

GENERAL NOTES - WATER

GENERAL NOTES - SANITARY SEWER

GENERAL NOTES - DRAINAGE

GENERAL NOTES - PAVING

GENERAL NOTES - LOT GRADING

- ALL WORK AND MATERIAL SHALL BE IN ACCORDANCE WITH THE TOWN OF ADDISON STANDARD SPECIFICATIONS AND GENERAL DESIGN STANDARDS.
- ALL 8" PIPE SHALL BE INSTALLED WITH MINIMUM 42" COVER OVER TOP OF PIPE, AND 10" OR LARGER PIPE WITH 48" COVER OVER TOP OF PIPE.
- ALL CONNECTIONS TO EXISTING SYSTEMS SHALL BE MADE UNDER PRESSURE UNLESS DRY CONNECTION WILL NOT CAUSE ANYONE TO BE WITHOUT WATER.
- ALL PIPE 6" IN DIAMETER AND LARGER USED IN DISTRIBUTION SYSTEM SHALL BE RATED FOR A MINIMUM OF 150 PSI, SHALL DISPLAY THE APPROPRIATE AWWA SPECIFICATION STAMP, SHALL DISPLAY THE NATIONAL SANITATION FOUNDATION (NSF) STAMP, SHALL CONFORM TO THE TEXAS STATE BOARD OF FIRE INSURANCE REQUIREMENTS, AND SHALL BE PVC AS APPROVED.
- PVC PIPE SHALL MEET THE REQUIREMENTS OF AWWA C-900 "STANDARD FOR PVC PRESSURE PIPE" PRESSURE CLASSES OF 150 PSI MINIMUM. PIPE SHALL BE FURNISHED IN 20 FOOT LAYING LENGTHS. THE EMBEDMENT FOR PVC PIPE SHALL BE A CLASS 4 EMBEDMENT. THE EMBEDMENT CONSISTS OF A CRADLE OF SAND A MINIMUM OF 6" BELOW THE PIPE AND THEN BROUGHT UP TO A POINT 12" ABOVE THE PIPE.
- THE COMPLETED LINE SHALL BE STERILIZED BEFORE BEING APPROVED FOR SERVICE. TEXAS STATE DEPARTMENT OF HEALTH APPROVED BACTERIOLOGICAL TESTS SHALL BE RECEIVED PRIOR TO ACCEPTANCE OF THE LINE OR PLACING LINE IN OPERATION. SAMPLES SHALL BE TAKEN IN THE PRESENCE OF THE DIRECTOR OF UTILITIES OF HIS DESIGNEE.
- THE CONTRACTOR SHALL FURNISH AND/OR INSTALL ALL LABOR, MATERIALS, AND EQUIPMENT NECESSARY FOR TESTING (NO PAY ITEM).
- FIRE HYDRANTS TO BE TOWN OF ADDISON APPROVED (MUELLER CENTURIUM MODEL).
- FIRE HYDRANTS SHALL BE PAINTED AS PER TOWN OF ADDISON STANDARDS AND SPECIFICATIONS AND LOCATED IN A PROTECTED AREA WITH 6" CURB OR BOLLARDS.
- STEAMER NOZZLES ON FIRE HYDRANTS SHALL BE 18" ABOVE THE TOP OF THE CURB ON FINISHED GRADE, AND FACE THE CENTER OF THE FIRE LANE OR STREET. FIRE HYDRANTS SHALL USUALLY BE LOCATED FOUR (4) FEET, BUT NOT LESS THAN TWO (2) FEET NOR MORE THAN SIX (6) FEET, BEHIND THE CURB.
- THE LOCATION OF ALL UTILITIES INDICATED ON THE PLANS ARE TAKEN FROM EXISTING PUBLIC RECORDS. THE EXACT LOCATION AND ELEVATION OF ALL PUBLIC UTILITIES MUST BE DETERMINED BY THE CONTRACTOR. IT SHALL BE THE DUTY OF THE CONTRACTOR TO ASCERTAIN WHETHER ANY ADDITIONAL FACILITIES OTHER THAN THOSE SHOWN ON THE PLAN MAY BE PRESENT.
- CONTRACTOR SHALL PROTECT ALL PUBLIC UTILITIES IN THE CONSTRUCTION OF THIS PROJECT. ALL MANHOLES, CLEANOUTS, VALVE BOXES, FIRE HYDRANTS, ETC. MUST BE ADJUSTED TO PROPER LINE AND GRADE BY THE UTILITY CONTRACTOR AFTER PLACEMENT OF PAVING.
- UTILITY CONTRACTOR SHALL BE RESPONSIBLE FOR PAVING OF INLET BLOCKOUTS, VARIABLE HEIGHT CURBS, AND INLET THROATS.
