

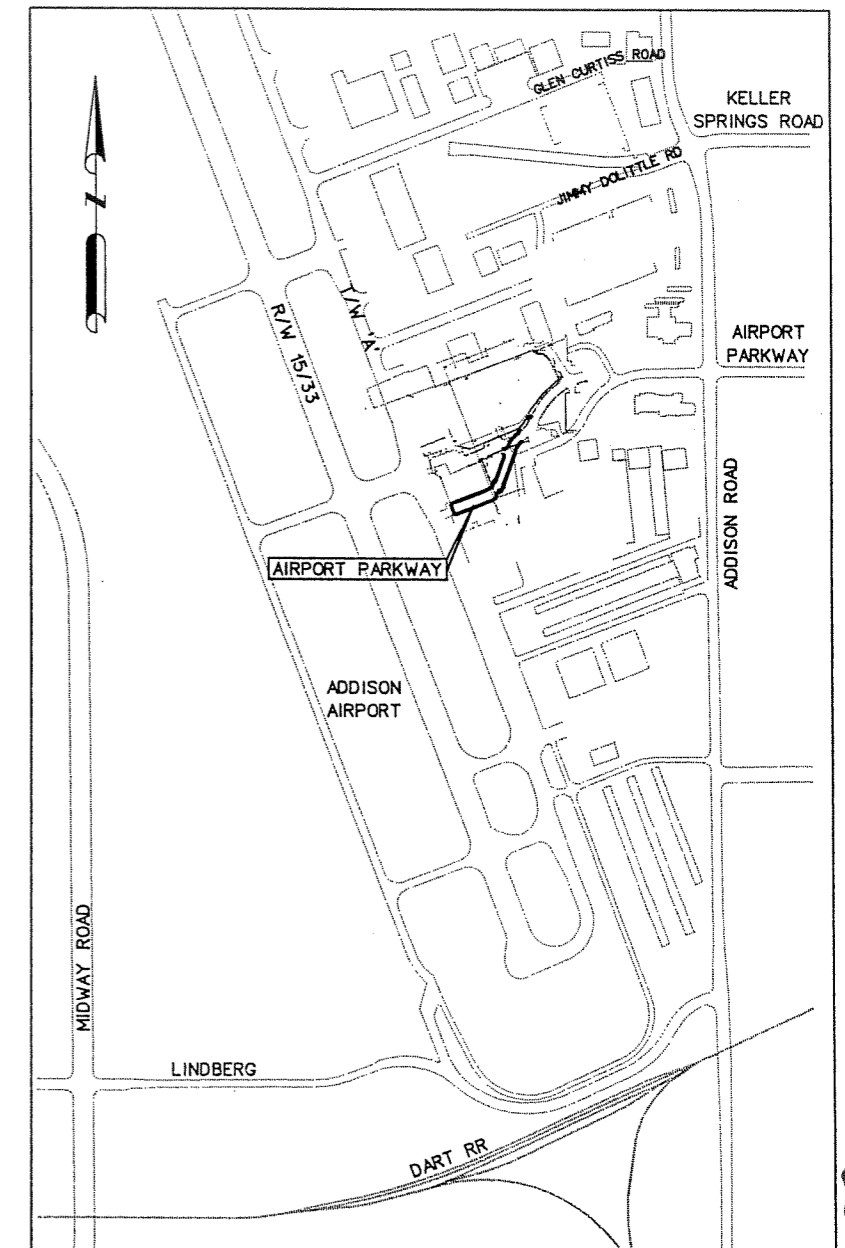
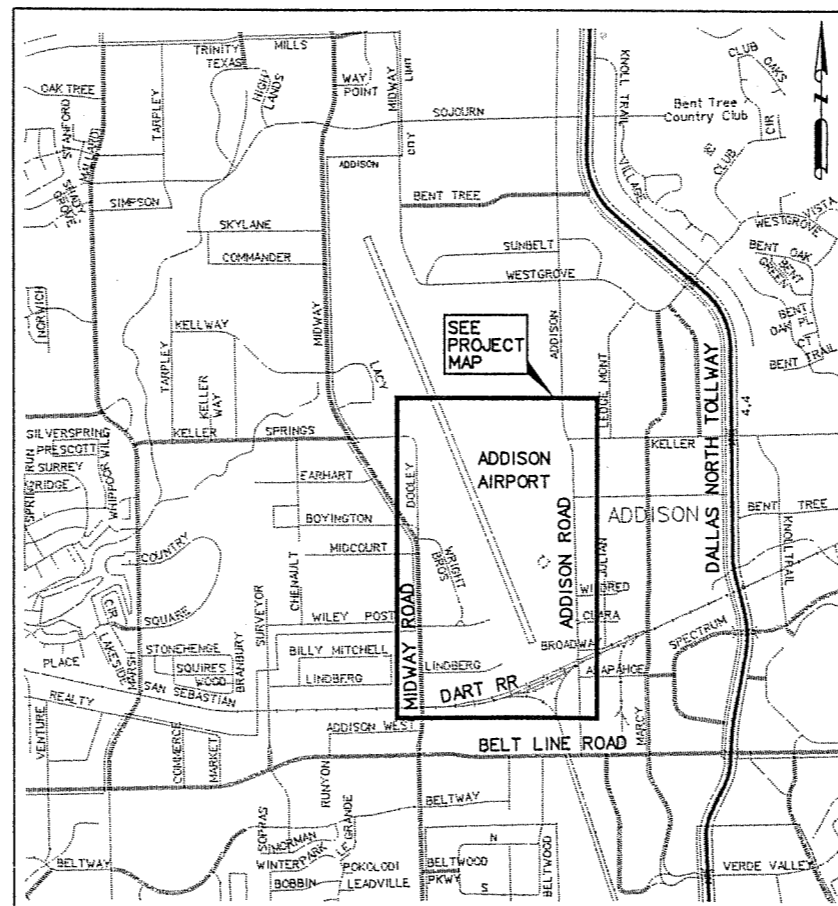
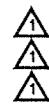
PLANS FOR THE CONSTRUCTION OF  
GRADING, PAVING AND STORM SEWER  
IMPROVEMENTS FOR

# AIRPORT PARKWAY REALIGNMENT

## ADDISON AIRPORT



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OWNER:  
TOWN OF ADDISON  
DEPARTMENT OF PUBLIC WORKS  
16801 WESTGROVE  
P.O. BOX 144  
ADDISON, TEXAS 75001  
(972) 450-2886

ENGINEER:  
HNTB CORPORATION  
5910 WEST PLANO PARKWAY, SUITE 200  
DALLAS, TEXAS 75093  
(972) 661-5626

LOCATION MAP  
NOT TO SCALE

PROJECT MAP  
NOT TO SCALE

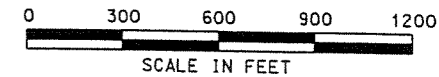
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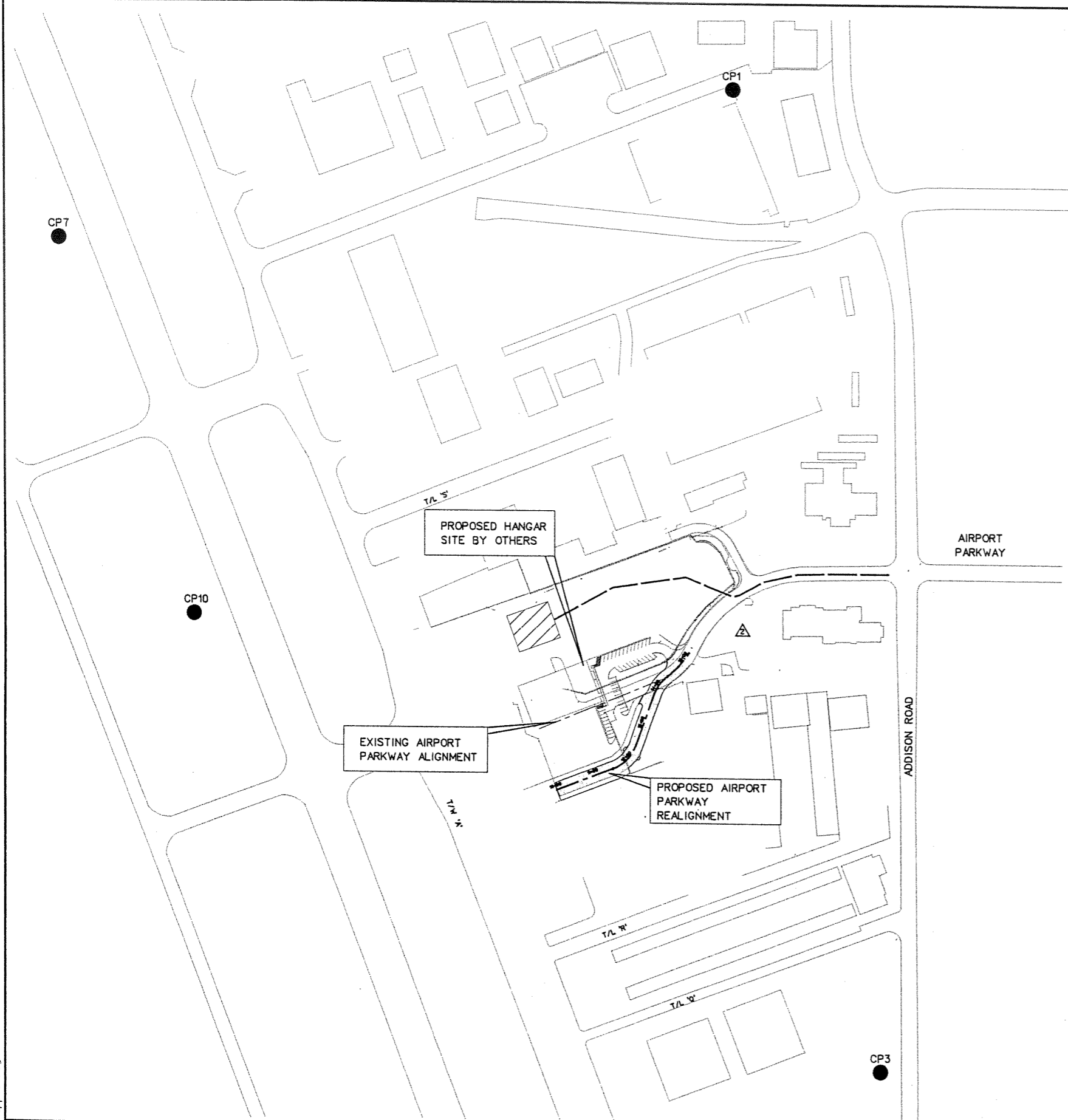


**NOTES:**

1. THE CONTRACTOR WILL NEED TO COORDINATE WITH ADDISON AIRPORT OPERATIONS THROUGHOUT THE CONSTRUCTION PROCESS. THE CONTRACTOR IS TO KEEP AIRPORT OPERATIONS UPDATED REGARDING THE SCHEDULE ON A WEEKLY BASIS. THE AIRPORT OPERATIONS WILL NEED 14 DAYS NOTICE BEFORE WORK CAN BEGIN ON THE NEXT PHASE OF THE PROJECT.
2. CONTRACTOR TO VERIFY THE LIMITS OF THE STAGING AREA WITH THE ADDISON AIRPORT AND PUBLIC WORKS INSPECTION PERSONNEL PRIOR TO BEGINNING WORK.
3. CONTROL MONUMENTS (NAD83, TEXAS NORTH CENTRAL - 4202)
4. CONTRACTOR TO COORDINATE WITH THE TOWN OF ADDISON PRIOR TO INSTALLATION OF TEMPORARY GATE. EXISTING IRRIGATION SYSTEM TO BE CUT AND PLUGGED BY THE TOWN OF ADDISON WITHIN THE LIMITS OF THE GATE. IRRIGATION SYSTEM WILL BE RECONNECTED AFTER CONSTRUCTION IS COMPLETED. CONTRACTOR TO PROVIDE ASPHALT TRANSITION AGAINST THE BACK OF CURBS FOR TEMPORARY CONSTRUCTION ACCESS. ASPHALT WILL BE REMOVED, AND CURB CLEANED AFTER CONSTRUCTION IS COMPLETE
5. AFTER CONSTRUCTION OF THE PROJECT THE CONTRACTOR IS TO REMOVE THE 20-FOOT GATE AND PUT THE FENCE BACK TO IT'S ORIGINAL CONDITION.

HORIZONTAL AND VERTICAL CONTROL MONUMENTATION IS BASED ON A DAL-TECH, 2002, SURVEY PROVIDED BY THE ADDISON AIRPORT

CP#	NORTH	EAST	ELEV	DESC.
1	7,039,689.078	2,480,245.682	638.30	AA-1
2	7,041,501.126	2,480,144.730	641.92	AA-2
3	7,037,202.352	2,480,632.193	638.03	AA-3
4	7,035,957.986	2,479,444.822	632.41	AA-4
5	7,039,506.863	2,478,102.163	637.40	AA-5
6	7,043,849.691	2,477,497.499	652.26	AA-6
7	7,039,305.132	2,478,531.657	640.77	AB2786
8	7,040,772.156	2,478,253.153	633.02	AB7648
9	7,043,042.492	2,477,532.021	637.40	AH7852
10	7,038,354.486	2,478,875.925	642.02	CS1308

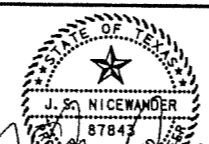


**LEGEND**

- CONTRACTOR HAUL ROUTE
- CONTRACTOR STAGING AREA
- CP# CONTROL POINT (SEE GENERAL NOTES FOR DETAILS)

"As-Built"

NO.	DATE	REVISION	APPROV.	NO.	DATE	REVISION	APPROV.
1.	10/10/03	REVISED NOTES, DISPLAYED PROPOSED SITE PLAN BY OTHERS	JSN				
2	11/19/03	REMOVE TEMPORARY GATE FROM PLANS	JSN				



**HNTB** ARCHITECTS ENGINEERS PLANNERS  
The HNTB Companies

DESIGN CHECK JSN  
DRAWN CHECK GFS  
DATE SEPTEMBER 2003

PROJECT NO. 39020



TOWN OF ADDISON  
AIRPORT PARKWAY REALIGNMENT

PROJECT LAYOUT

SHEET

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**CONSTRUCTION CONTROL NOTES**

**I. GENERAL REQUIREMENTS**

- A. IN ACCEPTING THE CONTRACTOR'S BID ON THIS PROJECT, THE TOWN OF ADDISON ASSUMES THE CONTRACTOR IS, OR HAS BECOME PRIOR TO SUBMITTING HIS/HER BID, KNOWLEDGEABLE OF THE CONSTRUCTION REQUIREMENTS, RESTRICTIONS, METHODS, MEANS, AND GENERAL CONSIDERATIONS OF DOING CONSTRUCTION WORK ON AN ACTIVE AIRPORT. WHILE A CONSCIENTIOUS AND GOOD FAITH EFFORT HAS BEEN MADE TO INCLUDE ALL APPROPRIATE AND RELEVANT REQUIREMENTS IN THESE PLANS AND SPECIFICATIONS, THE CONTRACTOR, AS A CONSIDERATION OF THIS CONTRACT, SHALL NOT USE A CLAIM OF LACK OF UNDERSTANDING OF THE COMPLEXITIES OF AIRPORT WORK AS A REASON TO CLAIM AGAINST THE TOWN OF ADDISON FOR ADDITIONAL TIME AND/OR COMPENSATION.
- B. THE GENERAL REQUIREMENTS PRESENTED IN THESE NOTES ARE SUPPLEMENTARY TO THOSE GIVEN IN THE VARIOUS SECTIONS OF THE SPECIFICATIONS. PARTICULAR ATTENTION IS DIRECTED TO THE TECHNICAL SPECIFICATIONS AND TO THE APPENDIX. <sup>1</sup>
- C. THE CONTRACTOR AND HIS/HER STAFF, INCLUDING SUBCONSULTANTS, WHO WILL BE DRIVING IN THE AIRPORT OPERATIONS AREA (AOA) WILL BE REQUIRED TO ATTEND THE ADDISON AIRPORT DRIVING SCHOOL PRIOR TO CONSTRUCTION. THEY WILL ALSO BE REQUIRED TO ADHERE TO ALL RULES AND REGULATIONS PRESENTED IN THE DRIVING SCHOOL.
- D. THE ENGINEER SHALL BE THE INDIVIDUAL, PARTNERSHIP, FIRM OR CORPORATION AUTHORIZED BY THE OWNER (SPONSOR) TO BE RESPONSIBLE FOR ENGINEERING SUPERVISION OF THE CONTRACT WORK AND ACTING DIRECTLY OR THROUGH THE AUTHORIZED REPRESENTATIVE.

**II. SITE AVAILABILITY**

- A. ALL WORK AREAS WILL BE AVAILABLE TO THE CONTRACTOR AT THE TIME OF NOTICE TO PROCEED (NTP) AS DETAILED ON THE PROPOSED CONSTRUCTION SCHEDULE CONTAINED IN THESE DOCUMENTS AND WITHIN THE AVAILABLE CLOSURE PERIODS.
- B. RUNWAY 15/33 AND TAXIWAY 'B' MUST BE OPENED FOR AIR TRAFFIC ARRIVALS/DEPARTURES EACH WEEK DAY MORNING AND EVENING. THE AVAILABLE TIME PERIOD EACH DAY FOR CONSTRUCTION ACTIVITIES DURING A TAXIWAY CLOSURE SHALL BE SET FROM 10:00 P.M. UNTIL 6:00 A.M. TYPICALLY THE OTHER TAXIWAYS WILL BE AVAILABLE TO THE CONTRACTOR DURING NORMAL BUSINESS HOURS.
- C. DUE TO WEATHER, WIND DIRECTION OR EMERGENCIES THE CONTRACTOR MAY BE DELAYED, RE-DIRECTED TO ANOTHER AREA OR SHUT DOWN.
- D. ALTHOUGH NOT ANTICIPATED FOR THIS PROJECT, FOR THE CONTRACTOR TO OPERATE WITHIN 250 FEET OF THE CENTERLINE OF ANY RUNWAY, THAT RUNWAY MUST BE CLOSED BY AIR TRAFFIC CONTROL TOWER (ATCT). ALL RUNWAY CLOSURES SHALL BE CONDUCTED BY AIRPORT PERSONNEL. A RUNWAY CLOSURE REQUIRES A 24-HOUR NOTICE TO AIRPORT OPERATIONS.
- E. THE CONTRACTOR SHALL NOT ENTER THE AIRPORT OPERATIONS AREA (AOA) NOR LEAVE HIS DESIGNATED AOA WORK AREA WITHOUT AN ESCORT FROM AIRPORT OPERATIONS OR PERMISSION FROM OPERATIONS TO PERFORM HIS OWN ESCORT.

**III. MAINTENANCE OF VEHICULAR TRAFFIC**

**A. VEHICULAR TRAFFIC**

- 1. THE CONTRACTOR IS ADVISED THAT AIRPORT RUNWAY 15/33, AND ALL TAXIWAYS WILL BE ACTIVE DURING THIS CONTRACT. THE CONTRACTOR WILL BE EXPECTED TO CONDUCT THE WORK SUCH THAT THE SAFETY OF OPERATIONS IS NOT DIMINISHED AND THAT VEHICULAR FLOW IS MAINTAINED AT ALL TIMES. THE CONTRACTOR IS ADVISED, AND SHALL ACCEPT AS AN IMPORTANT CONSIDERATION OF THE WORK, THAT THE MAINTENANCE OF SAFE AND EFFICIENT ACCESS TO THE AIRPORT VIA THE ROADWAY SYSTEM IS AN INTEGRAL PART OF THE WORK. ALL TRAFFIC MAINTENANCE SHALL BE DONE IN CONFORMANCE TO THE LATEST EDITION OF THE MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES.
- 2. ALL EXISTING ROADS OR PAVEMENTS USED AS ACCESS/HAUL ROADS SHALL BE MAINTAINED AND IMPROVED AS REQUIRED. THE ROAD OR PAVEMENT CONDITIONS WILL BE INSPECTED JOINTLY BY THE ENGINEER AND CONTRACTOR PRIOR TO AND AT THE END OF THE PROJECT. THE CONDITION OF THESE ROADS AND PAVEMENTS SHALL BE EQUAL OR BETTER THAN AT THE START OF THE PROJECT AS DETERMINED BY THE ENGINEER. MECHANICAL BROOMS WILL ALSO BE REQUIRED FOR THESE EXISTING PAVEMENT ACCESS ROADS AT ALL TIMES.

**B. AIRCRAFT TRAFFIC**

- 1. THE CONTRACTOR IS ADVISED THAT ALL AIRPORT TAXIWAYS, RUNWAYS AND RAMPS WILL BE ACTIVE DURING THIS CONTRACT WITH THE EXCEPTION OF THOSE PAVEMENTS CLOSED EACH DAY DURING THE DEFINED CLOSURE PERIODS. THE CONTRACTOR WILL BE EXPECTED TO CONDUCT THE WORK SUCH THAT THE SAFETY OF OPERATIONS IS NOT DIMINISHED AND THAT AIRCRAFT FLOW IS MAINTAINED AT ALL TIMES. THE CONTRACTOR IS FURTHER ADVISED, AND SHALL ACCEPT AS AN IMPORTANT CONSIDERATION OF THE WORK, THAT THE MAINTENANCE OF SAFE AND EFFICIENT OPERATION OF THE AIRPORT OPERATIONS AREA (AOA) IS AN INTEGRAL PART OF THE WORK. ALL CONSTRUCTION INTERFACE WITH AIRCRAFT PAVEMENTS, TAXIWAY CROSSINGS, AND SECURITY REQUIREMENTS AS CONTAINED IN FAA ADVISORY CIRCULAR 150/5370-2E WILL APPLY.
- 2. AOA NIGHT WORK SHALL BE DEFINED AS 10 P.M. TO 6 A.M. CONTRACTOR SHALL BEGIN CLEAN UP OPERATIONS NO LATER THAN 5:30 A.M. TO HAVE ALL AOA PAVEMENT OPEN NO LATER THAN 6:00 A.M.
- 3. AOA WEEKEND WORK SHALL BE DEFINED AS BEGINNING AT 9:00 A.M. SATURDAY AND MAY CONTINUE UNTIL 4:30 A.M. MONDAY.

**IV. WORK WITHIN AIRPORT OPERATIONS AREA (AOA)**

**A. GENERAL REQUIREMENTS**

ALL WORK TO BE PERFORMED WILL BE INSIDE OR ADJACENT TO THE AIRPORT OPERATIONS AREA (AOA). CONSEQUENTLY ALL WORK MUST BE PERFORMED SUCH THAT THE SECURITY OF THE AOA IS MAINTAINED.

THE CONTRACTOR WILL BE REQUIRED TO OBTAIN ALL VEHICLE PASSES AND TEMPORARY PERSONNEL SECURITY BADGES TO SATISFY ALL AIRPORT SECURITY REQUIREMENTS PRIOR TO BEGINNING WORK WITHIN THE AOA.

IN ADDITION, THE CONTRACTOR IS ADVISED THAT CERTAIN RULES AND RESTRICTIONS, AS CONTAINED IN FAA ADVISORY CIRCULAR 150/5370-2D AND AUGMENTED BY THESE PLANS AND SPECIFICATIONS, WILL APPLY TO THE WORK. THE CONTRACTOR SHALL BECOME FAMILIAR WITH ALL REQUIREMENTS APPLICABLE TO AIRPORT CONSTRUCTION AND COOPERATE WITH THE ENGINEER IN MAINTAINING A SAFE CONSTRUCTION SITE WHICH IS COMPATIBLE WITH AIRCRAFT AND AIRPORT OPERATIONS.

THE CONTRACTOR'S ACCESS TO THE JOB SITE AND WITHIN THE AIRCRAFT OPERATIONS AREA (AOA), SHALL BE LIMITED THROUGH THE EXISTING SECURITY GATES. THE CONTRACTOR WILL BE REQUIRED TO MAINTAIN SECURITY AT THE GATES THROUGHOUT THE DURATION OF THE PROJECT. GATES SHALL BE MANNED BY A SECURITY GUARD AT ALL TIMES WHEN THE SECURITY OF THE AIRCRAFT OPERATIONS AREA (AOA) IS BREACHED. THE GUARD WILL BE TRAINED BY THE CONTRACTOR IN THE USE OF THE RADIOS AND THE SECURITY REQUIREMENTS OF THE AOA. WHEN THE GATE IS UNGUARDED FOR WHATEVER REASON OR LENGTH OF TIME, IT SHALL BE LOCKED WITH LOCKS PROVIDED BY THE CONTRACTOR AND APPROVED BY THE ENGINEER. THE CONTRACTOR WILL BE LIABLE FOR ANY FINES ISSUED BY THE FAA FOR SECURITY (OR OTHER) VIOLATIONS FOR WHICH IT IS CITED. AN ASSESSMENT OF \$1,000 MAY BE ASSESSED FOR EACH AND EVERY OCCURRENCE WHERE THE GATE IS LEFT UNGUARDED AND UNLOCKED.

CONTRACTOR SHALL SUBMIT HIS 3 WEEK ROLLING SCHEDULE FOR DAILY WORK AND CLOSURE OF THE AIRFIELD PAVEMENTS. THE SCHEDULE SHALL BE APPROVED BY THE ENGINEER AND AIRPORT OPERATIONS PRIOR TO START OF WORK. DUE TO WEATHER, WIND DIRECTION OR UNFORESEEN EMERGENCIES THE CONTRACTOR MAY BE REQUIRED TO PULL OFF ANY GIVEN AREA AT SHORT NOTICE. EFFORTS WILL BE MADE TO ALLOW WORK IN OTHER AREAS BUT WILL NOT BE GUARANTEED. IF THE CONTRACTOR IS NOT ALLOWED TO WORK IN ANOTHER AREA IT MAY BE COUNTED AS A WEATHER DAY WITH NO OTHER COMPENSATION, ACCORDING TO THE ENGINEERS APPROVAL.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROVIDING HIS/HER OWN PROJECT OFFICE, TOILET FACILITIES AND OTHER NECESSARY BUILDINGS OR SHELTERS. THE AIRPORT SPONSOR WILL NOT PROVIDE ANY FACILITIES TO THE CONTRACTOR DURING CONSTRUCTION.

THE CONTRACTOR SHALL SUBMIT HIS/HER CONSTRUCTION WORK SCHEDULE TO THE ENGINEER PROJECTING HIS UPCOMING WORK FOR THE NEXT THREE WEEKS. THE ENGINEER AND AIRPORT MANAGER SHALL REVIEW THIS PLAN WEEKLY WITH THE CONTRACTOR SO THAT EVERYONE IS AWARE OF UPCOMING CONSTRUCTION EVENTS.

THE CONTRACTOR SHALL MAINTAIN A CLEAN AND SAFE CONSTRUCTION WORK AREA. THE CONTRACTOR SHALL PERFORM CLEAN-UP OPERATIONS ON A DAILY BASIS.

THE CONTRACTOR SHALL NOT DEVIATE FROM THE APPROVED CONSTRUCTION SEQUENCE WITHOUT FIRST OBTAINING APPROVAL FROM THE ENGINEER. THE CONTRACTOR SHALL BRING IN HIS/HER EQUIPMENT AND SHALL KEEP THAT EQUIPMENT ON SITE FOR THE DURATION OF ITS FUNCTION.

THE CONTRACTOR SHALL HAVE SUFFICIENT EQUIPMENT AND PERSONNEL ON SITE TO ACCOMPLISH EFFICIENT AND PROMPT CONSTRUCTION OF THE VARIOUS WORK ITEMS, INCLUDING WORK ON MORE THAN ONE WORK ITEM SIMULTANEOUSLY.

