

CONSTRUCTION DOCUMENTS FOR ADDISON AIRPORT ACCESS ROAD REALIGNMENT

CITY COUNCIL
JOE CHOW, MAYOR

GREGORY S. HIRSCH,
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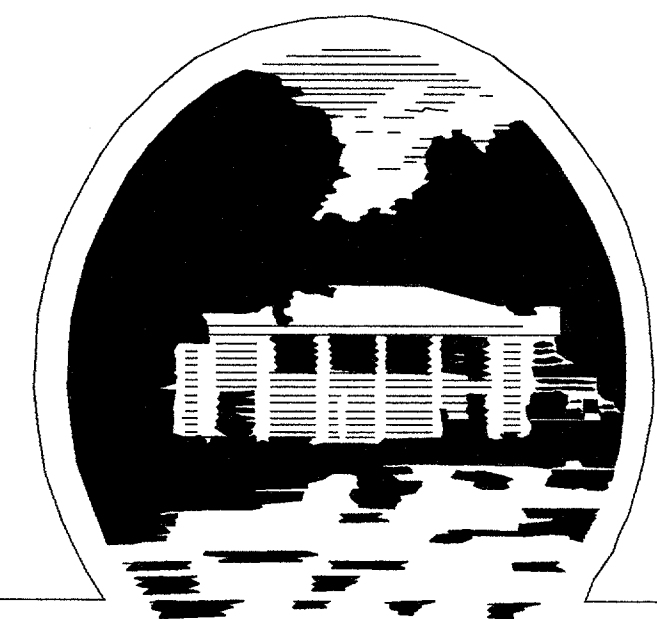
TODD MEIER

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RON WHITEHEAD

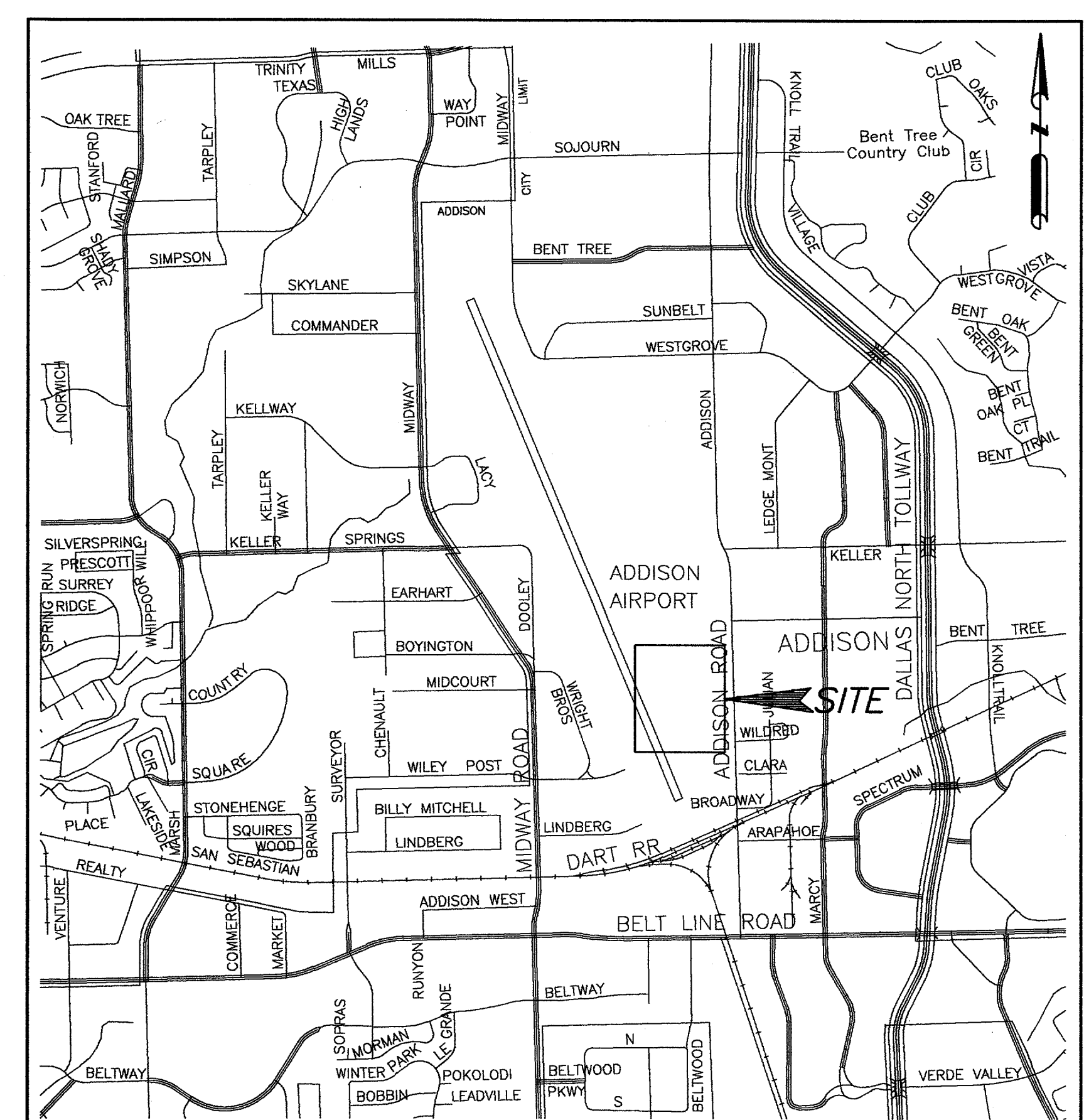
DIRECTOR OF PUBLIC WORKS
NANCY STRAUB CLINE, P.E.



T O W N O F
ADDISON

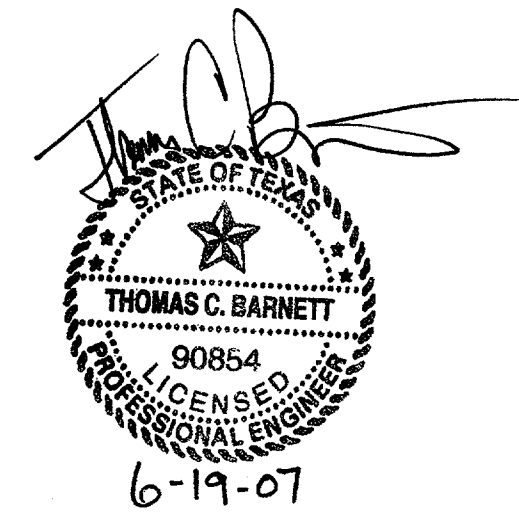
PUBLIC WORKS NO. 2007-004

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LOCATION MAP
NOT TO SCALE

ENGINEER
TOWN OF ADDISON
DEPARTMENT OF PUBLIC WORKS
16801 WESTGROVE DRIVE
ADDISON, TEXAS 75001
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JUNE, 2007

ADDISON AIRPORT ACCESS ROAD REALIGNMENT

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CONSTRUCTION CONTROL NOTES

I. GENERAL REQUIREMENTS

- A. IN ACCEPTING THE CONTRACTOR'S BID ON THIS PROJECT THE TOWN OF ADDISON ASSUMES THE CONTRACTOR IS OR HAS BECOME PRIOR TO SUBMITTING HIS/HER BID, KNOWLEDGEABLE OF THE CONSTRUCTION REQUIREMENTS, RESTRICTIONS, METHODS, MEANS, AND GENERAL CONSIDERATIONS OF DOING CONSTRUCTION WORK ON AN ACTIVE AIRPORT. WHILE A CONSCIENTIOUS AND GOOD FAITH EFFORT HAS BEEN MADE TO INCLUDE ALL APPROPRIATE AND RELEVANT REQUIREMENTS IN THESE PLANS AND SPECIFICATIONS, THE CONTRACTOR, AS A CONSIDERATION OF THIS CONTRACT, SHALL NOT USE A CLAIM OF LACK OF UNDERSTANDING OF THE COMPLEXITIES OF AIRPORT WORK AS A REASON TO CLAIM AGAINST THE TOWN OF ADDISON FOR ADDITIONAL TIME AND/OR COMPENSATION.
- B. THE CONTRACTOR AND HIS/HER STAFF, INCLUDING SUBCONSULTANTS, WHO WILL BE DRIVING IN THE AIRPORT OPERATIONS AREA (AOA) WILL BE REQUIRED TO ATTEND THE ADDISON AIRPORT DRIVING SCHOOL PRIOR TO CONSTRUCTION. THEY WILL ALSO BE REQUIRED TO ADHERE TO ALL RULES AND REGULATIONS PRESENTED IN THE DRIVING SCHOOL.
- C. THE ENGINEER SHALL BE THE INDIVIDUAL, PARTNERSHIP, FIRM OR CORPORATION AUTHORIZED BY THE OWNER (SPONSOR) TO BE RESPONSIBLE FOR ENGINEERING SUPERVISION OF THE CONTRACT WORK AND ACTING DIRECTLY OR THROUGH AN AUTHORIZED REPRESENTATIVE.
- D. THE RESIDENT PROJECT REPRESENTATIVE (RPR) WILL BE ONSITE AND WORK IN CONJUNCTION WITH THE ENGINEER THROUGHOUT THE DURATION OF THE PROJECT.

