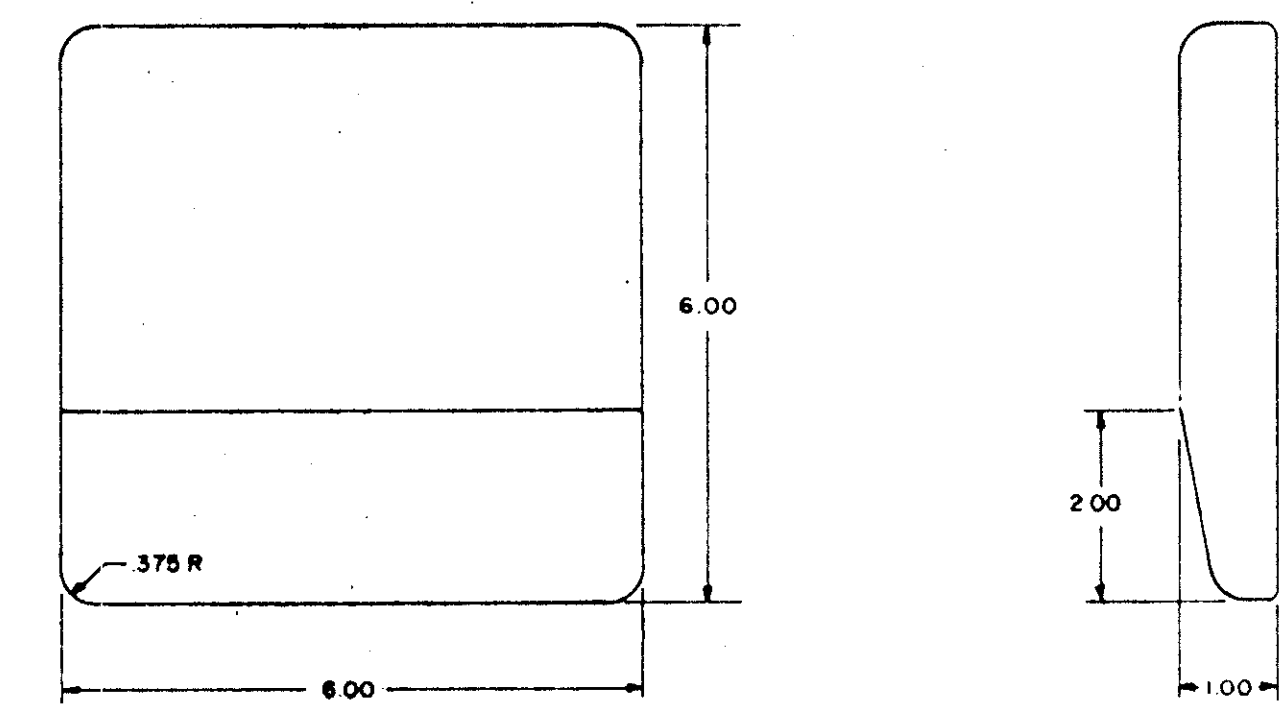
MEDIUM SETTING MARKER ADHESIVE

TYPE III and III-M :

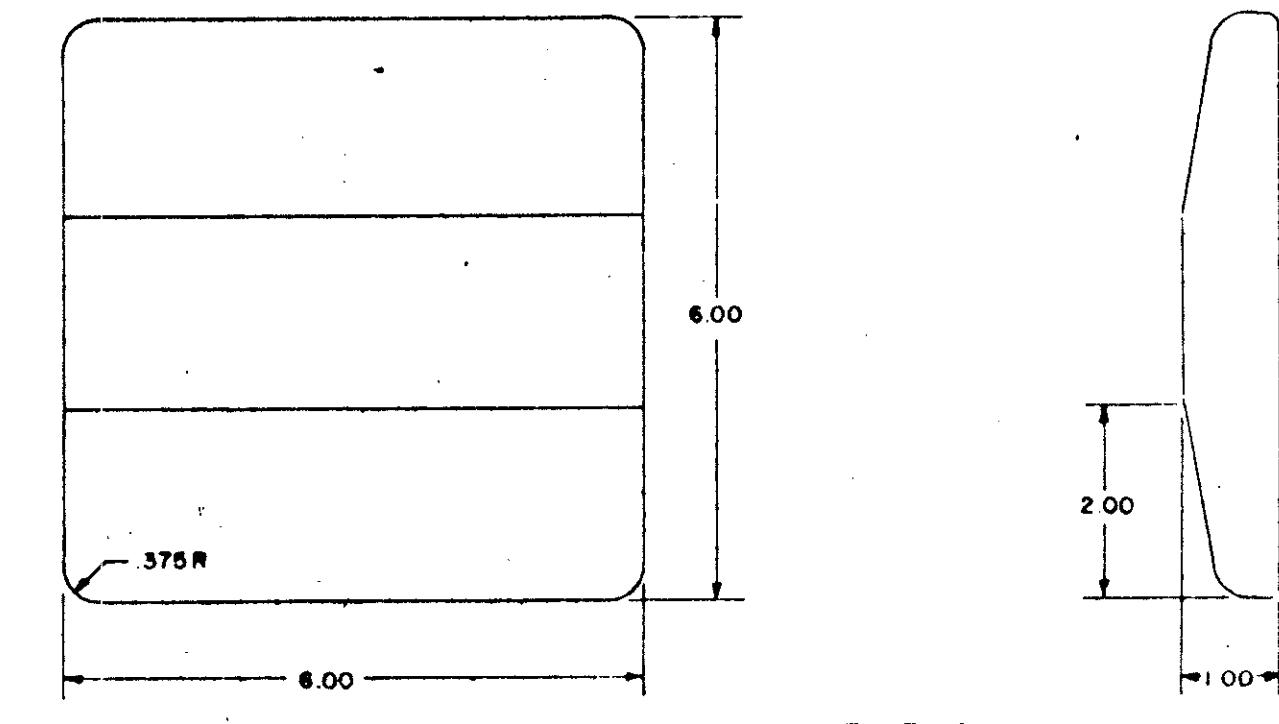
STANDARD SETTING MARKER ADHESIVE

PAVEMENT TEMP., F	APPROXIMATE SET TIME, HOURS	
	TYPE II & II-M	TYPE III & III-M
115	0.5	1
95	1	2
77	2	4
60	4	8
50	-	-

NOTE: ALL EPOXY SHALL BE MACHINE MIXED UNLESS PRIOR WRITTEN APPROVAL IS OBTAINED FROM CITY ENGINEER ALLOWING HAND MIXING.