- CONTRACTOR SHALL BE RESPONSIBLE FOR MAINTAINING TRENCH SAFETY REQUIREMENTS IN ACCORDANCE WITH TOWN STANDARDS, TEXAS STATE LAW, AND OSHA STANDARDS. CONTRACTOR TO PROVIDE TOWN WITH TRENCH SAFETY PLANS.
- CONTRACTOR SHALL PROVIDE A MINIMUM CLEARANCE OF 12 INCHES BETWEEN WATER AND STORM SEWER INLETS.
- UTILITY CONTRACTOR SHALL BE RESPONSIBLE FOR PROVIDING DESIGN ENGINEER WITH ONE SET OF "AS-BUILT" DRAWINGS UPON COMPLETION OF PROJECT. AS-BUILT DRAWINGS SHALL GIVE HORIZONTAL TIES TO ALL FIRE HYDRANTS, SEWER SERVICES AND OTHER KEY SUBTERRANEAN UTILITY IMPROVEMENTS. AS-BUILT DRAWINGS SHALL BE SUBMITTED PRIOR TO ACCEPTANCE OF SUBDIVISION BY THE TOWN OF ADDISON AND BEFORE FINAL PAYMENT IS MADE TO THE CONTRACTOR.
- GROUND ELEVATIONS SHOULD BE AT OR NEAR PROPOSED SUBGRADE ELEVATIONS IN STREET RIGHTS-OF-WAY PRIOR TO CONSTRUCTION OF ALL UTILITY IMPROVEMENTS. THE UTILITY CONTRACTOR SHALL BE RESPONSIBLE FOR RETURNING ALL GRADES WITHIN 0.2 FEET OF ESTABLISHED GRADES AFTER CONSTRUCTION OF ALL UTILITIES. DESIGN ENGINEER SHALL VERIFY GRADES PRIOR TO MOBILIZATION OF PAVING CONTRACTOR. ANY REVERIFICATION OF GRADES BY THE DESIGN ENGINEER SHALL BE THE UTILITY CONTRACTOR'S EXPENSE AND WILL BE PAID DIRECTLY TO THE DESIGN ENGINEER BY THE UTILITY CONTRACTOR. FAILURE TO MAKE PAYMENT SHALL RESULT IN THE ENGINEER DEDUCTING THE AMOUNT FROM UTILITY CONTRACTOR'S CONTRACT AT AN HOURLY RATE ESTABLISHED BY THE DESIGN ENGINEER.
- DESIGN ENGINEER TO PROVIDE ONE SET OF CONSTRUCTION STAKES FOR THOSE IMPROVEMENTS. ANY RESTAKING SHALL BE AT THE CONTRACTOR'S EXPENSE AND WILL BE PAID DIRECTLY TO THE ENGINEER BY THE UTILITY CONTRACTOR. FAILURE TO MAKE PAYMENT SHALL RESULT IN THE ENGINEER DEDUCTING THE AMOUNT FROM THE UTILITY CONTRACTOR'S CONTRACT AT AN HOURLY RATE ESTABLISHED BY THE DESIGN ENGINEER.
- BEDDING AND BACKFILL SHALL BE CONSIDERED ON SUBSIDIARY TO THE PAY ITEM FOR PIPE AND SHALL NOT BE PAID FOR DIRECTLY.
- ALL BARRICADES, WARNING SIGNS, LIGHTS, DEVICES, ETC. FOR THE GUIDANCE AND PROTECTION OF TRAFFIC AND PEDESTRIANS MUST CONFORM TO THE INSTALLATION SHOWING THE 1980 TEXAS MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES, AS CURRENTLY AMENDED, TEXAS STATE DEPARTMENT OF HIGHWAYS AND PUBLIC TRANSPORTATION AND SHALL BE INCIDENTAL TO THE COST OF CONSTRUCTION (NO PAY ITEM).
- UNDER NO CIRCUMSTANCES SHALL THE UTILITY CONTRACTOR PLACE UTILITY TRENCH SPOIL WITHIN THE 100 YEAR FLOODPLAIN OR DESIGNED DRAINAGE EASEMENT AS SHOWN ON THE FINAL PLAN FOR THIS ADDITION.
- UTILITY CONTRACTOR SHALL BE RESPONSIBLE FOR CLOSING ALL OPEN TRENCHES AT THE END OF EACH WORKING DAY.
- WHEN INSTALLING WATER MAINS AT SANITARY SEWER CROSSINGS, NO JOINT OF WATER MAIN SHALL BE LOCATED WITHIN NINE (9) FEET OR THE SEWER CROSSING.