II. SITE AVAILABILITY

- A. ALL WORK AREAS WILL BE AVAILABLE TO THE CONTRACTOR AT THE TIME OF NOTICE TO PROCEED (NTP).
- B. THE CONTRACTOR SHALL NOT ENTER THE AIRPORT OPERATIONS AREA (AOA) NOR LEAVE HIS DESIGNATED AOA WORK AREA WITHOUT AN ESCORT FROM AIRPORT OPERATIONS OR PERMISSION FROM OPERATIONS TO PERFORM HIS OWN ESCORT.

III. MAINTENANCE OF VEHICULAR TRAFFIC

- A. VEHICULAR TRAFFIC
 - 1. THE CONTRACTOR IS ADVISED THAT AIRPORT RUNWAY 15/33, AND ALL TAXIWAYS WILL BE ACTIVE DURING THIS CONTRACT. THE CONTRACTOR WILL BE EXPECTED TO CONDUCT THE WORK SUCH THAT THE SAFETY OF OPERATIONS IS NOT DIMINISHED AND THAT VEHICULAR FLOW IS MAINTAINED AT ALL TIMES. THE CONTRACTOR IS ADVISED, AND SHALL ACCEPT AS AN IMPORTANT CONSIDERATION OF THE WORK, THAT THE MAINTENANCE OF SAFE AND EFFICIENT ACCESS TO THE AIRPORT VIA THE ROADWAY SYSTEM IS AN INTEGRAL PART OF THE WORK. ALL TRAFFIC MAINTENANCE SHALL BE DONE IN CONFORMANCE TO THE LATEST EDITION OF THE MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES.
 - 2. ALL EXISTING ROADS OR PAVEMENTS USED AS ACCESS/HAIL ROADS SHALL BE MAINTAINED AND IMPROVED AS REQUIRED. THE ROAD OR PAVEMENT CONDITIONS WILL BE INSPECTED JOINTLY BY THE ENGINEER/RPR AND CONTRACTOR PRIOR TO AND AT THE END OF THE PROJECT. THE CONDITION OF THESE ROADS AND PAVEMENTS SHALL BE EQUAL OR BETTER THAN AT THE START OF THE PROJECT AS DETERMINED BY THE ENGINEER/RPR. MECHANICAL BROOMS WILL ALSO BE REQUIRED FOR THESE EXISTING PAVEMENT ACCESS ROADS AT ALL TIMES.
- B. AIRCRAFT TRAFFIC
 - 1. THE CONTRACTOR IS ADVISED THAT ALL AIRPORT TAXIWAYS, RUNWAYS AND RAMPS WILL BE ACTIVE DURING THIS CONTRACT. THE CONTRACTOR WILL BE EXPECTED TO CONDUCT THE WORK SUCH THAT THE SAFETY OF OPERATIONS IS NOT DIMINISHED AND THAT AIRCRAFT FLOW IS MAINTAINED AT ALL TIMES. THE CONTRACTOR IS FURTHER ADVISED, AND SHALL ACCEPT AS AN IMPORTANT CONSIDERATION OF THE WORK, THAT THE MAINTENANCE OF SAFE AND EFFICIENT OPERATION OF THE AIRPORT OPERATIONS AREA (AOA) IS AN INTEGRAL PART OF THE WORK. ALL CONSTRUCTION INTERFACE WITH AIRCRAFT PAVEMENTS, TAXIWAY CROSSINGS, AND SECURITY REQUIREMENTS AS CONTAINED IN FAA ADVISORY CIRCULAR 150/5370-2E WILL APPLY.
 - 2. AOA NIGHT WORK SHALL BE DEFINED AS 10 P.M. TO 6 A.M. CONTRACTOR SHALL BEGIN CLEAN UP OPERATIONS NO LATER THAN 5:30 A.M. TO HAVE ALL AOA PAVEMENT OPEN NO LATER THAN 6:00 A.M.
 - 3. AOA WEEKEND WORK SHALL BE DEFINED AS BEGINNING AT 9:00 A.M. SATURDAY AND MAY CONTINUE UNTIL 4:30 A.M. MONDAY.

IV. WORK WITHIN AIRPORT OPERATIONS AREA (AOA)

- A. GENERAL REQUIREMENTS
 - 1. ALL WORK TO BE PERFORMED WILL BE INSIDE OR ADJACENT TO THE AIRPORT OPERATIONS AREA (AOA). CONSEQUENTLY ALL WORK MUST BE PERFORMED SUCH THAT THE SECURITY OF THE AOA IS MAINTAINED.
 - 2. THE CONTRACTOR WILL BE REQUIRED TO OBTAIN ALL VEHICLE PASSES AND TEMPORARY PERSONNEL SECURITY BADGES TO SATISFY ALL AIRPORT SECURITY REQUIREMENTS PRIOR TO BEGINNING WORK WITHIN THE AOA.
 - 3. IN ADDITION, THE CONTRACTOR IS ADVISED THAT CERTAIN RULES AND RESTRICTIONS, AS CONTAINED IN FAA ADVISORY CIRCULAR 150/5370-2D AND AUGMENTED BY THESE PLANS AND SPECIFICATIONS, WILL APPLY TO THE WORK. THE CONTRACTOR SHALL BECOME FAMILIAR WITH ALL REQUIREMENTS APPLICABLE TO AIRPORT CONSTRUCTION AND COOPERATE WITH THE ENGINEER IN MAINTAINING A SAFE CONSTRUCTION SITE WHICH IS COMPATIBLE WITH AIRCRAFT AND AIRPORT OPERATIONS.
 - 4. THE CONTRACTOR'S ACCESS TO THE JOB SITE AND WITHIN THE AIRCRAFT OPERATIONS AREA (AOA), SHALL BE LIMITED THROUGH THE EXISTING SECURITY GATES. THE CONTRACTOR WILL BE REQUIRED TO MAINTAIN SECURITY AT THE GATES THROUGHOUT THE DURATION OF THE PROJECT. GATES SHALL BE MANNED BY A SECURITY GUARD AT ALL TIMES WHEN THE SECURITY OF THE AIRCRAFT OPERATIONS AREA (AOA) IS BREACHED. THE GUARD WILL BE TRAINED BY THE CONTRACTOR IN THE USE OF THE RADIOS AND THE SECURITY REQUIREMENTS OF THE AOA. WHEN THE GATE IS UNGUARDED FOR WHATEVER REASON OR LENGTH OF TIME, IT SHALL BE LOCKED WITH LOCKS PROVIDED BY THE CONTRACTOR AND APPROVED BY THE ENGINEER. THE CONTRACTOR WILL BE LIABLE FOR ANY FINES ISSUED BY THE FAA FOR SECURITY (OR OTHER) VIOLATIONS FOR WHICH IT IS CITED. AN ASSESSMENT OF \$1,000 MAY BE ASSESSED FOR EACH AND EVERY OCCURRENCE WHERE THE GATE IS LEFT UNGUARDED AND UNLOCKED.
 - 5. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROVIDING HIS/HER OWN PROJECT OFFICE, TOILET FACILITIES AND OTHER NECESSARY BUILDINGS OR SHELTERS. THE AIRPORT SPONSOR WILL NOT PROVIDE ANY FACILITIES TO THE CONTRACTOR DURING CONSTRUCTION. TOILET FACILITIES SHALL BE MAINTAINED IN WORKING CONDITION BY THE CONTRACTOR AT ALL TIMES. THE CONTRACTOR SHALL PAY ALL MONTHLY UTILITY BILLS AND INSTALLATION FEES.
 - 6. THE CONTRACTOR SHALL SUPPLY PORTABLE HAND-HELD RADIOS (AVIATION BAND), SET TO A PREDETERMINED FREQUENCY ESTABLISHED BY THE AIRPORT MANAGER, TO EACH FLAGMAN, SUPERVISORY INDIVIDUAL AND RPR SO THAT THEY MAY KEEP IN CONSTANT CONTACT AT ALL TIMES WITH THE AIRPORT OPERATIONS OFFICE. UPON COMPLETION OF THE PROJECT AND ACCEPTANCE BY THE OWNER, THE RPR SHALL RETURN THE RADIO SET TO THE CONTRACTOR.
 - 7. THE CONTRACTOR SHALL BE RESPONSIBLE FOR MAINTAINING ALL PORTABLE HAND-HELD RADIO SETS IN WORKING ORDER AT ALL TIMES FOR THE DURATION OF THE PROJECT.
 - 8. THE CONTRACTOR SHALL SUBMIT HIS/HER CONSTRUCTION WORK SCHEDULE TO THE ENGINEER PROJECTING HIS UPCOMING WORK FOR THE NEXT THREE WEEKS. THE ENGINEER, AIRPORT MANAGER AND RESIDENT PROJECT REPRESENTATIVE SHALL REVIEW THIS PLAN WEEKLY WITH THE CONTRACTOR SO THAT EVERYONE IS AWARE OF UPCOMING CONSTRUCTION EVENTS.