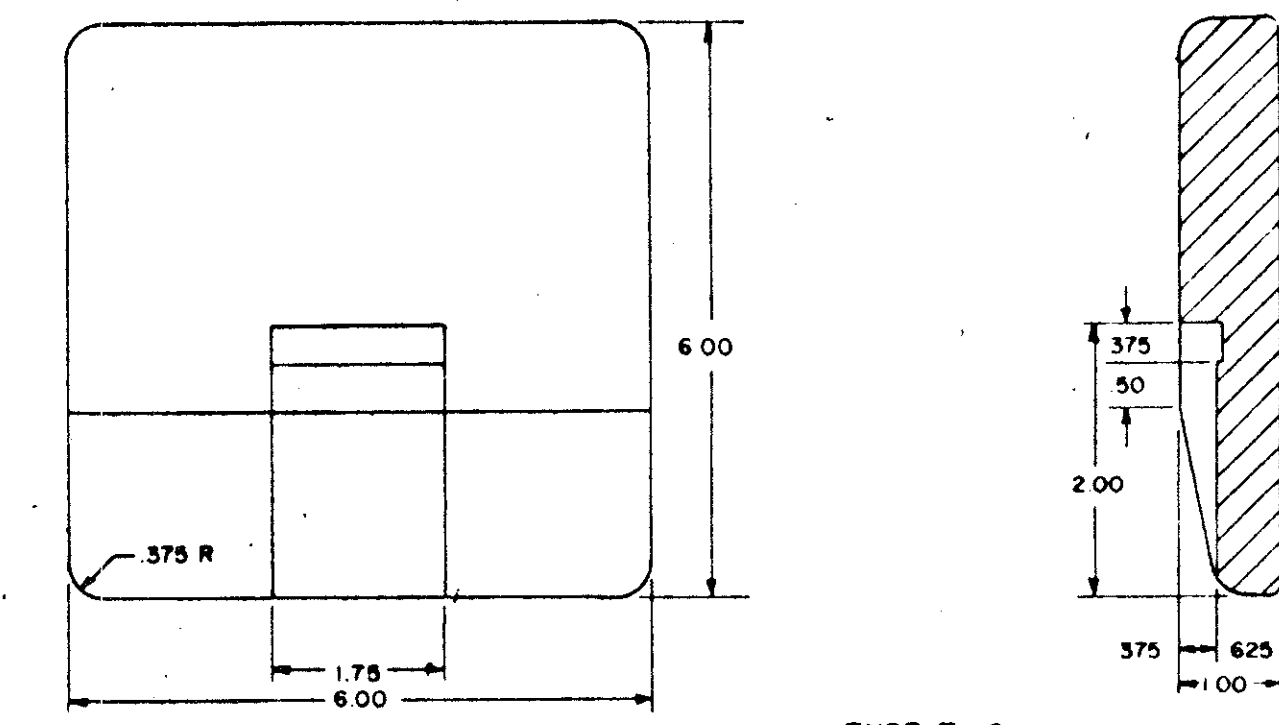


TYPE - 6

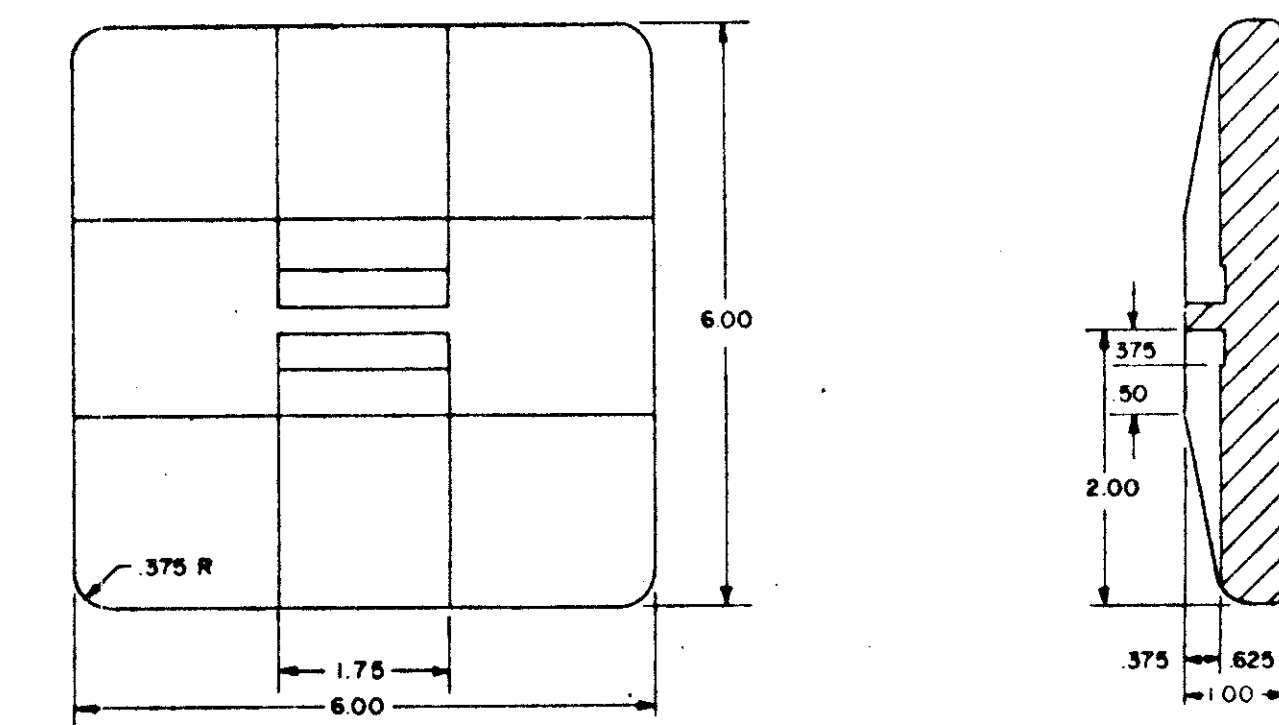


TYPE - 6

NON-REFLECTIVE

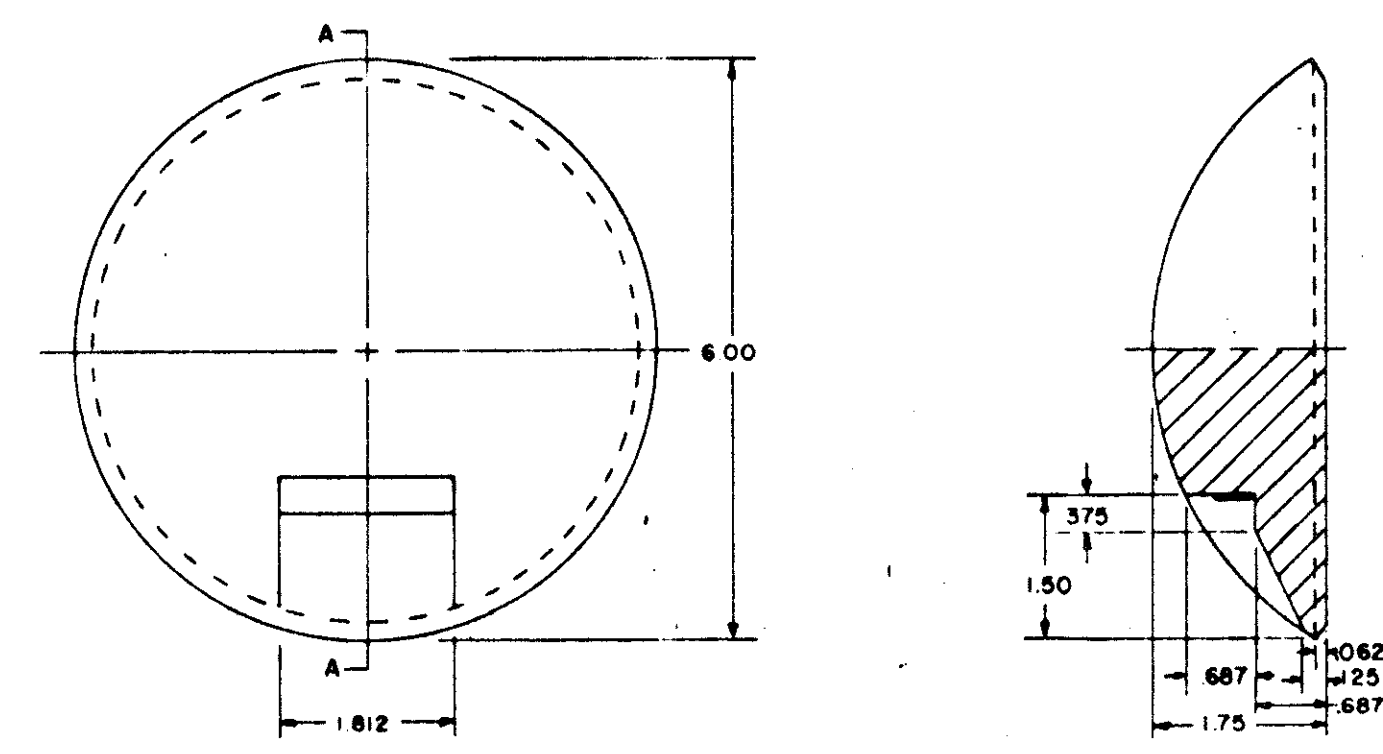


TYPE I - 6

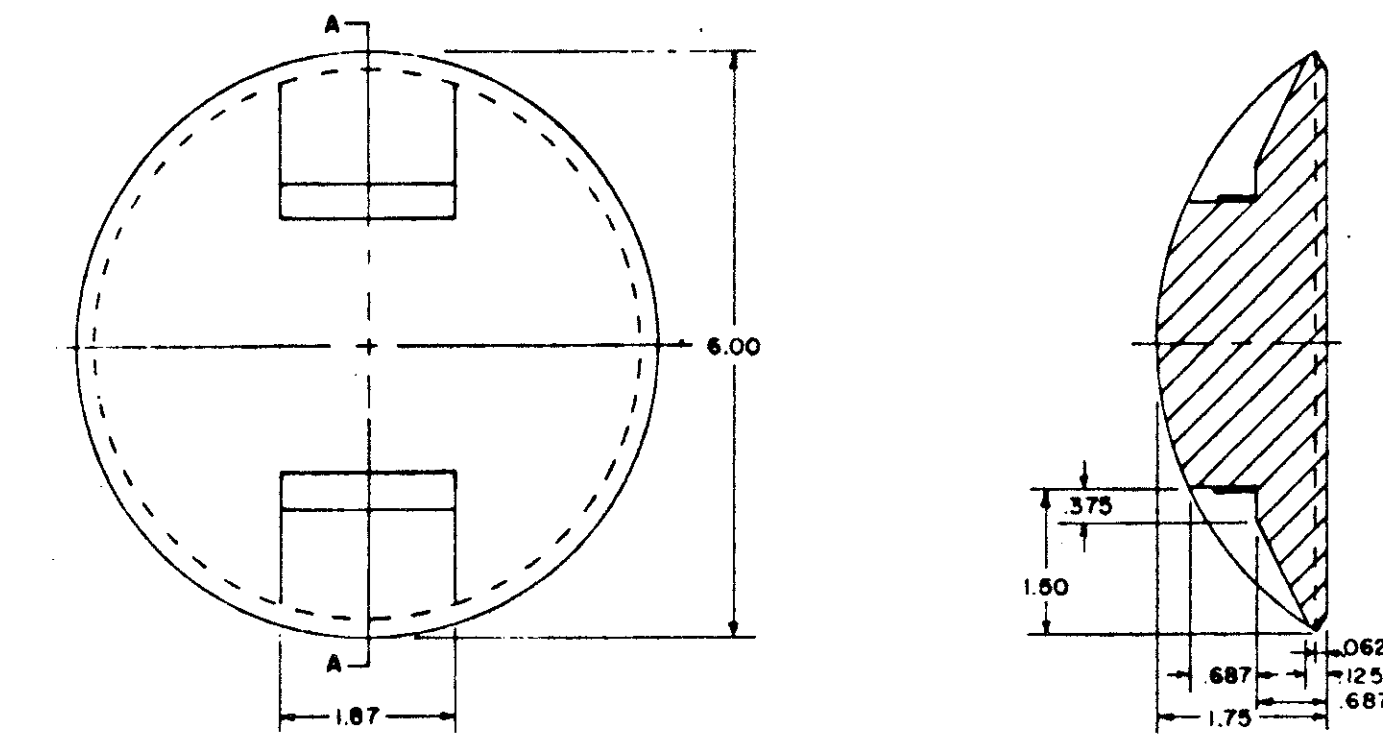


TYPE II - 6

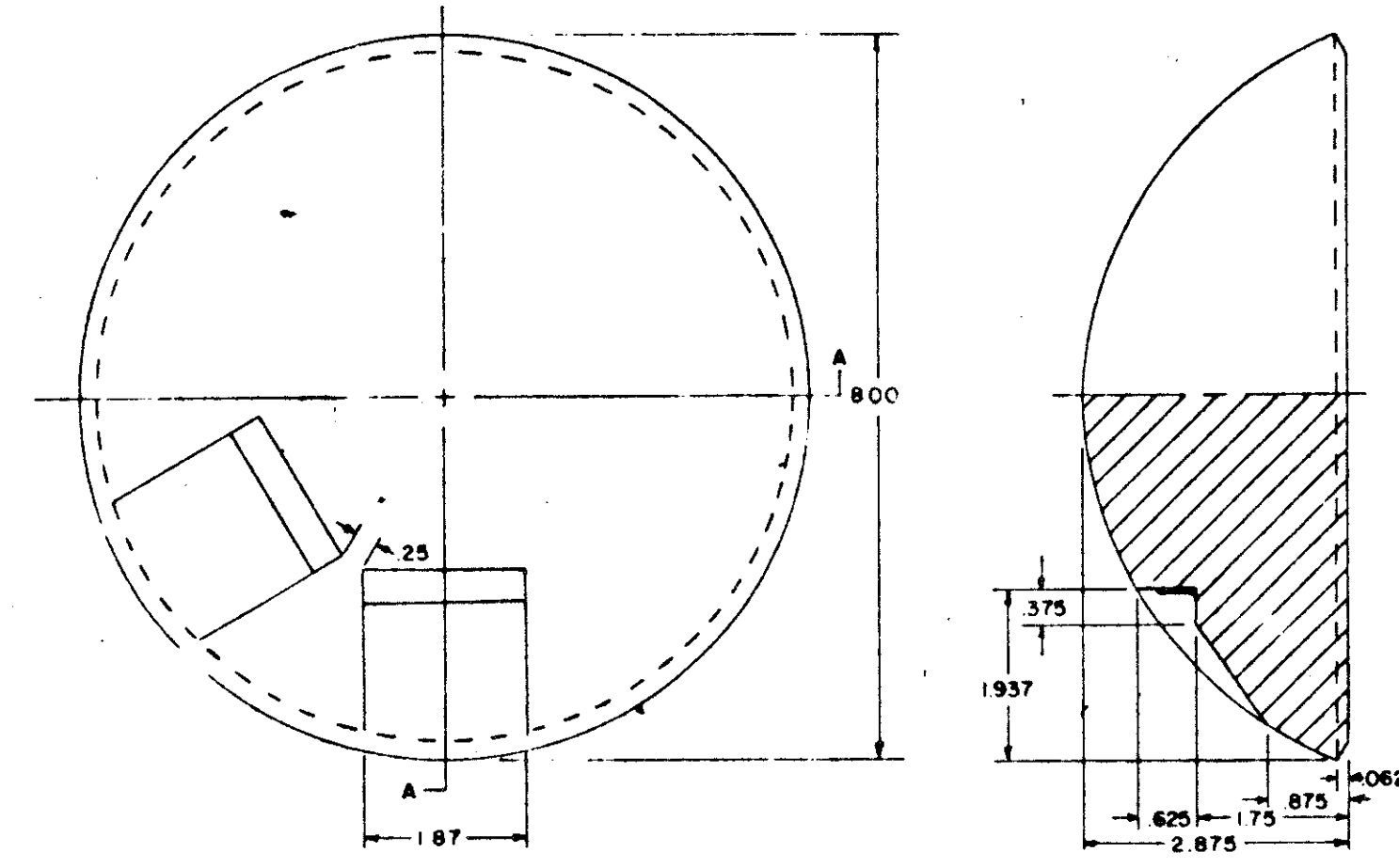