NO TRENCHES IN OR DIRECTLY ADJACENT TO OPERATIONAL PAVEMENT SHALL REMAIN OPEN OVERNIGHT OR WHEN THE CONTRACTOR FINISHES WORK FOR THE DAY IN THE AREA. TRENCHES NOT BACKFILLED SHALL BE COVERED WITH STEEL PLATES TO ALLOW FOR SAFE PASSAGES BY AIRCRAFT ACROSS THE TRENCH, IF APPROVED BY THE AIRPORT MANAGER.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR REPAIR OF ANY DAMAGE TO EXISTING FACILITIES NOT DESIGNATED FOR RECONSTRUCTION OR REPLACEMENT AT HIS/HER EXPENSE DUE TO MOVING OR USAGE OF HEAVY EQUIPMENT OR THE TRANSPORT OF MATERIALS TO OR ON THE SITE SHALL BE REPAIRED TO EQUAL OR BETTER QUALITY BY THE CONTRACTOR AT HIS/HER OWN EXPENSE.

THE CONTRACTOR SHALL VIDEO TAPE THE ENTIRE WORK AREA AFTER THE PRE-CONSTRUCTION MEETING AND PRIOR TO THE MOBILIZATION OF PERSONNEL AND EQUIPMENT. THE CONTRACTOR SHALL PROVIDE ONE (1) COPY OF THIS VIDEO TAPE(S) TO BOTH THE AIRPORT MANAGER AND THE ENGINEER. IN AREAS WHERE CONSTRUCTION EQUIPMENT CROSSES EXISTING PAVEMENTS, THE CONTRACTOR SHALL TAKE PICTURES OR VIDEO OF THE PAVEMENT PRIOR TO COMMENCING OPERATIONS. THE CONTRACTOR SHALL PROVIDE THE ENGINEER AND AIRPORT MANAGER WITH ONE COPY EACH OF THE PHOTOGRAPH OR VIDEO TAPE(S) TAKEN. THIS DOCUMENTATION SHALL BE USED TO DETERMINE THE AMOUNT OF DAMAGE, IF ANY, CAUSED TO THE PAVEMENTS AND OTHER STRUCTURES BY THE CONSTRUCTION EQUIPMENT CROSSINGS AND THE QUALITY OF CONSTRUCTION WHICH SHALL BE REQUIRED FOR THE REPAIRS. NO SEPARATE BID ITEM WILL BE SET UP FOR THIS ACTIVITY, IT SHALL BE CONSIDERED SUBSIDIARY TO OTHER BID ITEMS.

CONSTRUCTION EQUIPMENT AND VEHICLES SHALL TRAVEL A MINIMUM AMOUNT ON NEWLY CONSTRUCTED PAVEMENTS SO THAT THE NEWLY CONSTRUCTED AREAS WILL NOT BE DAMAGED.

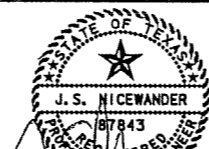
THE CONTRACTOR SHALL BE RESPONSIBLE FOR REPAIR OF ANY DAMAGE TO UNDERGROUND CABLES ENCOUNTERED. NO DIGGING SHALL BE PERFORMED WITHOUT FIRST CONTACTING THE AIRPORT MANAGER. ANY DAMAGE TO FAA CABLE OR OTHER UNDERGROUND CABLING OR FACILITIES SHALL BE REPAIRED IN ACCORDANCE WITH THE APPLICABLE FAA SPECIFICATIONS AND IN A MANNER ACCEPTABLE TO AIRPORT MANAGER, AND THE ENGINEER.

ALL ABOVE GROUND AND GROUND LEVEL ELECTRICAL RELATED APPURTENANCES (I.E., RUNWAY LIGHTS, CABLE BOXES, CABLE AND/OR DUCT MARKERS, CONDUIT, ETC.) SHALL BE PROTECTED AT ALL TIMES. ANY DAMAGE DONE TO SAID APPURTENANCES BY THE CONTRACTOR SHALL BE REPAIRED IMMEDIATELY TO LIKE QUALITY AT THE CONTRACTOR'S EXPENSE. THE REPAIRS SHALL BE PERFORMED TO THE SATISFACTION OF THE AIRPORT MANAGER, AND THE ENGINEER.

*"As-Built"*

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NO.	DATE	REVISION	APPROV.	NO.	DATE	REVISION	APPROV.
1	10/10/03	NOTE REVISIONS	JSN				



**HNTB** ARCHITECTS ENGINEERS PLANNERS  
The HNTB Companies

DESIGN CHECK	JSN	PROJECT NO.	39020
DRAWN	JOH		
CHECK	GFS		
	JSN		
DATE	SEPTEMBER 2003		



<b>TOWN OF ADDISON</b>		SHEET
AIRPORT PARKWAY REALIGNMENT		
<b>GENERAL NOTES</b>		

CONTRACTOR SHALL PROVIDE A MOTORIZED MECHANICAL SWEEPER, ALONG WITH A FOREIGN OBJECTS DAMAGE (FOD) PLAN, PRIOR TO BEGINNING WORK. THE FOD PLAN SHALL BE PRESENTED BY THE CONTRACTOR AT THE PRE-CONSTRUCTION MEETING. THE FOD PLAN IS SUBJECT TO APPROVAL BY THE AIRPORT MANAGER.

CONSTRUCTION WORKERS WILL NOT BE ALLOWED TO ESTABLISH OVERNIGHT RESIDENCE ON THE PREMISES. ALL CONSTRUCTION WORKERS SHALL LEAVE THE CONSTRUCTION SITE AND AIRPORT PROPERTY AT THE END OF THEIR WORK PERIOD.



WORK CANNOT COMMENCE UNTIL:

- A). SUFFICIENT BARRICADES ARE IN PLACE TO CONFINE THE AREA AND CREATE A BARRIER BETWEEN AIRCRAFT AND VEHICLE MOVEMENT AREAS AND THE CONSTRUCTION AREA.
- B). ALL SAFETY EQUIPMENT FOR PERSONNEL AND CONSTRUCTION EQUIPMENT IS IN PLACE AND OPERABLE.
- C). A NOTICE TO PROCEED HAS BEEN ISSUED BY THE TOWN OF ADDISON TO THE CONTRACTOR.

ALL MATERIAL SUBMITTALS FOR ITEMS TO BE USED IN CONSTRUCTION OF THE PROJECT SHALL BE SUBMITTED TO THE ENGINEER FOR REVIEW AND APPROVAL 21-DAYS PRIOR TO COMMENCEMENT OF WORK. THREE-WEEK LOOK AHEAD SCHEDULES TO BE PROVIDED ON A WEEKLY BASIS AND BE EXPANDED AND ENHANCED VERSIONS OF THE PROJECT SCHEDULE. WEEKLY SCHEDULES TO BE PRESENTED BY THE CONTRACTOR AT THE WEEKLY CONSTRUCTION MEETINGS.

A COMPLETE PROJECT SCHEDULE SHALL BE PROVIDED AT THE PRE-CONSTRUCTION MEETING AND PRESENTED BY THE CONTRACTOR TO THE MEETING ATTENDEES. RUNWAY SHUT-DOWN DATES MAY BE DETERMINED AT THE PRE-CONSTRUCTION MEETING, OR AT A LATER TIME, AS APPROVED BY THE AIRPORT MANAGER.

INTERIM PROJECT SCHEDULES TO BE PROVIDED ON THE FIRST OF EACH MONTH AND INCLUDE ORIGINAL BASELINE, UPDATED TO CURRENT CONSTRUCTION ACTIVITY.

**B. WORK ADJACENT TO AND ON RUNWAYS**

THE CONTRACTOR SHALL:

- 1. NOT ALLOW ANY WORK TO BE UNDERTAKEN INSIDE OF, OR ANY PERSONNEL, EQUIPMENT, OR VEHICLES TO ENTER THE TAXIWAY OR RUNWAY RESTRICTED ZONE (OBJECT FREE AREA) WHILE ANY TAXIWAY OR RUNWAY IS "OPEN". THE RESTRICTED ZONE FOR TAXIWAY 'B' IS DEFINED AS THE AREA WITHIN 93- FEET OF THE TAXIWAY CENTERLINE. THE ENGINEER SHALL, WHEN REQUESTED BY THE CONTRACTOR AND WHEN IN CONFORMANCE TO THE APPROVED CONSTRUCTION SCHEDULE, ARRANGE FOR THE CLOSURE OF AFFECTED TAXIWAYS AND RUNWAYS. A MINIMUM OF 24 HOURS ADVANCE NOTICE IS REQUIRED TO SCHEDULE ANY AIRFIELD PAVEMENT CLOSURES.
- 2. BE RESPONSIBLE FOR PROVIDING ALL TEMPORARY LIGHTING AND OTHER SPECIAL EQUIPMENT THAT MAY BE NEEDED FOR NIGHTTIME CONSTRUCTION IF REQUESTED BY CONTRACTOR AND APPROVED BY ENGINEER. THE COST OF THIS EQUIPMENT SHALL BE INCLUDED IN THE GENERAL COST OF THE WORK, NO SEPARATE PAYMENT WILL BE MADE.

REQUEST THROUGH THE ENGINEER AND IN CONFORMANCE WITH THE PHASING PLANS THE CLOSURES OF ANY AIRFIELD PAVEMENTS. THE AIRPORT MANAGER, IN COOPERATION WITH THE FAA, WILL CLOSE THE TAXIWAYS AND RUNWAYS TO AIRCRAFT ACTIVITY TO ALLOW CONSTRUCTION ACTIVITY WITHIN THE RUNWAY/TAXIWAY RESTRICTION ZONE. THESE ZONES ARE DEFINED AS AN AREA WITHIN 250- FEET OF A RUNWAY CENTERLINE AND 93- FEET OF A TAXIWAY 'A' CENTERLINE. THE CLOSURE PERIOD WILL BE SUBJECT TO THE FOLLOWING CRITERIA:

THE CLOSURE PERIODS WILL BE SCHEDULED IN GENERAL CONFORMANCE WITH THE PHASING PLANS. ADDITIONALLY, THE RUNWAY CLOSURE WILL BE DEPENDENT UPON THE WEATHER FORECAST; THE CONTRACTOR BEING FULLY MOBILIZED TO PURSUE THE WORK AT MAXIMUM EFFICIENCY (IN THE ENGINEER'S OPINION); AND ANY UNFORESEEN EMERGENCY WHICH, IN THE ENGINEER'S AND TOWN'S OPINIONS, MAKES THE CLOSURE UNFEASIBLE.

THE CONTRACTOR WILL BE REQUIRED TO FURNISH ALL BARRIERS, BARRICADES, AND TAXIWAY/RUNWAY CLOSED SYMBOLS AS NECESSARY. THESE ITEMS WILL NOT BE PAID FOR SEPARATELY, BUT WILL BE INCLUDED IN THE PRICE BID FOR M-101, MOBILIZATION.

AT THE CONCLUSION OF ANY TAXIWAY CLOSURE, THE TAXIWAY SAFETY AREA AND ASSOCIATED NAVAID CRITICAL AREA SHALL BE RESTORED TO ORIGINAL ELEVATIONS AND GRADES. THE CONTRACTOR MAY NOT CONCLUDE A DAYS WORK AND REMOVE HIS FORCES FROM A WORK AREA WITHOUT RESTORING THE TAXIWAY OR EXISTING TAXIWAY TO OPERATIONAL STATUS.

**V. STAGING AREAS - GENERAL REQUIREMENTS**

- A. THE LOCATION AND SIZE OF THE CONTRACTOR'S STAGING AREA IS SHOWN FOR REFERENCE ONLY. THE ACTUAL SIZE AND EXACT LOCATION WILL BE ESTABLISHED PRIOR TO CONSTRUCTION. THE FOLLOWING REQUIREMENTS WILL APPLY, HOWEVER:



ACCESS TO THE STAGING AREA SHALL BE OFF AIRPORT PARKWAY. ALL CONSTRUCTION SITE ACCESS SHALL BE VIA THE SECURITY GATE INSTALLED AS SHOWN ON SHEET PL-1 OR AS DIRECTED BY THE AIRPORT OPERATIONS. THE CONTRACTOR SHALL NOT PREVENT ACCESS TO THE SECURITY GATE BY AIRPORT STAFF, FAA PERSONNEL, THE ENGINEER OR THE ENGINEER'S SUBCONSULTANTS. ALL OF THE CONTRACTOR'S ACTIVITY WILL BE STAGED FROM THIS AREA. ALL FIELD OFFICE AND EMPLOYEE PARKING WILL BE LIMITED TO THE AREA ALONG AIRPORT PARKWAY. ONLY VEHICLES REQUIRED TO PERFORM LEGITIMATE FUNCTIONS BY THE CONTRACTOR ON THIS PROJECT AND FULLY COVERED UNDER THE CONTRACTOR'S INSURANCE AS SPECIFIED WITHIN THE SPECIAL PROVISIONS, WILL BE ALLOWED WITHIN THE AOA. THE CONTRACTOR WILL BE RESPONSIBLE FOR TRANSPORTING EMPLOYEES FROM THE STAGING AREA TO AND FROM THE WORK AREAS.

IT IS NOT ANTICIPATED THAT THE AIRPORT'S OBSTRUCTION HEIGHT REQUIREMENTS, AS DEFINED IN FAA'S FAR PART 77, WILL RESTRICT THE CONTRACTOR'S ACTIVITIES IN THE STAGING AREA. IT IS REQUIRED, HOWEVER, THAT THIS ASSUMPTION BE VERIFIED BY THE CONTRACTOR THROUGH THE ENGINEER PRIOR TO BEGINNING WORK.

THE CONTRACTOR WILL BE REQUIRED TO OBSERVE ALL EXISTING TRAFFIC FLOW DIRECTIONS WHEN ENTERING AND LEAVING THE STAGING AREA. NO COUNTER FLOW WILL BE ALLOWED ANY TIME.

THE CONTRACTOR SHALL PROVIDE PROFESSIONALLY-MADE SIGNS INDICATING THE NAME OF THE CONTRACTOR AND A MESSAGE DIRECTING ALL MATERIALS DELIVERIES TO THE STAGING AREA.

**VI. SCHEDULES**

- A. THE WORK IN THIS CONTRACT HAS BEEN SEQUENCED IN A MANNER WHICH WILL MINIMIZE DISRUPTION TO NORMAL AIRPORT OPERATION AND COMPLY WITH APPROPRIATE FAA SAFETY CRITERIA. THE CONTRACTOR'S DETAILED SCHEDULING OF HIS WORK MUST BE DONE WITHIN THE FRAMEWORK OF THE SPECIFIED SEQUENCE OF CONSTRUCTION AND THESE CONSTRUCTION CONTROL NOTES. FAILURE BY THE CONTRACTOR TO APPRECIATE AND UNDERSTAND THE COMPLEXITY OF THE WORK IN HIS SCHEDULING WILL NOT BE REASON FOR HIM/HER TO CLAIM FOR ADDITIONAL TIME AND/OR COMPENSATION.
- B. BECAUSE OF THE CIRCUMSTANCES OF THIS WORK, THE CONTRACTOR IS ADVISED THAT THE CONTRACT REQUIREMENTS FOR SCHEDULING OF THE WORK AND THE PENALTIES FOR FAILURE TO MAINTAIN AN APPROVED REALISTIC CONSTRUCTION SCHEDULE WILL BE STRICTLY ENFORCED. SHOULD THE CONTRACTOR FAIL TO MAINTAIN A CONSTRUCTION SCHEDULE THAT REASONABLY REFLECTS ACTUAL AND ANTICIPATED PROGRESS, ANY ADDITIONAL COSTS NECESSARY TO RESOLVE CONFLICTS WITH THE WORK THAT, IN THE ENGINEER'S OPINION, COULD HAVE OTHERWISE BEEN FORESEEN AND AVOIDED, WILL BE BORNE BY THE CONTRACTOR.

**VII. DUST CONTROL**

- A. THE CONTRACTOR SHALL PAY PARTICULAR ATTENTION TO THE DUST CONTROL REQUIREMENTS OF THIS CONTRACT. THE OPERATION OF RUNWAYS, TAXIWAYS, AND ASSOCIATED NAVAIDS ARE ESPECIALLY SENSITIVE TO DUST. THE ENGINEER RESERVES THE RIGHT TO STOP CONTRACTOR OPERATIONS, IF NECESSARY TO BRING DUST UNDER CONTROL. THE CONTRACTOR SHALL BE RESPONSIBLE FOR DUST CONTROL WITHIN THE CONSTRUCTION LIMITS AS WELL AS ALONG ANY ROADWAYS USED BY THE EQUIPMENT AND VEHICLES.
- B. WATER TRUCKS SHALL BE MAINTAINED AT ALL TIMES SUCH THAT THE ACCESS ROADS AND CONSTRUCTION AREAS CAN BE WETTED AS NECESSARY. THE CONTRACTOR SHALL BE PREPARED, AT NO EXTRA COST TO THE OWNER, TO USE ADDITIONAL WATER TRUCKS OR OTHER MEANS SHOULD IT BE NECESSARY TO MAINTAIN DUST TO AN ACCEPTABLE LEVEL. ALL WATER TRUCKS SHALL BE SUBSIDIARY TO THE VARIOUS BID ITEMS ON THE PROJECT.

**VIII. DEMOBILIZATION**

CONDITIONS OF THE PROJECT AREA UPON COMPLETION OF THE JOB SHALL BE GOOD AS OR BETTER THAN THE CONDITIONS PRIOR TO STARTING WORK, IN ADDITION TO THE WORK ITEMS LISTED.

THE PROJECT AREA SHALL BE FREE OF ANY CONTRACTOR STOCKPILE MATERIALS UPON COMPLETION OF THE JOB UNLESS OTHERWISE DIRECTED BY THE ENGINEER.

UPON COMPLETION OF THE PROJECT, ALL OF THE HAUL ROUTES SHALL BE PROPERLY CLEANED TO PREVENT OBSTRUCTION AND/OR CAUSE INCONVENIENCE TO NORMAL REGULAR TRAFFIC. ALL TEMPORARY HAUL ROUTES SHALL BE REMOVED AND BROUGHT BACK TO ORIGINAL CONDITION OR BETTER.

ANY PROPERTIES BELONGING TO THE AIRPORT SHALL BE RETURNED TO THE AIRPORT OWNER.

PROPER DRAINAGE (NO LOCALIZED PONDING) SHALL BE MAINTAINED, PRIOR TO, DURING AND AFTER MOBILIZATION.

DEMOBILIZATION SHALL BE DONE TO THE SATISFACTION OF THE ENGINEER.

DEMOBILIZATION SHALL BE DONE IN A MANNER THAT WILL NOT CAUSE ANY INCONVENIENCE TO AIRPORT OPERATIONS.

ANY DAMAGE TO THE AIRPORT PROPERTIES DURING DEMOBILIZATION SHALL BE REPAIRED AND PAID FOR AT THE CONTRACTOR'S OWN EXPENSE.

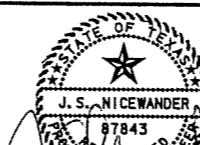
SAFETY REGULATIONS SHALL BE OBSERVED AT ALL TIMES DURING DEMOBILIZATION.

THE COST FOR DEMOBILIZATION SHALL BE CONSIDERED SUBSIDIARY TO ITEM 1, M-101, MOBILIZATION.

THE CONTRACTOR SHALL VIDEO TAPE THE ENTIRE WORK AREA AFTER THE CONSTRUCTION ACTIVITIES HAVE BEEN COMPLETED. THE CONTRACTOR SHALL PROVIDE ONE (1) COPY OF THIS VIDEO TAPE(S) TO BOTH THE AIRPORT MANAGER AND THE ENGINEER. THE COST OF PREPARING VIDEO SHALL BE SUBSIDIARY TO THE OTHER BID ITEMS.

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NO.	DATE	REVISION	APPROV.	NO.	DATE	REVISION	APPROV.
1	10/10/03	NOTE REMOVED, NOTE REVISION	JSN				



<b>HNTB</b> ARCHITECTS ENGINEERS PLANNERS The HNTB Companies		PROJECT NO. 39020
DESIGN CHECK	JSN	
DRAWN CHECK	JOH	
	GFS	
	JSN	
DATE	10/10/03	



<b>TOWN OF ADDISON</b>	
AIRPORT PARKWAY REALIGNMENT	
GENERAL NOTES	

"As-Built"

# ROADWAY CONSTRUCTION NOTES:

1. THE CONTRACTOR SHALL SCHEDULE OPERATIONS TO MINIMIZE POTENTIAL DROP-OFF HAZARDS. ANY DEVIATION TO THE CONSTRUCTION SEQUENCING PLANS CONTAINED IN THESE PLANS MUST BE APPROVED BY THE ENGINEER.
2. PRIOR TO FINAL ACCEPTANCE, ALL CURB SURFACES SHALL BE CLEARED OF ALL DISCOLORATION SUCH AS TIRE MARKS OR OTHER DISFIGUREMENT, AND PATCHED IF NECESSARY
3. TREES OUTSIDE OF TOE OF SLOPES SHALL NOT BE DISTURBED EXCEPT WITH APPROVAL OF THE OWNER. ONLY A MINIMUM AMOUNT OF TREE REMOVAL SHALL BE DONE. CONTRACTOR SHALL FLAG ALL TREES TO BE REMOVED FOR OWNER'S APPROVAL BEFORE REMOVAL.
4. THE CONTRACTOR SHALL BEGIN CONSTRUCTION WITHIN FIVE (5) CALENDAR DAYS OF THE ISSUANCE OF THE NOTICE TO PROCEED AND GIVE NOTICE TO THE TOWN, IN WRITING, BEFORE WORK BEGINS ON THE PROJECT.
5. THE CONTRACTOR SHALL SEQUENCE CONSTRUCTION AROUND BUSINESS DRIVEWAYS SO THAT ACCESS TO THE BUSINESS CAN BE OBTAINED DURING BUSINESS HOURS. NO PARKING LOT WILL BE SHUT OFF FROM ACCESS WITHOUT APPROVAL OF THE TOWN OF ADDISON.
6. ALL UTILITY CONSTRUCTION (STORM WATER) SHALL BE COMPLETED PRIOR TO SUBGRADE PREPARATION.
7. SUBGRADES SHALL BE PROOF ROLLED. NO ADDITIONAL COMPENSATION SHALL BE PAID FOR PROOF ROLLING, IT SHALL BE CONSIDERED SUBSIDIARY TO OTHER BID ITEMS.
8. THE CONTRACTOR SHALL FURNISH, AT HIS OWN COST, THE FOLLOWING TESTING SERVICES BY A REPUTABLE INDEPENDENT TESTING LABORATORY APPROVED BY THE TOWN.
  - A. FIELD DENSITY TESTS OF EMBANKMENT, SUBGRADE, OR BASE, AT LOCATIONS SPECIFIED BY THE INSPECTOR.
  - B. PAVEMENT TESTING SHALL BE DONE ACCORDING TO SECTION 5.8.6, PAVEMENT TESTING, OF THE NCTCOG SPECS (COMPRESSIVE STRENGTH).
9. THE CONTRACTOR SHALL ADJUST ALL VALVE BOXES, METER BOXES, MANHOLES, AND OTHER UTILITY STRUCTURES TO GRADE AS NEEDED PRIOR TO AND AFTER STREET PAVING HAS BEEN COMPLETED.
10. EROSION CONTROL SHALL START WITH INITIAL CONSTRUCTION AND BE PRACTICED THROUGHOUT THE PROJECT.
11. IT IS THE INTENT OF THESE PLANS TO MAINTAIN TRAFFIC FLOW THROUGHOUT THE PROJECT LIMITS AT ALL TIMES DURING CONSTRUCTION. IF A SITUATION ARISES THAT WARRANTS A ROAD CLOSURE, THIS WILL ONLY BE PERMITTED WITH THE WRITTEN PERMISSION FROM THE PUBLIC WORKS DIRECTOR.
12. PROJECT SIGNS SHALL BE MAINTAINED AT INTERSECTIONS WITH ADDISON ROAD AND THROUGHOUT DURATION OF PROJECT.
13. MAINTAIN/REPAIR EXISTING IRRIGATION SYSTEMS TO INSURE WATERING OCCURS ON PRIVATE LANDSCAPING, NO EXTRA PAY.
14. THE CONTRACTOR SHALL PAY EXTRA ATTENTION TO ITEM 6.2.9 "BACKFILL" OF THE NCTCOG SPECIFICATIONS. THE OWNER WILL EXPECT ALL TRENCHES TO BE BACKFILLED WITH PROPER CARE AND ANY SETTLEMENT TO BE REMEDIED IN A TIMELY FASHION.
15. CONTRACTOR SHALL KEEP PROJECT SITE CLEAN AND ORDERLY. IT WILL BE UNACCEPTABLE TO ALLOW TRASH TO BLOW ONTO ADJACENT PROPERTIES. STRICT ATTENTION WILL BE PAID TO THIS ITEM. IF CONTRACTOR FAILS TO ADHERE TO THIS STIPULATION, OWNER RESERVES THE RIGHT TO STOP WORK UNTIL TRASH IS CLEANED UP. THIS STOPPAGE OF WORK WILL NOT EXTEND THE ORIGINAL CONTRACT TIME.
16. FLYASH MAY BE USED PER NCTCOG SPECIFICATIONS ITEM 2.2.2.(D)

# UTILITY CONTACTS

THE INFORMATION SHOWN ON THESE DRAWINGS CONCERNING TYPE AND LOCATION OF UNDERGROUND UTILITIES IS NOT GUARANTEED TO BE ACCURATE OR ALL INCLUSIVE. THE CONTRACTOR IS RESPONSIBLE FOR MAKING HIS OR HER OWN DETERMINATION AS TO THE TYPE AND LOCATION OF UNDERGROUND UTILITIES AS MAY BE NECESSARY TO AVOID DAMAGE THERETO. THE CONTRACTOR SHALL MAINTAIN THE UTILITY LOCATION MARKINGS UNTIL THEY ARE NO LONGER NECESSARY.

CONTRACTOR SHALL CONTACT 1-800-DIG-TESS (344-8377) PRIOR TO ANY EXCAVATION.

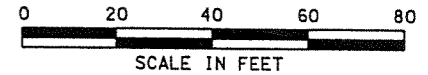
# TYPICAL IRRIGATION NOTES:

1. PRIOR TO DEMOLITION OR CONSTRUCTION ACTIVITIES, EXISTING IRRIGATION SYSTEMS SHALL BE TURNED ON. THE LOCATION OF HEADS, VALVES, AND PIPE SHALL BE MARKED WHERE DEMOLITION AND CONSTRUCTION WILL IMPACT THE SYSTEM. DEFICIENCIES IN THE SYSTEM SHALL BE NOTED AND REPORTED TO THE SYSTEM OWNER. A PHOTOGRAPHIC OR VIDEO RECORD SHALL BE MADE IF APPROPRIATE.
2. PIPING SHALL BE CUT, CAPPED AND MARKED AT THE LIMITS OF CONSTRUCTION.

