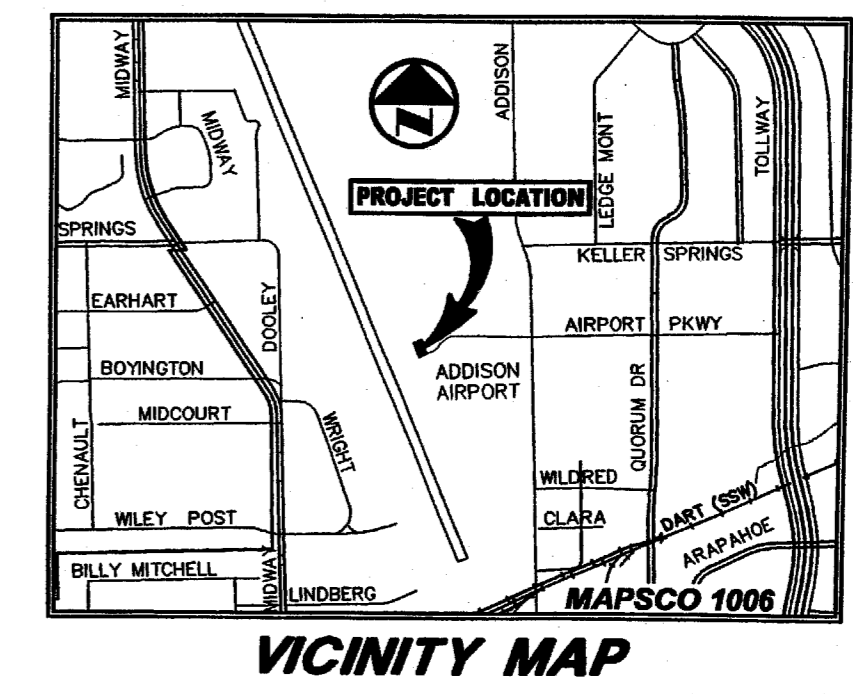


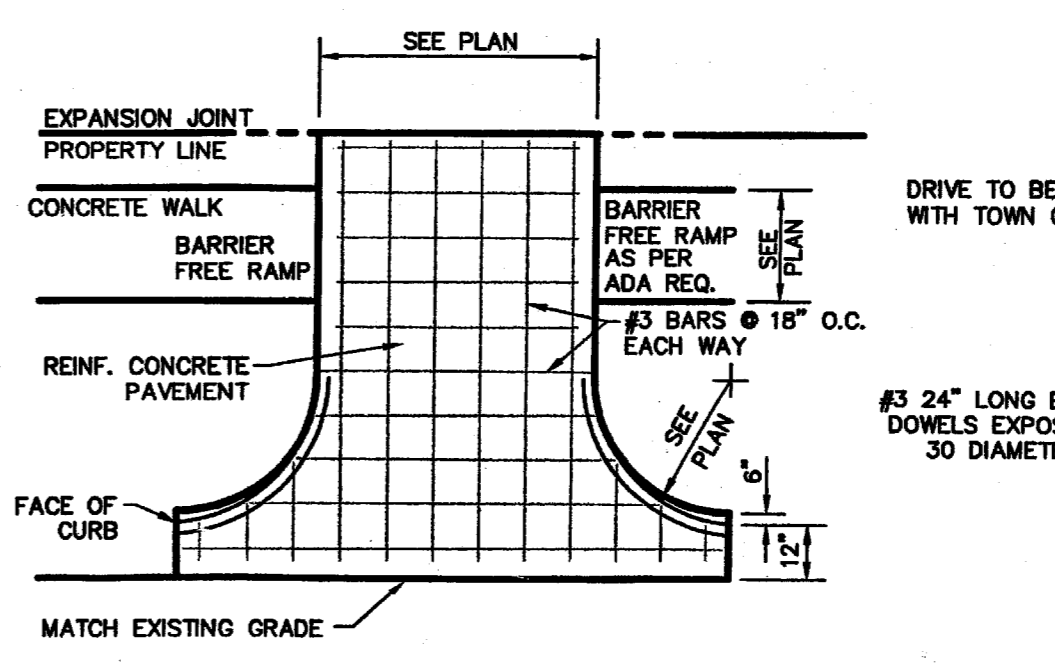
**12 ACCESS WAY**  
NOT TO SCALE

NOTES:  
1. ALL HANDICAPPED SIGNAGE AND MARKINGS TO BE IN ACCORDANCE WITH FEDERAL STATE AND LOCAL REGULATIONS.  
2. MAXIMUM SLOPE FOR HANDICAPPED ACCESSIBLE PATHS ARE 2% WITH A MAXIMUM CROSS FALL SLOPE OF 2% THE FIRST FIVE FEET FROM THE DOOR IS NOT TO EXCEED 2% IN ANY DIRECTION.