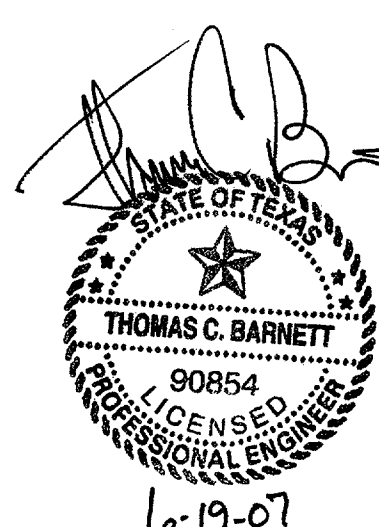
- 9. THE CONTRACTOR SHALL MAINTAIN A CLEAN AND SAFE CONSTRUCTION WORK AREA. THE CONTRACTOR SHALL PERFORM CLEAN-UP OPERATIONS ON A DAILY BASIS.
- 10. THE CONTRACTOR SHALL NOT DEVIATE FROM THE APPROVED CONSTRUCTION SEQUENCE WITHOUT FIRST OBTAINING APPROVAL FROM THE ENGINEER. THE CONTRACTOR SHALL BRING IN HIS/HER EQUIPMENT AND SHALL KEEP THAT EQUIPMENT ON SITE FOR THE DURATION OF ITS FUNCTION.
- 11. THE CONTRACTOR SHALL HAVE SUFFICIENT EQUIPMENT AND PERSONNEL ON SITE TO ACCOMPLISH EFFICIENT AND PROMPT CONSTRUCTION OF THE VARIOUS WORK ITEMS, INCLUDING WORK ON MORE THAN ONE WORK ITEM SIMULTANEOUSLY.
- 12. NO TRENCHES IN OR DIRECTLY ADJACENT TO OPERATIONAL PAVEMENT SHALL REMAIN OPEN OVERNIGHT OR WHEN THE CONTRACTOR FINISHES WORK FOR THE DAY IN THE AREA. TRENCHES NOT BACKFILLED SHALL BE COVERED WITH STEEL PLATES TO ALLOW FOR SAFE PASSAGES BY AIRCRAFT ACROSS THE TRENCH, IF APPROVED BY THE AIRPORT MANAGER.
- 13. THE CONTRACTOR SHALL BE RESPONSIBLE FOR REPAIR OF ANY DAMAGE TO EXISTING FACILITIES NOT DESIGNATED FOR RECONSTRUCTION OR REPLACEMENT AT HIS/HER EXPENSE DUE TO MOVING OR USAGE OF HEAVY EQUIPMENT OR THE TRANSPORT OF MATERIALS TO OR ON THE SITE SHALL BE REPAIRED TO EQUAL OR BETTER QUALITY BY THE CONTRACTOR AT HIS/HER OWN EXPENSE.
- 14. THE CONTRACTOR SHALL VIDEO TAPE THE ENTIRE WORK AREA AFTER THE PRE-CONSTRUCTION MEETING AND PRIOR TO THE MOBILIZATION OF PERSONNEL AND EQUIPMENT. THE CONTRACTOR SHALL PROVIDE ONE (1) COPY OF THIS VIDEO TAPE(S) TO BOTH THE AIRPORT MANAGER AND THE ENGINEER. IN AREAS WHERE CONSTRUCTION EQUIPMENT CROSSES EXISTING PAVEMENTS, THE CONTRACTOR SHALL TAKE PICTURES OR VIDEO OF THE PAVEMENT PRIOR TO COMMENCING OPERATIONS. THE CONTRACTOR SHALL PROVIDE THE ENGINEER AND AIRPORT MANAGER WITH ONE COPY EACH OF THE PHOTOGRAPH OR VIDEO TAPE(S) TAKEN. THIS DOCUMENTATION SHALL BE USED TO DETERMINE THE AMOUNT OF DAMAGE, IF ANY, CAUSED TO THE PAVEMENTS BY THE CONSTRUCTION EQUIPMENT CROSSINGS AND THE QUALITY OF CONSTRUCTION WHICH SHALL BE REQUIRED FOR THE REPAIRS.
- 15. CONSTRUCTION EQUIPMENT AND VEHICLES SHALL TRAVEL A MINIMUM AMOUNT ON NEWLY CONSTRUCTED PAVEMENTS SO THAT THE NEWLY CONSTRUCTED AREAS WILL NOT BE DAMAGED.
- 16. THE CONTRACTOR SHALL BE RESPONSIBLE FOR REPAIR OF ANY DAMAGE TO UNDERGROUND CABLES ENCOUNTERED. NO DIGGING SHALL BE PERFORMED WITHOUT FIRST CONTACTING THE AIRPORT MANAGER. ANY DAMAGE TO FAA CABLE OR OTHER UNDERGROUND CABLES OR FACILITIES SHALL BE REPAIRED IN ACCORDANCE WITH THE APPLICABLE FAA SPECIFICATIONS AND IN A MANNER ACCEPTABLE TO AIRPORT MANAGER, TXDOT AND THE ENGINEER.
- 17. ALL ABOVE GROUND AND GROUND LEVEL ELECTRICAL RELATED APPURTENANCES (I.E., RUNWAY LIGHTS, CABLE BOXES, CABLE AND/OR DUCT MARKERS, CONDUIT, ETC.) SHALL BE PROTECTED AT ALL TIMES. ANY DAMAGE DONE TO SAID APPURTENANCES BY THE CONTRACTOR SHALL BE REPAIRED IMMEDIATELY TO LIKE QUALITY AT THE CONTRACTOR'S EXPENSE. THE REPAIRS SHALL BE PERFORMED TO THE SATISFACTION OF THE AIRPORT MANAGER AND THE ENGINEER.
- 18. CONSTRUCTION WORKERS WILL NOT BE ALLOWED TO ESTABLISH OVERNIGHT RESIDENCE ON THE PREMISES. ALL CONSTRUCTION WORKERS SHALL LEAVE THE CONSTRUCTION SITE AND AIRPORT PROPERTY AT THE END OF THEIR WORK PERIOD.
- 19. WORK CANNOT COMMENCE UNTIL:
 - A). SUFFICIENT BARRICADES ARE IN PLACE TO CONFINE THE AREA AND CREATE A BARRIER BETWEEN AIRCRAFT AND VEHICLE MOVEMENT AREAS AND THE CONSTRUCTION AREA.
 - B). ALL SAFETY EQUIPMENT FOR PERSONNEL AND CONSTRUCTION EQUIPMENT IS IN PLACE AND OPERABLE.
- 20. ALL MATERIAL SUBMITTALS FOR ITEMS TO BE USED IN CONSTRUCTION OF THE PROJECT SHALL BE SUBMITTED TO THE ENGINEER FOR REVIEW AND APPROVAL 21-DAYS PRIOR TO COMMENCEMENT OF WORK. THREE-WEEK LOOK AHEAD SCHEDULES TO BE PROVIDED ON A WEEKLY BASIS AND BE EXPANDED AND ENHANCED VERSIONS OF THE PROJECT SCHEDULE. WEEKLY SCHEDULES TO BE PRESENTED BY THE CONTRACTOR AT THE WEEKLY CONSTRUCTION MEETINGS.
- 21. A COMPLETE PROJECT SCHEDULE SHALL BE PROVIDED AT THE PRE-CONSTRUCTION MEETING AND PRESENTED BY THE CONTRACTOR TO THE MEETING ATTENDEES. RUNWAY SHUT-DOWN DATES MAY BE DETERMINED AT THE PRE-CONSTRUCTION MEETING, OR AT A LATER TIME, AS APPROVED BY THE AIRPORT MANAGER.
- 22. INTERIM PROJECT SCHEDULES TO BE PROVIDED ON THE FIRST OF EACH MONTH AND INCLUDE ORIGINAL BASELINE, UPDATED TO CURRENT CONSTRUCTION ACTIVITY.
- B. WORK ADJACENT TO AND ON RUNWAYS
 - 1. NOT ALLOW ANY WORK TO BE UNDERTAKEN INSIDE OF, OR ANY PERSONNEL, EQUIPMENT, OR VEHICLES TO ENTER THE TAXIWAY OR RUNWAY RESTRICTED ZONE WHILE ANY TAXIWAY OR RUNWAY IS "OPEN". THE RESTRICTED ZONE FOR TAXIWAY "A" IS DEFINED AS THE AREA WITHIN 93 FEET OF THE TAXIWAY CENTERLINE. THE RESTRICTED ZONE FOR TAXIWAYS "R" "S" AND "T" IS DEFINED AS THE AREA WITHIN 57.5 FEET OF THE TAXIWAY CENTERLINES.
 - 2. BE RESPONSIBLE FOR PROVIDING ALL TEMPORARY LIGHTING AND OTHER SPECIAL EQUIPMENT THAT MAY BE NEEDED FOR NIGHTTIME CONSTRUCTION IF REQUESTED BY CONTRACTOR AND APPROVED BY ENGINEER. THE COST OF THIS EQUIPMENT SHALL BE INCLUDED IN THE GENERAL COST OF THE WORK, NO SEPARATE PAYMENT WILL BE MADE.