# GENERAL NOTES:

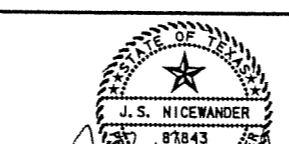
1. ALL MATERIALS AND WORKMANSHIP SHALL CONFORM TO THE TOWN OF ADDISON STANDARDS AND SPECIFICATIONS AND THE NORTH CENTRAL TEXAS COUNCIL OF GOVERNMENT (NCTCOG) STANDARDS AND SPECIFICATIONS, EXCEPT AS NOTED. IN THE EVENT OF A CONFLICT, THE TOWN OF ADDISON STANDARDS AND SPECIFICATIONS SHALL GOVERN.
2. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PUBLIC SAFETY DURING CONSTRUCTION AND WILL PROVIDE THE NECESSARY TRAFFIC BARRICADES AND WARNING SIGNAGE TO PROTECT THE CONSTRUCTION SITE. ALL BARRICADES, WARNING SIGNS, LIGHTS, DEVICES, ETC. FOR THE GUIDANCE AND PROTECTION OF TRAFFIC AND PEDESTRIANS MUST CONFORM TO THE 1980 TEXAS MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES, AS CURRENTLY AMENDED, BY THE TEXAS DEPARTMENT OF TRANSPORTATION.
3. NO PERSON SHALL OPEN, TURN OFF, INTERFERE WITH, ATTACH ANY PIPE OR HOSE TO OR TAP ANY WATER MAIN BELONGING TO THE TOWN UNLESS DULY AUTHORIZED TO DO SO BY THE TOWN OF ADDISON PUBLIC WORKS DEPARTMENT. ARRANGEMENTS FOR CONSTRUCTION WATER SHALL BE MADE THROUGH CITY OF ADDISON PUBLIC WORKS DEPARTMENT.
4. CONTRACTOR SHALL MAINTAIN ADEQUATE SANITARY FACILITIES FOR USE BY WORKERS THROUGHOUT CONSTRUCTION.
5. NO TRAFFIC SIGNS ARE TO BE RELOCATED OR REMOVED WITHOUT PRIOR APPROVAL OF THE TOWN OF ADDISON PUBLIC WORKS DEPARTMENT.
6. THE CONTRACTOR SHALL TAKE ALL NECESSARY PRECAUTIONS TO PROTECT EXISTING FACILITIES FROM DAMAGE. ANY DAMAGE TO EXISTING FACILITIES RESULTING FROM CONSTRUCTION WORK SHALL BE REPAIRED AT THE CONTRACTOR'S EXPENSE.
7. THESE PLANS DO NOT EXTEND TO OR INCLUDE DESIGNS OR SYSTEMS PERTAINING TO THE SAFETY OF THE CONSTRUCTION CONTRACTOR OR ITS EMPLOYEES, AGENT OR REPRESENTATIVES IN PERFORMANCE OF THE WORK. THE SEAL OF HNTB CORPORATION, REGISTERED PROFESSIONAL ENGINEER(S) HEREON DOES NOT EXTEND TO ANY SUCH SAFETY SYSTEMS THAT MAY NOW OR HEREAFTER BE INCORPORATED IN THESE PLANS. THE CONSTRUCTION CONTRACTOR SHALL PREPARE OR OBTAIN THE APPROPRIATE SAFETY SYSTEMS, INCLUDING THE PLANS AND SPECIFICATIONS.
8. IT IS THE CONTRACTOR'S RESPONSIBILITY TO MAINTAIN NEAT AND ACCURATE PLANS OF RECORD.

9. THE CONTRACTOR IS RESPONSIBLE FOR MAINTAINING ADEQUATE SITE DRAINAGE THROUGHOUT THE DURATION OF THIS PROJECT.
10. THE CONTRACTOR IS RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND APPROVALS BEFORE CONSTRUCTION BEGINS.
11. THE CONTRACTOR SHALL TAKE ALL NECESSARY PRECAUTIONS TO ENSURE THAT ELECTRIC POWER AND TELEPHONE POLES ARE NOT DISTURBED DURING CONSTRUCTION.
12. THE CONTRACTOR SHALL KEEP EXCAVATED TRENCHES FREE OF GROUNDWATER DURING CONSTRUCTION. IF NECESSARY, THE CONTRACTOR SHALL UTILIZE DEWATERING IN ORDER TO CONTROL GROUNDWATER DURING CONSTRUCTION SUCH THAT IT DOES NOT AFFECT HIS CONSTRUCTION WORK. (NO SEPARATE PAY ITEM)
13. EXISTING FACILITIES ARE SHOWN IN APPROXIMATE LOCATIONS PER INFORMATION AND RECORDS AVAILABLE. CONTRACTOR SHALL UNCOVER AND VERIFY HORIZONTAL AND VERTICAL LOCATION OF EXISTING FACILITIES PRIOR TO CONSTRUCTION.
14. ALL STORM SEWERS SHALL BE RCP CLASS III UNLESS OTHERWISE NOTED.
15. INSTALL CONCRETE COLLAR AT CONNECTION OF NEW RCP'S TO EXIST. RCP'S.
16. CONTRACTOR SHALL FIELD VERIFY FLOWLINE OF EXIST. RCP'S AND CONTACT ENGINEER IF A DISCREPANCY IS FOUND.
17. CONTRACTOR TO BE ADVISED, THERE WILL BE OTHER CONSTRUCTION ACTIVITIES GOING ON ADJACENT TO THIS PROJECT. COORDINATION BETWEEN CONTRACTORS WILL BE ESSENTIAL THROUGHOUT THE CONSTRUCTION OF THE PROJECT.



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1	10/10/03	NOTE MODIFIED	JSN				



10/10/03

**HNTB** ARCHITECTS ENGINEERS PLANNERS  
The HNTB Companies

DESIGN CHECK JSN  
 DRAWN JDH  
 CHECK GFS  
 DATE JSN

PROJECT NO. 39020



"As-Built"

**TOWN OF ADDISON**  
AIRPORT PARKWAY REALIGNMENT

GENERAL NOTES

ITEM NO.	DESCRIPTION	UNIT	EST. QTY.
ROADWAY IMPROVEMENTS			
101	MOBILIZATION	L.S.	1
102	PREPARE RIGHT-OF-WAY	L.S.	1
103	FULL DEPTH SAWCUT EXISTING PAVEMENT	L.F.	755
104	REMOVE AND DISPOSE OF EXISTING PAVEMENT, INCLUDING CURB	S.Y.	1,650
105	UNCLASSIFIED STREET EXCAVATION	C.Y.	500
106	EMBANKMENT	C.Y.	100
107	10" THICK REINFORCED CONCRETE PAVEMENT, 4000 PSI @ 28 DAYS	S.Y.	2,800
108	FURNISH, INSTALL 10' RECESSED CURB INLET	EA	2
109	CLASS III 18" RCP STORM DRAIN PIPE, INCLUDING FITTINGS	L.F.	108
110	REMOVE AND RELOCATE ROADSIDE SIGN	EA.	0
111	15 MPH SPEED LIMIT SIGN (R2-1)	EA.	2
112	FIRE LANE STRIPING	L.F.	1,100
113	SOLID SODDING	S.Y.	900
114	FURNISH, INSTALL, MAINTAIN AND REMOVE SILT FENCE	L.F.	300
115	INLET PROTECTION (DROP)	EA	2
116	IRRIGATION CONDUIT, 4" SCH 40 PVC	L.F.	114
117	FURNISH, INSTALL, SECURITY GATE WITH ACCESS CONTROL PANEL	LS	1
118	AOA SECURITY FENCE	LF	300
119	REMOVE AND DISPOSE OF TEMPORARY ROAD	LS	1
120	REMOVE AND DISPOSE OF SECURITY FENCE	LF	195
121	CLASS III 12" RCP STORM DRAIN PIPE	LF	95
122	FURNISH, INSTALL MAINTAIN AND REMOVE 20-FT SECURITY GATE	EA	0
123	CONDUIT, 2" SCH 40 PVC	LF	285
124	FOUNDATION, 30' STREET LIGHT POLE	EA	1
125	GROUND MOUNTED PULL BOX, TYPE A	EA	1
126	CABLE INSULATED, #8 AWG	LF	600
127	CABLE, BARE #8 AWG	LF	300

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1.	10/10/03	ADDED LINE ITEMS, MODIFIED QUANTITIES	JSN				
2.	11/18/03	MODIFIED QUANTITIES	JSN				



**HNTB** ARCHITECTS ENGINEERS PLANNERS  
The HNTB Companies

DESIGN CHECK	JSN	PROJECT NO.	39020
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DRAWN CHECK	GFS		
	JSN		
DATE	SEPTEMBER 2003		

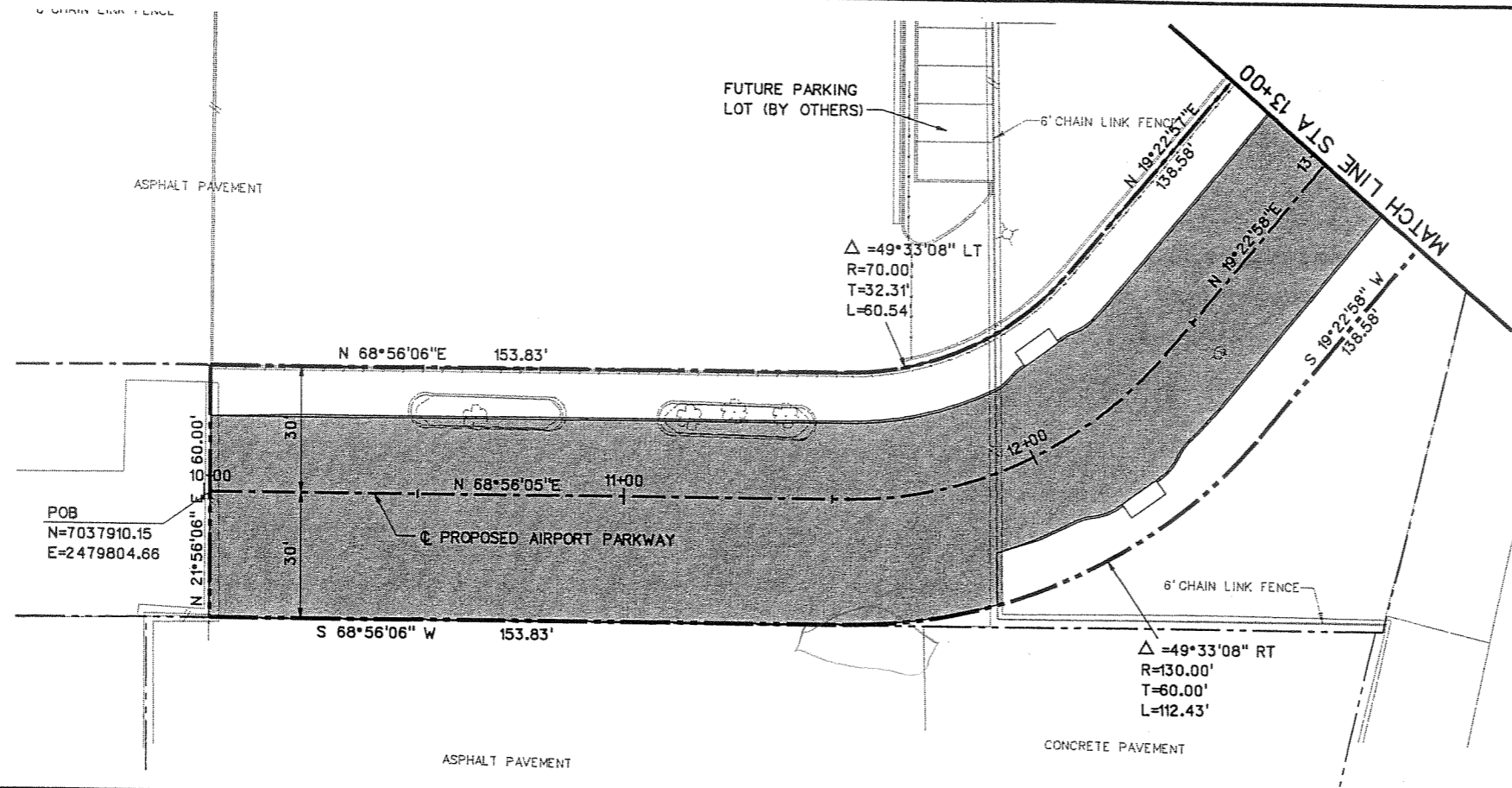
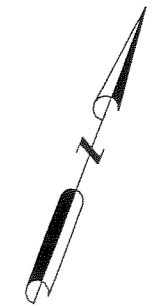
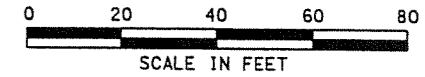


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**TOWN OF ADDISON**  
AIRPORT PARKWAY REALIGNMENT

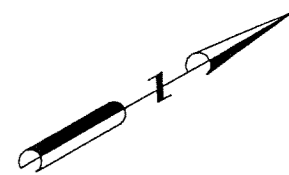
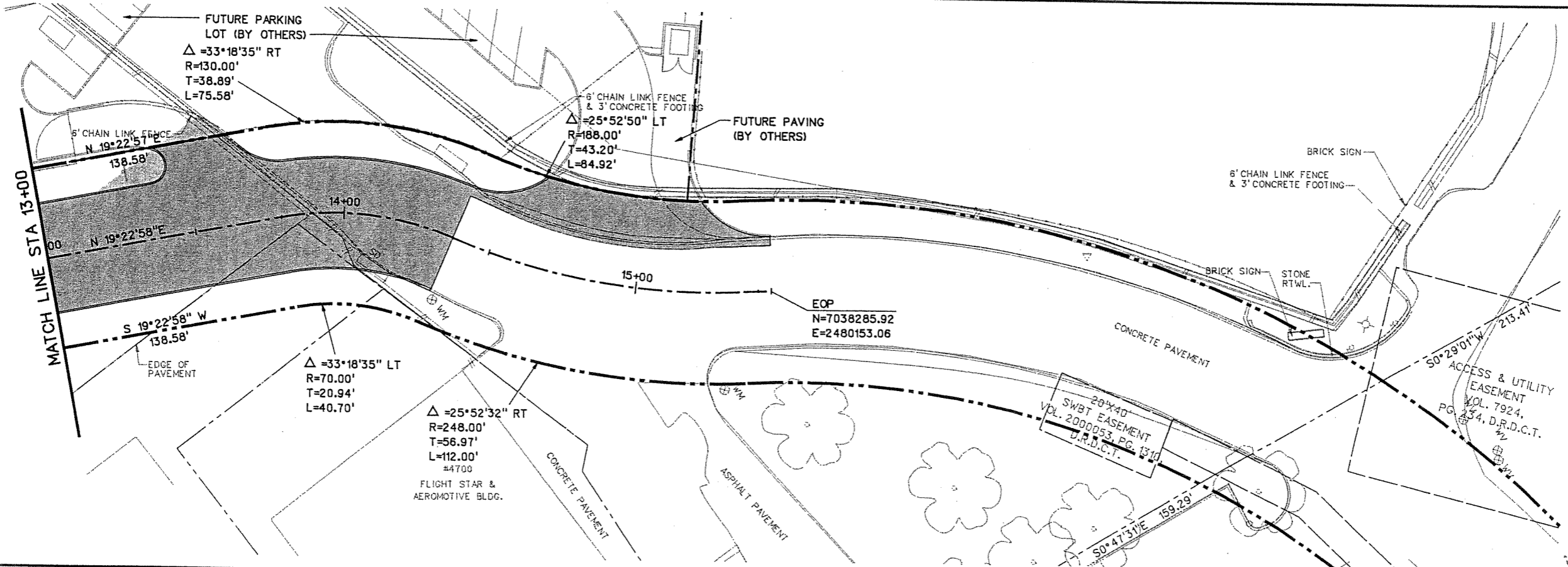
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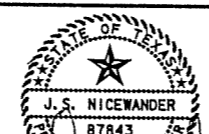


**LEGEND**

- CENTERLINE OF PROPOSED AIRPORT PARKWAY
- RIGHT OF WAY
- ▲ [Hatched Box] PAVING LIMITS OF NEW AIRPORT PARKWAY REALIGNMENT



NO.	DATE	REVISION	APPROV.	NO.	DATE	REVISION	APPROV.
1	10/10/03	EDITED LEGEND, HATCHED PROPOSED PAVEMENT, CLARIFIED TEXT	JSN				



**HNTB** ARCHITECTS ENGINEERS PLANNERS  
The HNTB Companies

DESIGN: JSN  
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DRAWN: GFS  
CHECK: JSN

PROJECT NO. 39020



**TOWN OF ADDISON**  
AIRPORT PARKWAY REALIGNMENT

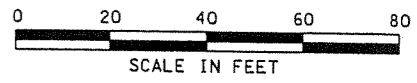
RIGHT OF WAY MAP  
STA 10+00 TO STA 15+45

SHEET

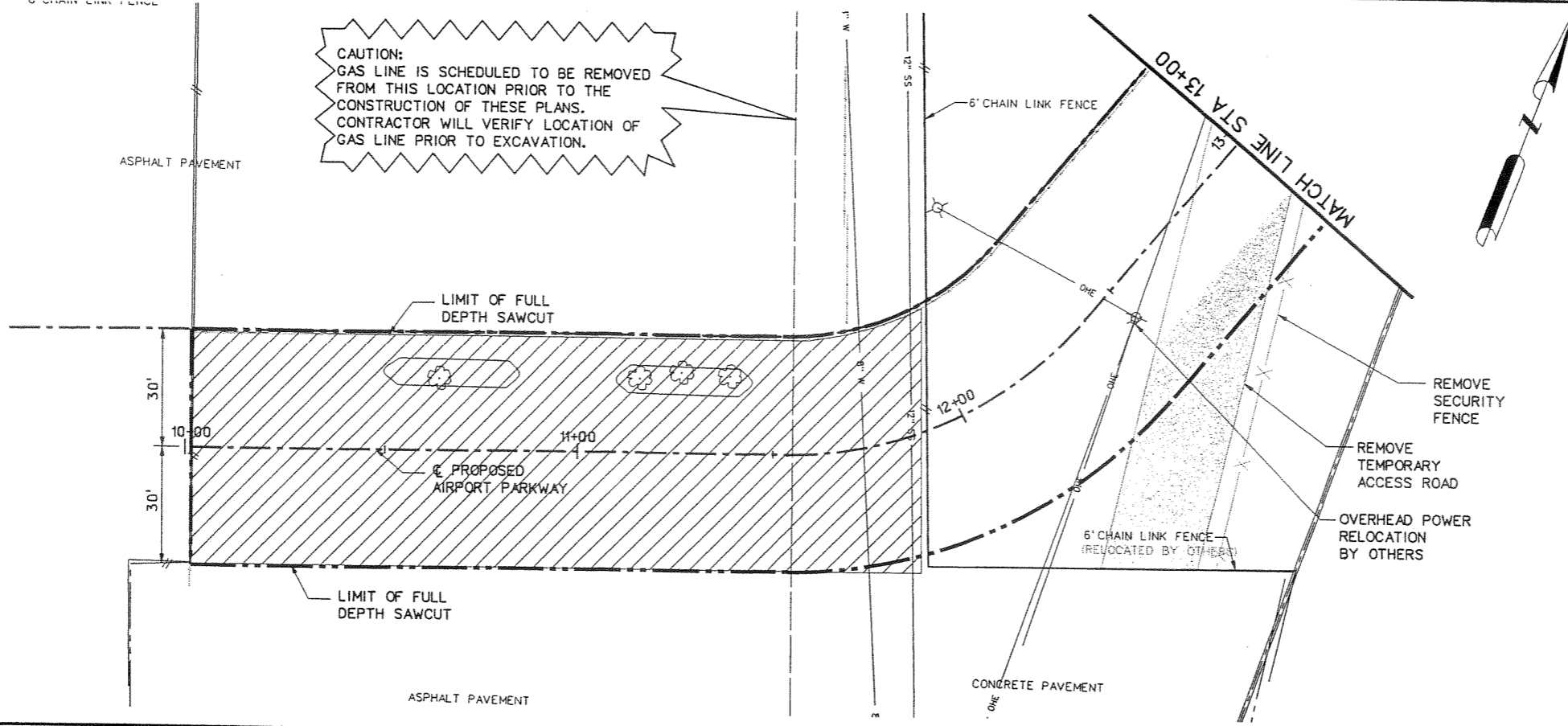
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CAUTION:  
GAS LINE IS SCHEDULED TO BE REMOVED FROM THIS LOCATION PRIOR TO THE CONSTRUCTION OF THESE PLANS. CONTRACTOR WILL VERIFY LOCATION OF GAS LINE PRIOR TO EXCAVATION.

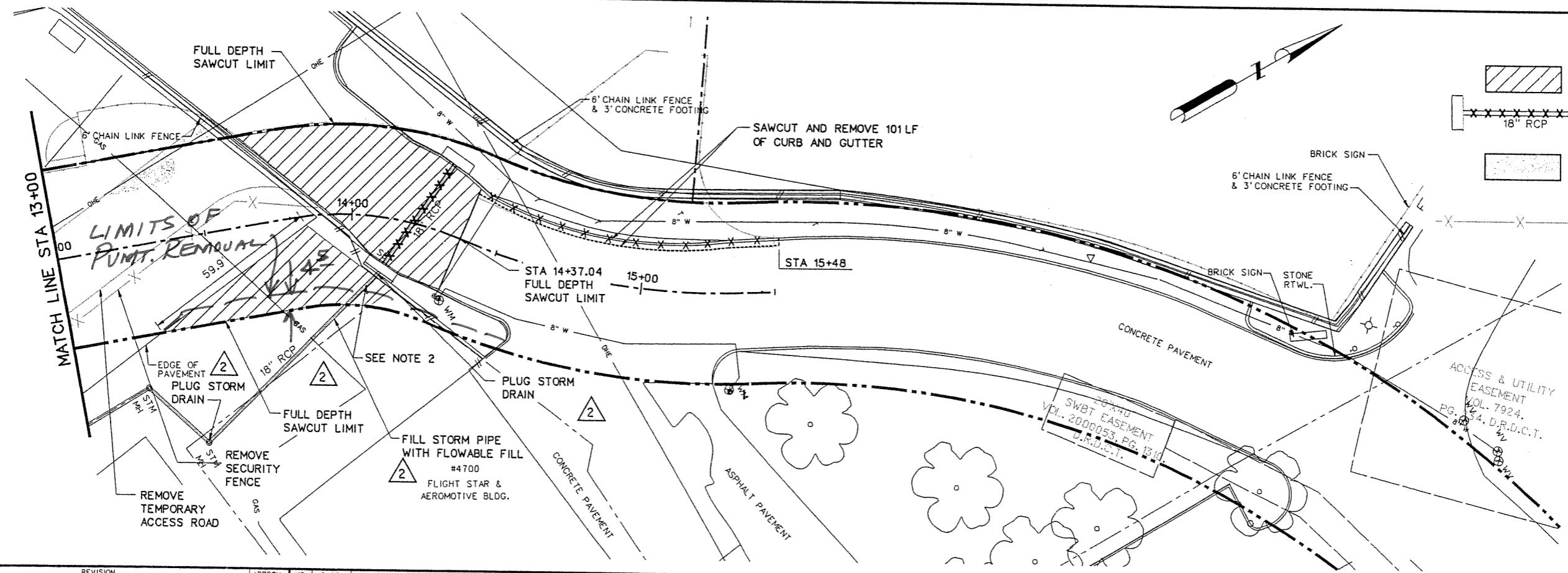


- NOTE:
1. SEE NCTCOG SPECIFICATION "PREPARING RIGHT-OF-WAY" FOR DETAILS.
  2. INLETS AND PIPE REMOVAL INCLUDED IN ITEM 102 'PREPARE RIGHT-OF-WAY' AND NOT A SEPARATE PAY ITEM.
  3. CONTRACTOR TO TAKE PRECAUTIONS NOT TO DISTURB WATERLINE OR WATER METER TO FLIGHT STAR. WATERLINE SHALL REMAIN IN SERVICE TO FLIGHT STAR THROUGHOUT CONSTRUCTION.
  4. UNDERGROUND UTILITIES EXIST WITHIN AND ADJACENT TO THE LIMITS OF CONSTRUCTION. AN ATTEMPT HAS BEEN MADE TO LOCATE THESE UTILITIES ON THE PLANS. ALL EXISTING UTILITIES MAY NOT BE SHOWN ON THE PLANS, AND THE LOCATION OF THE UTILITIES SHOWN MAY VARY FROM THE LOCATION ON THE PLANS. PRIOR TO THE BEGINNING OF ANY TYPE OF EXCAVATION, THE CONTRACTOR SHALL CONTACT THE UTILITIES INVOLVED AND MAKE ARRANGEMENTS FOR THE LOCATION OF THE UTILITY ON THE GROUND. THE CONTRACTOR SHALL MAINTAIN THE UTILITY LOCATION MARKINGS UNTIL THEY ARE NO LONGER NECESSARY.
  5. SECURITY FENCE SHALL NOT BE REMOVED UNTIL PERMANENT AOA FENCING IS IN PLACE.
  6. CONTRACTOR SHALL BACKFILL TEMPORARY ACCESS ROAD AFTER REMOVAL OF GRAVEL. BACKFILL SHALL BE GRADED TO DRAIN AND SODDED. SOD SHALL MATCH GRASS TYPE OF SURROUNDING GRASS. BACKFILL AND SOD SHALL BE SUBSIDIARY TO REMOVAL OF ROAD.

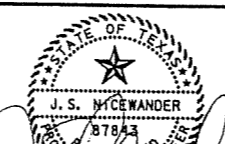


LEGEND

- PAVEMENT REMOVAL
- STORM DRAIN REMOVAL  
18" RCP
- TEMPORARY PAVEMENT REMOVAL
- TEMPORARY FENCE REMOVAL



NO.	DATE	REVISION	APPROV.	NO.	DATE	REVISION	APPROV.
1	10/10/03	SHEET REISSUED	JSN				
2	11/19/03	STORM DRAIN PIPE TO REMAIN AND BE PLUGGED	JSN				



**HNTB** ARCHITECTS ENGINEERS PLANNERS  
The HNTB Companies

DESIGN CHECK: JSN, JDH  
DRAWN CHECK: GFS, JSN  
DATE: SEPTEMBER 2003

PROJECT NO. 39020



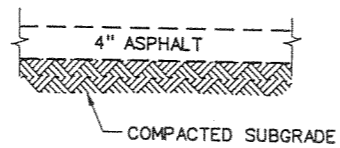
TOWN OF ADDISON  
AIRPORT PARKWAY REALIGNMENT

REMOVAL PLAN  
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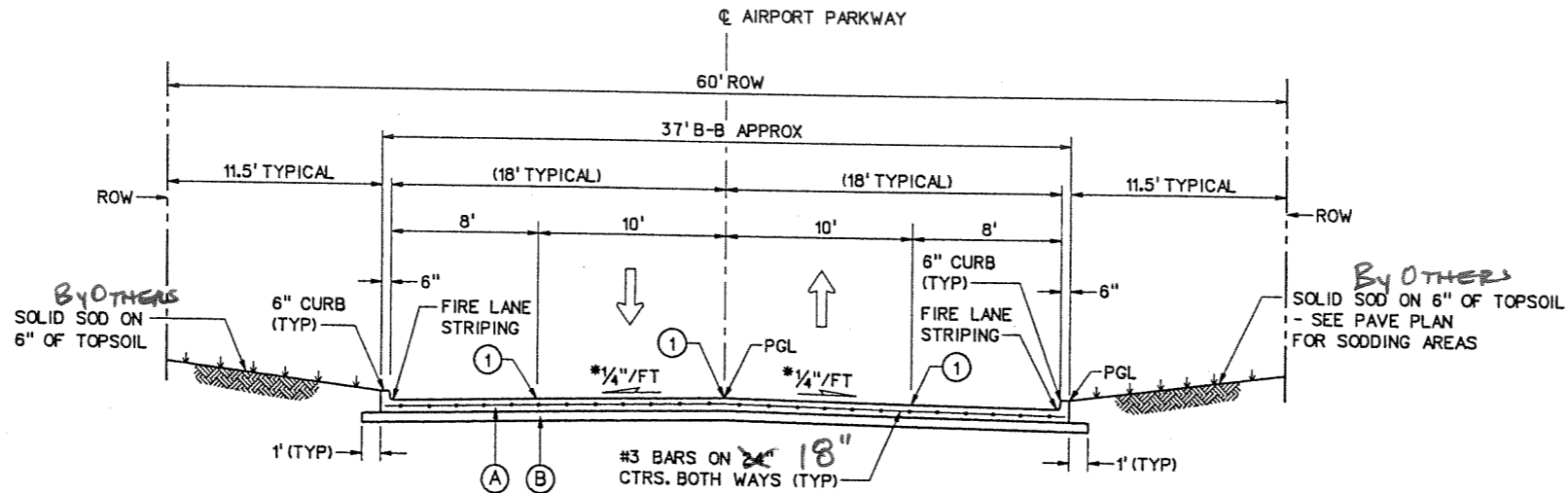




**EXISTING ASPHALT SECTION**  
 (PER SOILS REPORT -  
 SEE SPECIFICATIONS FOR BORINGS)  
 NO SCALE

**NOTES:**

1. INTEGRAL CURB & GUTTER SHALL CONFORM TO NCTCOG STD DWG NO. 2120. THE INTEGRAL CURB AND GUTTER SHALL BE SUBSIDIARY TO THE 10" REINFORCED CONCRETE PAVEMENT PAY ITEM.
2. ALL TRANSVERSE JOINTS SHALL BE ON 1' CENTERS MAX.
3. PROFILE GRADES SHOWN ON THE PAVING PLAN & PROFILE SHEETS ARE TOP OF CURB AT BACK OF CURB UNLESS NOTED OTHERWISE ON THE PLANS.
4. THE FINAL FINISH ON THE CONCRETE STREET PAVEMENT SHALL CONSIST OF A COMBINATION OF A LONGITUDINAL CARPET DRAG AND TRANSVERSE METAL TINE FINISH AS DESCRIBED IN TxDOT STANDARD SPECIFICATIONS FOR ITEM 360 - CONCRETE PAVEMENT UNLESS SPECIFIED OTHERWISE IN THE PLANS.