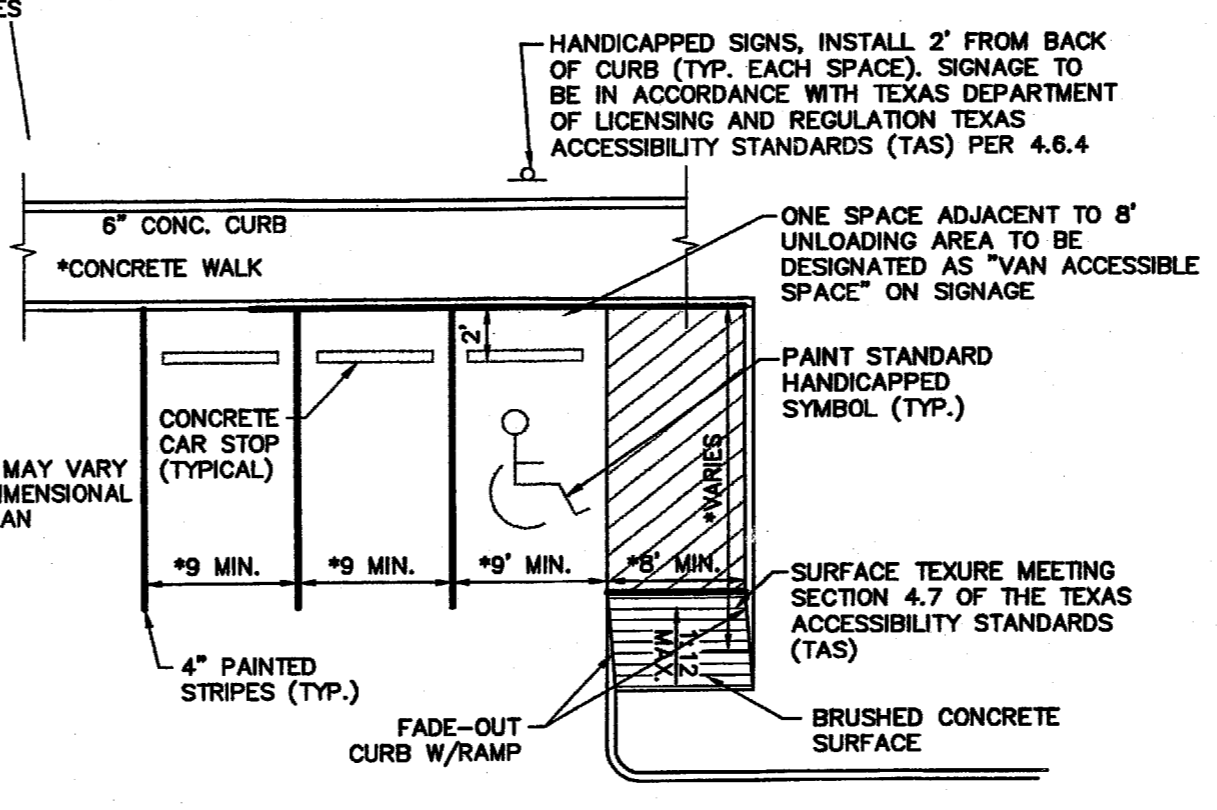
**LEGEND**

B	BOLLARD
EM	ELECTRIC METER
PP	POWER POLE
LS	LIGHT STANDARD
WM	WATER METER
WV	WATER VALVE
ICV	IRRIGATION CONTROL VALVE
FD	FIRE HYDRANT
CL	CLEANOUT
MA	MANHOLE
TSC	TRAFFIC SIGNAL CONTROL
TSP	TRAFFIC SIGNAL POLE
TB	TELEPHONE BOX
FL	FLOOD LIGHT
FP	FLAG POLE
TS	TRAFFIC SIGN
IR	1/2\"/>



