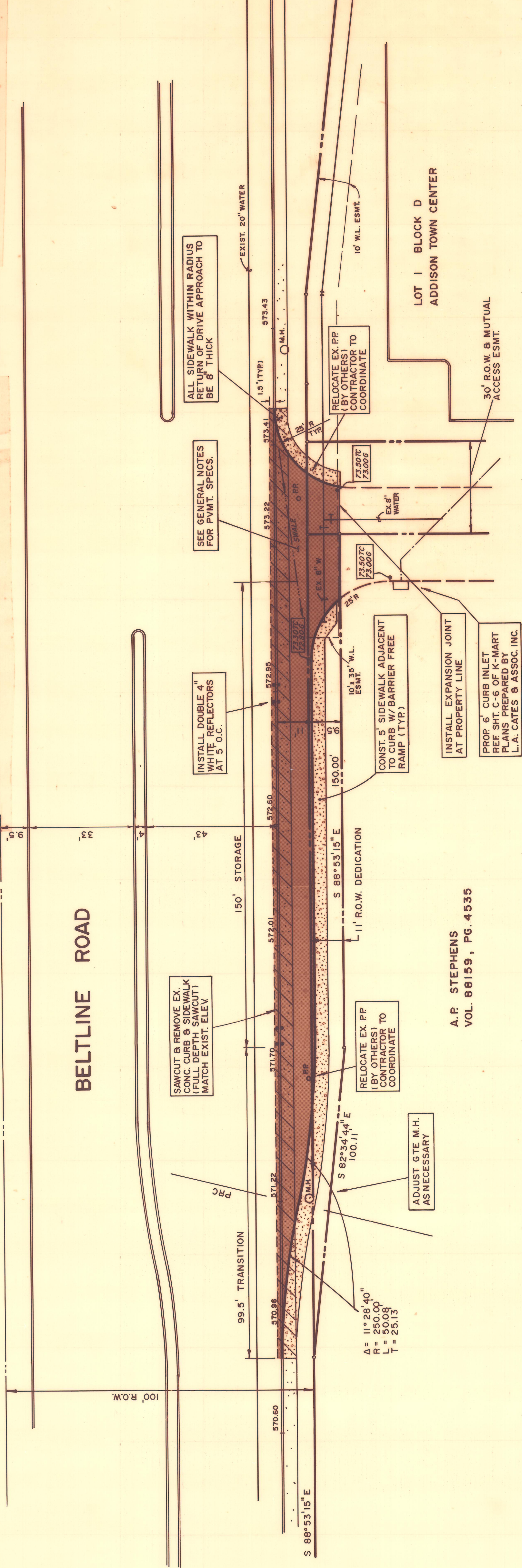
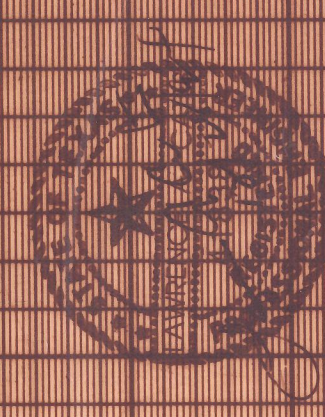


- GENERAL NOTES**
1. ALL CURBS TO BE PLACED INTERIOR WITH PAVEMENT AND SHALL MEET THE SAME COMPRESSIVE STRENGTH AS THE PROPOSED CONCRETE PAVEMENT.
 2. ARRANGEMENT OF JOINTS SHALL MATCH JOINTS IN EXISTING PAVEMENT.
 3. ALL BAR LAYS SHALL BE 30" DIAMETERS.
 4. BAR CHAIRS SHALL BE FURNISHED.
 5. CROSS SLOPE OF PROPOSED TURN LANE WILL BE 1/4" PER FOOT.
 6. CONCRETE PAVEMENT FOR TURN LANE WILL BE 8" THICK, 3600 PSI COMPRESSIVE STRENGTH AT 28 DAYS WITH A MIXTURE THAT IS MINIMUM FIVE BAGS PER CUBIC YARD. REINFORCEMENT WILL BE #3 BARS AT 18" O.C.E.V.
 7. SUBGRADE WILL BE SCARIFIED TO A DEPTH OF 6" AND RECOMPACTED TO 95% STANDARD PROCTOR DENSITY. SUBGRADE TO BE STABILIZED WITH 6% BY WEIGHT OF HYDRATED LIME. SUBGRADE SHALL EXTEND 1 FOOT BACK OF PROPOSED CURB. # 3 CTB MAY BE ALLOWED AS ALTERNATE TO LIME STABILIZATION.
 8. ALL JOINTS IN CONCRETE PAVEMENT TO BE SEALED WITH DABS ASPHALT OR RUBBER BASED COMPOUND.
 9. INSTALLATION OF DRIVE APPROACHES, SIDEWALKS, RIGHT-TURN LANE AND MEDIAN IMPROVEMENTS SHALL MEET TOWN OF ADDISON STANDARDS.



A.P. STEPHENS
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DECELERATION LANE			
BELTLINE RD. EAST OF MARSH LN.			
KMART / STEPHENS APPROACH			
TOWN OF ADDISON			
LAWRENCE A. CATES & ASSOC. CONSULTING ENGINEERS DALLAS, TEXAS			
DESIGN	LAC	DATE	8/16/93
DRAWN	LAC	SCALE	1" = 20'
NOTES		FILE	92023
NO.			C-16A

ALL CURBS AND DRIVE APPROACHES TO BE CONSTRUCTED IN ACCORDANCE WITH THE SELECTION OF CONSTRUCTION PLANS AND SHALL BE FURNISHED AS SHOWN.