**TYPICAL SECTION**  
**AIRPORT PARKWAY**

STA 10+00.00 TO STA 14+38.60



**NOTE:**

LIMITS OF REMOVAL/REPLACEMENT FOR AIRPORT PKWY. ARE SHOWN ON THE REMOVAL PLANS (RM-1) AND THE PLAN AND PROFILE SHEETS (PP-1 AND PP-2). SEE THESE SHEETS FOR SPECIFIC LIMITS REGARDING AIRPORT PKWY.

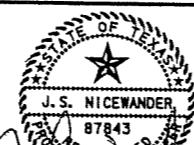
\*PAVEMENT CROSS SLOPES VARY FROM THE TYPICAL 1/4"/FT. SEE PAVING PLANS FOR PROPOSED GRADES.

**LEGEND**

- (A) 10" 4000 p.s.i. @28 DAYS REINFORCED PORTLAND CEMENT CONCRETE PVMT.
- (B) SCARIFY 6" DEPTH AND RECOMPACT TO 95% STANDARD PROCTOR DENSITY, SUBSIDIARY TO 10" REINFORCED CONCRETE PAVEMENT PAY ITEM
- (1) CONSTRUCTION JOINT (FULL WIDTH PAVING IS ALLOWED WHERE APPROVED BY THE TOWN OF ADDISON). IF FULL WIDTH PAVING IS APPROVED, SAWED JOINT

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NO.	DATE	REVISION	APPROV.	NO.	DATE	REVISION	APPROV.
1	10/10/03	MODIFIED NOTE	JSN				



**HNTB** ARCHITECTS ENGINEERS PLANNERS  
 The HNTB Companies

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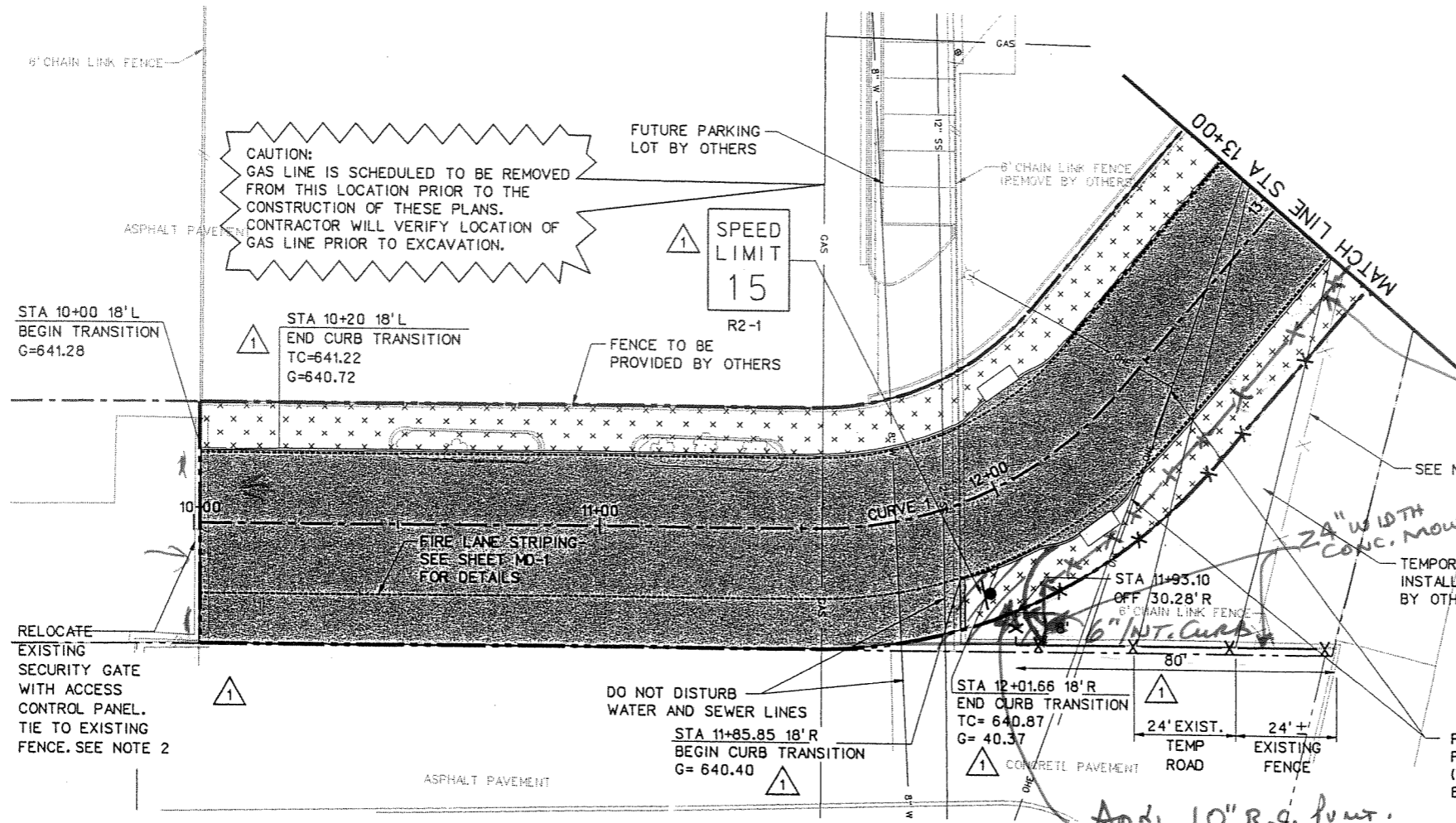
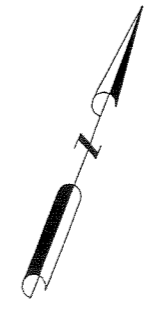
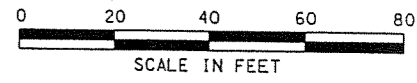
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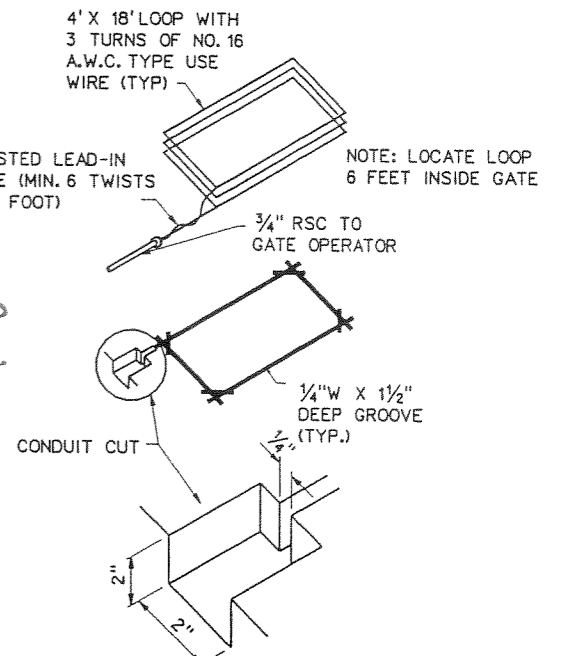
"As-Built"

**TOWN OF ADDISON**  
 AIRPORT PARKWAY REALIGNMENT

SHEET  
 TY-1

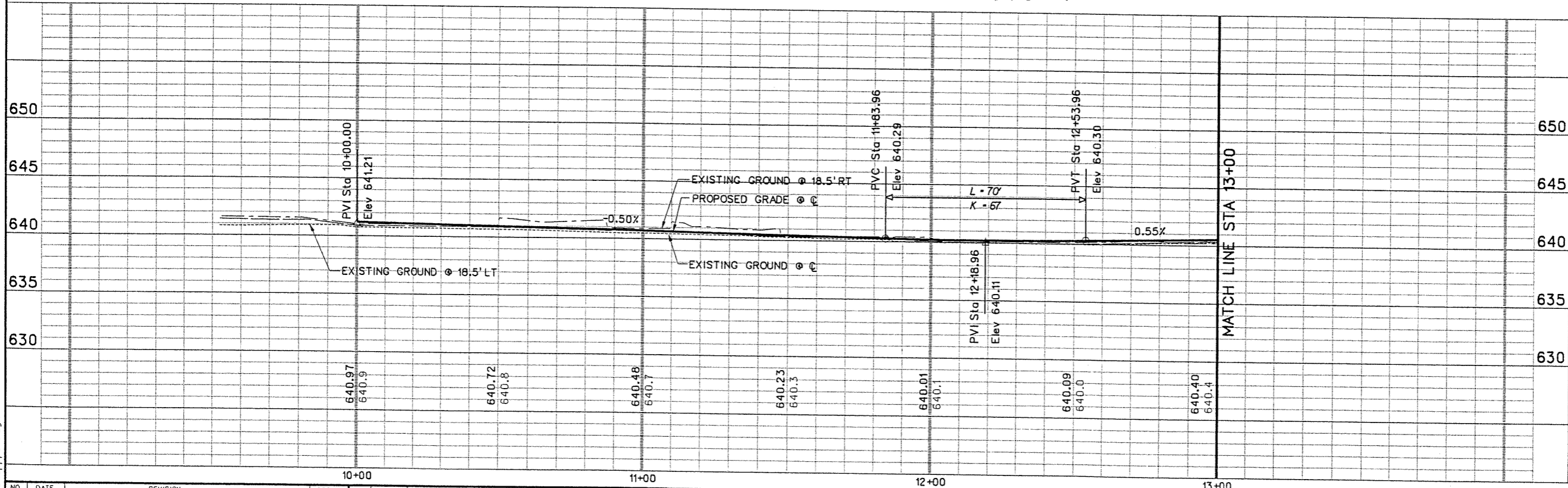


6" C.L. FENCE  
IN 36" WIDTH  
CONC. MOW STRIP  
5" FENCE - B.C.



TRAFFIC EXIT LOOP DETAIL  
NOT TO SCALE

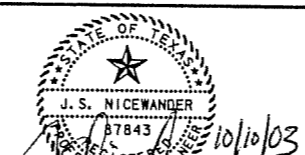
**CURVE 1**  
 $\Delta = 49^\circ 33' 08.66''$  LT  
 $R = 100.00'$   
 $T = 46.16'$   
 $L = 86.49'$   
 $PC = STA 11+55.40$   
 $PI = STA 12+01.55$   
 $PT = STA 12+41.88$



- LEGEND**
- SOLID SODDING
  - AOA SECURITY FENCE
  - FIRE LANE STRIPING
  - 10" PCCP
  - TEMPORARY ROAD
- NOTES:**
1. SECURITY FENCE TO BE PLACED IN COORDINATION WITH OTHER CONSTRUCTION PROJECTS GOING ON IN THE AREA AT THE SAME TIME.
  2. CONTRACTOR TO CONTACT DAVE FOSTER WITH THE ADDISON AIRPORT FOR SECURITY GATE EQUIPMENT.

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NO.	DATE	REVISION	APPROV.	NO.	DATE	REVISION	APPROV.
1.	10/10/03	NOTES ADDED / SPEED LIMIT SIGN	JSN				



**HNTB** ARCHITECTS ENGINEERS PLANNERS  
 THE HNTB Companies

DESIGN CHECK JSN  
 DRAWN CHECK GFS  
 DATE SEPTEMBER 2003

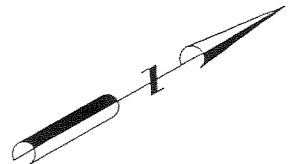
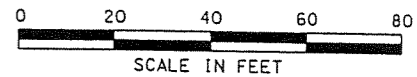
PROJECT NO. 39020



TOWN OF ADDISON  
 AIRPORT PARKWAY REALIGNMENT

PAVING PLAN/PROFILE  
 STA 10+00 TO STA 13+00

SHEET



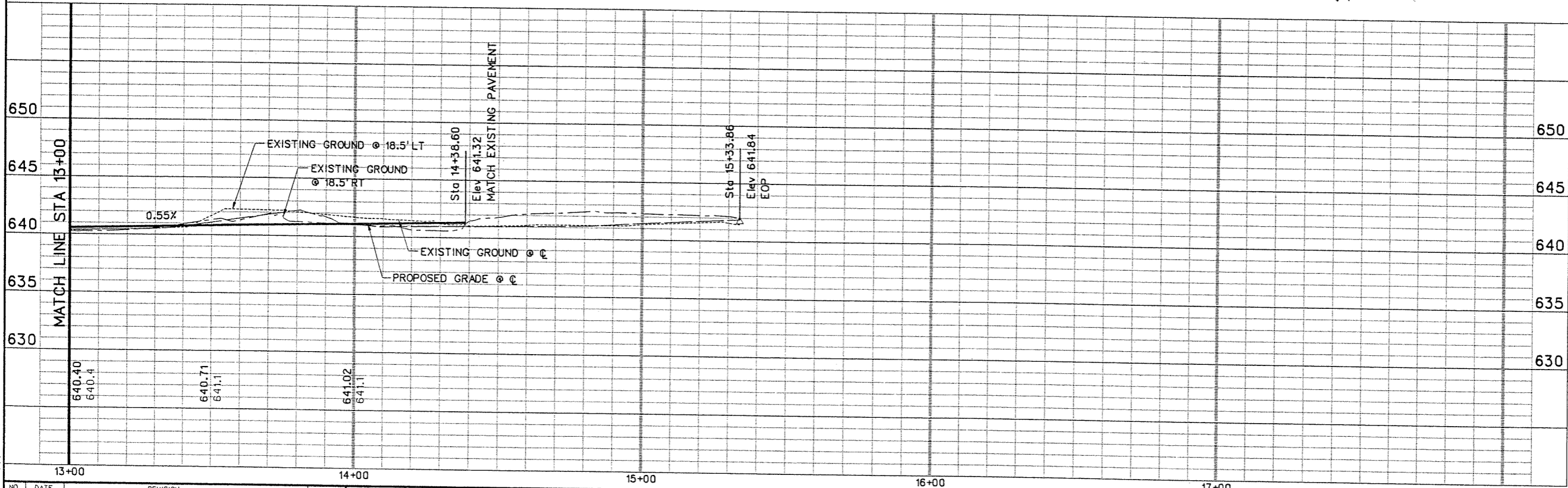
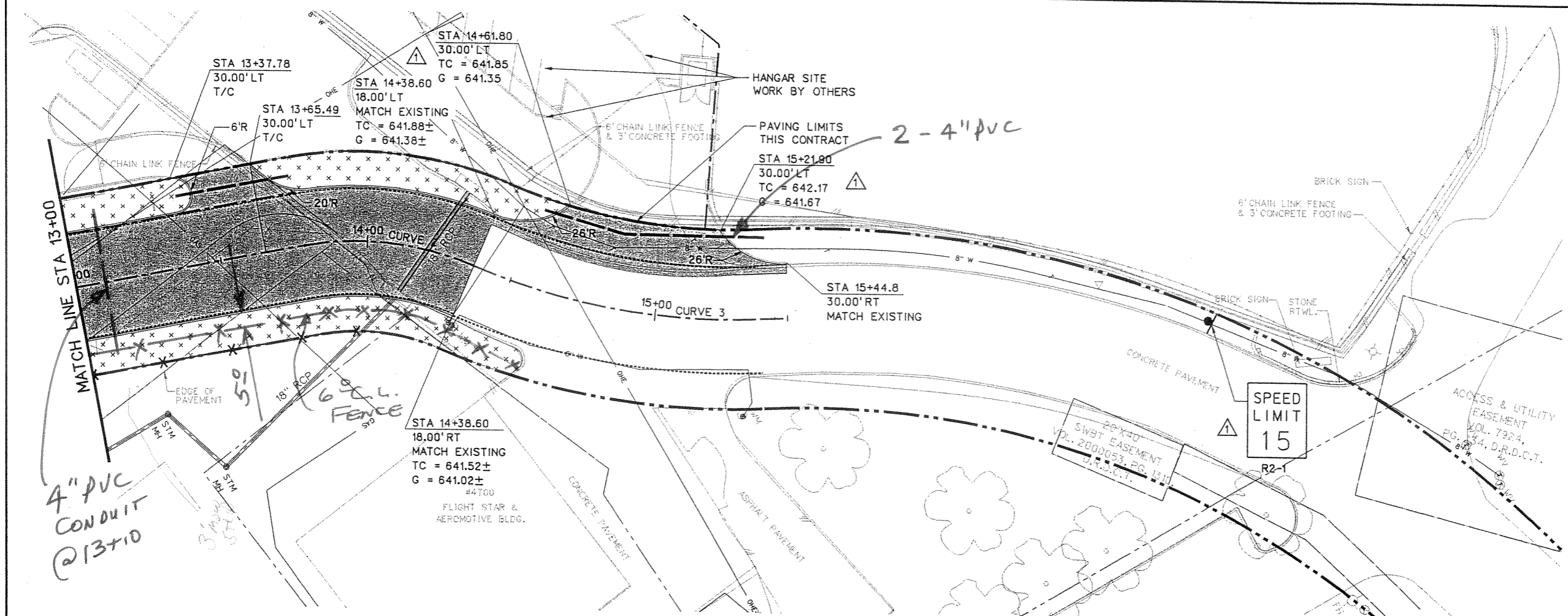
**LEGEND**

- SOLID SODDING
- AOA SECURITY FENCE
- FIRE LANE STRIPING
- 4" PVC CONDUIT, SCH 40
- 10" PCCP
- TEMPORARY ROAD

**CURVE 2**  
 $\Delta = 33^\circ 18' 35.56''$  RT  
 R=100.00'  
 T=29.92'  
 L=58.14'  
 PC STA = 13+80.46  
 PI STA = 14+10.38  
 PT STA = 14+38.60

**CURVE 3**  
 $\Delta = 25^\circ 49' 08.88''$  LT  
 R=218.00'  
 T=49.97'  
 L=98.24'  
 PRC STA = 14+38.60  
 PI STA = 14+88.57  
 PT STA = 15+36.84

NOTES:  
 1. ALL DIMENSIONS ARE FROM FACE OF CURB TO FACE OF CURB UNLESS OTHERWISE NOTED.



"As-Built"

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NO.	DATE	REVISION	APPROV.	NO.	DATE	REVISION	APPROV.
1.	10/10/03	NOTE REVISIONS, CHANGES TO LEGEND, SPEED LIMIT SIGN	JSN				

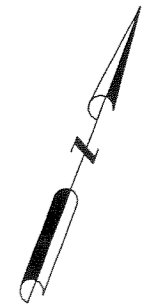
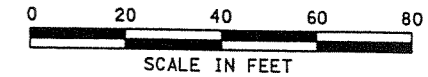


10/10/03

**HNTB** ARCHITECTS ENGINEERS PLANNERS  
 The HNTB Companies  
 PROJECT NO. 39020  
 DESIGN CHECK JSN  
 DRAWN CHECK GFS  
 DATE SEPTEMBER 2003



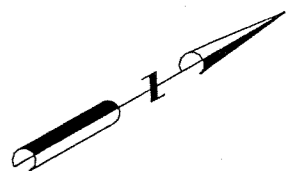
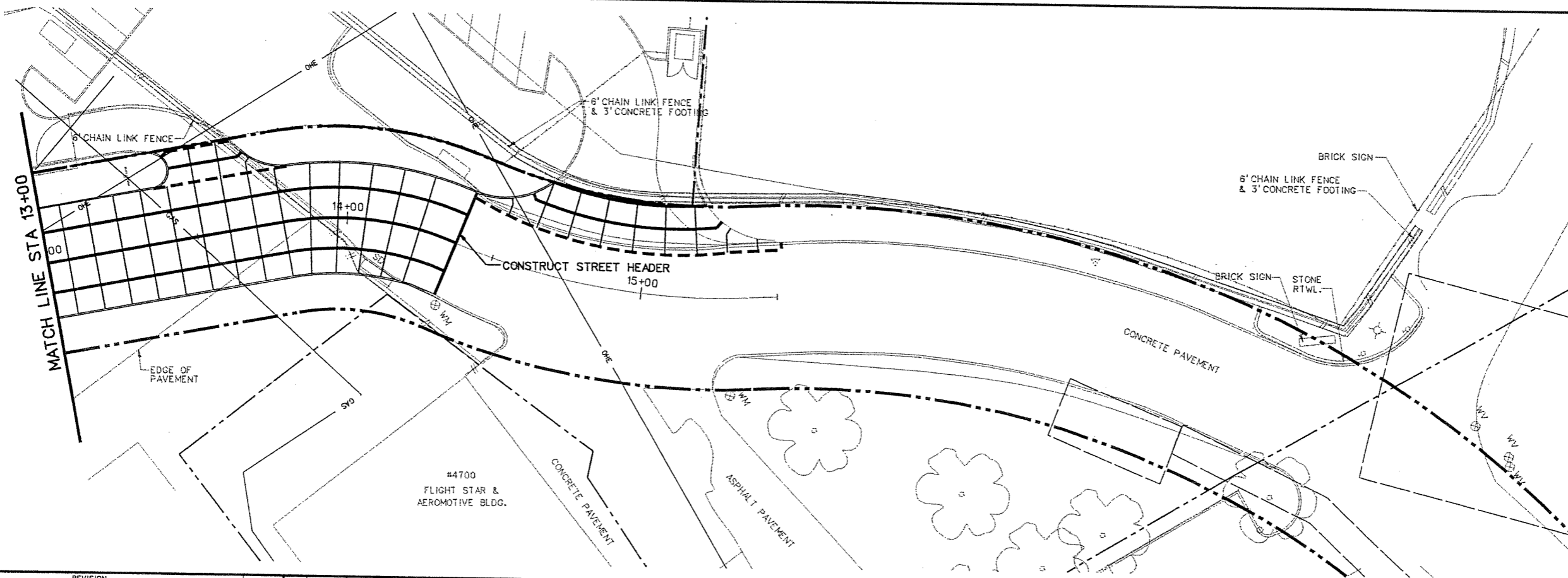
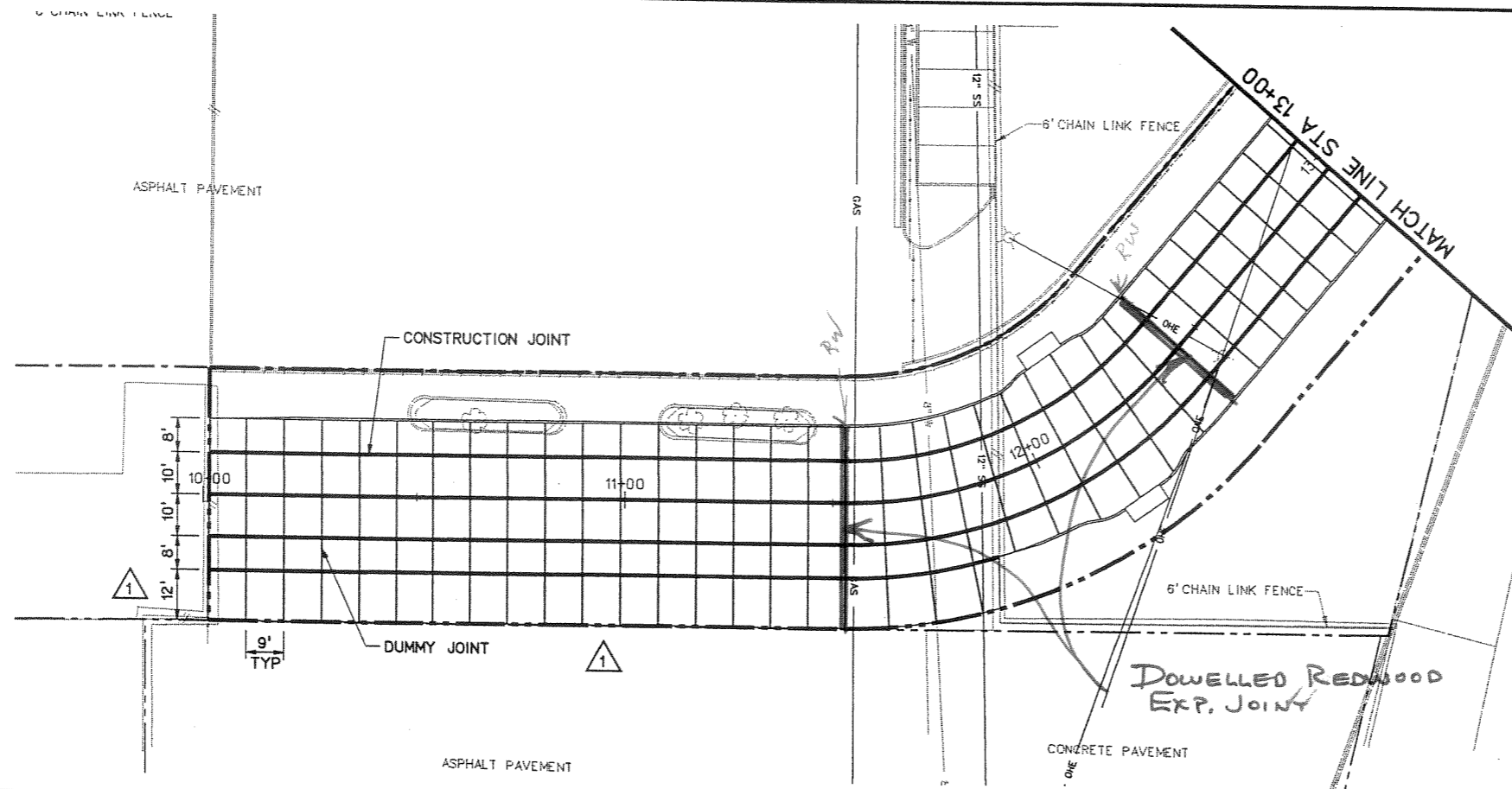
TOWN OF ADDISON  
 AIRPORT PARKWAY REALIGNMENT  
 PAVING PLAN/PROFILE  
 STA 13+00 TO STA 14+37  
 SHEET



- NOTE:
- DUMMY JOINTS ON 9' CENTERS
  - SEE PAVING DETAILS, SHEET PD-1, FOR JOINT DETAILS

**LEGEND**

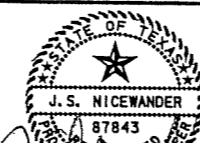
- LONGITUDINAL BUTT JOINT
- CONSTRUCTION JOINT
- DUMMY JOINT



*As-Built*

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NO.	DATE	REVISION	APPROV.	NO.	DATE	REVISION	APPROV.
L	10/10/03	NOTE, JOINT REVISION	JSN				