- ALL WORK AND MATERIAL SHALL BE IN ACCORDANCE WITH THE TOWN OF ADDISON STANDARD SPECIFICATIONS AND GENERAL DESIGN STANDARDS.
- ALL SANITARY SEWER PIPE UTILIZED SHALL BE PVC PIPE TYPE SDR-35.
- SEWER LATERALS SHALL BE EXTENDED TO RIGHT-OF-WAY LINES AND SHALL EXTEND ABOVE THE GROUND PER DETAILS.
- PIPE AND FITTINGS SHALL BE EXTRA STRENGTH PIPE CONFORMING TO ASTM C700.
- MANHOLES SHALL BE CONSTRUCTED OF CAST-IN-PLACE 3000 PSI CONCRETE AND SHALL BE 4" INSIDE DIAMETER.
- CONSTRUCTION WILL BEING AT DOWNSTREAM END OF PROJECT AN CONTINUE UPSTREAM WITH BELLS FACING UPSTREAM. CONSTRUCTION OF BRANCH MAIN WILL NOT BEGIN BEFORE MAIN IS CONSTRUCTED TO BRANCH POINT. NOT MORE THAN 300 FEET OF TRENCH SHALL BE OPENED IN ADVANCE OF PIPE INSTALLATION.
- THE ENTIRE SEWER SYSTEM SHALL BE FLUSHED AS DIRECTED BY THE DIRECTOR OF UTILITIES.
- THE CONTRACTOR SHALL FURNISH ADEQUATE PERSONNEL AND EQUIPMENT REQUIRED TO PERFORM TESTS (NO PAY ITEM).
- UPON COMPLETION OF PVC SANITARY SEWER PIPE INSTALLATION, AND AFTER 30 DAYS, THE CONTRACTOR SHALL PULL A MANDREL THROUGH THE PIPE TO TEST FOR A MAXIMUM OF 5% DEFLECTION (NO PAY ITEM).
- THE CONTRACTOR WILL BE REQUIRED TO RETAIN A QUALIFIED COMPANY TO PERFORM A VIDEO INSPECTION OF THE SEWER MAINS IN THE SUBDIVISION AT THE CONTRACTOR'S EXPENSE. PRIOR TO VIDEO INSPECTION SEWER MAINS WILL BE COMPLETELY FLUSHED. THE TOWN WILL MAKE THE FINAL DETERMINATION IF REPAIRS ARE REQUIRED AND THAT DECISION WILL BE FINAL. THE FINAL SET OF TAPES AND LOGS WILL BE GIVEN TO THE INSPECTOR OF THE CONSTRUCTION (NO PAY ITEM).
- THE LOCATION OF ALL UTILITIES INDICATED ON THE PLANS ARE TAKEN FROM EXISTING PUBLIC RECORDS. THE EXACT LOCATION AND ELEVATION OF ALL PUBLIC UTILITIES MUST BE DETERMINED BY THE CONTRACTOR. IT SHALL BE THE DUTY OF THE CONTRACTOR TO ASCERTAIN WHETHER ANY ADDITIONAL FACILITIES OTHER THAN THOSE SHOWN ON THE PLAN MAY BE PRESENT.
- CONTRACTOR SHALL PROTECT ALL PUBLIC UTILITIES IN THE CONSTRUCTION OF THIS PROJECT. ALL MANHOLES, CLEANOUTS, VALVE BOXES, FIRE HYDRANTS, ETC. MUST BE ADJUSTED TO PROPER LINE AND GRADE BY THE UTILITY CONTRACTOR AFTER PLACEMENT OF PAVING.
- UTILITY CONTRACTOR SHALL BE RESPONSIBLE FOR PAVING OF INLET BLOCKOUTS, VARIABLE HEIGHT CURBS, AND INLET THROATS.
- CONTRACTOR SHALL BE RESPONSIBLE FOR PROVIDING DESIGN ENGINEER WITH ONE SET OF "AS-BUILT" DRAWINGS UPON COMPLETION OF PROJECT. AS-BUILT DRAWINGS SHALL GIVE HORIZONTAL TIES TO ALL FIRE HYDRANTS, SEWER SERVICES AND OTHER KEY SUBTERRANEAN UTILITY IMPROVEMENTS. AS-BUILT DRAWINGS SHALL BE SUBMITTED PRIOR TO ACCEPTANCE OF SUBDIVISION BY THE TOWN OF ADDISON AND BEFORE FINAL PAYMENT IS MADE TO THE CONTRACTOR.
- GROUND ELEVATIONS SHOULD BE AT OR NEAR PROPOSED SUBGRADE ELEVATIONS IN STREET RIGHTS-OF-WAY PRIOR TO CONSTRUCTION OF ALL UTILITY IMPROVEMENTS. THE UTILITY CONTRACTOR SHALL BE RESPONSIBLE FOR RETURNING ALL GRADES WITHIN 0.2 FEET OF ESTABLISHED GRADES AFTER CONSTRUCTION OF ALL UTILITIES. DESIGN ENGINEER SHALL VERIFY GRADES PRIOR TO MOBILIZATION OF PAVING CONTRACTOR. ANY REVERIFICATION OF GRADES BY THE DESIGN ENGINEER SHALL BE THE UTILITY CONTRACTOR'S EXPENSE AND WILL BE PAID DIRECTLY TO THE DESIGN ENGINEER BY THE UTILITY CONTRACTOR. FAILURE TO MAKE PAYMENT SHALL RESULT IN THE ENGINEER DEDUCTING THE AMOUNT FROM UTILITY CONTRACTOR'S CONTRACT AT AN HOURLY RATE ESTABLISHED BY THE DESIGN ENGINEER.
- DESIGN ENGINEER TO PROVIDE ONE SET OF CONSTRUCTION STAKES FOR THOSE IMPROVEMENTS. ANY RESTAKING SHALL BE AT THE CONTRACTOR'S EXPENSE AND WILL BE PAID DIRECTLY TO THE ENGINEER BY THE UTILITY CONTRACTOR. FAILURE TO MAKE PAYMENT SHALL RESULT IN THE ENGINEER DEDUCTING THE AMOUNT FROM THE UTILITY CONTRACTOR'S CONTRACT AT AN HOURLY RATE ESTABLISHED BY THE DESIGN ENGINEER.
- BEDDING AND BACKFILL SHALL BE CONSIDERED ON SUBSIDIARY TO THE PAY ITEM FOR PIPE AND SHALL NOT BE PAID FOR DIRECTLY.
- ALL BARRICADES, WARNING SIGNS, LIGHTS, DEVICES, ETC. FOR THE GUIDANCE AND PROTECTION OF TRAFFIC AND PEDESTRIANS MUST CONFORM TO THE INSTALLATION SHOWING THE 1980 TEXAS MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES, AS CURRENTLY AMENDED, TEXAS STATE DEPARTMENT OF HIGHWAYS AND PUBLIC TRANSPORTATION AND SHALL BE INCIDENTAL TO THE COST OF CONSTRUCTION (NO PAY ITEM).
- UTILITY CONTRACTOR SHALL BE RESPONSIBLE FOR CLOSING ALL OPEN TRENCHES AT THE END OF EACH WORKING DAY.
- WHEN INSTALLING WATER MAINS AT SANITARY SEWER CROSSINGS, NO JOINT OF WATER MAIN SHALL BE LOCATED WITHIN NINE (9) FEET OF THE SANITARY CROSSING.