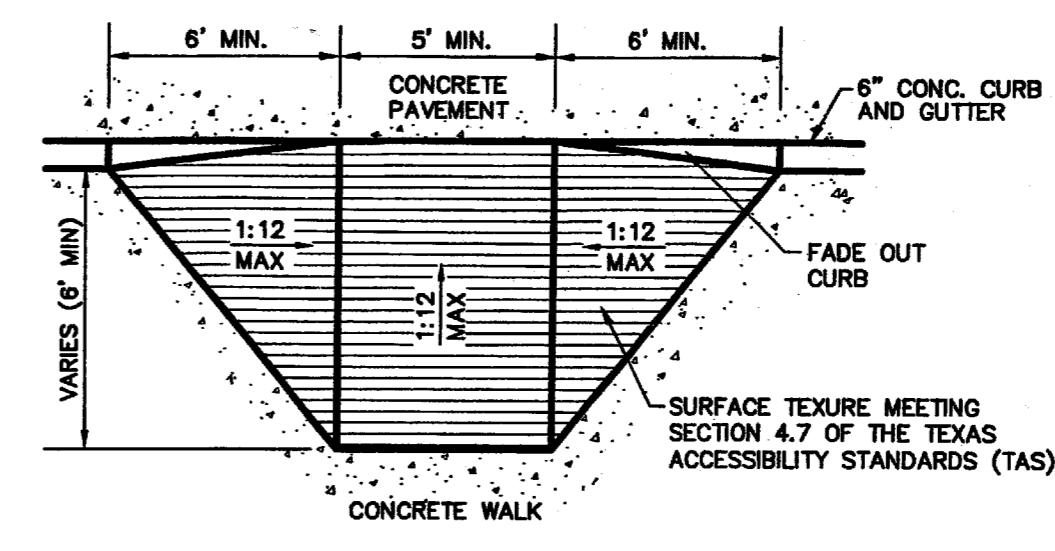
**11 DRIVEWAY APPROACH**  
NOT TO SCALE

NOTES:  
1. ALL HANDICAPPED SIGNAGE AND MARKINGS TO BE IN ACCORDANCE WITH FEDERAL STATE AND LOCAL REGULATIONS.  
2. MAXIMUM SLOPE FOR HANDICAPPED ACCESSIBLE PATHS ARE 2% WITH A MAXIMUM CROSS FALL SLOPE OF 2% THE FIRST FIVE FEET FROM THE DOOR IS NOT TO EXCEED 2% IN ANY DIRECTION.



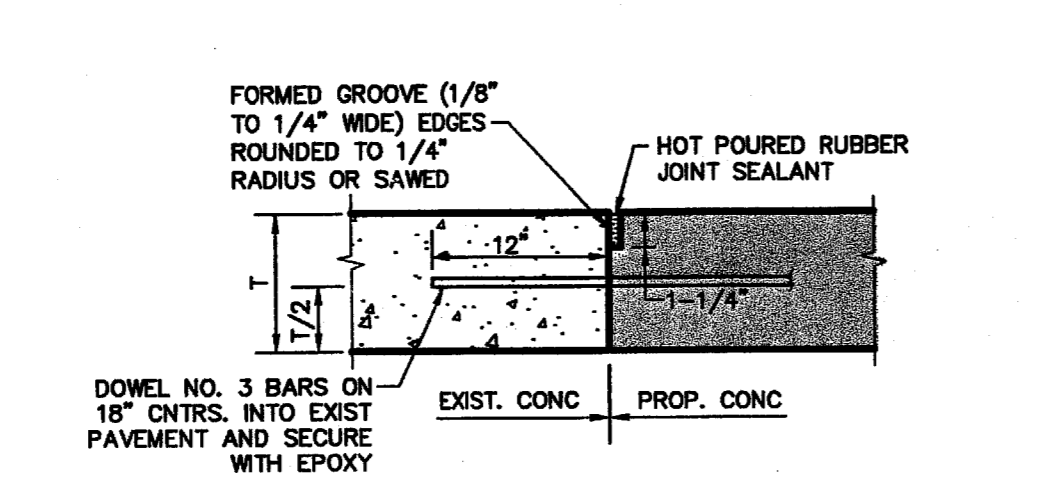
**9 HANDICAP PARKING**  
NOT TO SCALE

NOTES:  
1. MAXIMUM SLOPE FOR ALL PATHS 5% MAX CROSS FALL IS 2% FOR THE FIRST FIVE FEET FROM THE DOOR A 2% SLOPE (MAX) MUST BE MAINTAINED.  
2. SIGNAGE AND MARKINGS TO BE IN ACCORDANCE WITH FEDERAL STATE AND LOCAL REGULATIONS.