V. DUST CONTROL

- A. THE OPERATION OF RUNWAYS, TAXIWAYS, AND ASSOCIATED NAVAIDS ARE ESPECIALLY SENSITIVE TO DUST. THE ENGINEER RESERVES THE RIGHT TO STOP CONTRACTOR OPERATIONS, IF NECESSARY TO BRING DUST UNDER CONTROL. THE CONTRACTOR SHALL BE RESPONSIBLE FOR DUST CONTROL WITHIN THE CONSTRUCTION LIMITS AS WELL AS ALONG ANY ROADWAYS USED BY THE EQUIPMENT AND VEHICLES.
- B. WATER TRUCKS SHALL BE MAINTAINED AT ALL TIMES SUCH THAT THE ACCESS ROADS AND CONSTRUCTION AREAS CAN BE WETTED AS NECESSARY. THE CONTRACTOR SHALL BE PREPARED, AT NO EXTRA COST TO THE OWNER, TO USE ADDITIONAL WATER TRUCKS OR OTHER MEANS SHOULD IT BE NECESSARY TO MAINTAIN DUST TO AN ACCEPTABLE LEVEL.

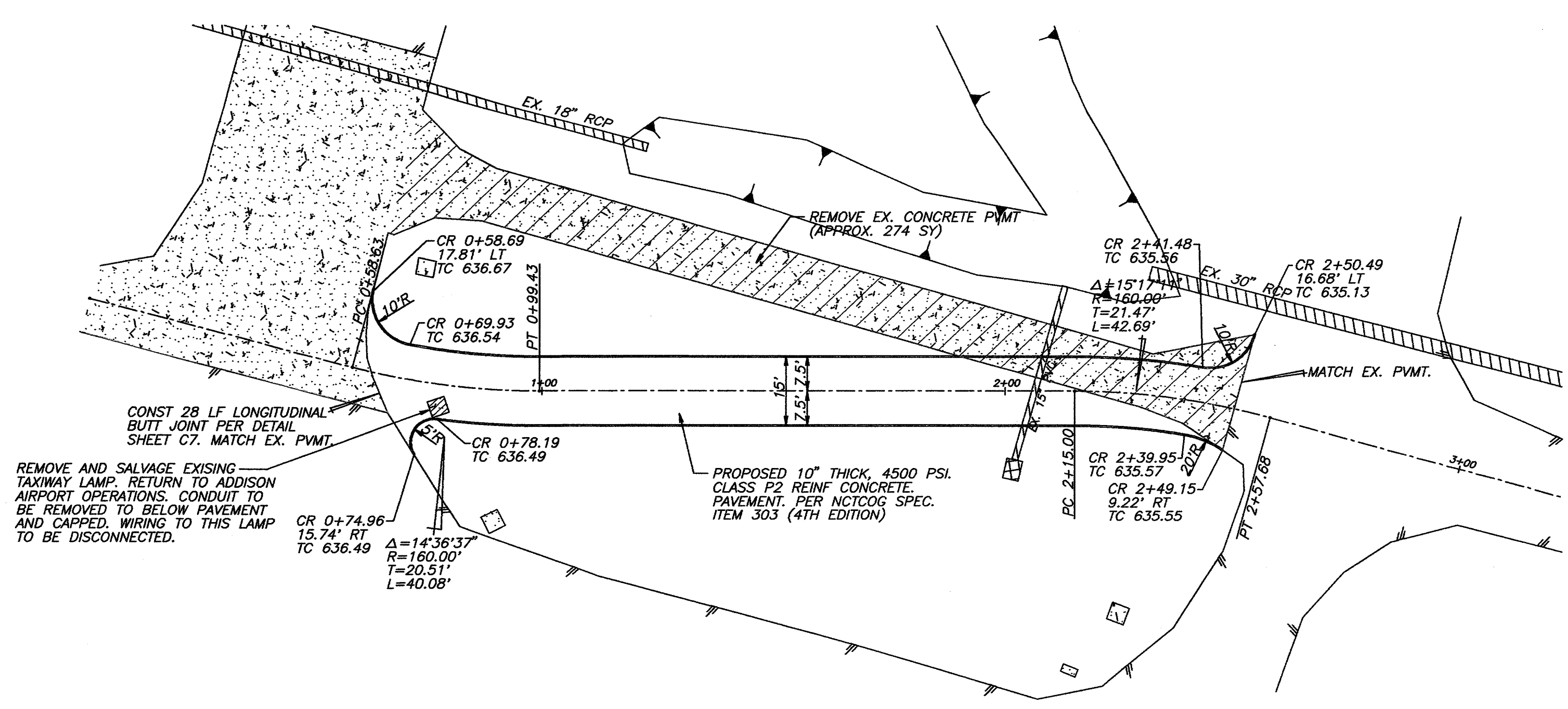
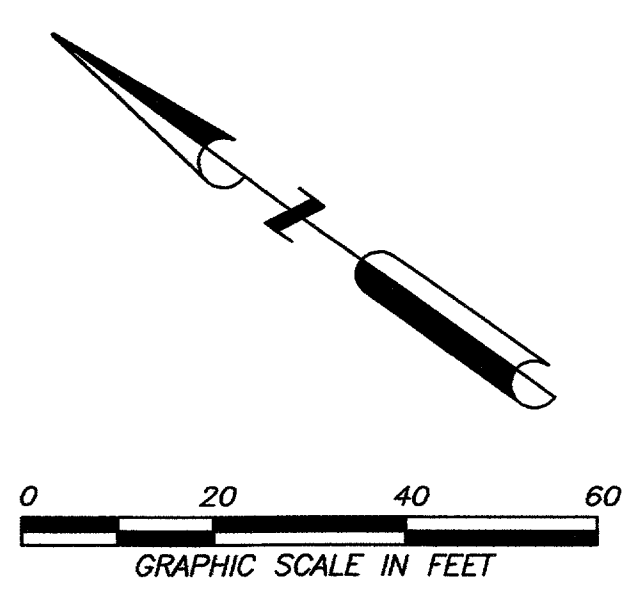
VI. DEMOBILIZATION

- A. CONDITIONS OF THE PROJECT AREA UPON COMPLETION OF THE JOB SHALL BE AS GOOD AS OR BETTER THAN THE CONDITIONS PRIOR TO STARTING WORK, IN ADDITION TO THE WORK ITEMS LISTED.
- B. THE PROJECT AREA SHALL BE FREE OF ANY CONTRACTOR STOCKPILE MATERIALS UPON COMPLETION OF THE JOB UNLESS OTHERWISE DIRECTED BY THE ENGINEER.
- C. ALL OF THE CONSTRUCTION EQUIPMENT, AND ANY FACILITIES TEMPORARILY PLACED ON SITE FOR THE PROJECT SHALL BE REMOVED FROM THE SITE.
- D. ANY PROPERTIES BELONGING TO THE AIRPORT SHALL BE RETURNED TO THE AIRPORT OWNER.
- E. PROPER DRAINAGE (NO LOCALIZED PONDING) SHALL BE MAINTAINED, PRIOR TO, DURING AND AFTER MOBILIZATION.
- F. DEMOBILIZATION SHALL BE DONE TO THE SATISFACTION OF THE ENGINEER.
- G. DEMOBILIZATION SHALL BE DONE IN A MANNER THAT WILL NOT CAUSE ANY INCONVENIENCE TO AIRPORT OPERATIONS.
- H. ANY DAMAGE TO THE AIRPORT PROPERTIES DURING DEMOBILIZATION SHALL BE REPAIRED AND PAID FOR AT THE CONTRACTOR'S OWN EXPENSE.
- I. SAFETY REGULATIONS SHALL BE OBSERVED AT ALL TIMES DURING DEMOBILIZATION.