- ALL MATERIALS AND CONSTRUCTION SHALL BE IN ACCORDANCE WITH THE TOWN OF ADDISON STANDARD SPECIFICATIONS AND GENERAL DESIGN STANDARDS.
- ALL STORM SEWER PIPE SHALL BE CLASS III RCP, ASTM C76.
- ALL DITCHES SHALL BE TAMPED AND COMPACTED TO 95% STANDARD PROCTOR DENSITY.
- SEE WATER AND SANITARY SEWER PLAN AND PAVING PLAN/PROFILE FOR ADDITIONAL INFORMATION RELATED TO PAVING, DRAINAGE AND OTHER UTILITY CONSTRUCTION.
- THE LOCATION OF EXISTING UTILITIES INDICATED ON THESE PLANS ARE TAKEN FROM PUBLIC RECORDS. THE EXACT LOCATION AND ELEVATION OF ALL PUBLIC UTILITIES SHALL BE DETERMINED BY THE CONTRACTOR TO ASCERTAIN WHETHER ANY ADDITIONAL FACILITIES OTHER THAN THOSE SHOWN IN THESE PLANS MAY BE PRESENT.
- ALL CONCRETE USED IN THE CONSTRUCTION OF STORM SEWER MANHOLES, INLETS, ETC. SHALL HAVE A MINIMUM OF 3,600 PSI AT 28 DAYS. (5.5 SACK MIX)
- NO UTILITY TRENCHES SHALL BE LEFT OPEN OVER NIGHT DURING THE CONSTRUCTION OF ALL UNDERGROUND DRAINAGE FACILITIES.
- ALL BARRICADES, WARNING SIGNS, LIGHTS, ETC. FOR THE GUIDANCE AND PROTECTION OF TRAFFIC AND PEDESTRIANS MUST CONFORM TO THE INSTALLATION SHOWING THE 1980 TEXAS MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES, AS CURRENTLY AMENDED, TEXAS STATE DEPARTMENT OF HIGHWAYS AND PUBLIC TRANSPORTATION, AND SHALL BE INCIDENTAL TO THE COST OF CONSTRUCTION (NO PAY ITEM).
- ALL CAST IRON CASTINGS SHALL CONFORM TO ASTM A 48, CLASS 30, GRAY CAST IRON.
- CONSTRUCTION SHALL BEGIN AT DOWNSTREAM END OF PROJECT AND CONTINUE UPSTREAM WITH PIPE GROOVES FACING UPSTREAM. PIPE SHALL BE LAID ON UNDISTURBED SOIL OR GRANULAR MATERIAL UNLESS OTHERWISE SPECIFIED ON APPROVED CONSTRUCTION DRAWINGS. NO MORE THAN 1/2 OF THE TONGUE LENGTH SHALL BE EXPOSED BEFORE JOINT MATERIAL IS APPLIED.
- UTILITY CONTRACTOR SHALL BE RESPONSIBLE FOR PAVING OF INLET BLOCKOUTS, VARIABLE HEIGHT CURBS, AND INLET THROATS. (NO PAY ITEM).
- CONTRACTOR SHALL BE RESPONSIBLE FOR MAINTAINING TRENCH SAFETY REQUIREMENTS IN ACCORDANCE WITH TOWN STANDARDS, TEXAS STATE LAW AND OSHA STANDARDS. CONTRACTOR TO PROVIDE TOWN WITH TRENCH SAFETY PLANS.
- CONTRACTOR SHALL PROVIDE A MINIMUM CLEARANCE OF 12 INCHES BETWEEN WATER AND STORM SEWER INLETS.
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- BEDDING AND BACKFILL SHALL BE CONSIDERED ON SUBSIDIARY TO THE PAY ITEM FOR PIPE AND SHALL NOT BE PAID FOR DIRECTLY.