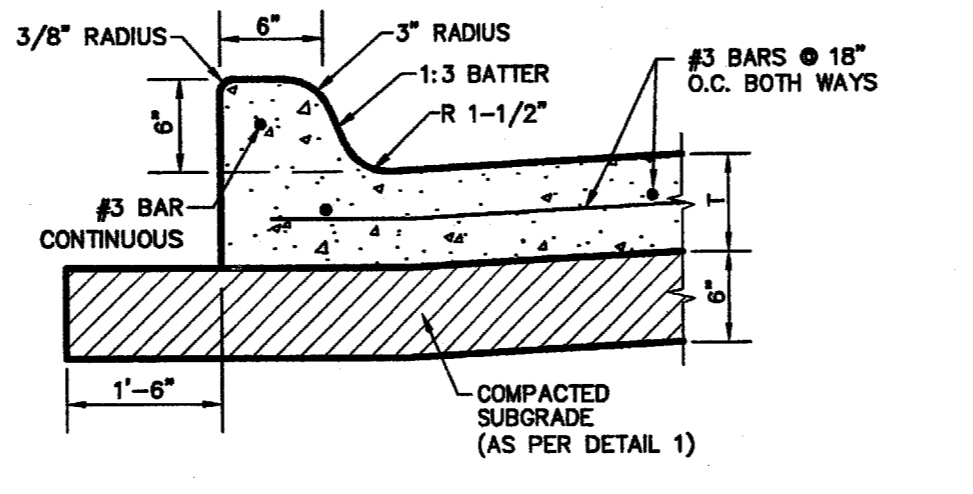


**10 ACCESS RAMP**  
NOT TO SCALE

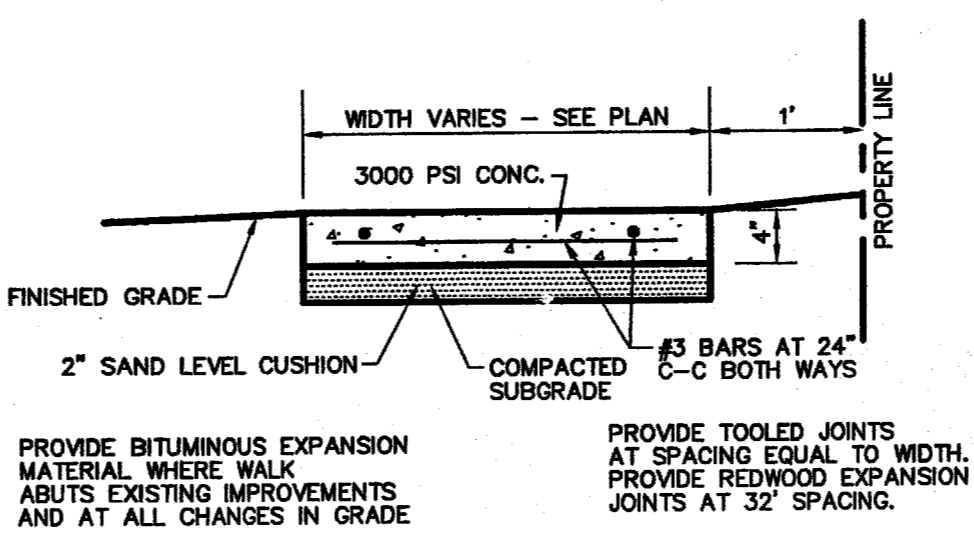
NOTES:  
1. ALL HANDICAPPED SIGNAGE AND MARKINGS TO BE IN ACCORDANCE WITH FEDERAL STATE AND LOCAL REGULATIONS.  
2. MAXIMUM SLOPE FOR HANDICAPPED ACCESSIBLE PATHS ARE 2% WITH A MAXIMUM CROSS FALL SLOPE OF 2% THE FIRST FIVE FEET FROM THE DOOR IS NOT TO EXCEED 2% IN ANY DIRECTION.