REFLECTIVE
JIGGLE BARS



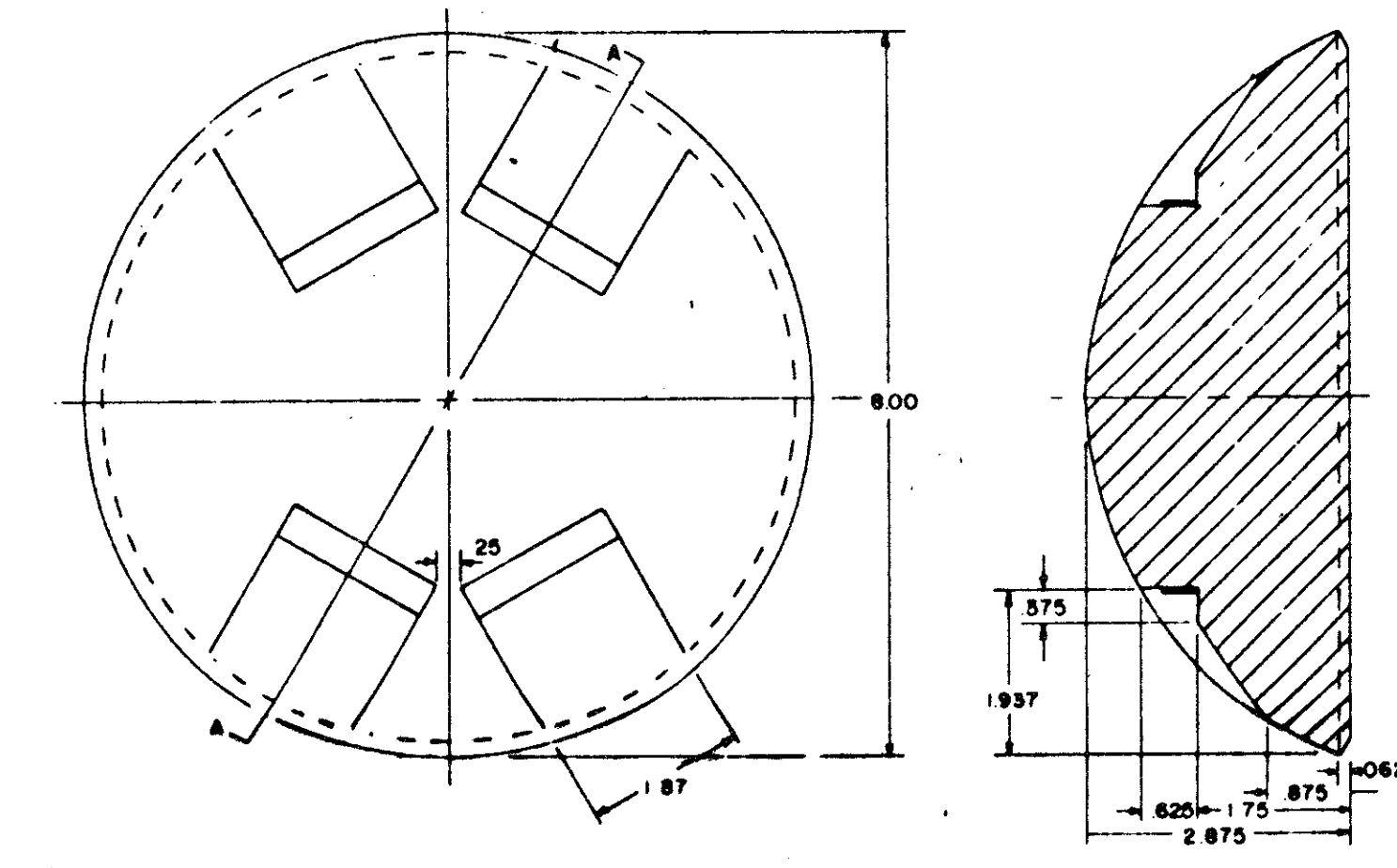
TYPE I - 6



TYPE II - 6

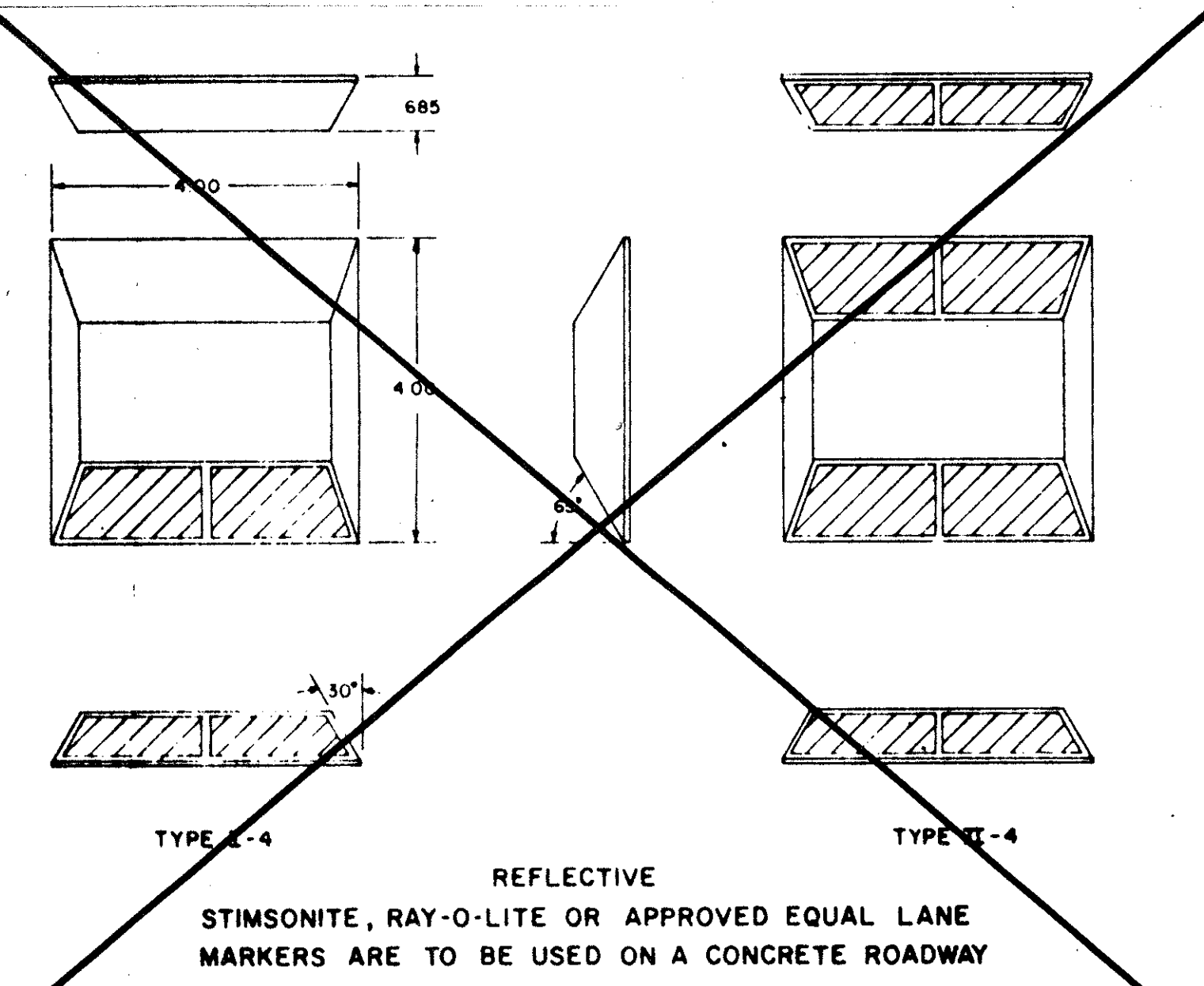


TYPE I - 8

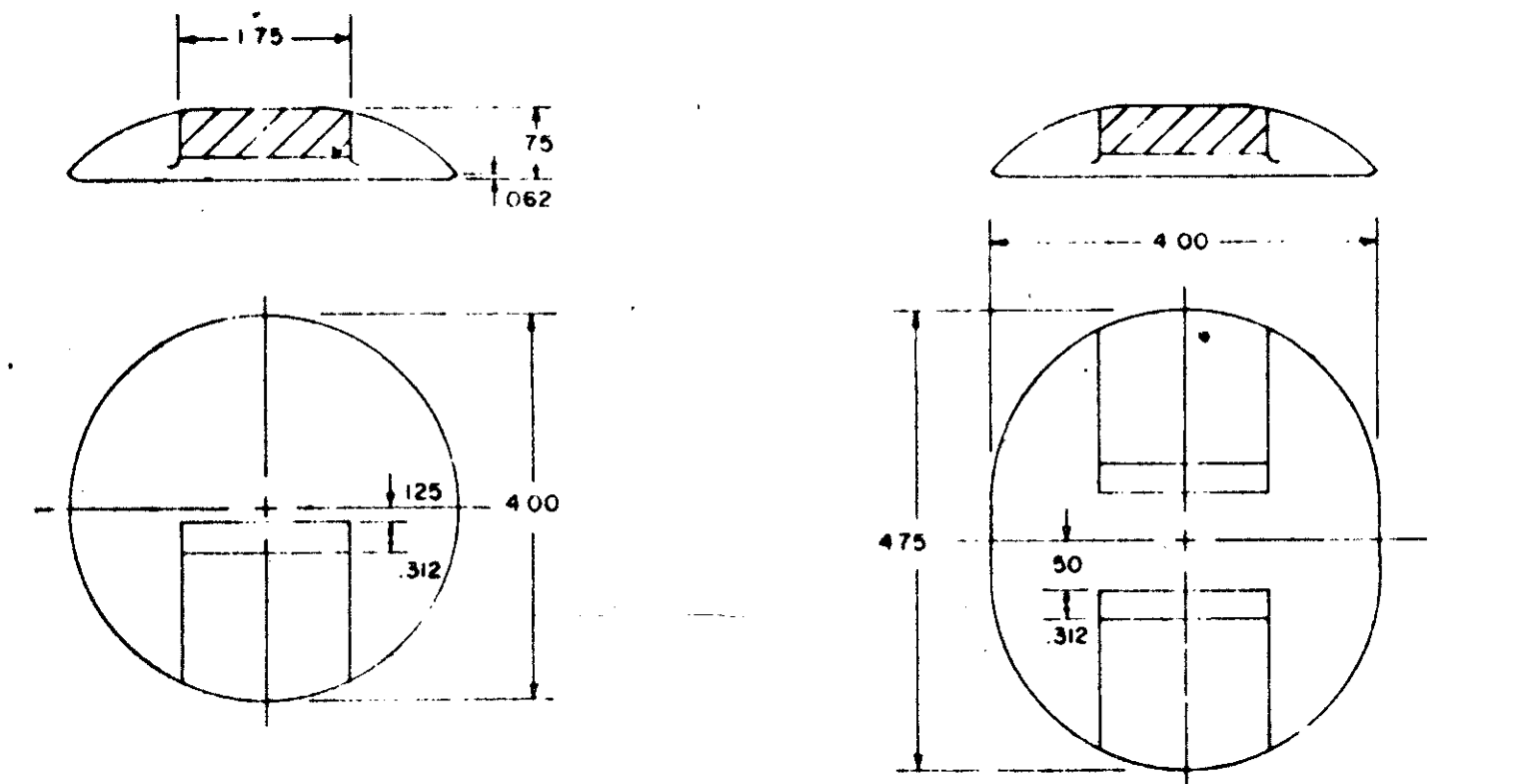


TYPE II - 8

CHANNEL MARKERS



REFLECTIVE