- ALL MATERIALS AND WORKMANSHIP SHALL CONFORM TO THE "STANDARD SPECIFICATIONS FOR PUBLIC WORKS CONSTRUCTION" PUBLISHED BY THE NORTH CENTRAL TEXAS COUNCIL OF GOVERNMENTS - LATEST EDITION AND THE LATEST TOWN OF ADDISON CONSTRUCTION STANDARDS AND SPECIFICATIONS FOR ROADS, STREETS, STRUCTURES AND UTILITIES. ALL TESTING WILL BE PERFORMED BY A CERTIFIED TECHNICIAN IN THE PRESENCE OF A TOWN INSPECTOR.
  - RESIDENTIAL STREETS SHALL BE CONSTRUCTED OF REINFORCED CONCRETE HAVING A MINIMUM THICKNESS OF SIX INCHES WITH MONOLITHIC CURB AND GUTTERS. THE MINIMUM WIDTH OF RESIDENTIAL STREETS SHALL BE 27 FEET MEASURED FROM BACK OF CURBS.
  - WHEN REQUESTED, THE PAVING CONTRACTOR SHALL PROVIDE THE TOWN ENGINEER WITH A CONCRETE MIX DESIGN, PREPARED BY AN APPROVED INDEPENDENT TESTING LABORATORY.
  - STEEL REINFORCING BARS SHALL BE NEW BILLET STEEL GRADE 60 OR GRADE 40, CONFORMING TO ASTM A615. BARS THAT REQUIRE BENDING SHALL BE GRADE 40. ALL REINFORCING BARS SHALL BE THE DEFORMED TYPE.
  - CONCRETE SHALL NOT BE POURED WHEN THE TEMPERATURE IS BELOW 40 DEGREES F. AND FALLING. CONCRETE MAY BE POURED WHEN THE TEMPERATURE IS ABOVE 35 DEGREES F. AND RISING. SALT OR OTHER CHEMICAL ADDITIVES SHALL NOT BE ADDED TO CONCRETE TO PREVENT FREEZING. THE PAVING CONTRACTOR SHALL BE RESPONSIBLE FOR REPLACING ANY CONCRETE THAT FREEZES DURING CURING.
  - IMMEDIATELY UPON UNINTENDED STOPPAGE OF POURING OPERATION, A STANDARD BULKHEAD SHALL BE INSTALLED AT RIGHT ANGLES TO THE CENTERLINE OF PAVEMENT.
  - JOINT SEALING MATERIAL SHALL BE PLACED IN SAWED AND OTHER JOINTS AS REQUIRED.
  - PAVEMENT SHALL BE FINISHED WITH A BELT FINISH OR AS DIRECTED BY THE TOWN ENGINEER. AFTER FINISHING IS COMPLETE AND THE CONCRETE IS STILL WORKABLE, THE GUTTER SURFACE SHALL BE TESTED FOR TRUENESS WITH AN APPROVED 10 FOOT STEEL STRAIGHT EDGE BY THE CONTRACTOR. THE MAXIMUM ORDINATE MEASUREMENT SHALL BE 1/16 INCH.
  - ALL SUB-GRADE IS TO BE COMPACTED TO A MINIMUM F 95% OF STANDARD PROCTOR AT +/-2% OF OPTIMUM MOISTURE AND LIME STABILIZED ACCORDING TO THE FOLLOWING TABLE:
- | PLASTICITY INDEX | % LIME REQUIRED |
|------------------|-----------------|
| 0 - 25           | 0               |
| 21 - 25          | 5               |
| 15 - 20          | 4               |
| 14 OR LESS       | 0               |
- TESTING OF MATERIALS REQUIRED FOR THE CONSTRUCTION OF THE SPECIFIED STREET IMPROVEMENTS SHALL BE PERFORMED BY AN APPROVED AGENCY FOR TESTING MATERIALS IN THE PRESENCE OF A TOWN INSPECTOR. THE NOMINATION OF THE TESTING LABORATORY AND THE PAYMENT OF SUCH TESTING SERVICES SHALL BE MADE BY THE CONTRACTOR. THE ENGINEER SHALL APPROVE THE LABORATORY NOMINATED TO DO THE TESTING OF MATERIALS. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO SHOW BY STANDARD TESTING PROCEDURES THAT THE WORK CONSTRUCTED DOES MEET THE REQUIREMENTS OF THE SPECIFICATIONS.
  - THE CONTRACTOR SHALL FURNISH A MAINTENANCE BOND TO THE TOWN TO RUN ONE (1) YEAR FROM THE DATE OF ACCEPTANCE OF IMPROVEMENTS BY THE TOWN.
  - ALL DIMENSIONS ARE TO BACK OF CURB UNLESS OTHERWISE NOTED.
  - THE CONCRETE SHALL BE A MINIMUM OF 5.5 SACK CEMENT WITH COMPRESSIVE STRENGTH OF 3600 PSI AT 28 DAYS. SLUMP SHALL BE A 3" +/- 1/2" WITH MECHANICAL FINISHING, OR 4" +/- 1/2" IF BY HAND FINISHING. AIR CONTENT SHALL BE 5% +/- 1%. WEIGHT/C.F. WITH A YIELD OF 27.0 +/- 0.2. CONCRETE MAXIMUM TEMPERATURE OF 95. ALL TESTING WILL BE PERFORMED BY A CERTIFIED TECHNICIAN IN THE PRESENCE OF A TOWN INSPECTOR. THE FOLLOWING TESTS ARE TO BE PERFORMED FOR EACH 100 YARDS OF CONCRETE OR PORTION THEREOF, PLACED ON ANY GIVEN DAY.
    - STRENGTH TEST, MOLD THREE (3) BEAMS TO BE TESTED AT 7 DAY, 14 DAY AND 28 DAY.
    - SLUMP
    - AIR CONTENT
    - WEIGHT AND YIELD
    - TEMPERATURE