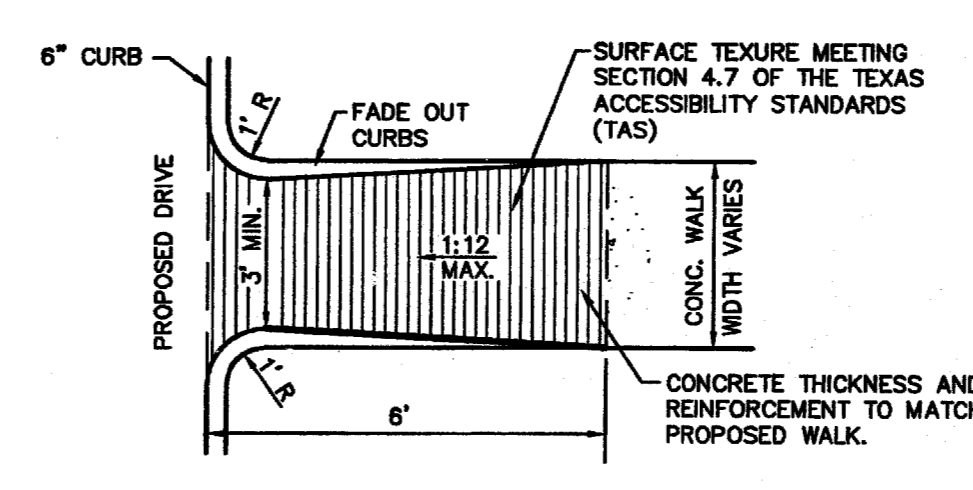
**5 PAVEMENT CONNECTION**  
NOT TO SCALE