STIMSONITE, RAY-O-LITE OR APPROVED EQUAL LANE MARKERS ARE TO BE USED ON A CONCRETE ROADWAY



REFLECTIVE
AMERICAN CLAY CERAMIC OR APPROVED EQUAL LANE MARKERS ARE TO BE USED ON A ASPHALT ROADWAY

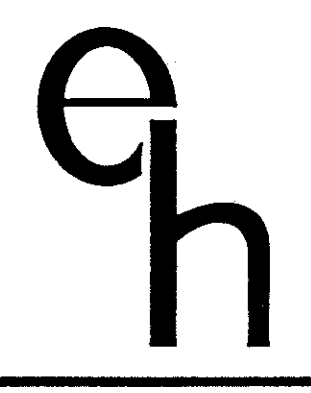
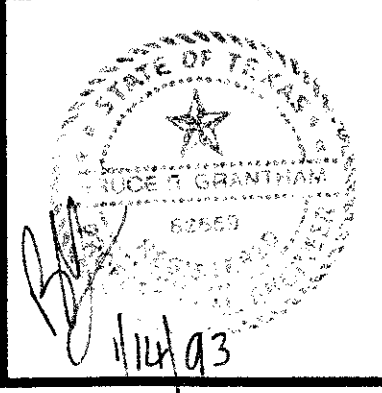