NO.	DATE	REVISION DESCRIPTION				
GENERAL NOTES						
ADDISON AIRPORT ACCESS ROAD REALIGNMENT						
TOWN OF ADDISON DALLAS COUNTY, TEXAS						
TOWN OF ADDISON PUBLIC WORKS DEPARTMENT						
DESIGN	DRAWN	DATE	SCALE	NOTES	JOB NUMBER	SHEET
TCB	TCB	06/19/2007		TCB		C2

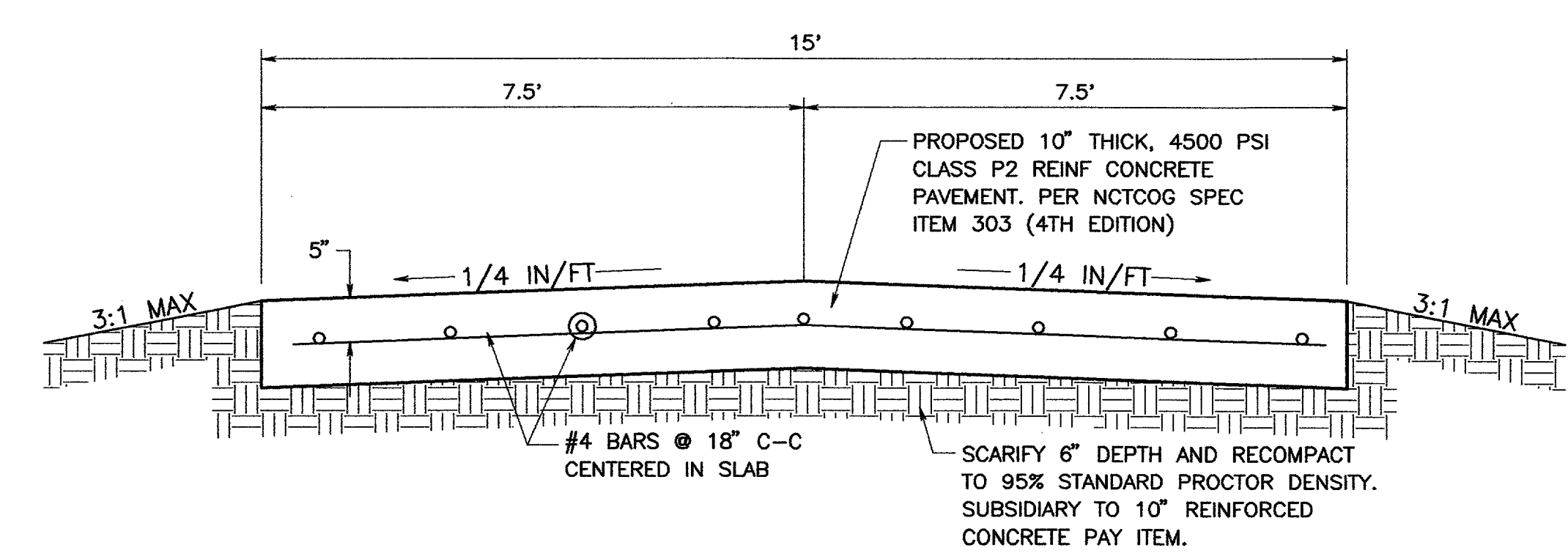
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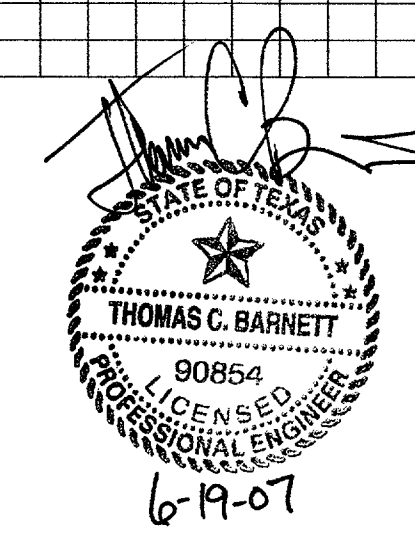
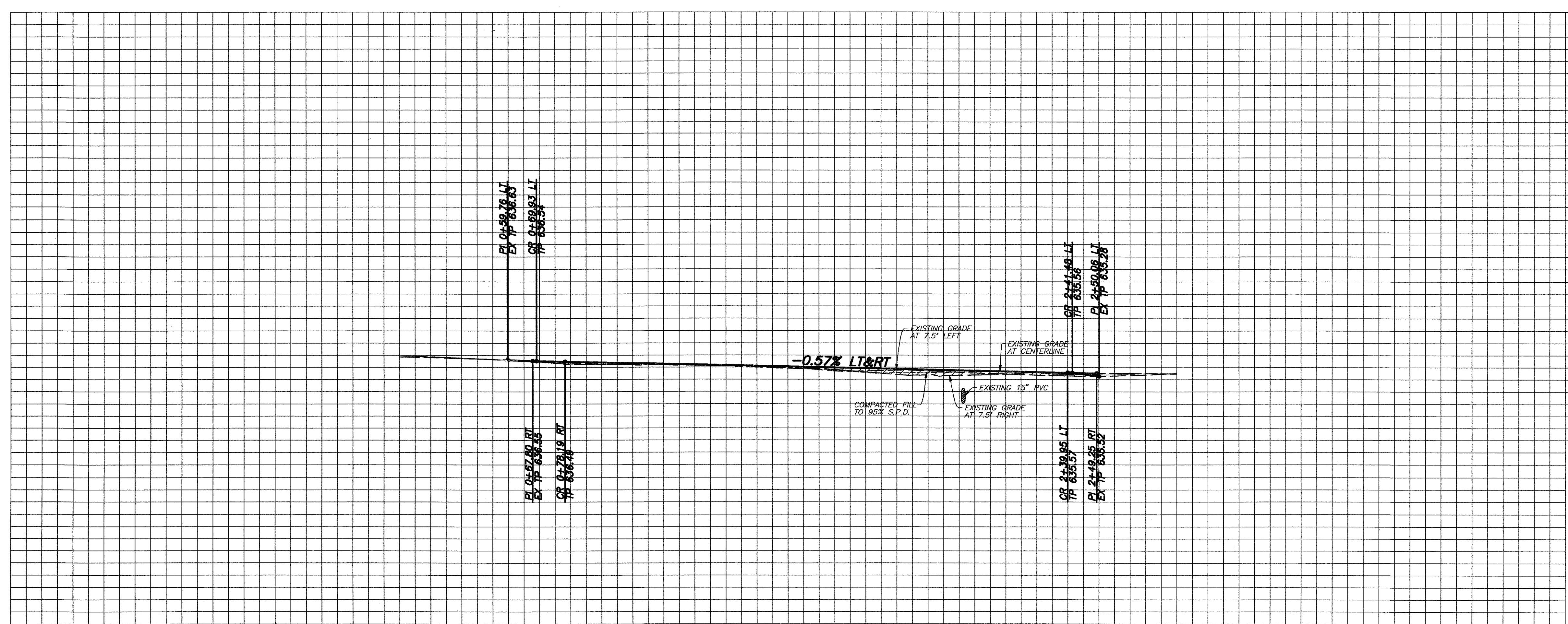
HORIZONTAL AND VERTICAL CONTROL MONUMENTS			
CTRL. PNT.	NORTHING	EASTING	ELEV.
AA-1	7,039,689.078	2,480,245.682	638.30
AA-2	7,041,501.126	2,480,144.730	641.92
AA-4	7,035,957.986	2,479,444.822	632.41
AA-5	7,039,506.863	2,478,102.163	637.40
AA-6	7,043,848.691	2,477,497.499	652.26
AA-7	7,036,903.681	2,480,589.730	636.69

REMOVE AND SALVAGE EXISTING TAXIWAY LAMP. RETURN TO ADDISON AIRPORT OPERATIONS. CONDUIT TO BE REMOVED TO BELOW PAVEMENT AND CAPPED. WIRING TO THIS LAMP TO BE DISCONNECTED.