**HNTB** ARCHITECTS ENGINEERS PLANNERS  
The HNTB Companies

DESIGN CHECK JSN JQH  
DRAWN CHECK GFS JSN

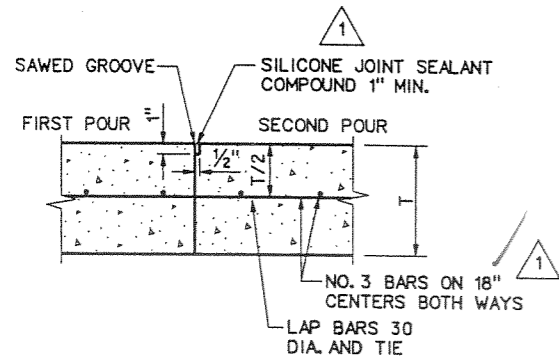
PROJECT NO. 39020



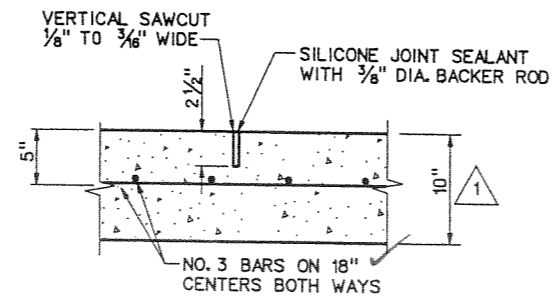
TOWN OF ADDISON  
AIRPORT PARKWAY REALIGNMENT

JOINT LAYOUT  
STA 10+00 TO STA 14+37

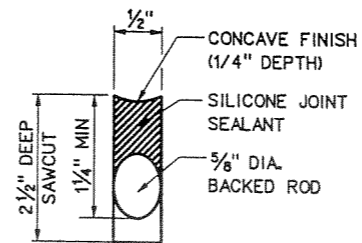
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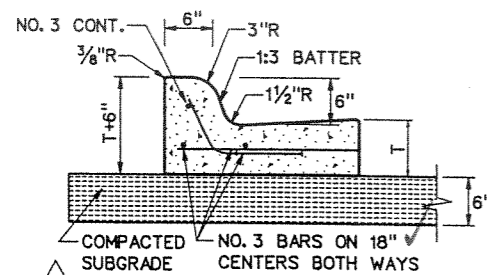
**CONSTRUCTION JOINT**  
NOT TO SCALE



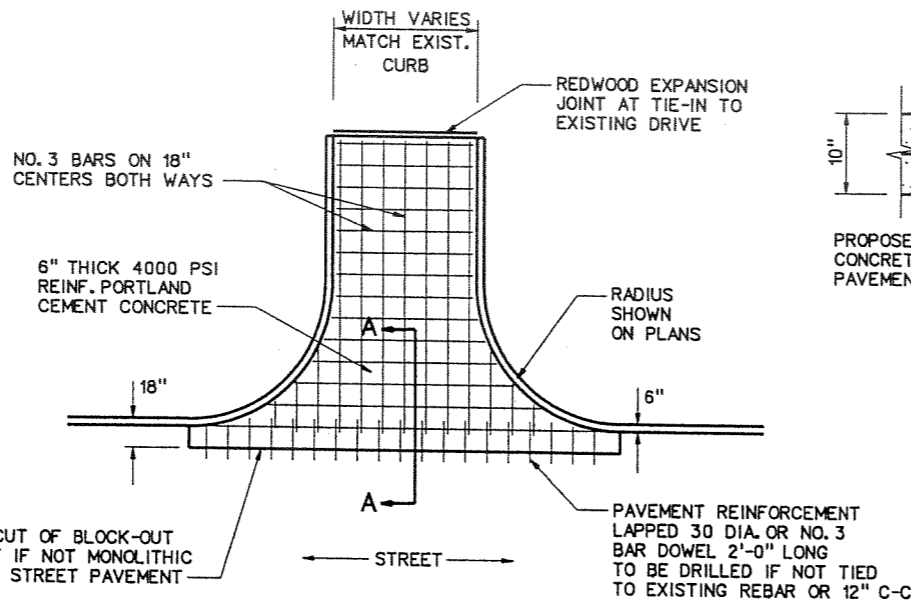
**SAWED DUMMY JOINT**  
NOT TO SCALE



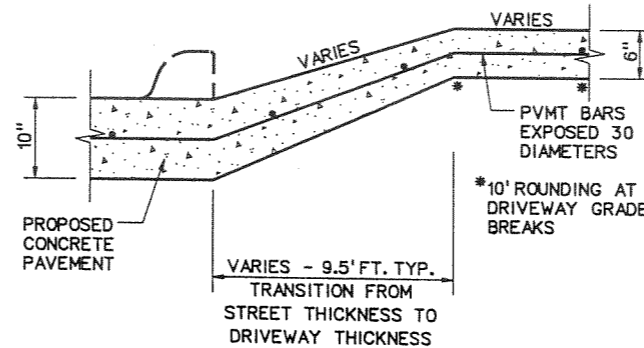
**TYPICAL JOINT DETAIL**  
NOT TO SCALE



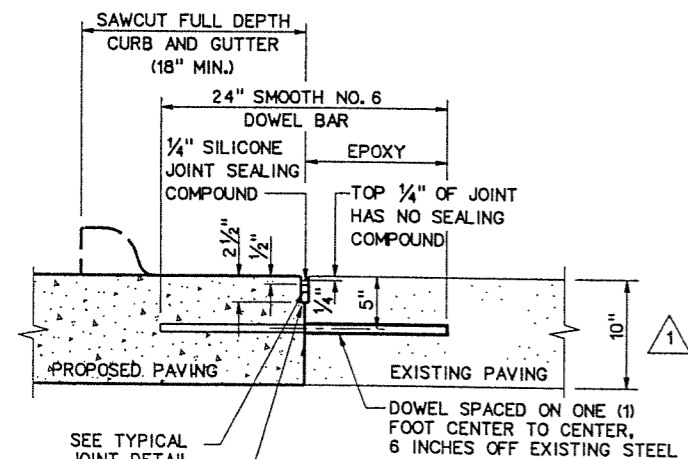
**INTEGRAL CURB**  
NOT TO SCALE



**DRIVE APPROACH**  
NOT TO SCALE

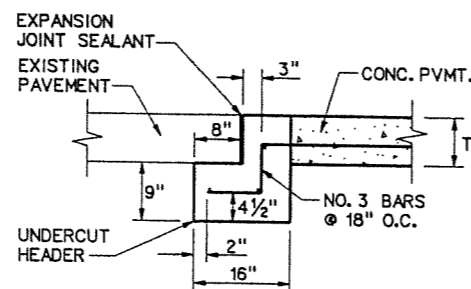


**SECTION A-A**  
NOT TO SCALE



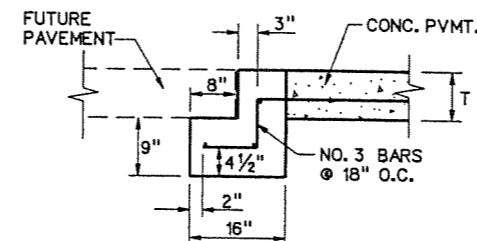
**LONGITUDINAL BUTT JOINT**  
NOT TO SCALE

NOTE:  
DOWEL BARS SHALL BE DRILLED INTO EXIST. PAVEMENT HORIZONTALLY BY USE OF A MECHANICAL RIG. DRILLING BY HAND IS NOT ACCEPTABLE. PUSHING DOWEL BARS INTO GREEN CONCRETE IS NOT ACCEPTABLE.



**STREET HEADER**  
NOT TO SCALE

PAVEMENT BARS TO BE BENT DOWN INTO HEADER. HEADER AND PAVEMENT TO BE MONOLITHIC.



NOTE:

1. STEEL DOWELS WILL BE USED FOR LOAD TRANSFER AT ALL TRAVERSE CONTRACTION JOINTS (SAWED DUMMY JOINTS). THE DOWELS WILL BE 24 INCHES LONG, PLACED 12 O-C AND LOCATED MID-HEIGHT IN THE SLAB. NO. 6 SMOOTH DOWELS WILL BE USED. NO SEPARATE PAY ITEM WILL BE ESTABLISHED FOR THIS ITEM.

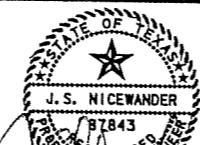
2. T=10"

NOTES:

1. UNLESS SPECIFICALLY STATED OTHERWISE IN PLANS OR CONTRACT DOCUMENTS, THE CONTROLLING SPECIFICATIONS FOR ALL WORK WITHIN PUBLIC RIGHT-OF-WAY AND EASEMENTS SHALL BE THE STANDARD SPECIFICATIONS FOR PUBLIC WORKS CONSTRUCTION, THIRD EDITION 1998, AS AMENDED BY THE NORTH CENTRAL TEXAS COUNCIL OF GOVERNMENTS (THE "STANDARD SPECIFICATIONS"). COPIES OF THE STANDARD SPECIFICATIONS MAY BE PURCHASED BY MAIL OR OVER THE COUNTER FROM THE NORTH CENTRAL TEXAS COUNCIL OF GOVERNMENTS, 616 SIX FLAGS DRIVE, P.O. BOX DRAWER COG, ARLINGTON, TEXAS 76005-5888. PHONE METRO 817/640-3300. BULK DISCOUNTS ARE AVAILABLE. THIS DOCUMENT IS COPYRIGHTED.
2. ROUGH GRADING SHALL BE ACCOMPLISHED TO WITHIN +/- 0.10 FEET OF PLAN ELEVATION.
3. ALL TRAFFIC CONTROL NECESSARY FOR THE WORK SHALL BE PROVIDED BY THE CONTRACTOR. ALL BARRICADES, WARNING SIGNS, LIGHTS, DEVICES, ETC. FOR THE GUIDANCE AND PROTECTION OF TRAFFIC AND PEDESTRIANS MUST CONFORM TO THE INSTALLATIONS SHOWN IN THE LATEST ISSUE OF THE "TEXAS MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES", AS CURRENTLY AMENDED, TEXAS DEPARTMENT OF TRANSPORTATION.
4. ALL FILL SHALL BE PLACED IN MAXIMUM 8-INCH LIFTS COMPACTED TO 95% OF STANDARD PROCTOR DENSITY BETWEEN 0% AND +3% OF OPTIMUM MOISTURE CONTENT.
5. ALL TREES, STUMPS, BRUSH, GRASSES AND SURFACE ORGANICS WITHIN PROPOSED RIGHT-OF-WAY ARE TO BE REMOVED AND PROPERLY DISPOSED OF OFF-SITE. TREE REMOVAL PERMITS, IF REQUIRED, WILL BE OBTAINED BY THE OWNER.
6. EXISTING UTILITY POLES, IF ANY, WILL BE REMOVED OR RELOCATED BY THE UTILITY COMPANIES THROUGH COORDINATION BY THE OWNER. CONTRACTOR SHALL BRING TO THE OWNER'S ATTENTION ANY FACILITIES THAT APPEAR TO BE IN CONFLICT SO THAT THE OWNER HAS SUFFICIENT TIME TO ACCOMPLISH THE NECESSARY RELOCATIONS.
7. WHERE DEEP VERTICAL EXCAVATIONS (IN EXCESS OF 3 FEET) ARE INDICATED, CUTS SHALL BE LAID BACK AT A STABLE SLOPE (ON OWNER'S PROPERTY) UNTIL WALLS ARE CONSTRUCTED. BACKFILL MATERIAL SHALL BE STOCKPILED ON-SITE AT THE DIRECTION OF THE OWNER.
8. ALL REINFORCING STEEL AND DOWEL BARS IN PAVEMENT SHALL BE SUPPORTED AND MAINTAINED AT THE CORRECT CLEARANCES BY THE USE OF BAR CHAIRS OR OTHER APPROVED SUPPORT.
9. THE USE OF WOOD FORMS FOR PAVEMENT CONSTRUCTION WILL BE PERMITTED.
10. DUMMY JOINTS SHALL BE SAWED IN THE PAVEMENT IN THIS PROJECT. SPACING VARIATIONS SHALL BE MADE AT BLOCKOUTS, CONSTRUCTION JOINTS, AND STREET INTERSECTIONS TO LINE UP WITH EXISTING PAVING JOINTS AS SHOWN ON THE PLANS. THE SPACING BETWEEN ANY JOINT SHALL NOT BE LESS THAN 8 FEET NOR MORE THAN 12 FEET. ALL DUMMY JOINTS SHALL BE SAWED NO SOONER THAN 4 HOURS AND NO LATER THAN 12 HOURS AFTER THE PLACEMENT OF THE PAVEMENT.
11. TWO-WAY TRAFFIC MUST BE MAINTAINED AT ALL TIMES ON PUBLIC STREETS ADJACENT TO THIS PROJECT UNLESS SPECIFIED OTHERWISE IN THE PLANS AND SPECIFICATIONS, OR DIRECTED BY THE ENGINEER. THE TRAVELWAY WIDTH SHALL NOT BE LESS THAN 10 FEET.
12. SEE TYPICAL SECTIONS SHEET FOR ADDITIONAL DETAILS AND NOTES.
13. ALL CURBS SHALL BE PLACED INTEGRAL WITH PAVEMENT.
14. CURBS SHALL MEET THE SAME COMPRESSIVE STRENGTH AS SPECIFIED FOR THE CONCRETE PAVEMENT.
15. BAR LAPS SHALL BE 30 DIAMETERS.

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NO.	DATE	REVISION	APPROV.	NO.	DATE	REVISION	APPROV.
1	10/10/03	MODIFIED DETAILS	JSN				



10/10/03

**HNTB** ARCHITECTS ENGINEERS PLANNERS  
The HNTB Companies

DESIGN CHECK JSN  
DRAWN CHECK GFS  
DATE SEPTEMBER 2003

PROJECT NO. 39020

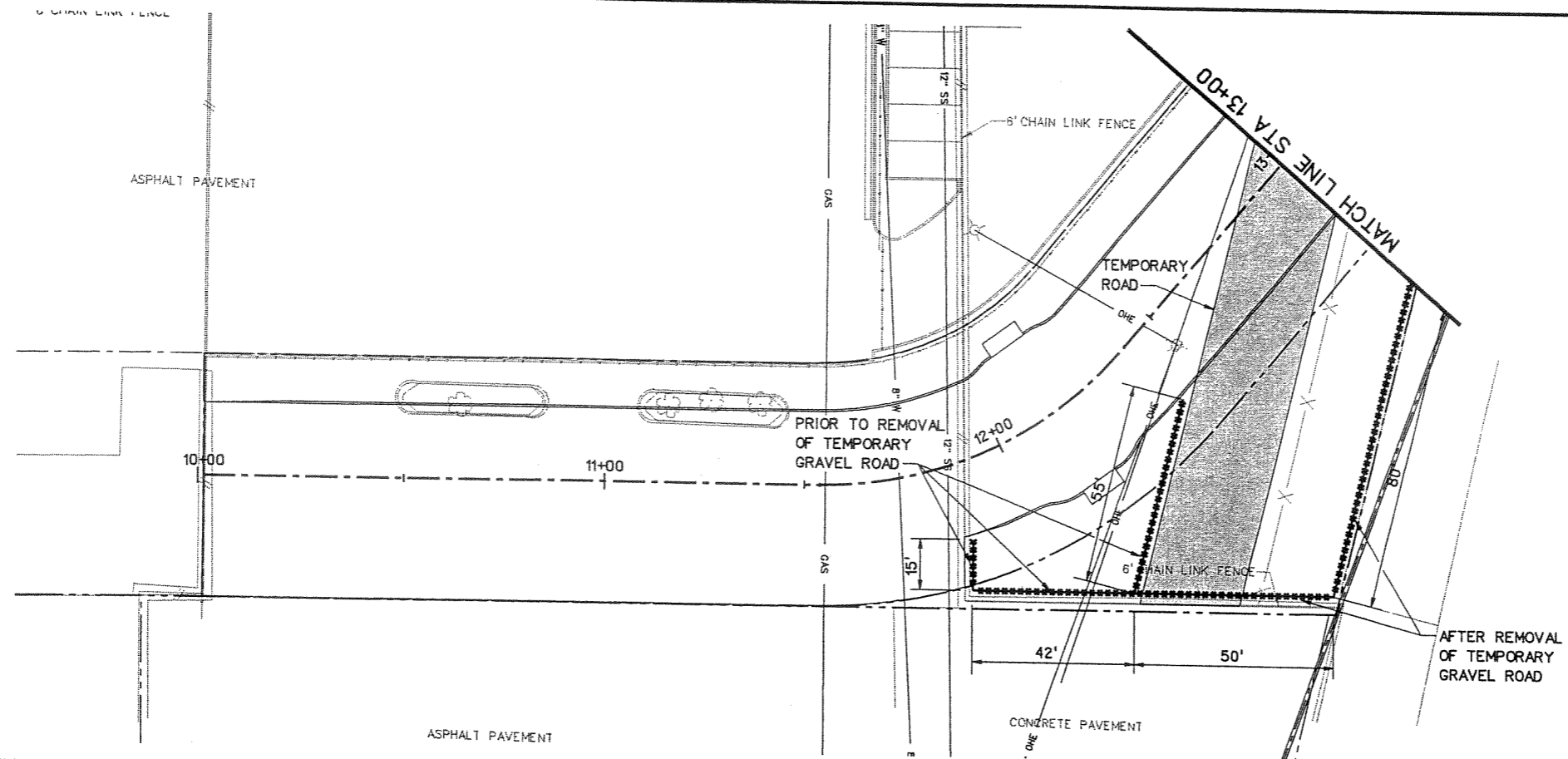
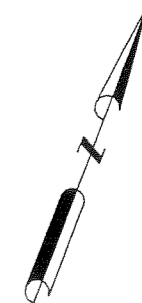
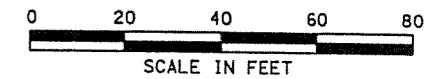


TOWN OF ADDISON  
AIRPORT PARKWAY REALIGNMENT

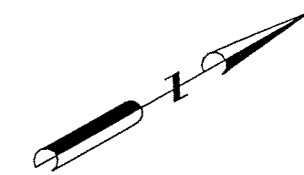
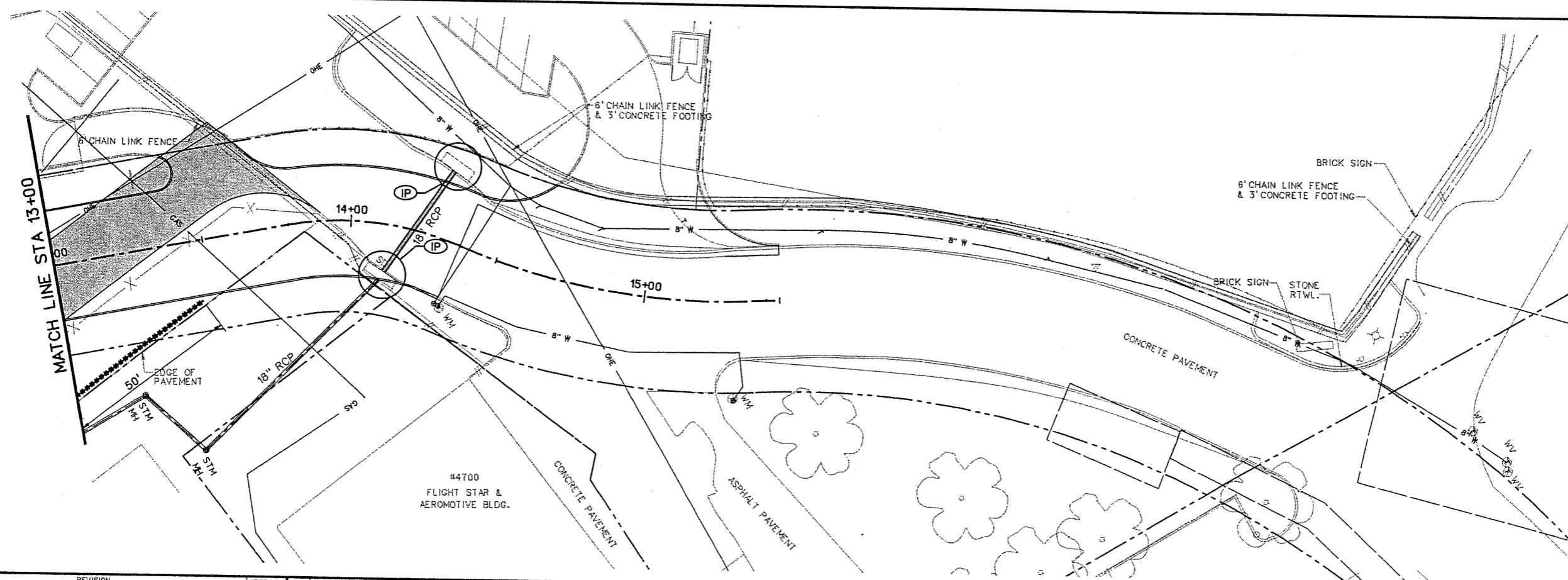
PAVING DETAILS

SHEET

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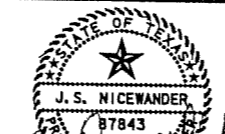


- LEGEND**
- \*\*\*\*\* SILT FENCE
  - (IP) INLET PROTECTION
  - ▲ TEMPORARY ROAD



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NO.	DATE	REVISION	APPROV.	NO.	DATE	REVISION	APPROV.
1	10/10/03	LEGEND MODIFIED	JSN				



10/10/03

**HNTB** ARCHITECTS ENGINEERS PLANNERS  
The HNTB Companies

DESIGN CHECK	JSN	PROJECT NO.	39020
DRAWN	JDH		
CHECK	GFS		
DATE	SEPTEMBER 2003		

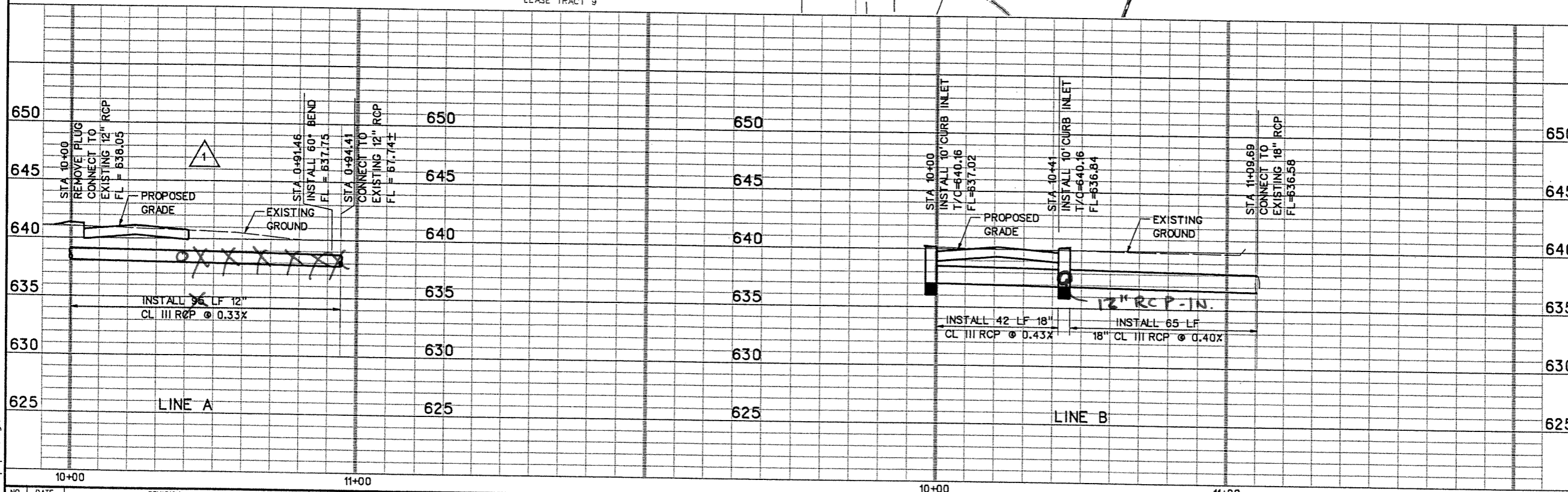
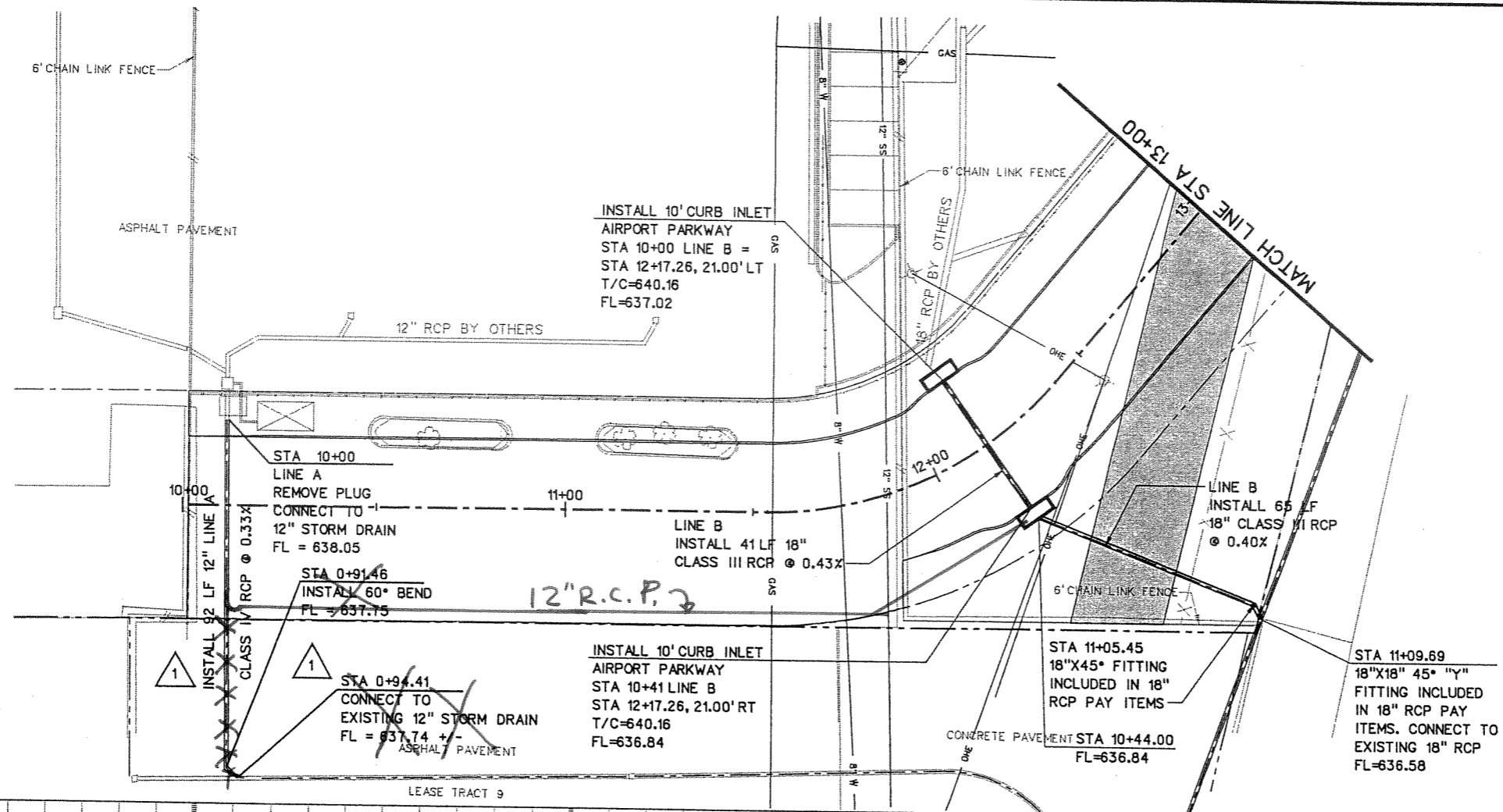
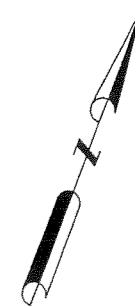
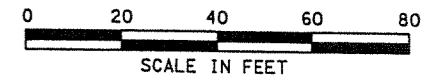