NON-REFLECTIVE
AMERICAN CLAY CERAMIC OR APPROVED EQUAL LANE MARKERS CAN BE USED ON ASPHALT AND CONCRETE ROADWAYS

LANE MARKERS

NO.	REVISION	BY	DATE

DESIGNED BY: _____
 DRAWN BY: _____
 CHECKED BY: _____
 SCALE: _____
 DATE: _____



ESPEY, HUSTON & ASSOCIATES, INC.
 Engineering & Environmental Consultants
 13800 Montfort Drive Suite 230 Dallas, Texas 75240
 (214) 387-0771

PAVEMENT MARKING DETAILS
 RAILROAD CROSSING IMPROVEMENTS AND
 TRACK REALIGNMENT FOR ADDISON ROAD
 for
 THE TOWN OF ADDISON

SHEET NO. 13
 OF 15 SHEETS
 JOB NO. 14163

CONSTRUCTION SIGN NOTES

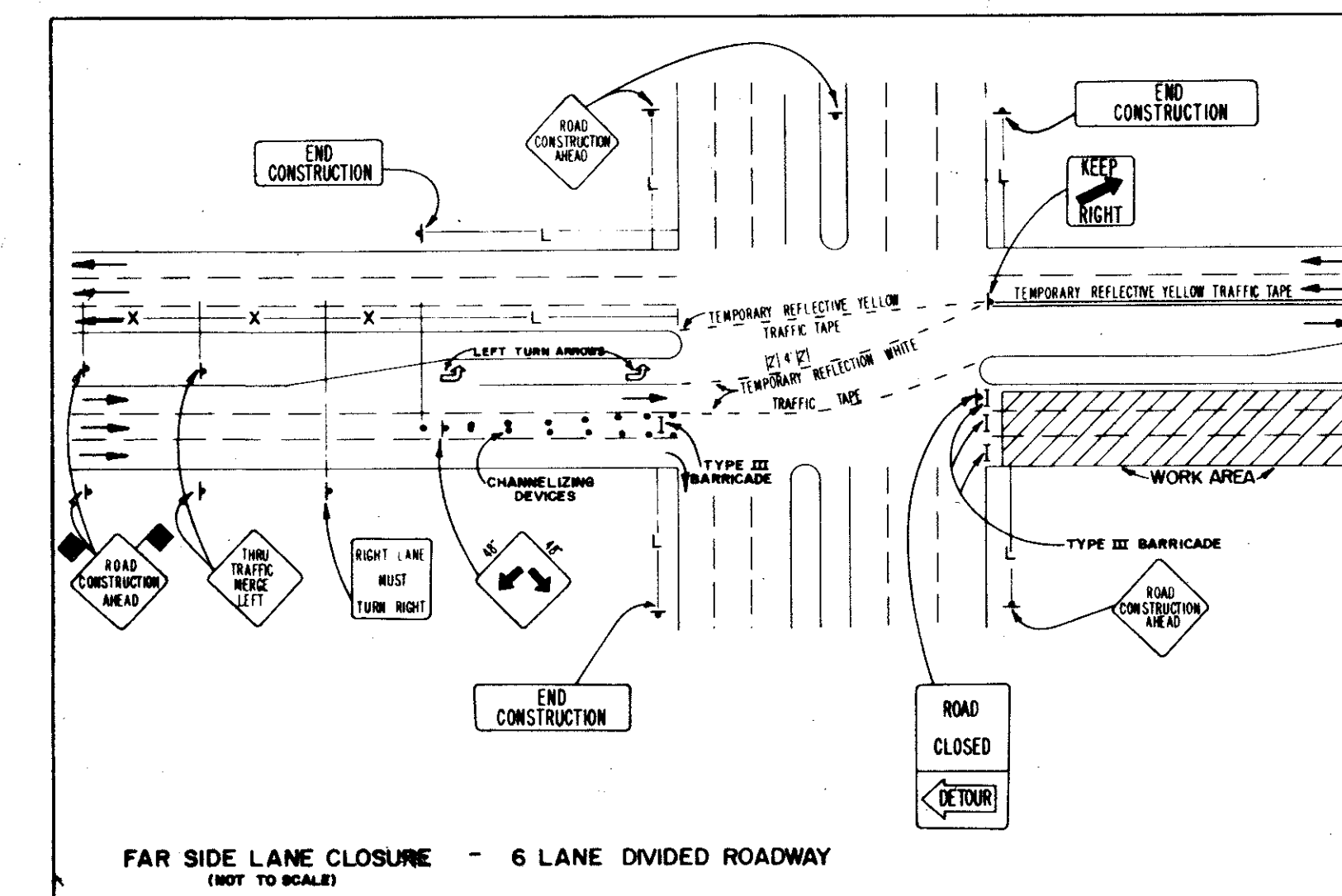
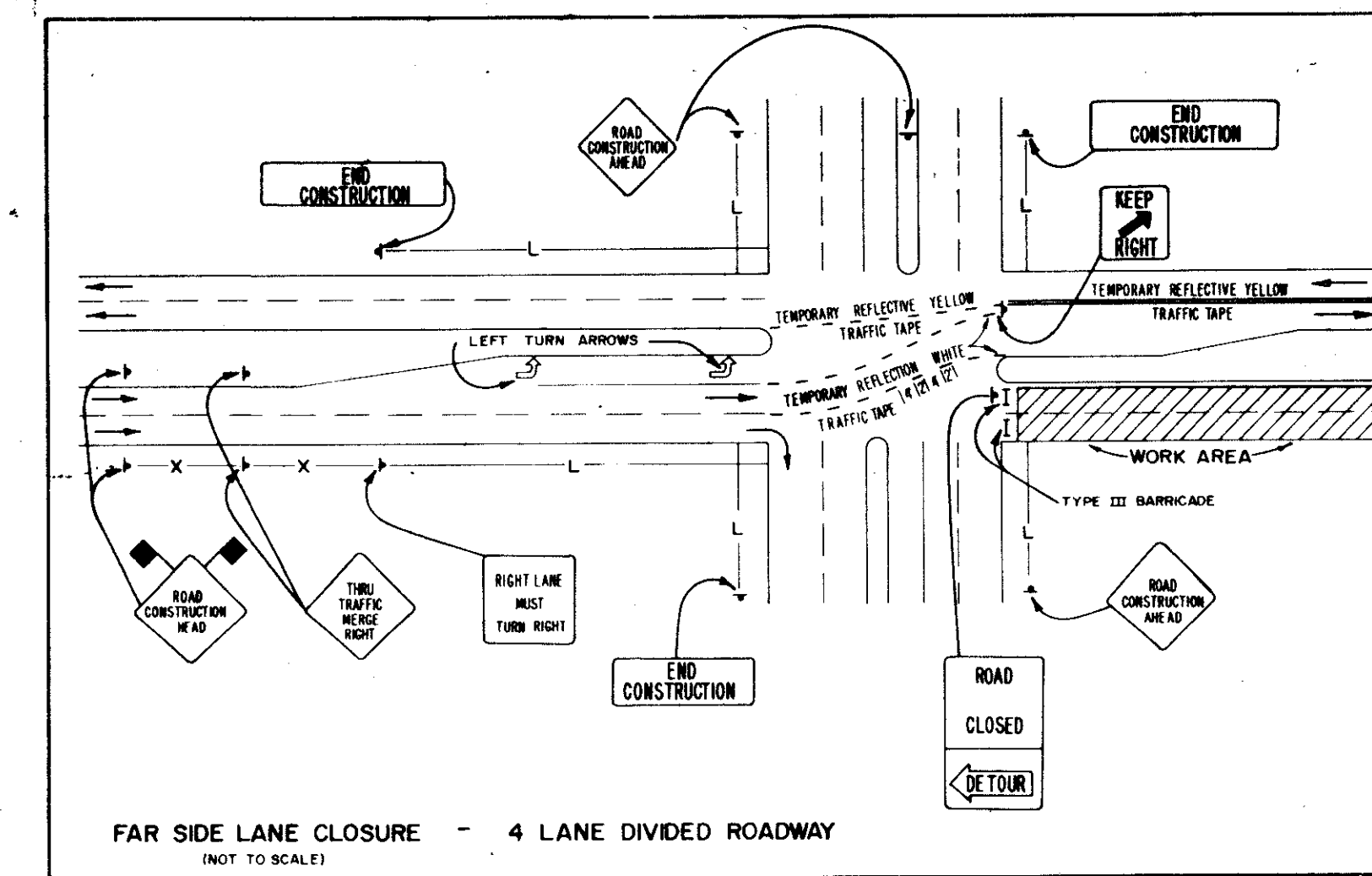
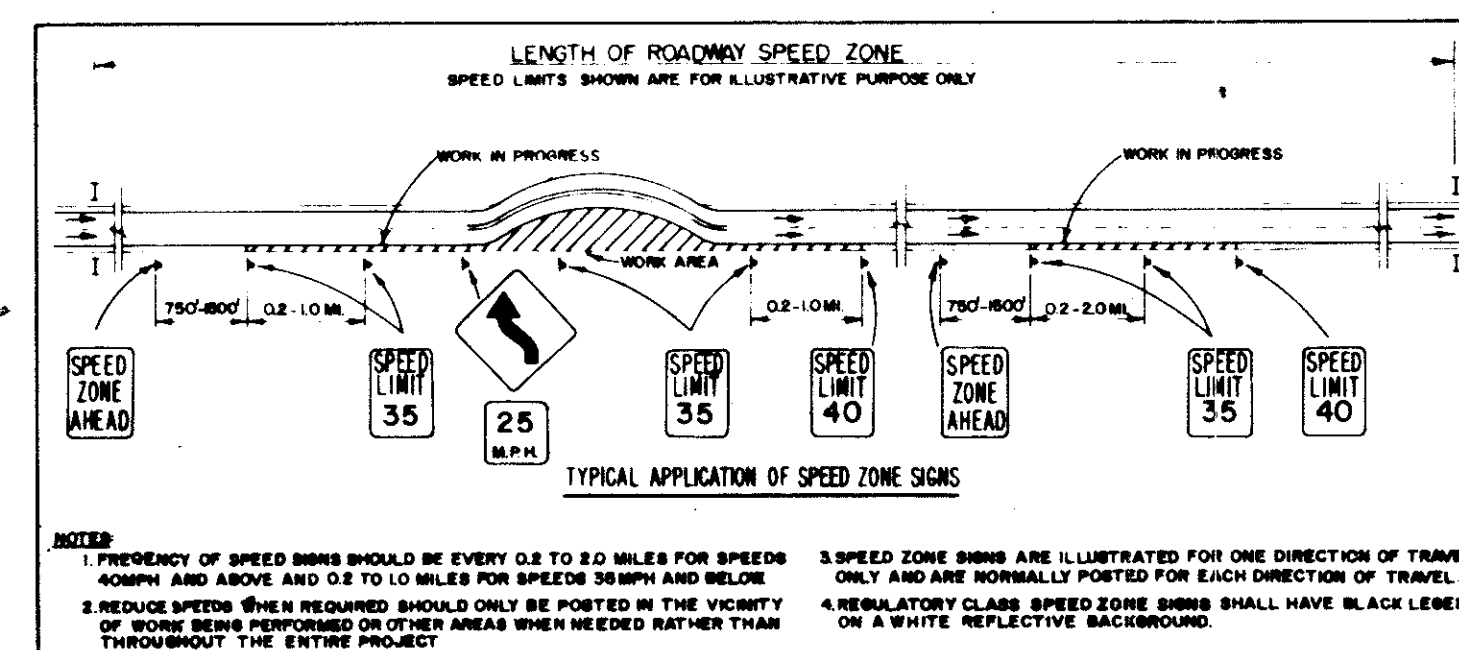
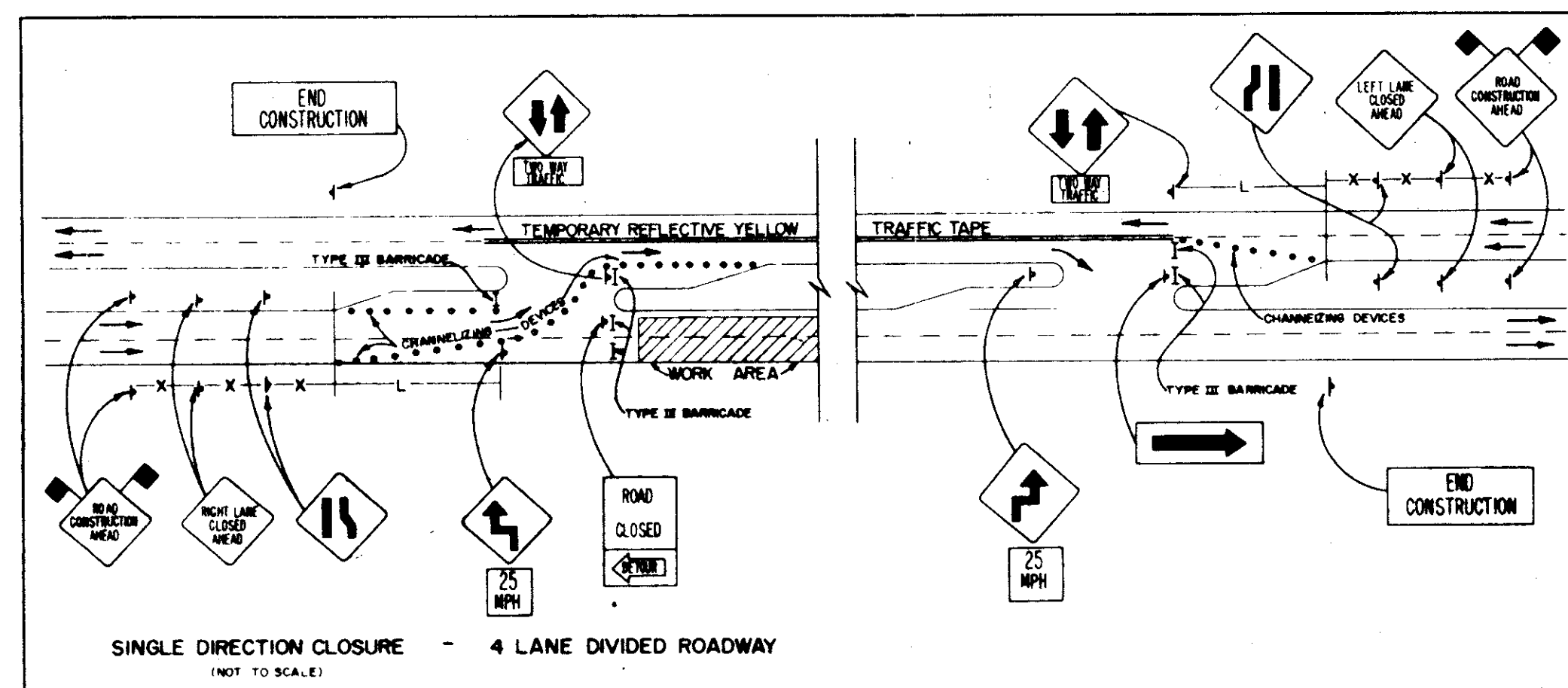
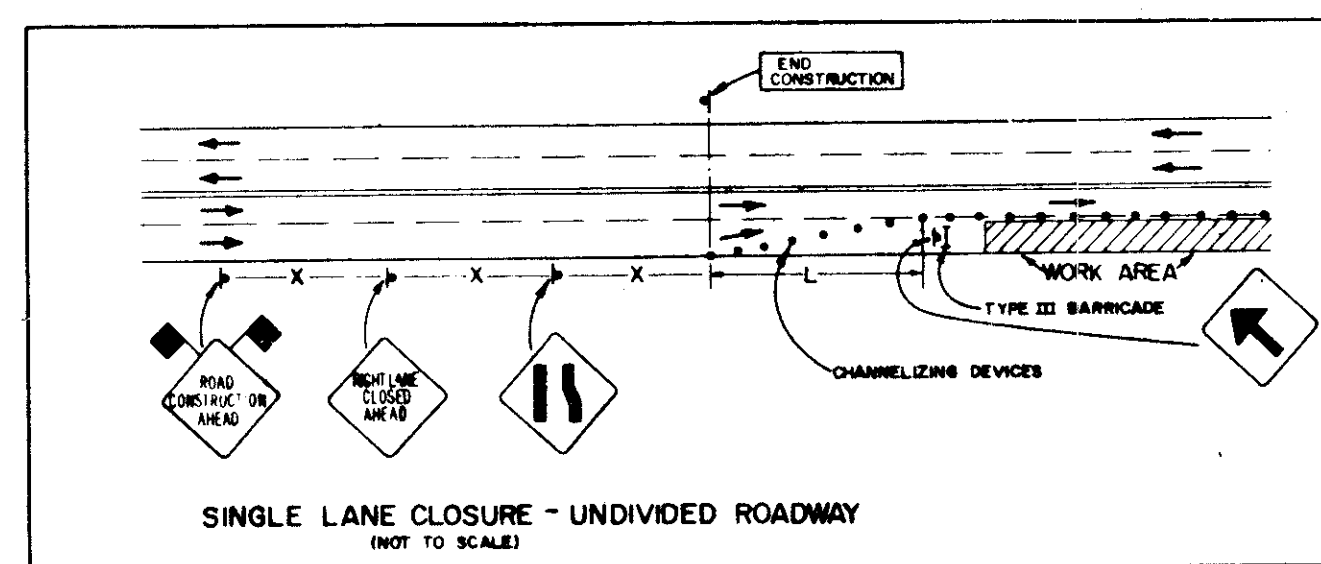
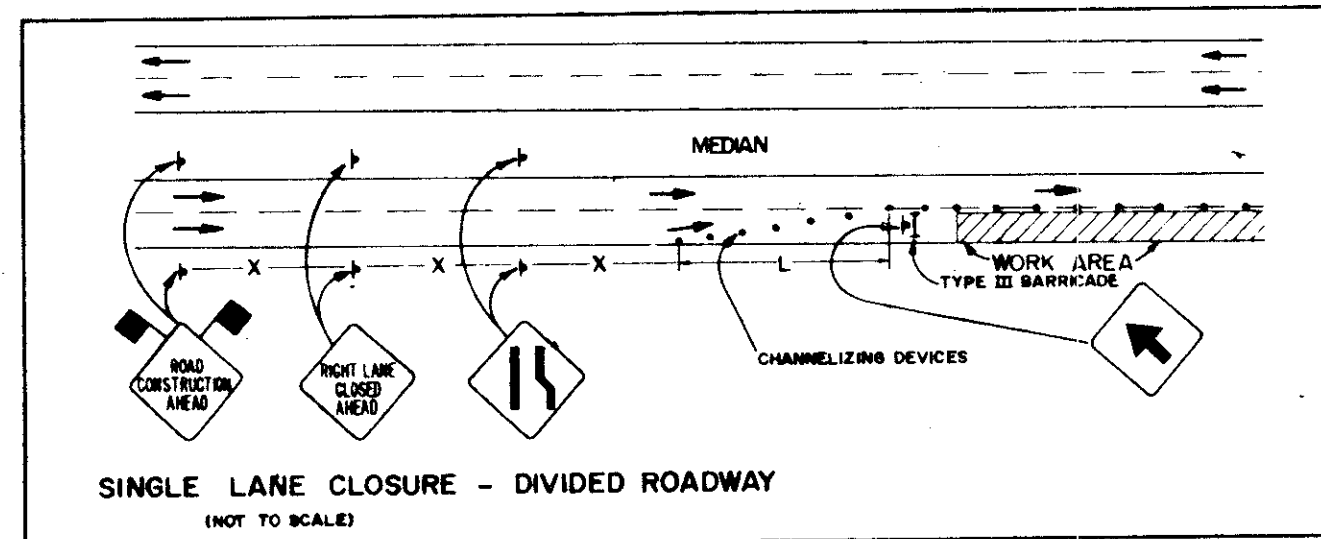
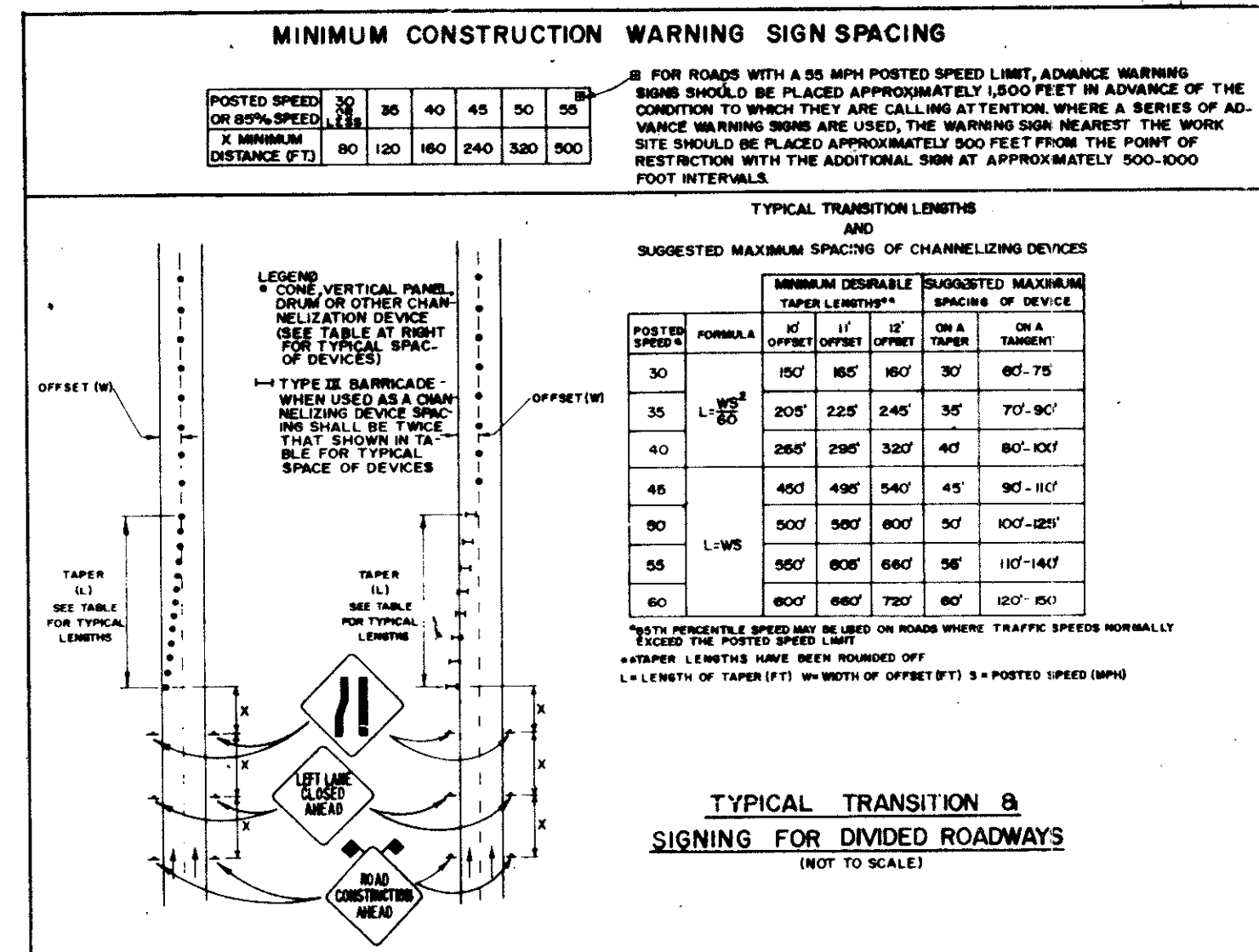
- ALL SIGNS, SIGN COLOR, SIGN LETTERING AND SIGN REFLECTORIZATION SHALL CONFORM WITH THE TEXAS MANUAL ON TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS. THE CONTRACTOR SHALL MAINTAIN EACH SIGN IN A CLEAN AND GOOD CONDITION.