PROPOSED 10" THICK, 4500 PSI. CLASS P2 REINF. CONCRETE. PAVEMENT. PER NCTCOG SPEC. ITEM 303 (4TH EDITION)

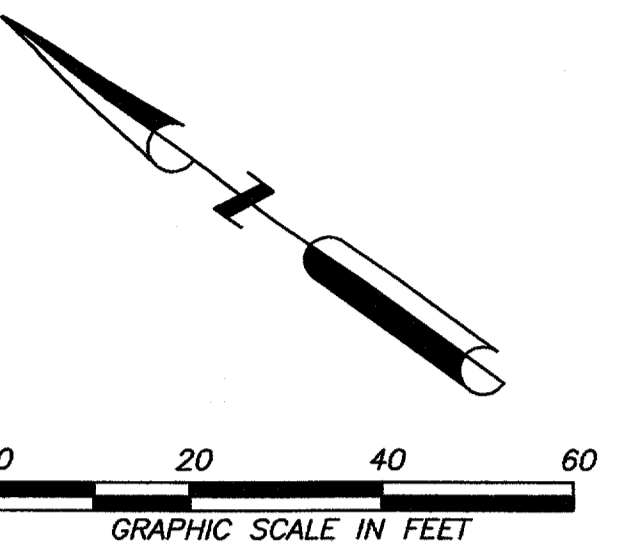


TYPICAL SECTION
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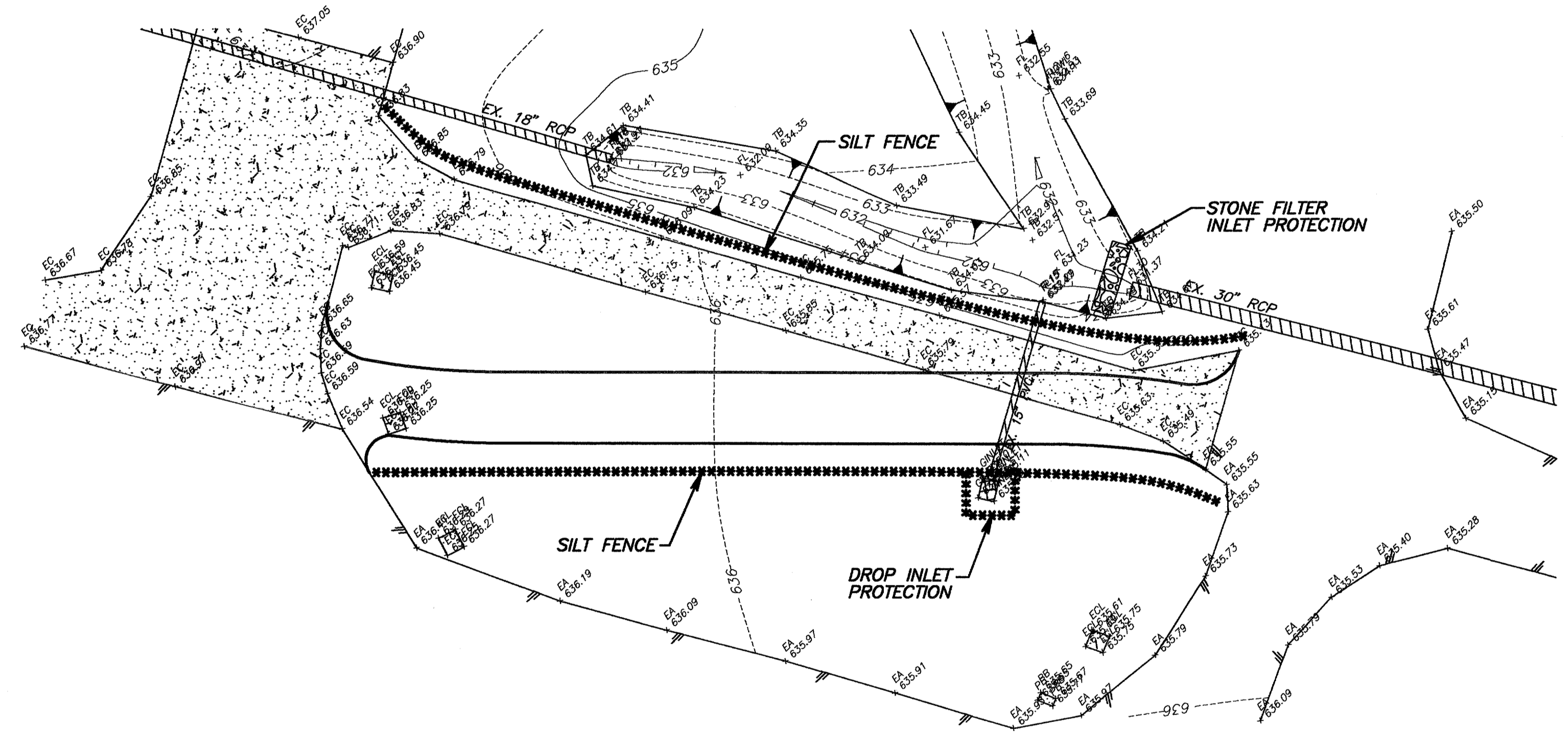


NO.	DATE	REVISION DESCRIPTION				
PAVING PLAN						
ADDISON AIRPORT ACCESS ROAD REALIGNMENT						
TOWN OF ADDISON DALLAS COUNTY, TEXAS						
TOWN OF ADDISON PUBLIC WORKS DEPARTMENT						
DESIGN	DRAWN	DATE	SCALE	NOTES	JOB NUMBER	SHEET
TCB	TCB	06/19/2007	(H) 1" = 20' (V) 1" = 5'	TCB		C3

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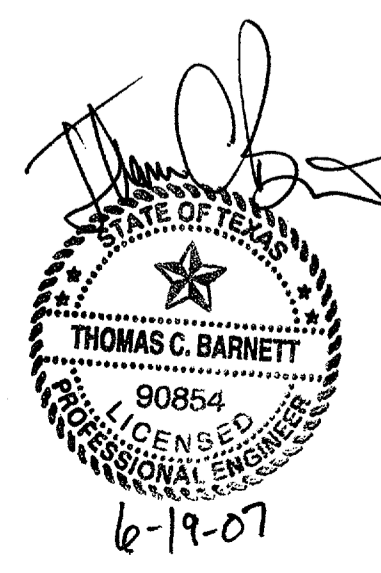


- LEGEND:**
- 6.35 --- EXISTING CONTOUR
 - FLOW ARROW
 - EXISTING EDGE OF PVMT
 - PROPOSED EDGE OF PVMT
 - EXISTING STORM SEWER MAIN
 - ***** SILT FENCE/INLET PROTECTION
 - ☒ SILT FENCE/INLET PROTECTION



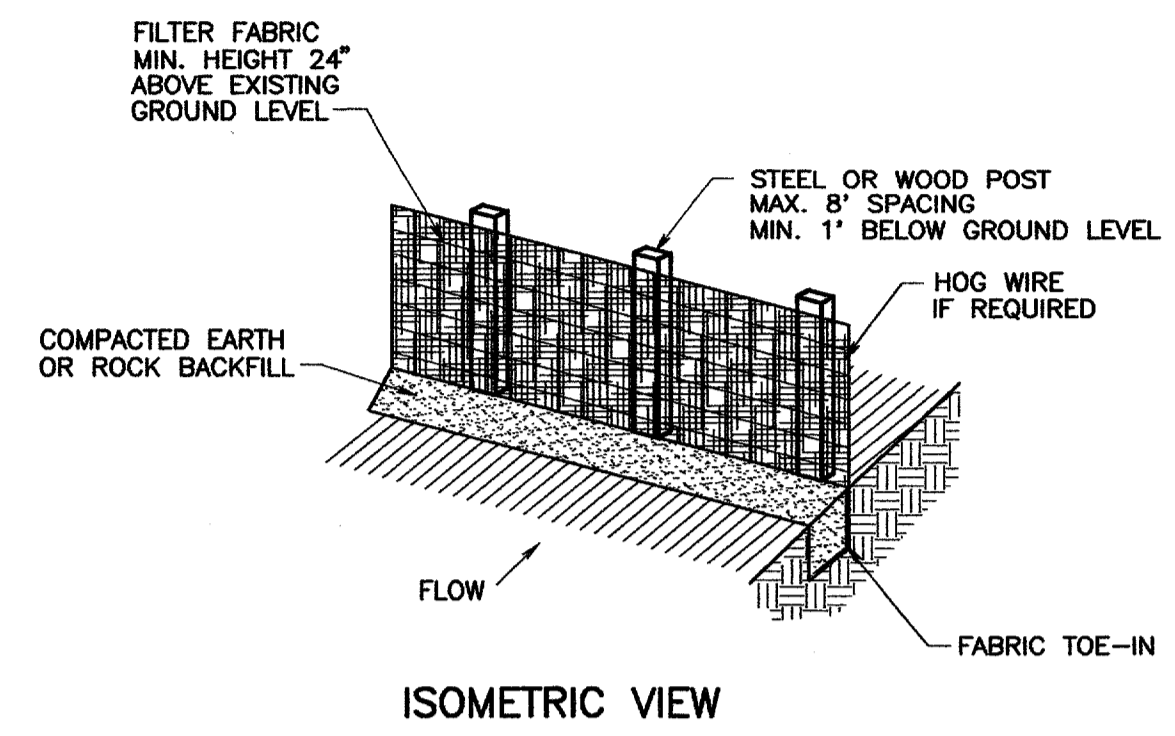
HORIZONTAL AND VERTICAL CONTROL MONUMENTS

CTRL PNT	NORTHING	EASTING	ELEV
AA-1	7,039,689.078	2,480,245.682	638.30
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AA-6	7,043,848.691	2,477,497.499	652.26
AA-7	7,036,903.681	2,480,589.730	636.69