**TOWN OF ADDISON**  
AIRPORT PARKWAY REALIGNMENT

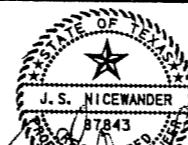
**EROSION CONTROL PLAN**  
STA 10+00 TO STA 14+37

SHEET

'As-Built'



NO.	DATE	REVISION	APPROV.	NO.	DATE	REVISION	APPROV.
1	10/10/03	ADDED STORM DRAIN LINE A, PLAN AND PROFILE	JSN				



**HNTB** ARCHITECTS ENGINEERS PLANNERS  
The HNTB Companies

DESIGN CHECK JSN  
DRAWN GFS  
CHECK JSN  
DATE SEPTEMBER 2003

PROJECT NO. 39020



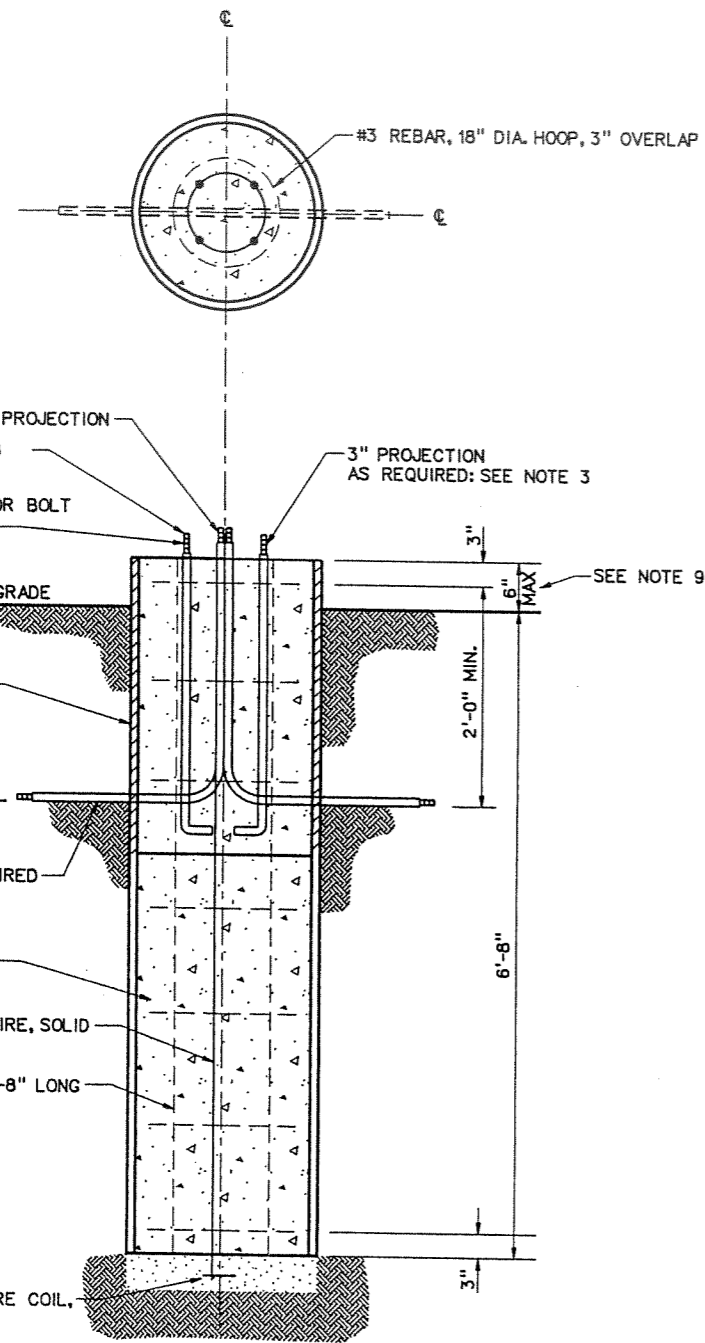
TOWN OF ADDISON  
AIRPORT PARKWAY REALIGNMENT

STORM SEWER PLAN/PROFILE  
STA 10+00 TO STA 13+00

SHEET

"As-Built"

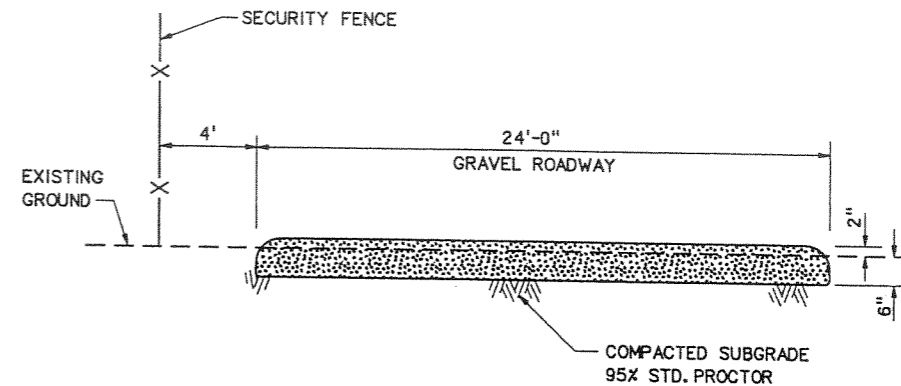
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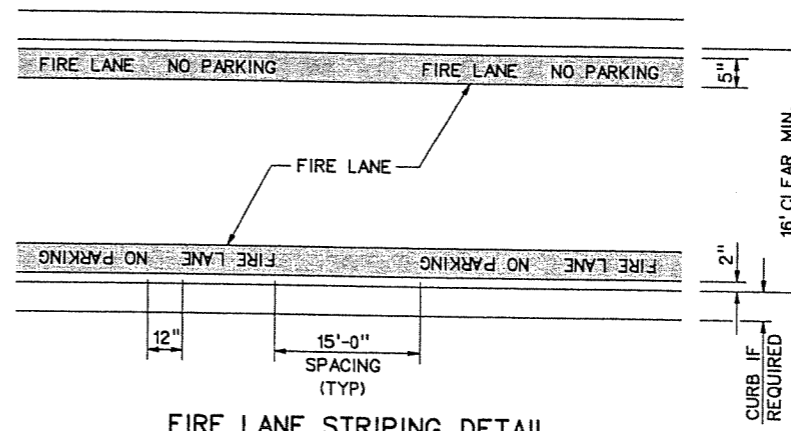
**STREET LIGHT FOUNDATION DETAIL**  
NOT TO SCALE

**STREET LIGHT FOUNDATION NOTES:**

1. CONCRETE TO BE A MINIMUM 3000 PSI AT 28 DAYS. MAXIMUM AGGREGATE 3/4". TOP OF FOUNDATION TO BE TROWELED TO A FLAT AND LEVEL SURFACE. AVOID EXCESSIVE TROWELING. CONCRETE TO SET A MINIMUM OF 72 HOURS BEFORE POLE INSTALLATION.
2. REBAR HOOPS ARE TIED BEGINNING 3" BELOW TOP OF CONCRETE FORM AND REPEATED AT APPROXIMATE 1FT. INTERVALS TO BOTTOM OF FOUNDATION.
3. ANCHOR BOLTS TO BE SUPPLIED WITH POLE. USE TEMPLATE FURNISHED BY POLE MANUFACTURER FOR ALIGNING ANCHOR BOLTS. PROJECTION OF 3" ON 25' & 30' SQUARE AND 3 1/2" ON 40' SQUARE AND 37' ROUND POLES.
4. CONCRETE FORM OF SONOTUBE TO EXTEND TO BOTTOM OF TRENCH OR AS NEEDED.
5. PROVIDE 2'-0" PIGTALE FOR CONNECTION OF GROUND WIRE TO POLE
6. A MINIMUM OF 12" OF BARE #6 SD CU WIRE TO BE PLACED IN BOTTOM OF HOLE AND COVERED WITH 2" OF DIRT.
7. IF SOIL HAS BEEN DISTURBED, EXTEND FOUNDATION BY DEPTH OF DISTURBED SOIL.
8. CONTRACTOR SHALL CONTACT DAVE FOSTER WITH ADDISON AIRPORT (972-392-4852) REGARDING ANCHOR BOLT DIAMETER AND LAYOUT FOR REUSED STREET LIGHT ON FOUNDATION LOCATED AT STA 10+10, 28' RT.
9. FOUNDATION AT STA 10+10, 28' RT SHALL EXTEND FOUR FEET ABOVE FINISHED GRADE THE DISTANCE INTO THE GROUND SHALL REMAIN THE SAME



**TEMPORARY ROADWAY TYPICAL SECTION**  
NOT TO SCALE



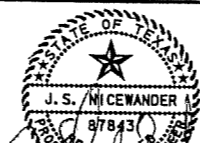
**FIRE LANE STRIPING DETAIL**  
NOT TO SCALE

**STRIPING DETAIL NOTES:**

1. A. PAINT:
  1. STRIPE SHALL BE FIVE (5) INCHES WIDE PAINTED WITH AN EXTERIOR LATEX PAINT. COLOR SHALL BE "TRAFFIC RED" GLIDDEN NO. 63251 OR EQUAL.
  2. LETTERS SHALL BE FOUR (4) INCHES HIGH PAINTED WITH AN EXTERIOR ACRYLIC LATEX PAINT. COLOR SHALL BE "TRAFFIC WHITE" GLIDDEN NO. 563245 OR EQUAL.
- B. APPLICATION:
  1. STRIPE MAY BE BRUSHED OR SPRAYED, ONE COAT TO FINISH.
  2. LETTERS SHALL BE STENCIL FORMED, BRUSH APPLIED AND SPACED AS DETAILED ON THIS SHEET.
2. "FIRE LANE NO PARKING" TO BE PAINTED ENTIRE LENGTH OF DEDICATED FIRE LANE.
3. WHERE PRACTICAL ALL FIRE LANE MARKINGS SHALL BE REQUIRED TO BE PLACED ON THE CURB.
4. FOR ADDITIONAL FIRE LANE SPECIFICATIONS, PLEASE CONTACT THE FIRE DEPARTMENT.

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NO.	DATE	REVISION	APPROV.	NO.	DATE	REVISION	APPROV.
1	10/10/03	DETAILS ADDED	JSN				



**HNTB** ARCHITECTS ENGINEERS PLANNERS  
The HNTB Companies

DESIGN CHECK: JSN, JDH  
DRAWN CHECK: GFS, JSN  
DATE: 10/10/03

PROJECT NO. 39020



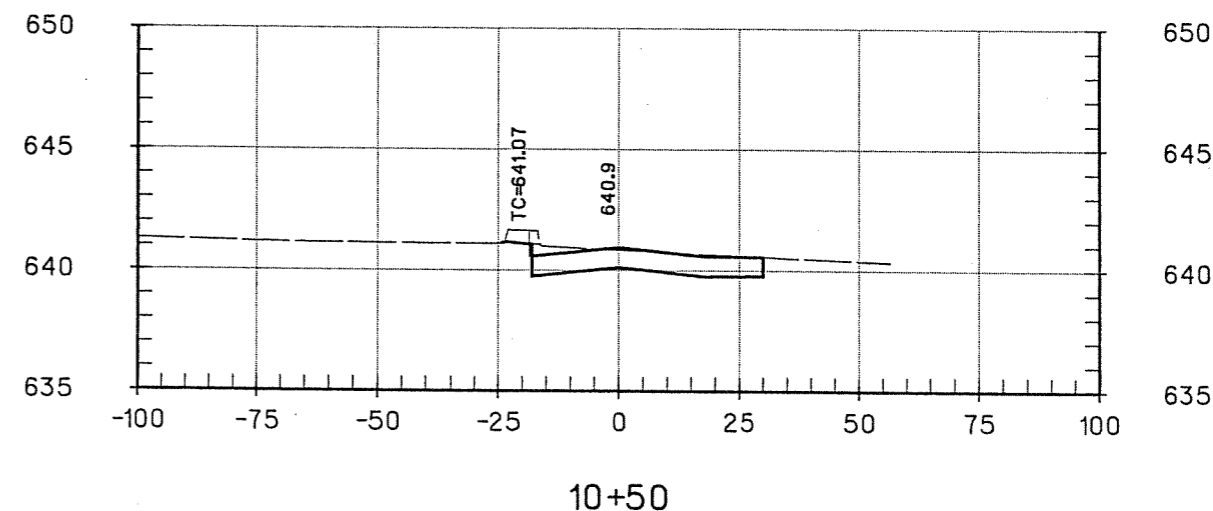
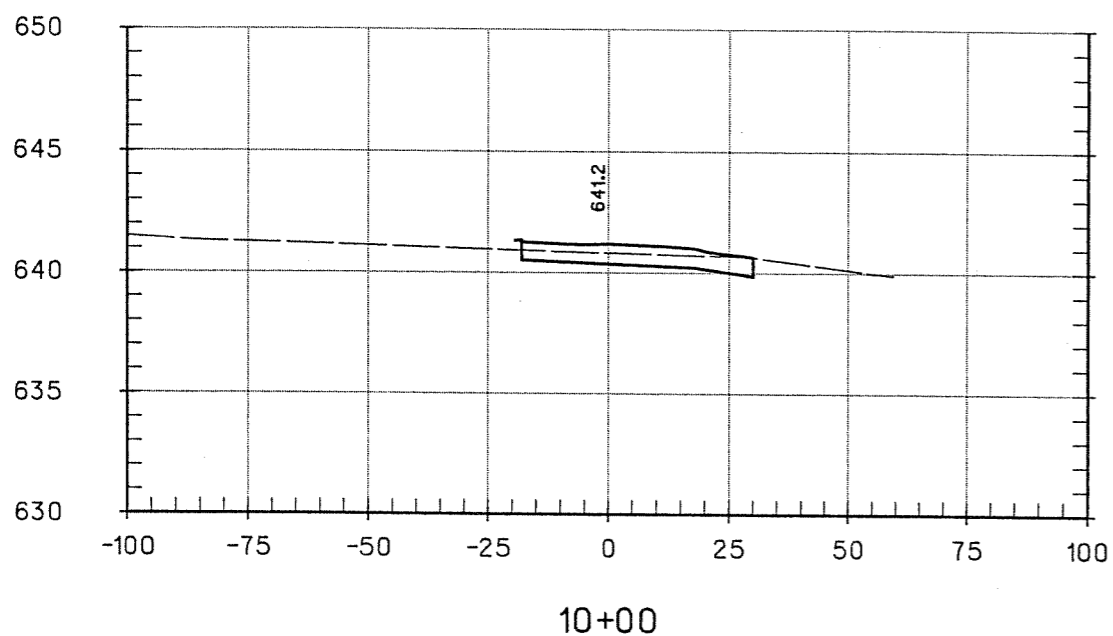
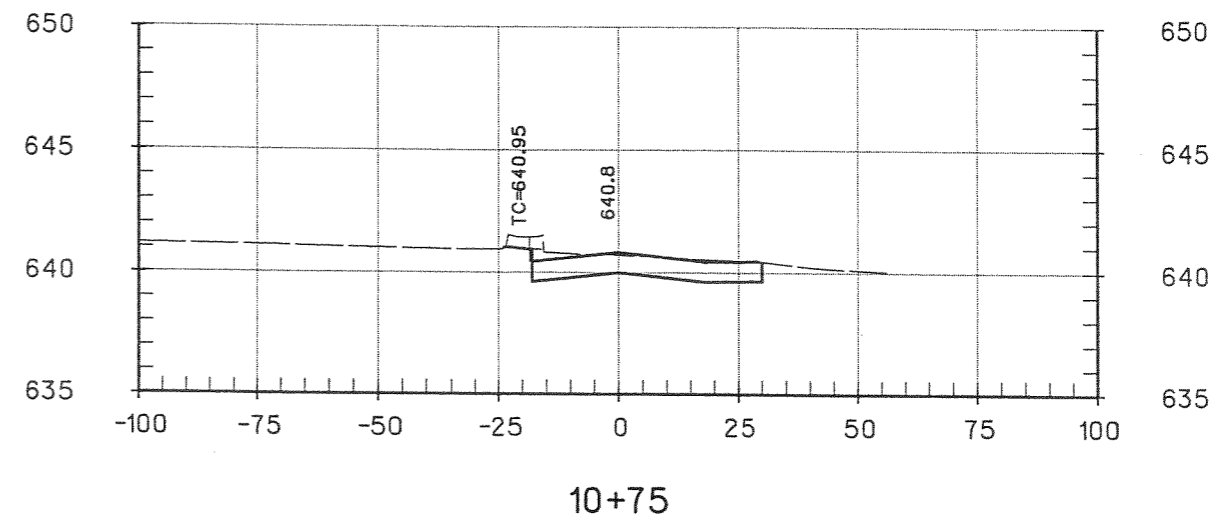
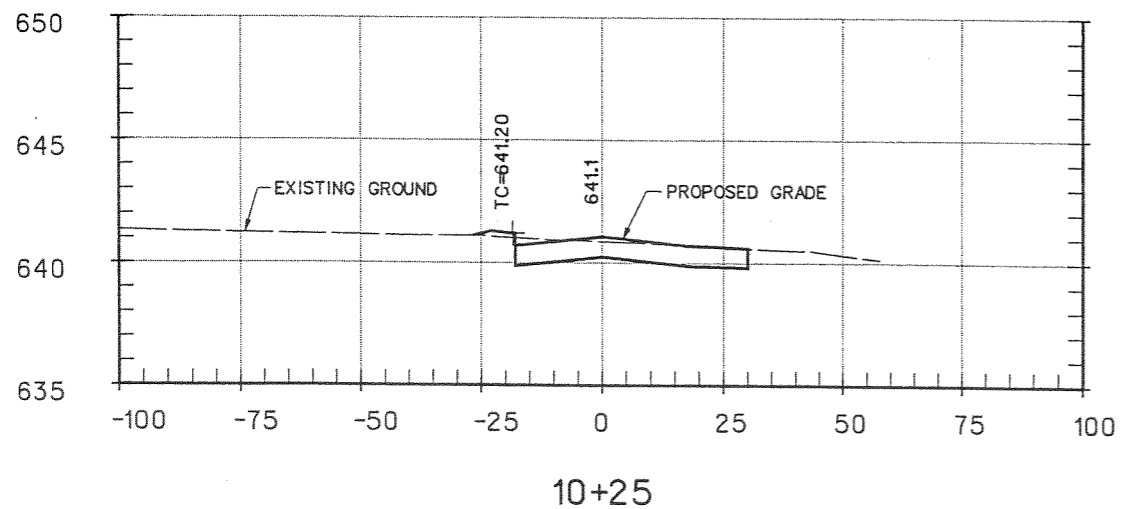
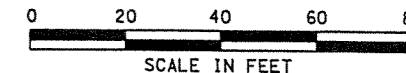
TOWN OF ADDISON  
AIRPORT PARKWAY REALIGNMENT

MISCELLANEOUS DETAILS

SHEET

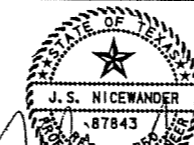
*As-Built*





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NO.	DATE	REVISION	APPROV.	NO.	DATE	REVISION	APPROV.
1	10/10/03	SHEET ISSUED	JSN				



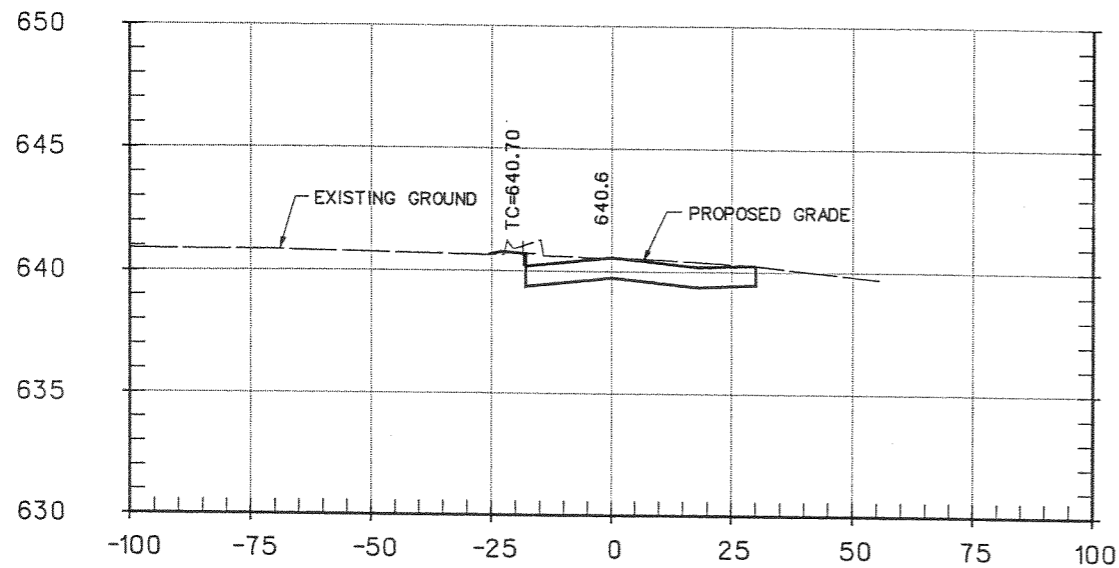
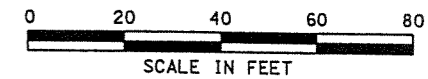
10/10/03

<b>HNTB</b> ARCHITECTS ENGINEERS PLANNERS <small>The HNTB Companies</small>		PROJECT NO. 39020
DESIGN CHECK JSN		
DRAWN CHECK JQH		
DRAWN CHECK GFS		
DATE SEPTEMBER 2003		

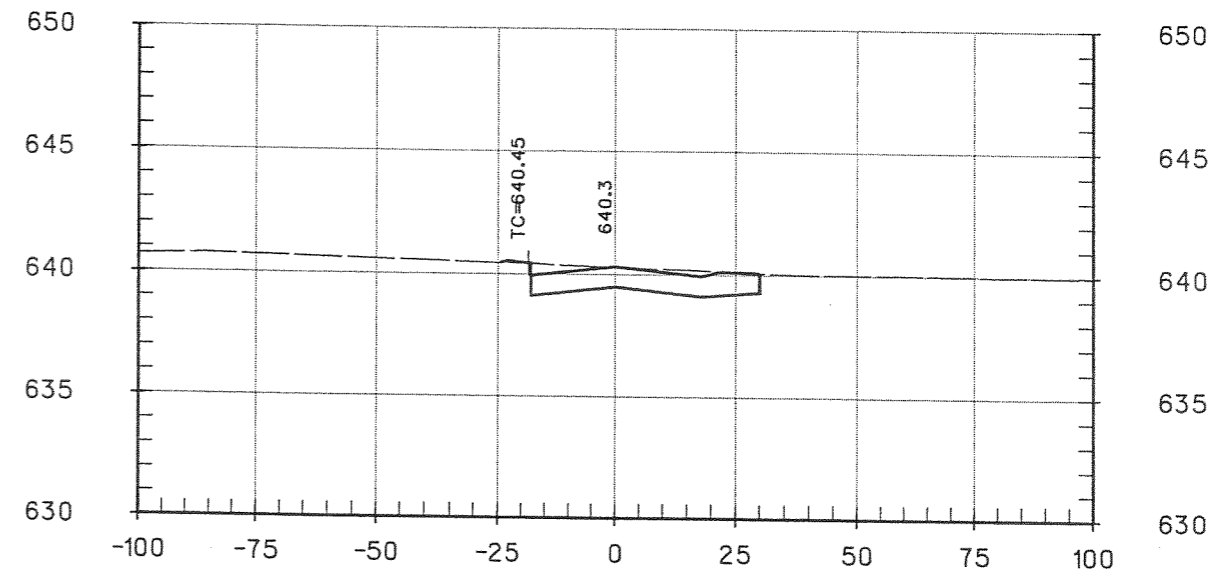


TOWN OF ADDISON	SHEET
AIRPORT PARKWAY REALIGNMENT	
CROSS SECTIONS	

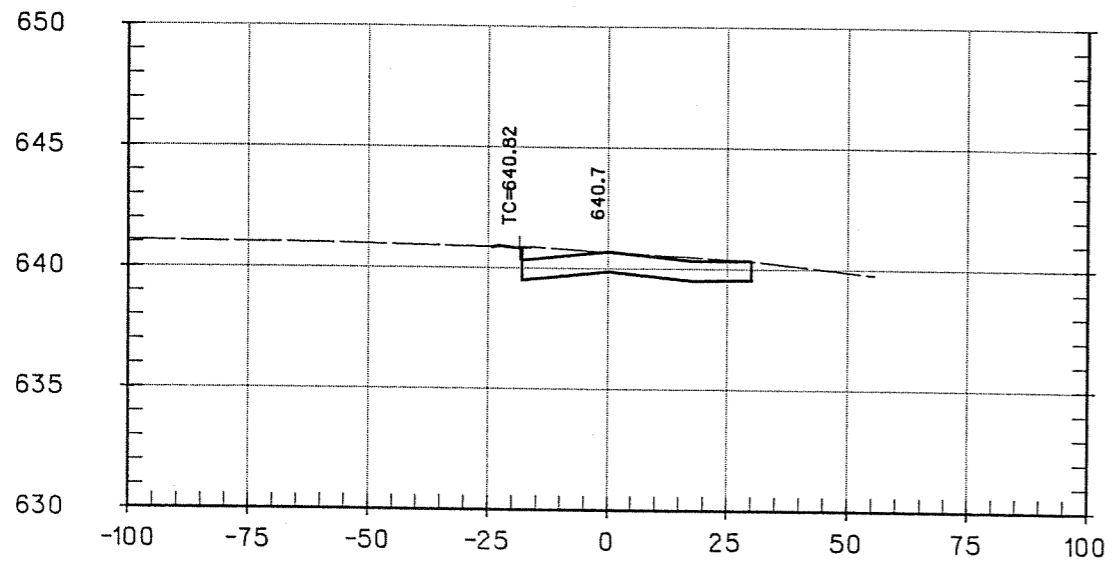
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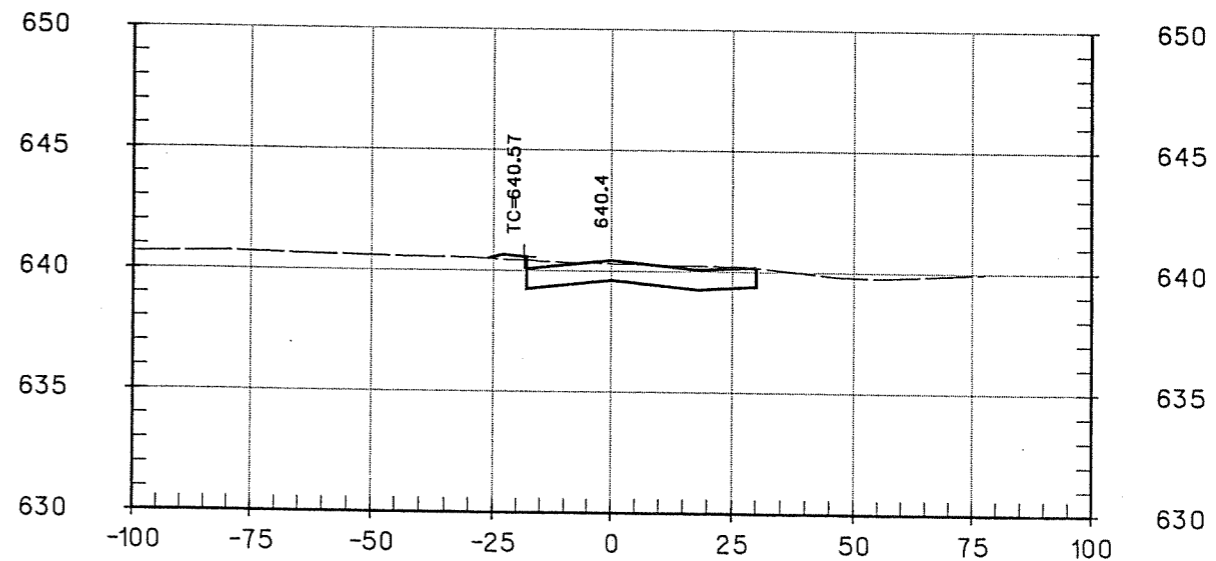
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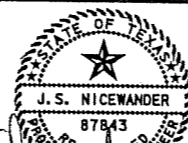
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NO.	DATE	REVISION	APPROV.	NO.	DATE	REVISION	APPROV.
1	10/10/03	SHEET ISSUED	JSN				