- CONSTRUCTION SIGNS SHALL BE MADE FROM WOOD OR METAL. THE DESIGNATION METAL AND WOOD AS PRIMARY MATERIALS FOR SIGNS SHALL NOT BE INTERPRETED TO EXCLUDE OTHER SUITABLE RIGID MATERIALS NOW OR HERE-AFTER AVAILABLE.
- SIGNS SHALL BE REMOVED UPON COMPLETION OF THE WORK. INDIVIDUAL WARNING SIGNS SHALL BE REMOVED, TURNED AWAY FROM THE VIEW OF TRAFFIC, OR COVERED UNLESS THE SPECIFIC DANGER OF WHICH THE PARTICULAR SIGN WARNS HAS CEASED TO EXIST, EITHER PERMANENTLY OR TEMPORARILY. THE SIGN MUST BE PROMPTLY REPLACED, TURNED INTO VIEW, OR UNCOVERED WHEN THE SITUATION AGAIN BECOMES A POTENTIAL DANGER.
- SIGNS ERECTED ON PORTABLE SUPPORTS FOR USE ON CONSTRUCTION PROJECTS NORMALLY MEAN SIGNS WHICH ARE USED DURING THE DAY TO WARN OR GUIDE TRAFFIC THROUGH AND OR AROUND THE ACTUAL CONSTRUCTION AREA, BUT AT THE END OF THE WORKDAY SUCH SIGNS ARE EITHER REMOVED OR TURNED AWAY FROM THE VIEW OF TRAFFIC. PORTABLE SUPPORTS SHALL BE AS SHOWN OR AS APPROVED BY THE ENGINEER. THE BOTTOM OF THE SIGN SHALL BE A MINIMUM OF ONE (1) FOOT ABOVE THE PAVEMENT EDGE. SIGNS REQUIRED FOR NIGHTTIME USAGE SHOULD NOT NORMALLY BE MOUNTED ON PORTABLE SUPPORTS EXCEPT WHEN APPROVED BY THE ENGINEER.