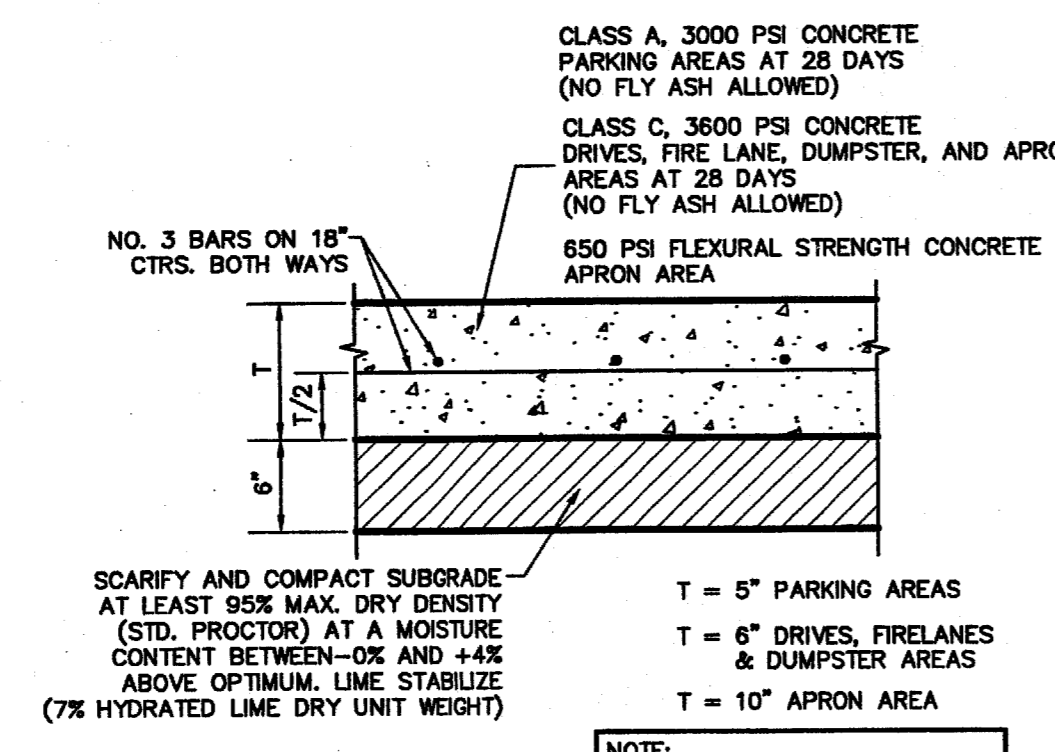
**6 INTEGRAL CURB**  
NOT TO SCALE



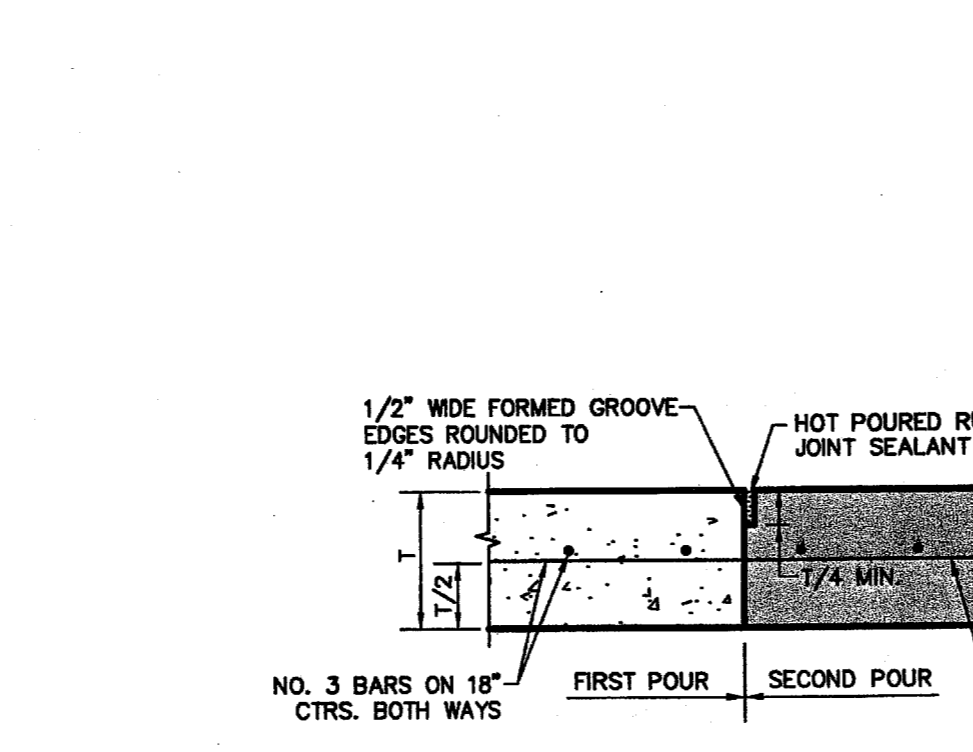
**7 CONCRETE WALK**  
NOT TO SCALE



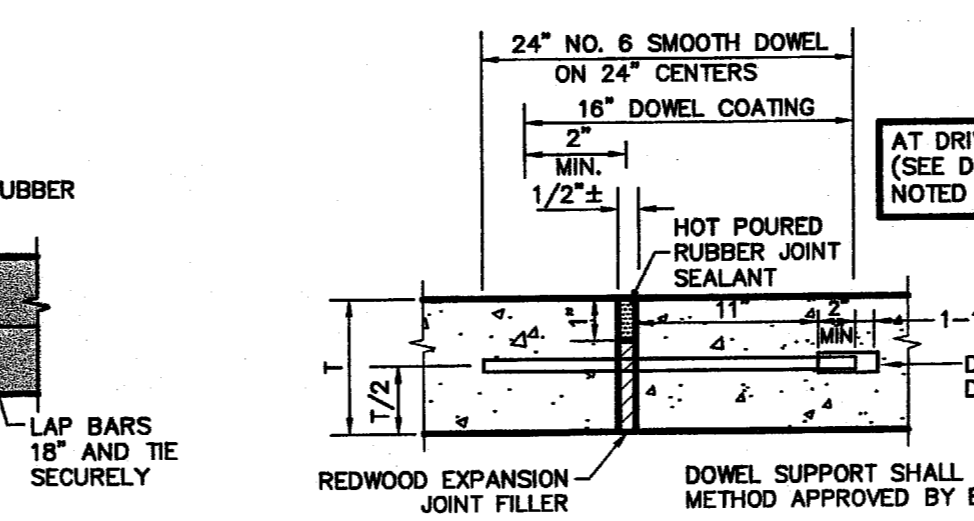
**8 ACCESS RAMP**  
NOT TO SCALE



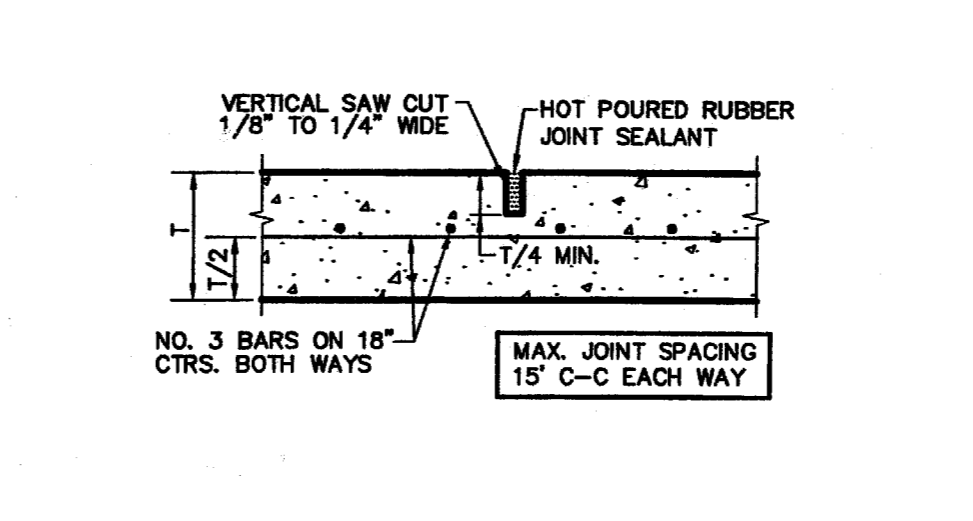
**1 CONCRETE PAVEMENT SECTION**  
NOT TO SCALE



**2 CONSTRUCTION JOINT**  
NOT TO SCALE



**3 EXPANSION JOINT**  
NOT TO SCALE



**4 SAWED DUMMY JOINT**  
NOT TO SCALE

- PAVING NOTES**
1. ALL DIMENSIONS ARE FROM BACK OF CURB UNLESS OTHERWISE NOTED.
  2. UNLESS OTHERWISE NOTED, ALL CONCRETE SHALL BE CLASS "A", (3000 PSI).
  3. UNLESS NOTED, ALL FILL PLACED UNDER PAVING SHALL BE COMPACTED TO MEET STANDARD PROCTOR DENSITY IN 6 INCH LIFTS. REFER TO STRUCTURAL SPECIFICATION FOR FILL PLACED BENEATH BUILDING AREAS. ALL OTHER FILL AREAS TO BE COMPACTED TO 90% STANDARD PROCTOR.