APART FROM THE BEGINNING AND END OF EACH DAY'S RUN OR AT COLD JOINT AND AT THE CURB RETURNS.

DOWEL: FOR 6" PAVEMENT 3/4" DOWEL FOR 8" PAVEMENT 1" DOWEL

REINFORCEMENT: #3 BARS @ 24" OC BOTH WAYS WITH A MINIMUM OF 75% TIED.

- A CONSTRUCTION JOINT SHALL BE USED IN ALL BLOCK OUTS OR LANE PAVEMENT. THE RE-BARS SHALL EXTEND PAST THE EDGE OF THE PAVEMENT 18".
- BARRIER-FREE CURBS SHALL BE CONSTRUCTED AT ALL STREET INTERSECTIONS. SEE SHEET SD-10.
- CHAIRS APPROVED BY THE ENGINEER SHALL BE USED TO SUPPORT REINFORCING STEEL AND SHALL BE PLACED AT THE INTERSECTION OF LONGITUDINAL AND TRANSVERSE BARS AT 4'-0" SPACING.
- PAVEMENT LAYOUT WILL NECESSITATE THAT ALL CONSTRUCTION AND WARPING JOINTS COINCIDE WITH LANE LINES. THRU-LANE CONSTRUCTION WILL BE CONTINUOUS WITH ALL LEFT TURN LANES AND TRANSITIONS TO BE POURED AS FILL-INS SUBJECT TO APPROVAL BY THE ENGINEER.