- SIGNS ERECTED ON FIXED SUPPORTS FOR USE ON CONSTRUCTION PROJECTS NORMALLY MEAN SIGNS THAT ARE TO REMAIN IN PLACE FOR BOTH DAY AND NIGHT USAGE TO REGULATE, WARN AND GUIDE TRAFFIC IN ADVANCE OF AND WITHIN THE LIMITS OF THE PROJECT INCLUDING THE CROSSROAD APPROACHES. HOWEVER, UNDER CERTAIN CONDITIONS, SUCH AS WHERE A SIGN MAY BE REQUIRED FOR A FEW DAYS' DURATION AND THEN IS NO LONGER NEEDED, OR WHERE A SIGN IS MOVED FROM LOCATION TO LOCATION EVERY FEW DAYS OR WHERE IT IS NOT PRACTICAL OR DESIRABLE TO PROVIDE A FIXED MOUNTING, SUCH SIGNS MAY BE ERECTED ON A TEMPORARY TYPE OF SUPPORT. TEMPORARY SUPPORTS SHALL BE AS SHOWN OR AS APPROVED BY THE ENGINEER. SIGNS ERECTED ON TEMPORARY SUPPORTS SHOULD BE AT A MINIMUM HEIGHT OF 3 FEET. SIGNS ERECTED ON FIXED SUPPORTS SHOULD BE AT A MINIMUM OF SEVEN (7) FEET, REGARDLESS OF THE TYPE OF SUPPORT USED, REGULATORY SIGNS SHOULD NOT BE ERECTED AT HEIGHT LESS THAN 7-FOOT MINIMUM SPECIFIED ABOVE UNLESS A LOWER HEIGHT IS APPROVED BY THE ENGINEER. POSTS FOR FIXED SUPPORTS SHOULD BE SET IN THE GROUND WITHOUT CONCRETE FOOTINGS.
- WHERE PORTABLE OR TEMPORARY SUPPORTS REQUIRE THE USE OF WEIGHTS TO KEEP A SIGN OR BARRICADE FROM TURNING OVER, THE USE OF SOME TYPE OF SANDBAG IS RECOMMENDED. THE USE OF PIECES OF CONCRETE, ROCKS, IRON, STEEL OR OTHER SOLID OBJECTS WILL NOT BE PERMITTED.