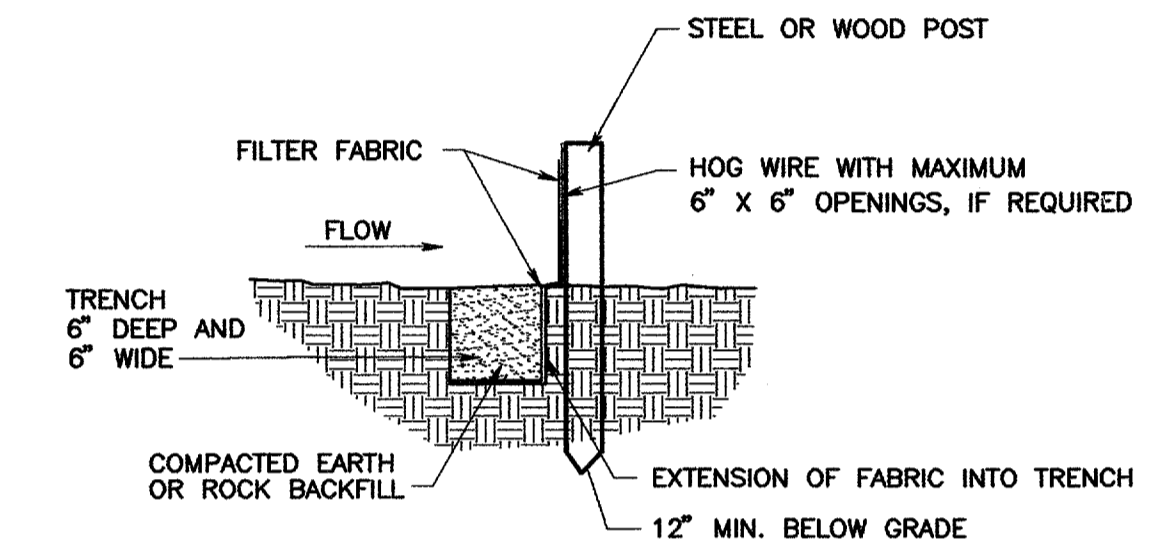


NO.	DATE	REVISION DESCRIPTION				
EROSION & SEDIMENTATION CONTROL PLAN						
ADDISON AIRPORT ACCESS ROAD REALIGNMENT						
TOWN OF ADDISON DALLAS COUNTY, TEXAS						
TOWN OF ADDISON PUBLIC WORKS DEPARTMENT						
DESIGN	DRAWN	DATE	SCALE	NOTES	JOB NUMBER	SHEET
TCB	TCB	06/19/2007	1"=20'	TCB		C4

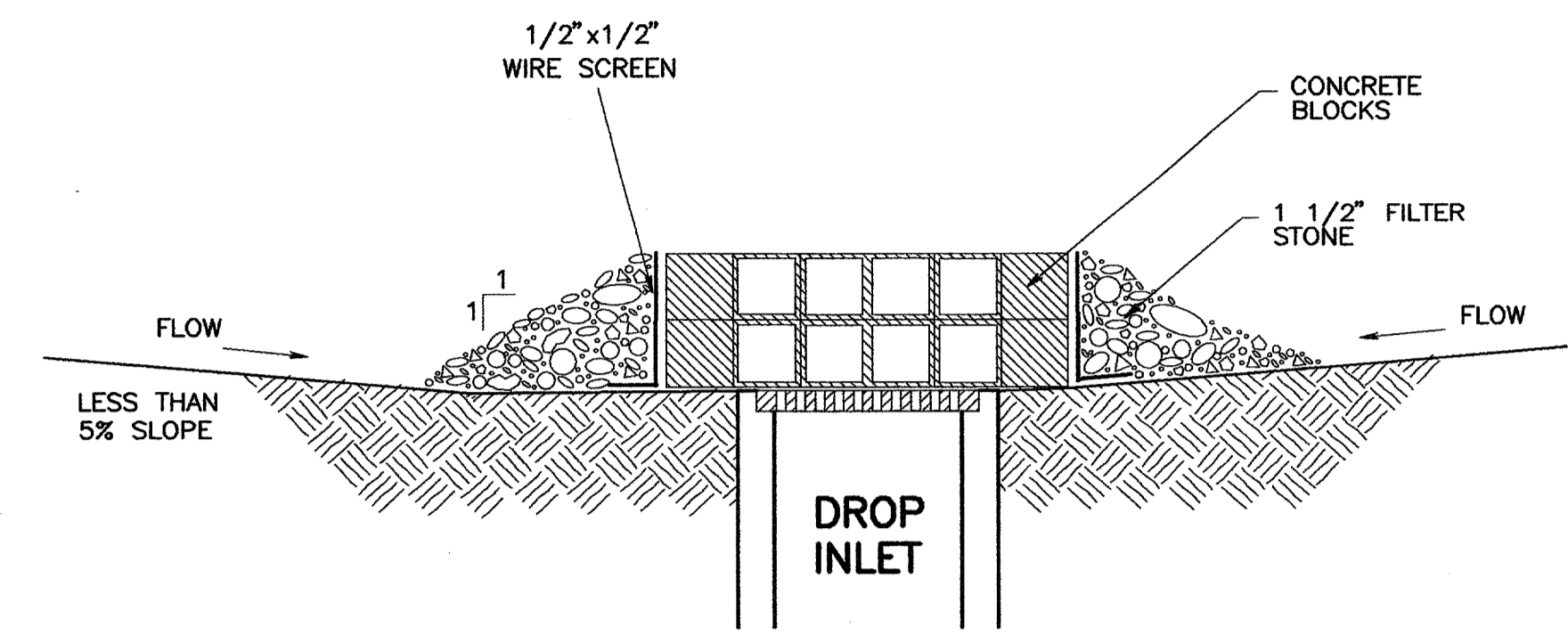
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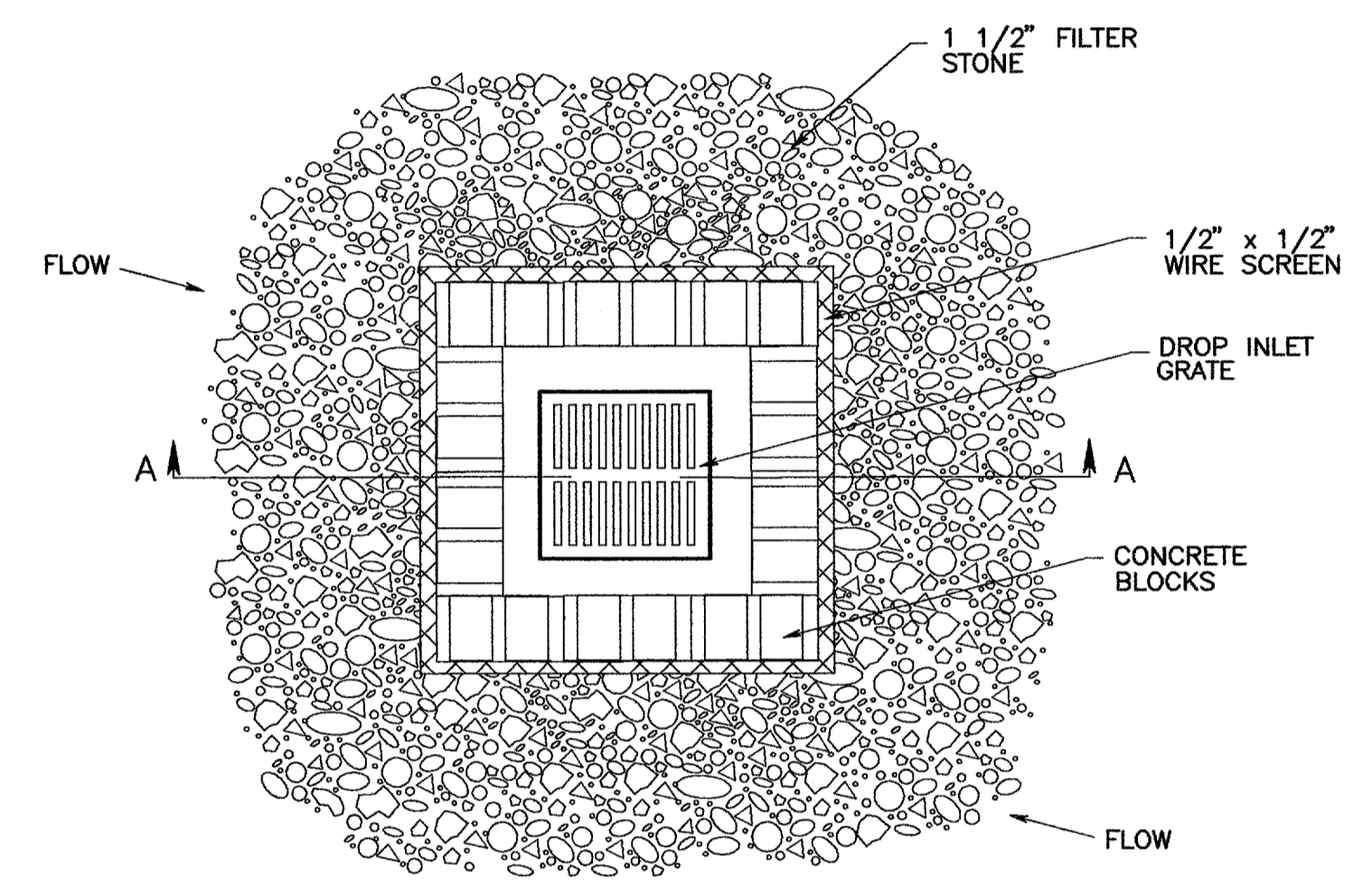
ISOMETRIC VIEW