- ALL CONSTRUCTION TO BE IN ACCORDANCE WITH PROVISIONS OUTLINED IN F.H.A. DATA SHEET 796 AND SPECIFICATIONS PREPARED BY REED ENGINEERING GROUP, REED PROJECT NO. 1357, DATED FEBRUARY, 1993.
- ALL SELECT FILL SHOULD CONTAIN NO DELETERIOUS MATERIAL AND SHOULD BE COMPACTED TO A DRY DENSITY OF AT LEAST 95 PERCENT STANDARD PROCTOR MAXIMUM DRY DENSITY (ASTM D 698) AT OR WITHIN THREE PERCENTAGE POINTS OF THE MATERIAL'S OPTIMUM MOISTURE CONTENT.
- ALL CLAY SOILS USED AS FILL SHOULD BE COMPACTED TO A DRY DENSITY OF AT LEAST 95 PERCENT OF STANDARD PROCTOR AND NOT EXCEEDING 98 PERCENT. THE COMPACTED MOISTURE CONTENT OF THE CLAYS DURING PLACEMENT SHOULD BE A LEAST OPTIMUM AND NOT EXCEEDING 4 PERCENTAGE POINTS ABOVE OPTIMUM.
- LIMESTONE OR OTHER ROCK-LIKE MATERIALS USED AS FILL SHOULD BE COMPACTED TO AT LEAST 95 PERCENT OF STANDARD PROCTOR MAXIMUM DRY DENSITY. NO INDIVIDUAL ROCK PIECES LARGER THAN ABOUT 6 INCHES DIAMETER SHOULD BE USED IN FILL. ADDITIONALLY, NO ROCK FILL SHOULD BE USED WITHIN 1 FOOT BELOW THE BOTTOM OF FLOOR SLABS.
- COMPACTION SHOULD BE ACCOMPLISHED BY PLACING THE FILL IN ABOUT 6 INCH TO 8 INCH THICK LIFTS AND COMPACTING EACH LIFT TO AT LEAST THE SPECIFIED MINIMUM DRY DENSITY. FIELD DENSITY TESTS SHOULD BE PERFORMED ON EACH LIFT AS NECESSARY TO EVALUATE FOR ADEQUATE COMPACTION.
- THE UPPER ONE (1) FOOT IN BUILDING PAD AREAS WHICH CONTAIN FILL SOILS SHALL BE REMOVED AND THE EXPOSED SURFACE SCARIFIED TO A DEPTH OF AT LEAST SIX (6) INCHES AND RECOMPACTED TO AT LEAST 95 PERCENT STANDARD PROCTOR MAXIMUM DRY DENSITY (ASTM D 698) AND NOT MORE THAN 98 PERCENT AT OR ABOVE OPTIMUM. THE EXPOSED SURFACE SHALL BE PROFFROLLED WITH HEAVY EQUIPMENT AND FURTHER TESTED & PROBED AS NECESSARY. AFTER RECONSTRUCTION, PROFFROLLING AND TESTING THE EXPOSED SURFACE, ANY WEEK OR HIGHLY ORGANIC SOILS NOTED SHALL BE REMOVED. UPON COMPLETION OF THE ABOVE PROFFROLLING AND TESTING, THE SOILS PREVIOUSLY REMOVED COULD BE REPLACED PROVIDED THEY ARE FREE OF NY DELETERIOUS MATERIALS AND COMPACTED TO AT LEAST 98 PERCENT STANDARD PROCTOR AT OR ABOVE OPTIMUM.
- ALL EXISTING FILL AREAS SHOULD BE PROFFROLLED WITH HEAVY EQUIPMENT. IF ANY UNSUITABLE MATERIALS THUS EXPOSED SHOULD BE REMOVED AND REPLACED WITH WELL-COMPACTED MATERIALS DESCRIBED IN NOTES NUMBERED 2-5.
- FRONT YARDS TO BE "DISHED OUT" FOR UTILITY SPOIL.
- ALL BOULDERS ENCOUNTERED DURING THE EXCAVATION PHASE OF THIS PROJECT, IF ANY UNSUITABLE MATERIALS THUS EXPOSED SHOULD BE REMOVED AND REPLACED WITH WELL-COMPACTED MATERIALS DESCRIBED IN NOTES NUMBERED 2-5.
- THE EXCAVATION WITHIN STREET RIGHTS-OF-WAY SHALL BE CUT/FILLED TO PLUS OR MINUS 0.1 FEET OF DESIGN GRADE.
- DESIGN ENGINEER SHALL PROVIDE ONE SET OF GRADE STAKES. ANY RESTAKING SHALL BE AT THE CONTRACTOR'S EXPENSE.
- AFTER THE PAVING PHASE HAS BEEN COMPLETED, THE EXCAVATION CONTRACTOR SHALL MOVE BACK ON SITE AND "FINAL BENCH" ALL LOTS OR THOSE LOTS IDENTIFIED BY THE OWNER, DESIGN ENGINEER OR ITS REPRESENTATIVE. ALL PADS SHALL BE PLUS OR MINUS 0.25 FEET OF DESIGN GRADE.