  4. THE CONTRACTOR SHALL SUBMIT A JOINT SPACING PLAN TO THE ENGINEER FOR APPROVAL. UNLESS NOTED, EXPANSION JOINT SPACING SHALL BE 90' MAXIMUM EACH WAY WITH NO KEYWAYS AND SAVED DUMMY JOINTS SHALL BE 15' EACH WAY.
  5. TRANSVERSE CONSTRUCTION JOINTS SHALL BE USED AT THE END OF EACH DAY'S PAVING AND WHERE INTERRUPTIONS SUSPEND OPERATIONS FOR 30 MINUTES OR MORE.
  6. ALL PAVEMENTS TO BE REMOVED SHALL BE SAWCUT TO A NEAT LINE, MINIMUM 1-1/2\"/>

NO.	DATE	REVISION

**Pacheco Koch Consulting Engineers**  
8300 N CENTRAL EXPWY SUITE 1000 DALLAS, TEXAS 75206 972.235.3031

**PAVING PLAN & DETAILS**  
**FRITO LAY (FLNA) CORPORATE HANGER**  
**AIRPORT PARKWAY @**  
**ADDISON AIRPORT**  
**TOWN OF ADDISON, TEXAS**

DESIGN	DRAWN	DATE	SCALE	NOTES	FILE	NO.
BJM	DRI	SEPT 2003	N.T.S.			<b>C6.1</b>

BRAD MOSS 03/28/03  
14.000-18.15.03-03-207CV.DWG

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70900  
PROFESSIONAL ENGINEER  
10/15/2003