CONSTRUCTION PAVEMENT MARKINGS

- WHEN REQUIRED ELSEWHERE IN THE PLANS, THE CONTRACTOR WILL BE RESPONSIBLE FOR MAINTAINING PAVEMENT MARKINGS ON ALL ROADWAYS THAT ARE OPEN TO TRAFFIC WITHIN THE LIMITS OF THE PROJECT. ON PROJECTS INVOLVING ROADWAY SURFACING WHICH WILL REQUIRE PAVEMENT MARKING FOR CONTROL OF TRAFFIC DURING CONSTRUCTION, THE MARKINGS MAY INCLUDE BOTH STANDARD AND ABBREVIATED MARKINGS AS DEFINED BELOW:
 - STANDARD PAVEMENT MARKINGS** -- STANDARD MARKINGS PLACED IN CONFORMANCE WITH THE REQUIREMENTS OF THE TEXAS MUTCD. SUCH MARKINGS SHOULD BE PLACED ON ALL ROADWAYS OPEN TO TRAFFIC DURING CONSTRUCTION, INCLUDING NEW PAVEMENT, RESURFACING, DETOURS OR OTHER ROADWAYS WHERE CONSTRUCTION ACTIVITIES MAY HAVE COVERED OR OBLITERATED EXISTING MARKINGS. STANDARD MARKINGS SHOULD BE PLACED AS SOON AS POSSIBLE AND PRACTICAL. WHEN IT IS NOT PRACTICAL OR POSSIBLE TO PLACE STANDARD MARKINGS AT THE END OF EACH DAY'S WORK, ABBREVIATED MARKINGS MAY BE UTILIZED FOR SHORT PERIODS UNTIL STANDARD MARKINGS CAN BE PLACED.
 - ABBREVIATED PAVEMENT MARKINGS** -- ABBREVIATED PAVEMENT MARKINGS ARE SHORTER IN LENGTH THAN STANDARD MARKINGS. ABBREVIATED PAVEMENT MARKINGS MAY BE USED TO DELINEATE LANE CONTINUITY ONLY UNTIL SUCH TIME AS STANDARD MARKINGS CAN BE PLACED. THEY ARE NOT INTENDED TO SUBSTITUTE FOR STANDARD MARKINGS FOR PERIODS GREATER THAN TWO (2) WEEKS. TO SEPARATE TRAFFIC FLOWS IN OPPOSING DIRECTIONS, THE PAVEMENT MARKINGS SHALL BE YELLOW. WHITE PAVEMENT MARKINGS SHALL BE USED TO DELINEATE THE SEPARATION OF TRAFFIC FLOWS IN THE SAME DIRECTION.
 - PAVEMENT MARKINGS, MATERIALS** -- TEMPORARY PAVEMENT MARKING MAY BE ACCOMPLISHED BY USE OF STANDARD TRAFFIC PAVEMENT MARKING PAINT OR BY PAVEMENT MARKING TAPE (SCOTCHLANE OR STAMARK BRAND OR AS APPROVED BY THE ENGINEER). TEMPORARY PAVEMENT MARKINGS SHALL BE REMOVED IMMEDIATELY WHEN NO LONGER APPLICABLE.
- WHEN ABBREVIATED PAVEMENT MARKINGS ARE USED, A DO NOT PASS SIGN SHALL BE USED TO MARK THE BEGINNING OF THE SECTION WHERE PASSING IS TO BE PROHIBITED AND A PASS WITH CARE SIGN SHALL BE USED TO MARK THE BEGINNING OF A SECTION WHERE PASSING IS PERMITTED.

REMOVAL OF PAVEMENT MARKINGS

- REMOVAL OF PAVEMENT MARKINGS -- INCLUDES CENTERLINE, BARRIER LINES, LANE LINES, EDGE LINES, AND RAISED PAVEMENT MARKINGS.
- IMMEDIATELY UPON OPENING A DETOUR TO TRAFFIC, ANY PAVEMENT MARKINGS ON THE EXISTING ORIGINAL ROADWAY IN THE DETOUR TRANSITION AREA THAT ARE NO LONGER APPLICABLE AND WHICH MAY CREATE CONFUSION OR DIRECT A MOTORIST TOWARD OR INTO THE CLOSED PORTION OF THE ROADWAY, SHALL BE REMOVED OR OBLITERATED. IN ADDITION, WHEN A DETOUR IS TO BE DISCONTINUED, ANY PAVEMENT MARKINGS USED TO TRANSITION TRAFFIC INTO THE DETOUR WHICH MAY CREATE CONFUSION OR DIRECT A MOTORIST INTO THE DISCONTINUED DETOUR SHALL LIKEWISE BE REMOVED OR OBLITERATED. THE ABOVE SHALL NOT APPLY TO DETOURS OF A SHORT TIME DURATION OF A FEW HOURS WHERE FLAGMEN OR SUFFICIENT CHANNELIZING DEVICES ARE USED TO OUTLINE THE DETOUR ROUTE AND THE DETOUR IS NOT TO BE MAINTAINED OVERNIGHT.
- THE REMOVAL OF PAVEMENT MARKINGS SHALL BE AN INTEGRAL PART OF ESTABLISHING THE DETOUR. DETOURS SHALL BE PLANNED AND SCHEDULED WELL ENOUGH IN ADVANCE TO ALLOW ADEQUATE TIME TO COMPLETE ALL PHASES OF THE OPERATION PRIOR TO DARKNESS. IF INCLEMENT WEATHER OR DARKNESS BECOMES A FACTOR, IT WILL BE THE CONTRACTOR'S DECISION TO CONTINUE WITH THE DETOUR OPERATION OR RETAIN THE EXISTING TRAVELWAY OPEN TO TRAFFIC WHEN ANY OR ALL OF THE REQUIREMENTS OF THE DETOUR CANNOT BE ACCOMPLISHED.
- PAVEMENT MARKINGS SHALL BE REMOVED TO THE FULLEST EXTENT POSSIBLE, SO AS NOT TO LEAVE A DISCRETE MARKING, BY ANY METHOD THAT DOES NOT MATERIALLY DAMAGE THE SURFACE OR TEXTURE OF THE PAVEMENT. SUBJECT TO THE APPROVAL OF THE ENGINEER, ANY METHOD THAT PROVES TO BE SUCCESSFUL ON A PARTICULAR TYPE PAVEMENT MAY BE USED. OVERPAINTING OF THE MARKING WILL NOT BE PERMITTED. REMOVAL OF RAISED PAVEMENT MARKINGS SHALL BE AS DIRECTED BY THE ENGINEER.
- WHERE MECHANICAL MEANS OF MARKING REMOVAL HAVE BEEN EMPLOYED TO COMPLETELY REMOVE THE MARKING AND ITS REFLECTIVITY, PAINT OF A COLOR MATCHING THE PAVEMENT SURFACE OR USED CRANKCASE OIL MAY BE EMPLOYED IF NECESSARY AS A MEANS OF COVERING CONTRASTING PAVEMENT TEXTURE. NIGHTTIME INSPECTIONS ARE NEEDED TO VERIFY THE CONTINUED EFFECTIVENESS OF THE CHANGE.
- PAVEMENT MARKINGS TO BE REMOVED SHALL BE AS SHOWN IN THE PLANS OR AS DIRECTED BY THE ENGINEER. REMOVAL OF PAVEMENT MARKINGS WILL BE CONSIDERED SUBSIDIARY TO THE ITEM BARRICADES, SIGNS AND TRAFFIC HANDLING.



THE TOWN OF ADDISON						
CONSTRUCTION SIGNING AND BARRICADE DETAILS						
DESIGN	DRAWN	DATE	SCALE	NOTES	FILE	NO.
D.H.D.	A.R.M.	8/00	NONE			111