GENERAL NOTES

- PRIOR TO FINAL ACCEPTANCE BY THE TOWN OF ADDISON:
  - A TEXAS REGISTERED PROFESSIONAL ENGINEER SHALL CERTIFY THAT THE PORTION OF THE PROJECT BEING DEDICATED TO ADDISON WAS CONSTRUCTED IN ACCORDANCE WITH THE PLANS AND SPECIFICATIONS APPROVED BY THE TOWN OF ADDISON.
  - A FIVE (5) FOOT SIDEWALK SHALL BE INSTALLED ALONG BELTWAY DRIVE AS SHOWN. FIVE (5) FOOT SIDEWALK TO BE RECONSTRUCTED WITH DECEL LANES AS SHOWN ON SHEETS C-15 AND C-16.
  - A ONE (1) YEAR MAINTENANCE BOND IS REQUIRED FOR THE CITY'S PORTION OF THE INFRASTRUCTURE.
  - CONTRACTOR SHALL DEMONSTRATE THAT THE WATER AND SANITARY SEWER SYSTEMS MEET THE PROPER PRESSURE, BACTERIA AND MANHOLE TESTS. IN ADDITION, THE OWNER SHALL PROVIDE A VHS FORMAT VIDEO TAPE OF THE SANITARY SEWER.
- PRIOR TO STARTING CONSTRUCTION, THE CONTRACTOR SHALL CONTACT THE UTILITY COMPANIES TO LOCATE EXISTING FACILITIES. THESE INCLUDE, BUT MAY NOT BE LIMITED TO, THE FOLLOWING:
  - TOWN OF ADDISON
  - LONE STAR GAS
  - SOUTHWESTERN BELL
  - STORER CABLE
  - PLANNED CABLE SYSTEMS
  - TU ELECTRIC
- PRIOR TO BEGINNING CONSTRUCTION, THE OWNER OR HIS AUTHORIZED REPRESENTATIVE SHALL CONVEY A PRE-CONSTRUCTION CONFERENCE BETWEEN THE TOWN OF ADDISON, CONSULTING ENGINEER, CONTRACTOR(S), UTILITY COMPANIES AND ANY OTHER AFFECTED PARTIES. NOTIFY BRUCE ELLIS (450-2847) AT LEAST 48 HOURS PRIOR TO THE TIME OF THE CONFERENCE AND 48 HOURS PRIOR TO BEGINNING OF CONSTRUCTION.
- ANY EXISTING PAVEMENT, CURB(S) AND/OR SIDEWALKS DAMAGED OR REMOVED WILL BE REPAIRED BY THE CONTRACTOR AT THEIR EXPENSE.
- LOT PINS SHALL BE IN PLACE DURING CONSTRUCTION AND PRIOR TO FINAL ACCEPTANCE. CONCRETE MONUMENTS SHALL BE PLACED AT 80 BOUNDARY CORNERS, BLOCK CORNERS, CURVE POINTS AND ANGLE POINTS IN PUBLIC RIGHT-OF-WAY. CONCRETE MONUMENTS SHALL BE SIX (6) INCHES IN DIAMETER AND TWENTY-FOUR (24) INCHES LONG. A COPPER PIN ONE-FOURTH INCH IN DIAMETER EMBEDDED AT LEAST THREE (3) INCHES IN THE MONUMENT AT THE EXACT INTERSECTION POINT OF THE MONUMENT. THE MONUMENTS SHALL BE SET AT SUCH AN ELEVATION THAT AFTER CONSTRUCTION, THE TOP OF THE MONUMENT WILL NOT BE LESS THAN TWELVE (12) INCHES BELOW THE GROUND SURFACE.
- THE CONTRACTOR SHALL STAMP A 2-INCH "S" IN THE CURB AT THE LOCATION OF THE SEWER SERVICE LINE.
- AT INTERSECTIONS THAT HAVE VALLEY DRAINAGE, THE CROWN OF THE INTERSECTION STREETS WILL BE QUADRATE IN A DISTANCE OF FORTY (40) FEET FROM THE INTERSECTING CURB LINE UNLESS OTHERWISE NOTED.
- TEMPORARY OR PERMANENT STREET BARRICADES SHALL REMAIN AT ALL POINTS OF INGRESS AND EGRESS TO PREVENT PUBLIC USE UNTIL SUCH STREET RECEIVED FINAL ACCEPTANCE.
- CONTRACTOR SHALL OBTAIN A RIGHT-OF-WAY PERMIT BY THE TOWN OF ADDISON FOR WORKING WITHIN THE PUBLIC RIGHT-OF-WAY EASEMENTS.
- DURING CONSTRUCTION, THE OWNER SHALL PROVIDE A QUALIFIED GEOTECHNICAL LAB TO PERFORM MATERIALS TESTING DURING THE CONSTRUCTION, AT THE REQUEST OF THE TOWN OF ADDISON.
- THE CONTRACTOR SHALL SUBMIT MATERIAL SHEETS TO THE TOWN OF ADDISON FOR APPROVAL PRIOR TO INCORPORATING MATERIALS INTO THE JOB.

REV. 05-12-93 PER CITY COMMENTS

GENERAL NOTES

ADDISON TOWN CENTER

SUBDIVISION

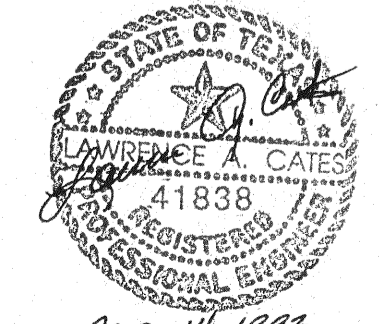
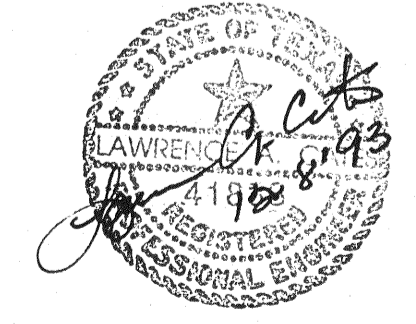
TOWN OF ADDISON, TEXAS

LAWRENCE A. CATES & ASSOC. CONSULTING ENGINEERS DALLAS, TEXAS

DESIGN	DRAWN	DATE	SCALE	NOTES	FILE	NO.
LAC	LAC	5/01/93	D.P.		93056 GENNOTES.DWG	GN-12

AS-BUILTS

I CERTIFY THIS PROJECT WAS CONSTRUCTED IN GENERAL CONFORMANCE WITH THESE CONSTRUCTION PLANS AND WILL FUNCTION AS DESIGNED.



May 14, 1993