**HNTB** ARCHITECTS ENGINEERS PLANNERS  
The HNTB Companies

DESIGN CHECK: JSN, JDH  
DRAWN CHECK: GFS, JSN  
DATE: SEPTEMBER 2003

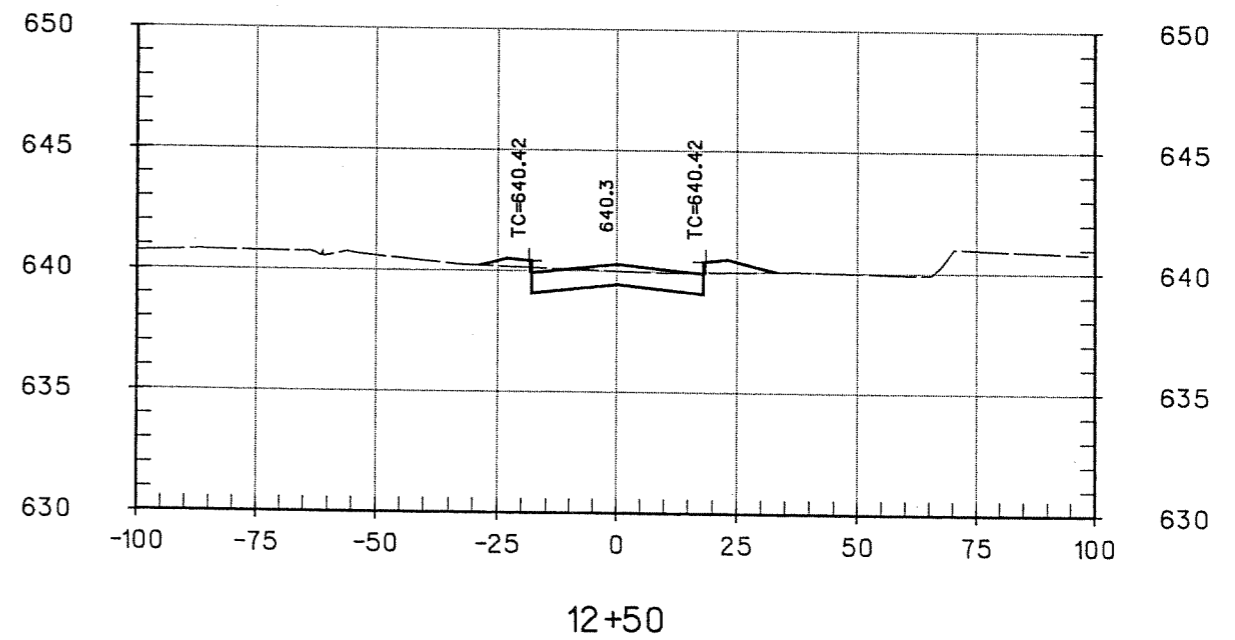
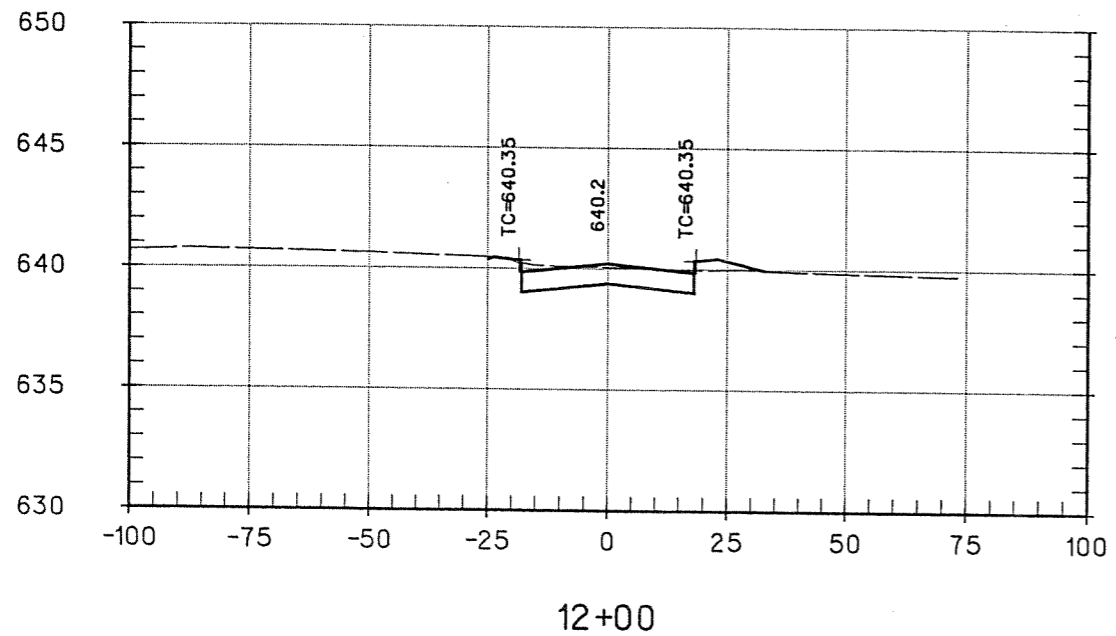
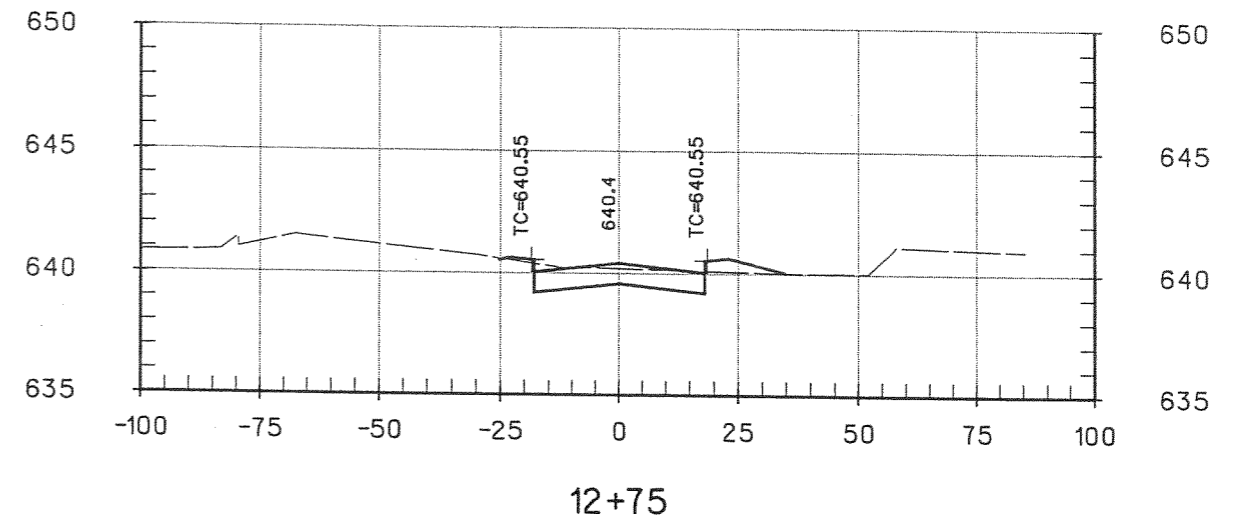
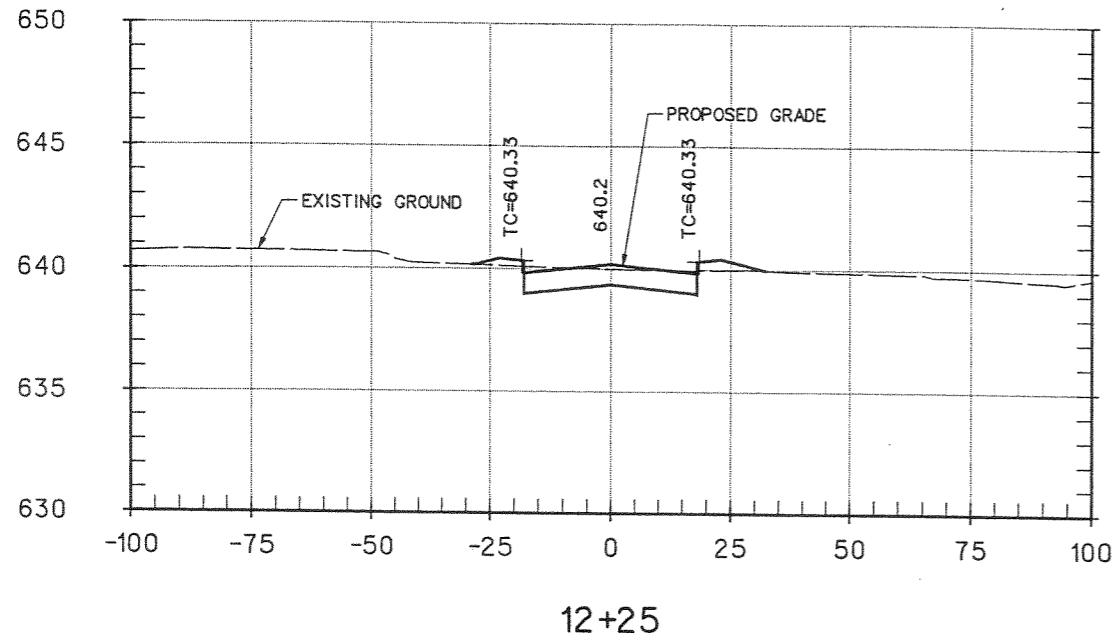
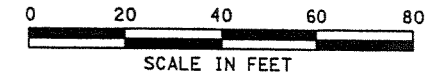
PROJECT NO. 39020



TOWN OF ADDISON  
AIRPORT PARKWAY REALIGNMENT

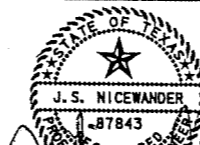
CROSS SECTIONS

"As-Built" SHEET



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NO.	DATE	REVISION	APPROV.	NO.	DATE	REVISION	APPROV.
1	10/10/03	SHEET ISSUED	JSN				



10/10/03

**HNTB** ARCHITECTS ENGINEERS PLANNERS  
The HNTB Companies

DESIGN CHECK JSN  
DRAWN GFS  
CHECK JSN  
DATE SEPTEMBER 2003

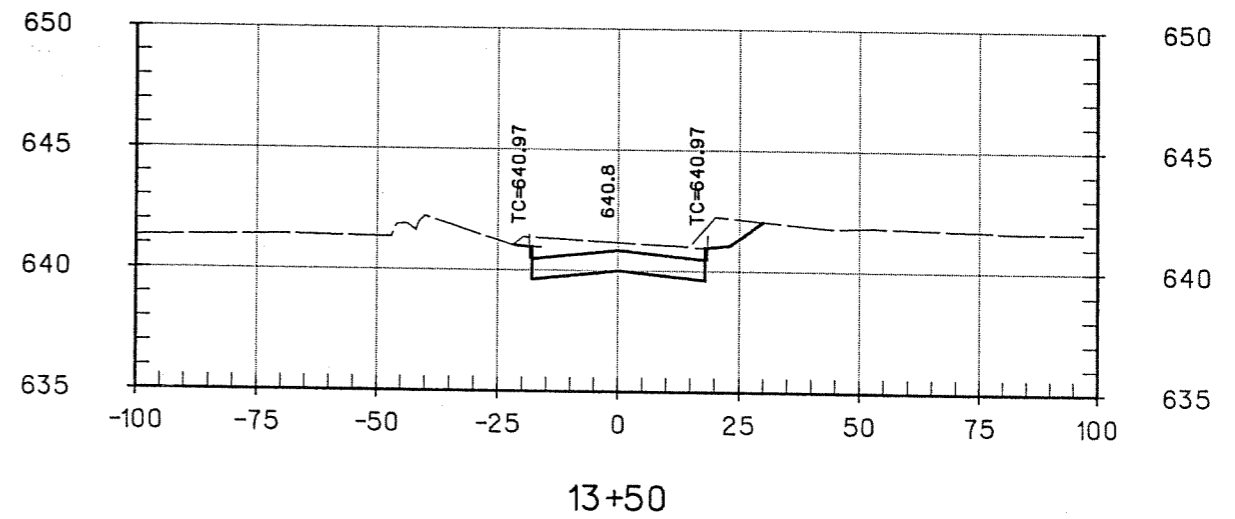
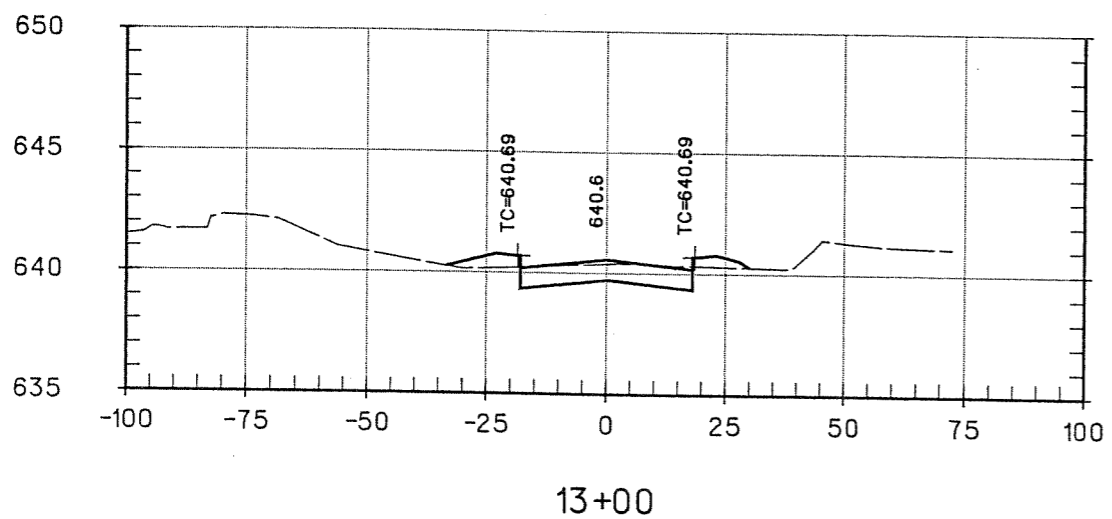
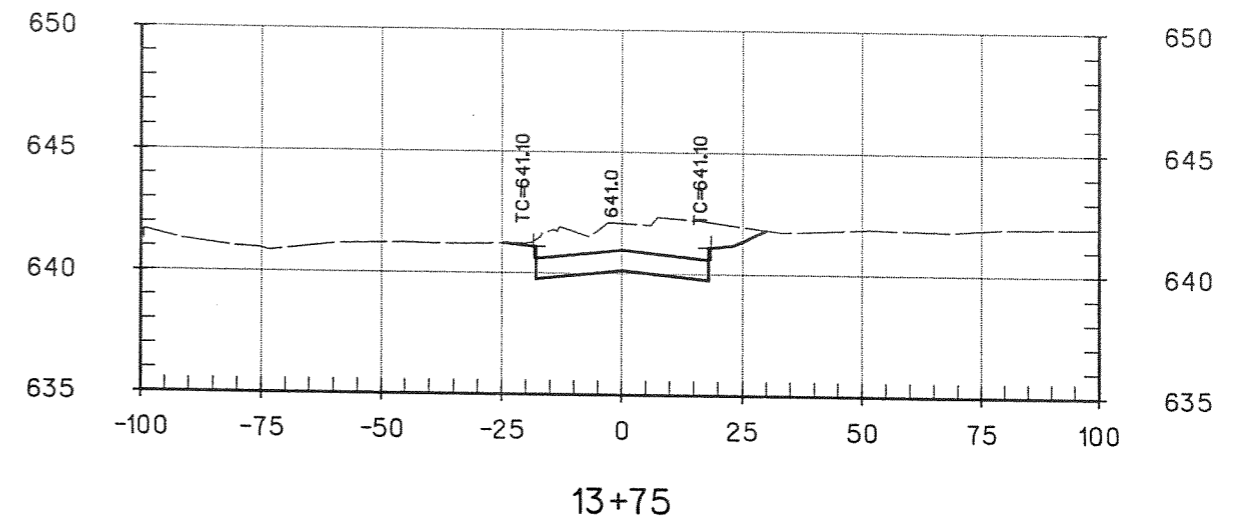
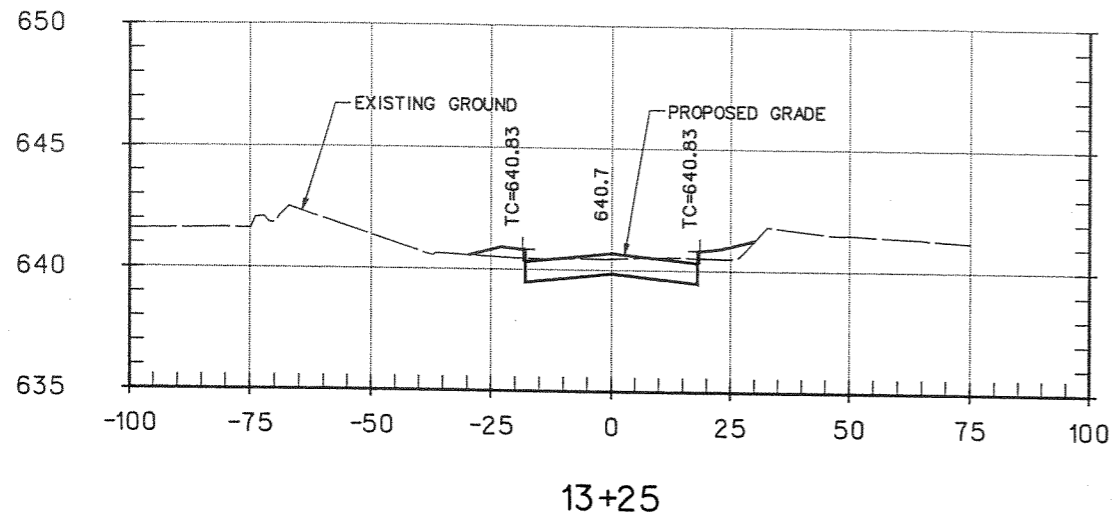
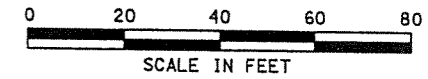
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TOWN OF ADDISON  
AIRPORT PARKWAY REALIGNMENT

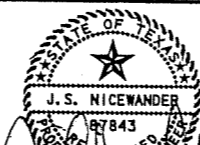
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"As-Built" SHEET



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10/16/03

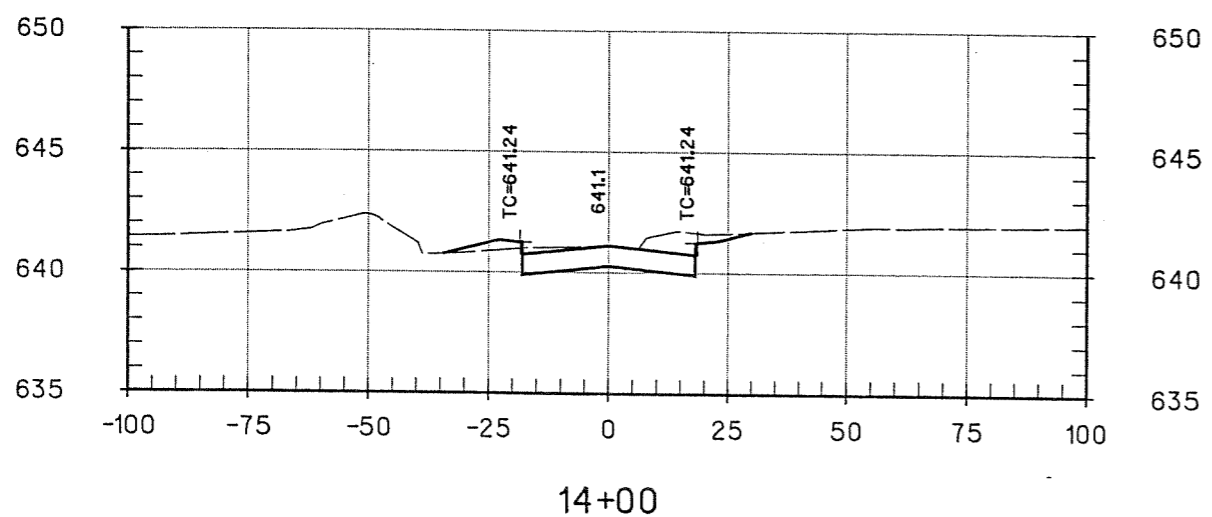
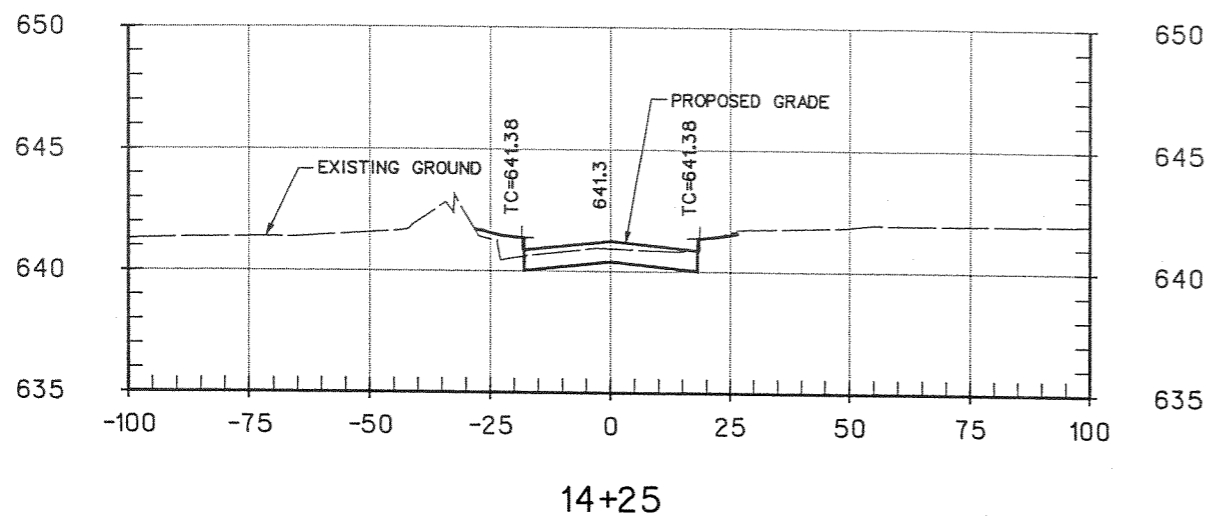
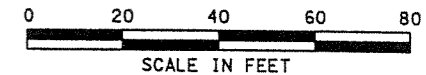
<b>HNTB</b> ARCHITECTS ENGINEERS PLANNERS <small>The HNTB Companies</small>	
DESIGN CHECK	JSN
DRAWN	GFS
CHECK	JSN
DATE	SEPTEMBER 2003
PROJECT NO.	39020



TOWN OF ADDISON
AIRPORT PARKWAY REALIGNMENT
CROSS SECTIONS

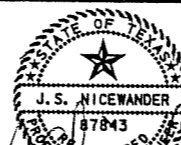
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SHEET



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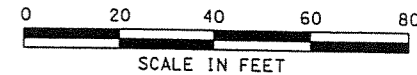
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<b>HNTB</b>		ARCHITECTS ENGINEERS PLANNERS The HNTB Companies
DESIGN CHECK	JSN	PROJECT NO. 39020
DRAWN	JDH	
CHECK	GFS	
DATE	JSN	
	SEPTEMBER 2003	



TOWN OF ADDISON	SHEET
AIRPORT PARKWAY REALIGNMENT	
CROSS SECTIONS	

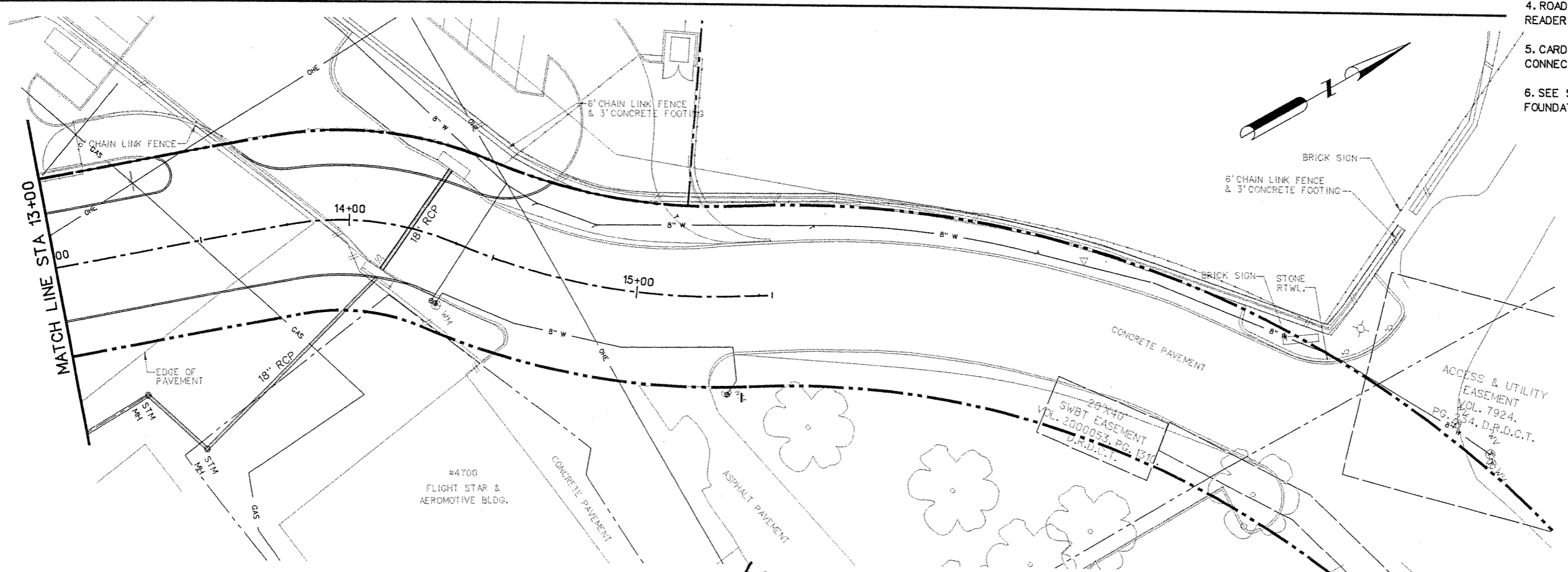
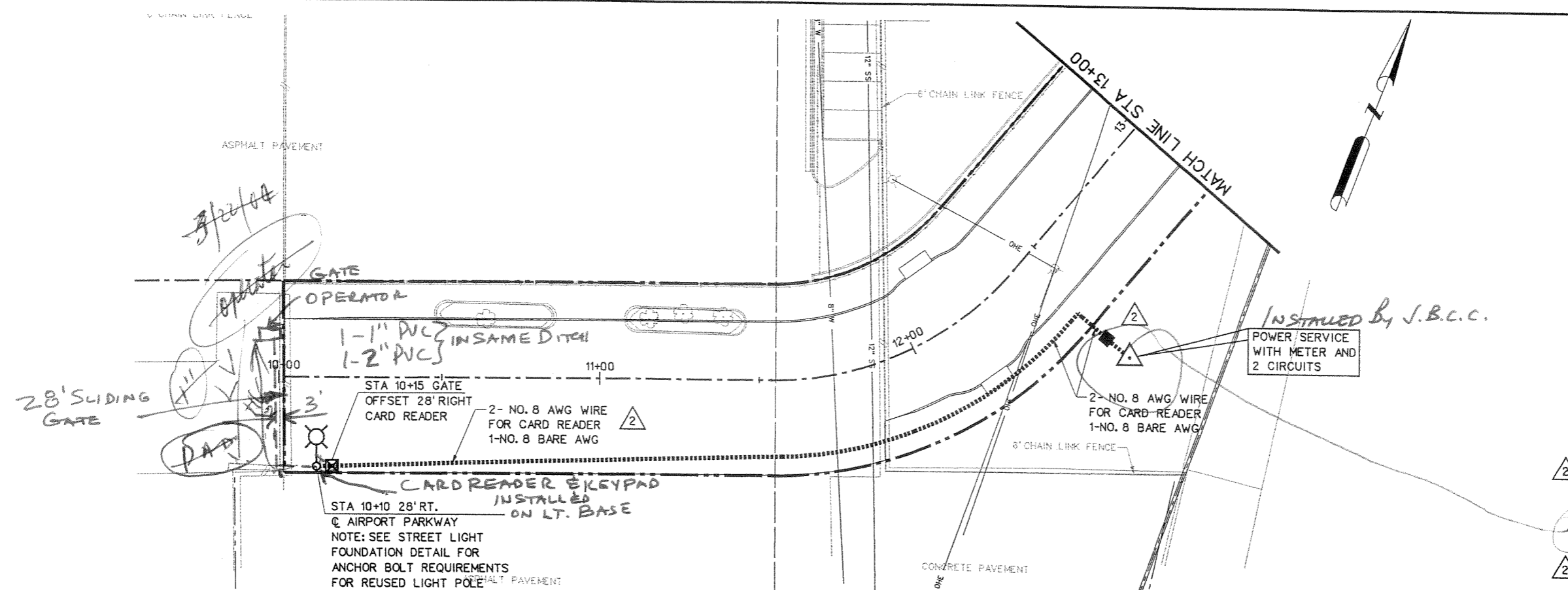
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**LEGEND**

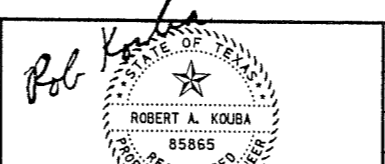
- REUSED STREET LIGHT SUPPLIED BY ADDISON AIRPORT
- CONDUIT, 2" SCH 40 PVC
- GATE CARD READER
- PULL BOX
- POWER SERVICE

- NOTE:
1. CONTRACTOR TO INSTALL POLE FOUNDATION AND CONDUIT. TXU TO INSTALL CIRCUIT CONDUCTORS.
  2. CONTRACTOR TO CONNECT TO EXISTING TXU STUBOUT.
  3. STREET LIGHTING FOUNDATION SHALL BE 6- FEET BEHIND CURB.
  4. ROADWAY LIGHTING CIRCUIT AND CARD READER CIRCUIT WILL SHARE 2" CONDUIT.
  5. CARD READER CIRCUIT SHALL NOT BE CONNECTED TO ROADWAY LIGHTING PHOTOCCELL.
  6. SEE SHEET MD-1 FOR STREET LIGHT FOUNDATION DETAIL.



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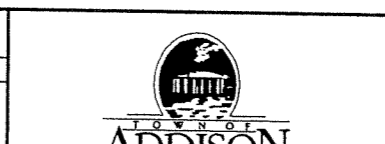
NO.	DATE	REVISION	APPROV.	NO.	DATE	REVISION	APPROV.
1	10/10/03	SHEET ISSUED	JSN				
2	11/12/03	NOTES MODIFIED, STREET LIGHTING REMOVED	RAK				
		CARD READER RELOCATED					
		PULL BOX ELIMINATED					



**HNTB** ARCHITECTS ENGINEERS PLANNERS  
The HNTB Companies

DESIGN CHECK JSN  
DRAWN GFS  
DATE SEPTEMBER 2003

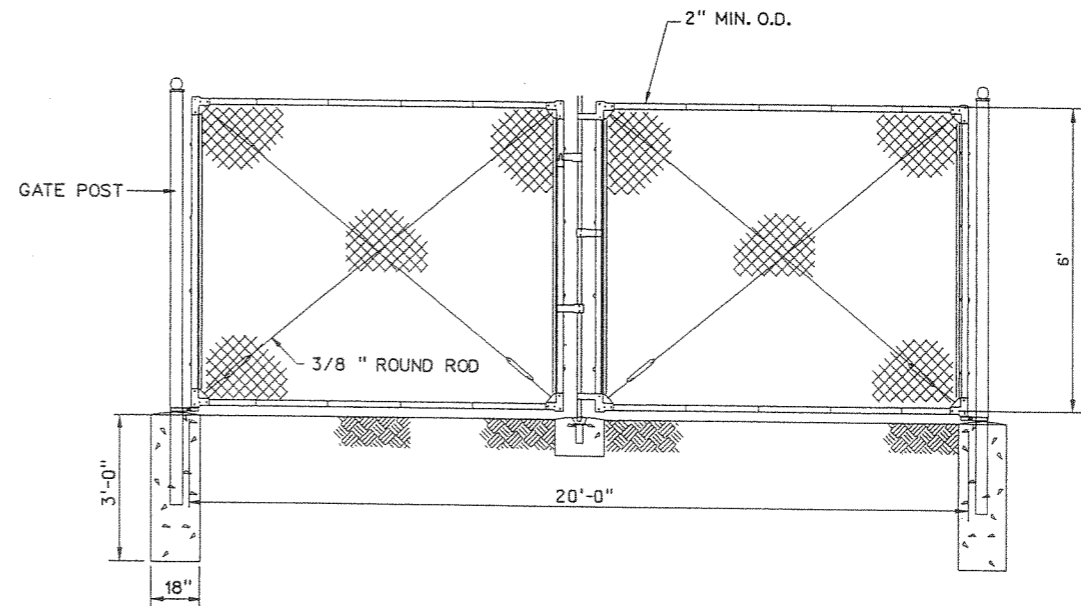
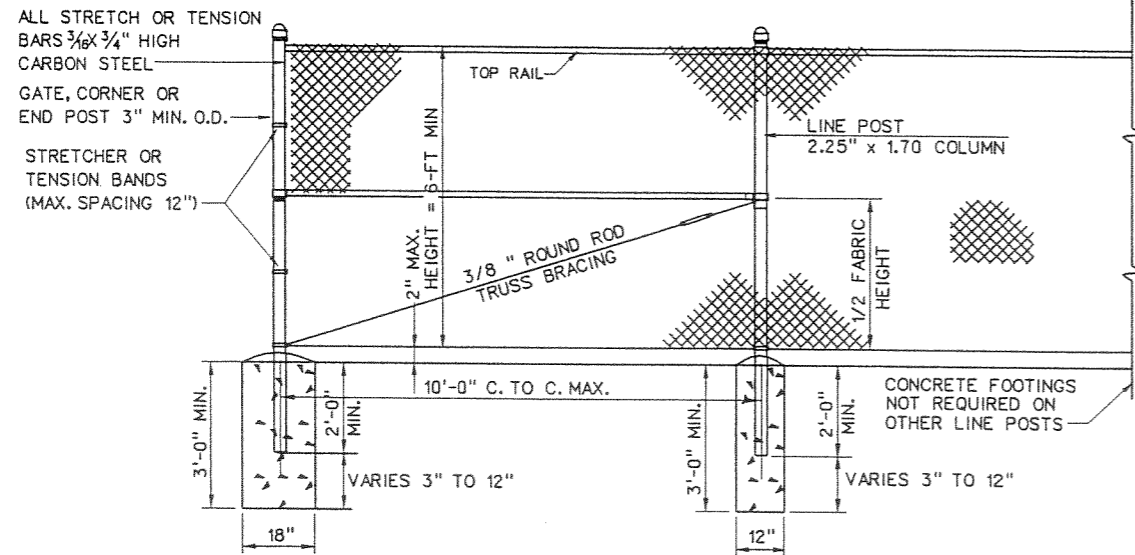
PROJECT NO. 39020



TOWN OF ADDISON  
AIRPORT PARKWAY REALIGNMENT

ILLUMINATION PLAN  
STA 10+00 TO STA 15+45

AS-BUILT SHEET

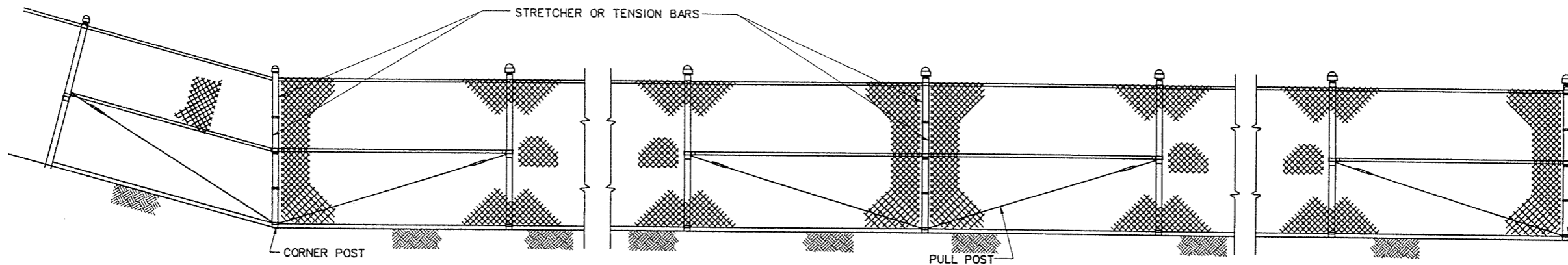


CHAIN-LINK WIRE FENCE WITH EXTENSION ARMS

DOUBLE SWING GATE

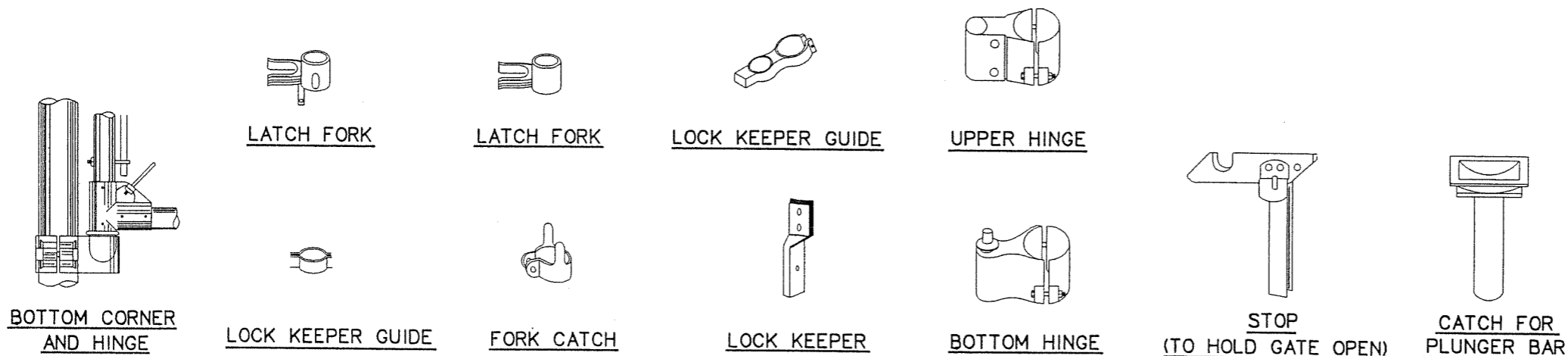
~~AND BARB WIRE~~  
NOT TO SCALE

NOT TO SCALE



METHOD OF BRACING END, CORNER, AND PULL POST

NOT TO SCALE



NOTES:

- HORIZONTAL BRACE RAILS, TRUSS BRACING AND STRETCHER OR TENSION BARS ATTACHED TO POST BY MEANS OF BANDS, SHALL BE USED FOR END, CORNER, ANGLE, AND INTERMEDIATE BRACE POSTS AND WHERE REQUIRED BY THE ENGINEER.
- FITTINGS SHOWN ARE SUGGESTED ONLY. SIMILAR DESIGNS MEETING THE APPROVAL OF THE ENGINEER MAY BE USED. ALL FITTINGS TO BE MALLEABLE IRON, CAST IRON, OR PRESSED STEEL.
- MATERIALS AND CONSTRUCTION SHALL BE IN ACCORDANCE WITH F.A.A. STANDARD SPECIFICATIONS AND THE SPECIAL PROVISIONS OF THIS CONTRACT.

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NO.	DATE	REVISION	APPROV.	NO.	DATE	REVISION	APPROV.
1.	10/10/03	SHEET ISSUED	JSN				

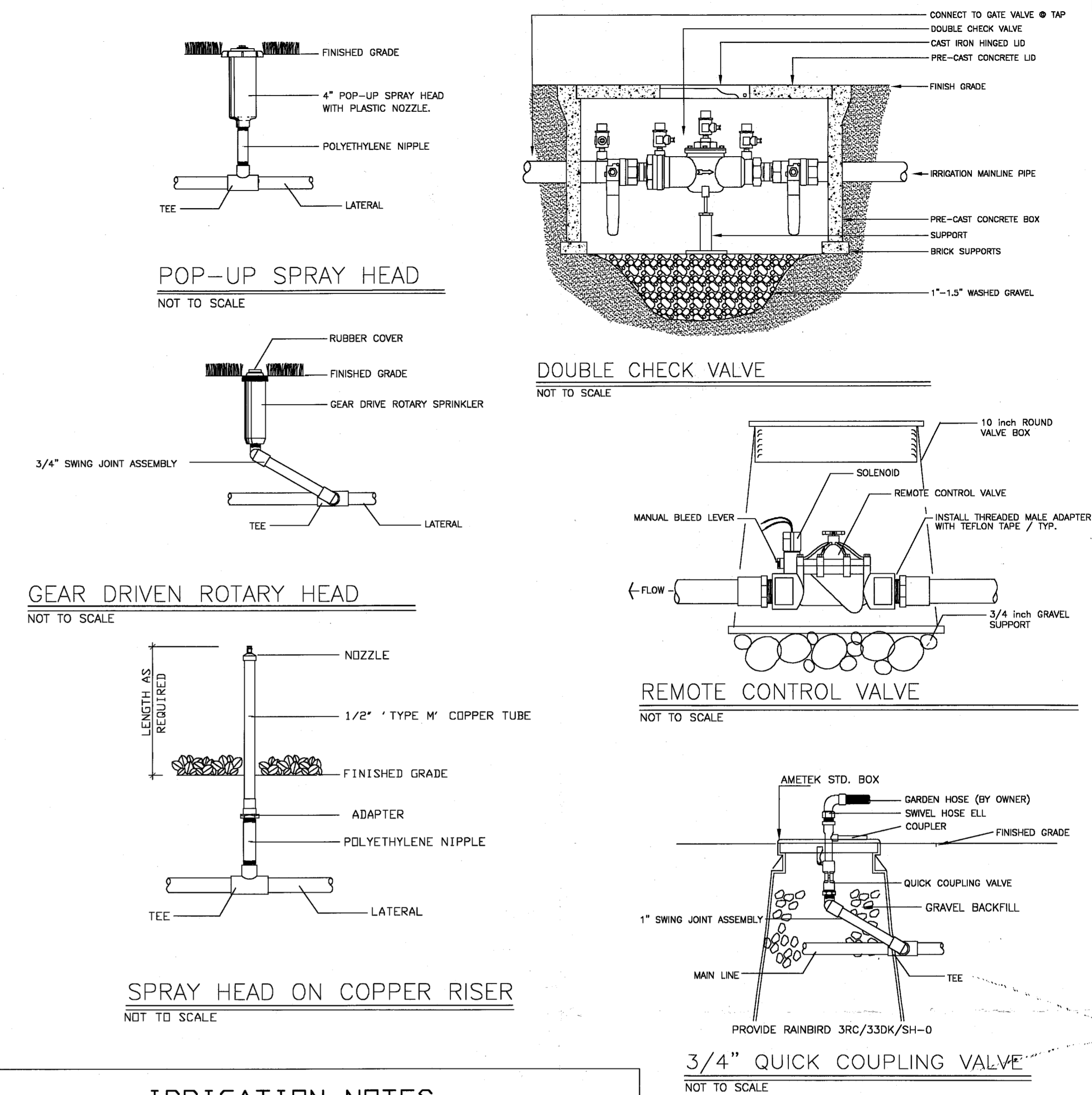
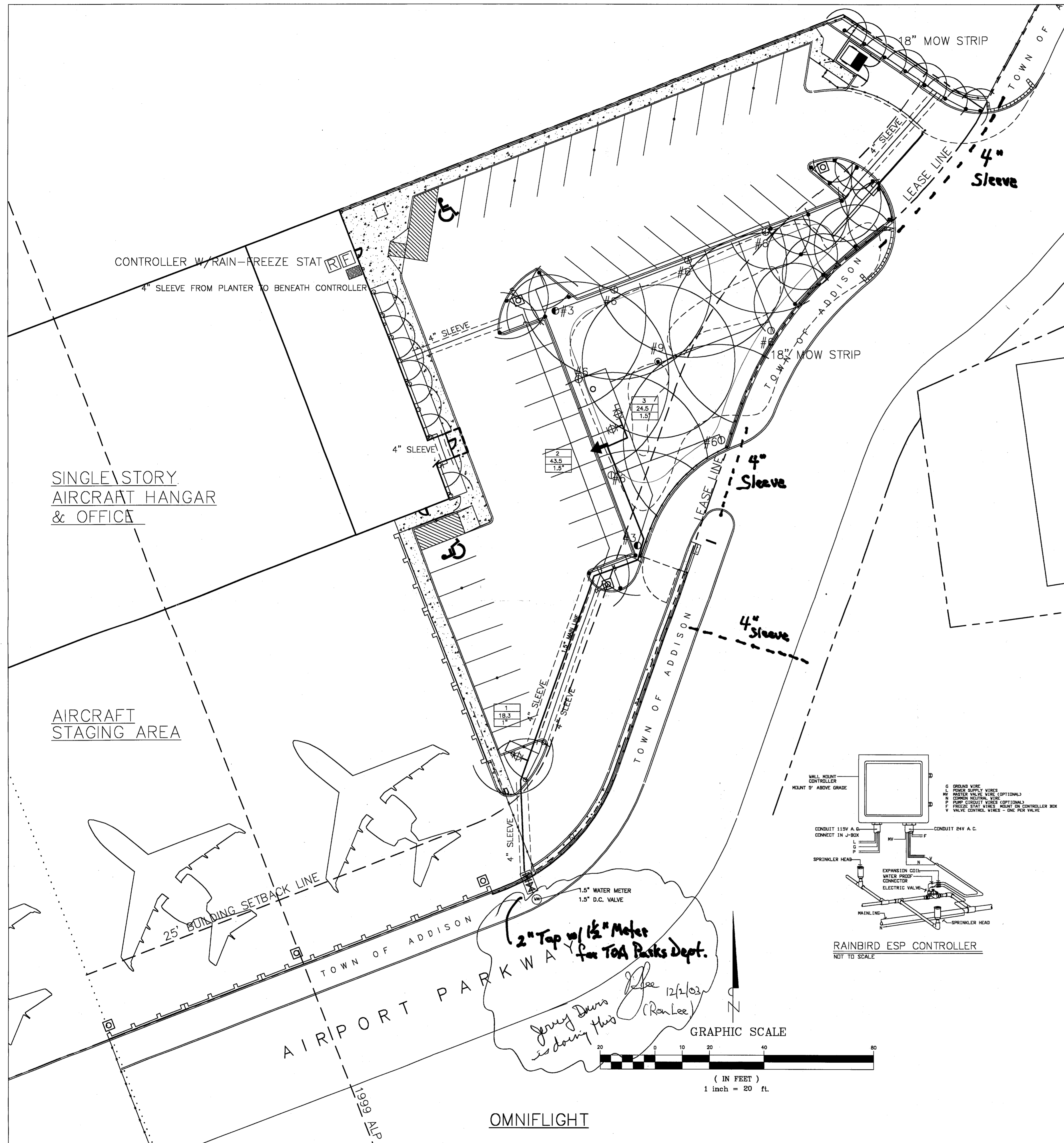


**HNTB** ARCHITECTS ENGINEERS PLANNERS  
The HNTB Companies

DESIGN CHECK	JSN	PROJECT NO.	39020
DRAWN	JDH		
CHECK	GFS		
DATE	SEPTEMBER 2003		



TOWN OF ADDISON	SHEET
AIRPORT PARKWAY REALIGNMENT	
FENCE DETAILS	



- ### IRRIGATION NOTES
- GENERAL:**
1. AN AUTOMATIC IRRIGATION SHALL BE PROVIDED AS INDICATED ON THE PLAN. IRRIGATION DESIGN IS FOR GRAPHIC PURPOSES ONLY. INSTALL SYSTEM COMPONENTS INDICATED IN HARDSCAPE AREAS IN ADJACENT LANDSCAPE AREAS.
  2. 10 DAYS PRIOR TO BEGINNING WORK I.L.C. TO CONFIRM THE STATIC WATER PRESSURE OF A MIN. OF 65 PSI. IF STATIC PRESSURE IS LESS THAN 65 PSI, ADJUSTMENTS IN THE PLAN WILL BE REQUIRED TO ACCOMMODATE THE EXISTING PRESSURES.
  3. THE IRRIGATION CONTRACTOR SHALL PROVIDE THE PROPER ARC AND RADIUS FOR EACH NOZZLE TO INSURE 100% COVERAGE OF ALL REQUIRED AREAS. PROVIDE NOZZLES AND HEAD PLACEMENTS TO MINIMIZE WATER BEING DIRECTLY SPRAYED INTO STREETS OR PARKING AREAS OR ONTO BUILDINGS, FENCES AND OTHER STRUCTURES.
  4. THE IRRIGATION CONTRACTOR SHALL COORDINATE INSTALLATION OF THE SYSTEM WITH THE LANDSCAPE CONTRACTOR SO THAT ALL PLANT MATERIALS WILL BE WATERED IN ACCORDANCE WITH THE INTENT OF THE PLANS AND SPECIFICATIONS.
  5. SLEEVES - SCHEDULE 40 PVC PIPE SLEEVES FOR PIPE AND WIRING SHALL BE PROVIDED AS SHOWN ON THE PLANS. SLEEVES SHALL BE INSTALLED 24" BELOW FINISH GRADE OF THE PAVEMENT, EXTEND 24" BEYOND THE EDGE OF THE PAVEMENT, AND, LOCATIONS TO BE FLAGGED WITH 2" PVC PIPE EXTENDED 24" ABOVE FINISH GRADE. COORDINATE INSTALLATION OF THE SLEEVES WITH THE PAVEMENT CONTRACTOR.
  6. IRRIGATION CONTRACTOR TO BE PROVIDED WITH THE REQUIRED METER(S) AND GATE VALVE(S) LOCATED IN A CONCRETE BOX. IRRIGATION CONTRACTOR SHALL PROVIDE THE SPRINKLER SYSTEM BEGINNING AT THE INSTALLATION OF THE BACKFLOW PREVENTER.
  7. 110V. ELECTRICAL POWER CIRCUITS SHALL BE PROVIDED FOR THE IRRIGATION CONTRACTOR AT THE LOCATION REQUIRED. IRRIGATION CONTRACTOR SHALL PROVIDE OTHER EQUIPMENT AND CONNECTIONS NECESSARY FOR THE REQUIRED CONTROLLER. ELECTRICAL COMPONENTS SHALL BE DIRECT WIRING INTO JUNCTION BOXES. PROVIDE COMPLETE INSTALLATION OF THE RAIN/FREEZE SENSORS.
  8. 24 V. WIRING TO BE MINIMUM 14 GA., SINGLE STRAND 4/64" INSULATION WITH 3M DBY SPLICES. SPLICES NOT MADE AT THE VALVE OR CONTROLLER TO BE IN A 10" RND. VALVE BOX. ALL WIRING TO BE PROVIDED WITH ONE COMMON WIRE (WHITE) AND INDIVIDUAL WIRES TO VALVES (RED).
  9. LATERAL LINES TO BE INSTALLED AT A MINIMUM OF 12" COVER. MAINLINES TO HAVE MIN. 18" COVER.
  10. INSTALL ALL VALVE IN 10" ROUND VALVE BOXES (COVERED) WITH 6" PEA GRAVEL IN BOTTOM.
  11. ALL POP UP SPRAY HEADS TO BE INSTALLED ON 6" X 1/2" CUT-OFF POLY NIPPLES.
  12. ALL SPRAY HEADS TO BE A MINIMUM OF 2" FROM CURBS AND SIDEWALKS.
  13. I.L.C. IS RESPONSIBLE FOR CONTACTING ALL UTILITY COMPANIES AND COORDINATING THE WORK WITH THE GENERAL CONTRACTOR TO HAVE UNDERGROUND UTILITIES LOCATED.

### IRRIGATION LEGEND

	VALVE NUMBER
	GALLONS PER MINUTE
	VALVE SIZE
	QUICK COUPLER VALVE
	CONTROLLER RAINBIRD ESP
	FREEZE-STAT AND RAIN-STAT
	POP-UP SPRAY HEAD (LAWN)
	SPRAY HEAD ON COPPER RISER
	HIGH-POP SPRAY HEAD
	END STRIP SPRAY HEAD
	CENTER STRIP SPRAY HEAD
	PART CIRCLE ROTARY HEAD - PGP#3 NOZZLE
	PART CIRCLE ROTARY HEAD - PGP#6 NOZZLE
	FULL CIRCLE ROTARY HEAD - PGP#9 NOZZLE
	ELECTRIC VALVE
	1.5" DOUBLE CHECK VALVE
	MAIN LINE - CLASS 200 PVC
	LATERAL LINES - CLASS 200 PVC

### PIPE SIZES

PIPE SIZE	UP TO GALLONS PER MINUTE
1/2"	4
3/4"	12
1"	20
1.25"	35
1.5"	50
2"	50+

VELOCITIES SHALL NOT EXCEED 5 FPS.

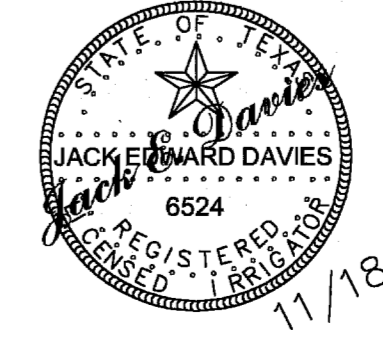
### Revision Log

No.	Date	Description

**FRITO LAY NORTH AMERICA**  
**CORPORATE HANGER**  
**AIRPORT PARKWAY**  
**ADDISON AIRPORT**

**Parkway Landscape, Inc.**  
**LANDSCAPE ARCHITECTURE**  
1508 N. COLLINS BLVD., #100 RICHARDSON, TEXAS 75080 972-689-4000

Date: **NOVEMBER 18, 2003**  
Project No. **0346**  
Drawn By: **JD**  
Checked By: **JD**  
Sheet Title: **IRRIGATION PLAN**  
Drawing No. **L-2**



**DFW Sprinkler Systems, Inc.**  
P. O. Box 595205 Dallas, Texas 75359  
Tel. 214.343.8433 Fax 214.824.6630

**1 IRRIGATION PLAN**  
SCALE: 1" = 20'