SECTION VIEW
SILT FENCE
NTS

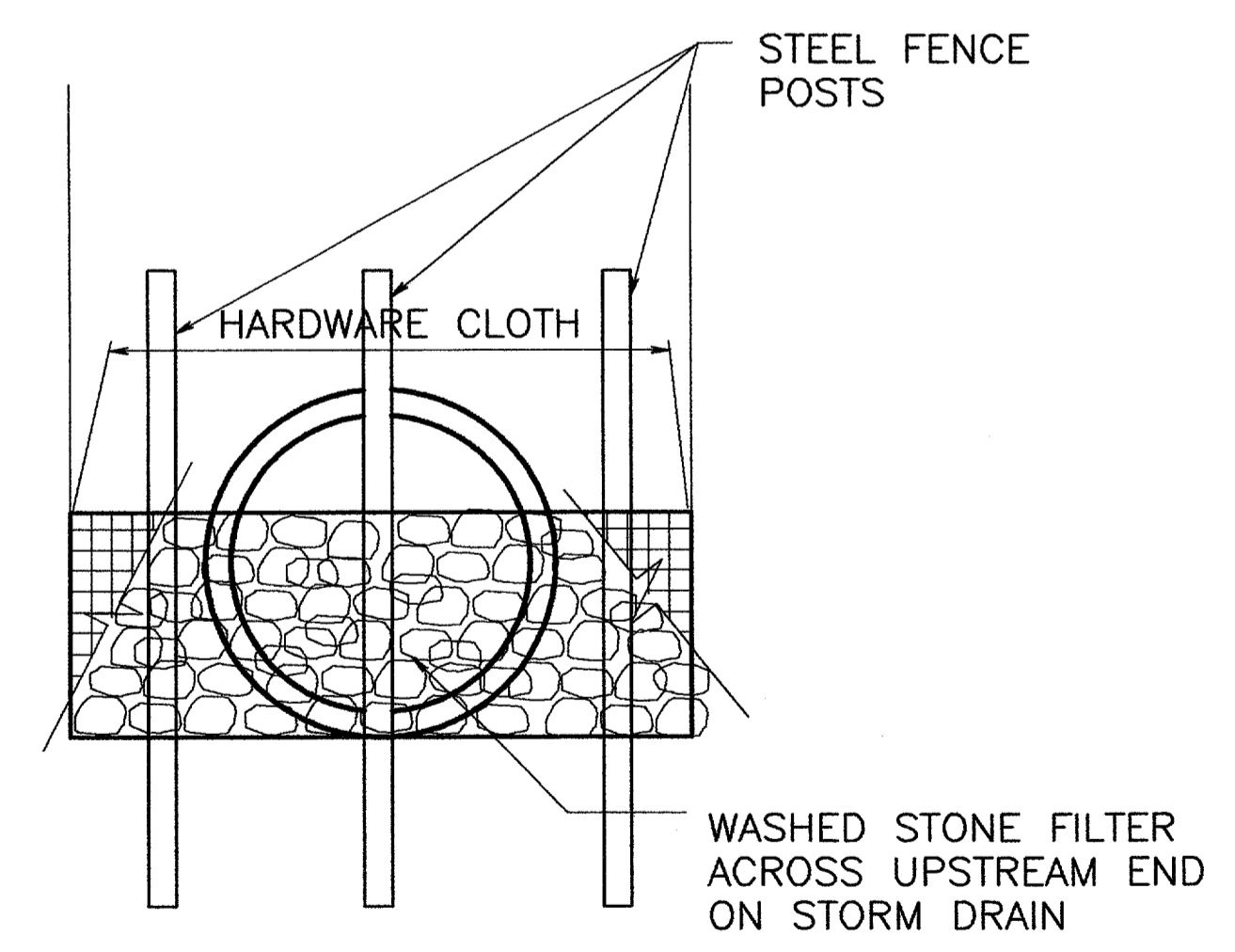


SECTION A-A

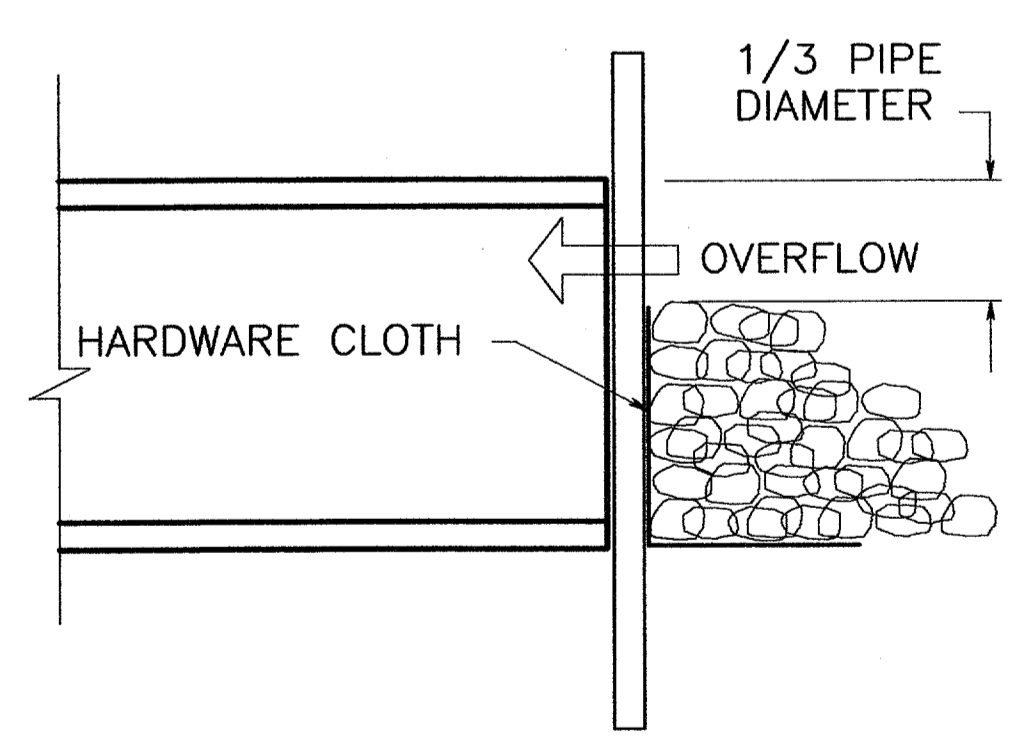


PLAN VIEW

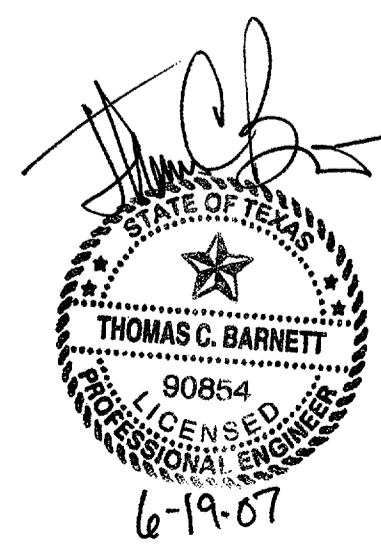
BLOCK/GRAVEL DROP INLET PROTECTION
NTS



STONE FILTER INLET PROTECTION FOR STORM DRAIN UNDER CONSTRUCTION AND FOR EXISTING CULVERTS
NTS

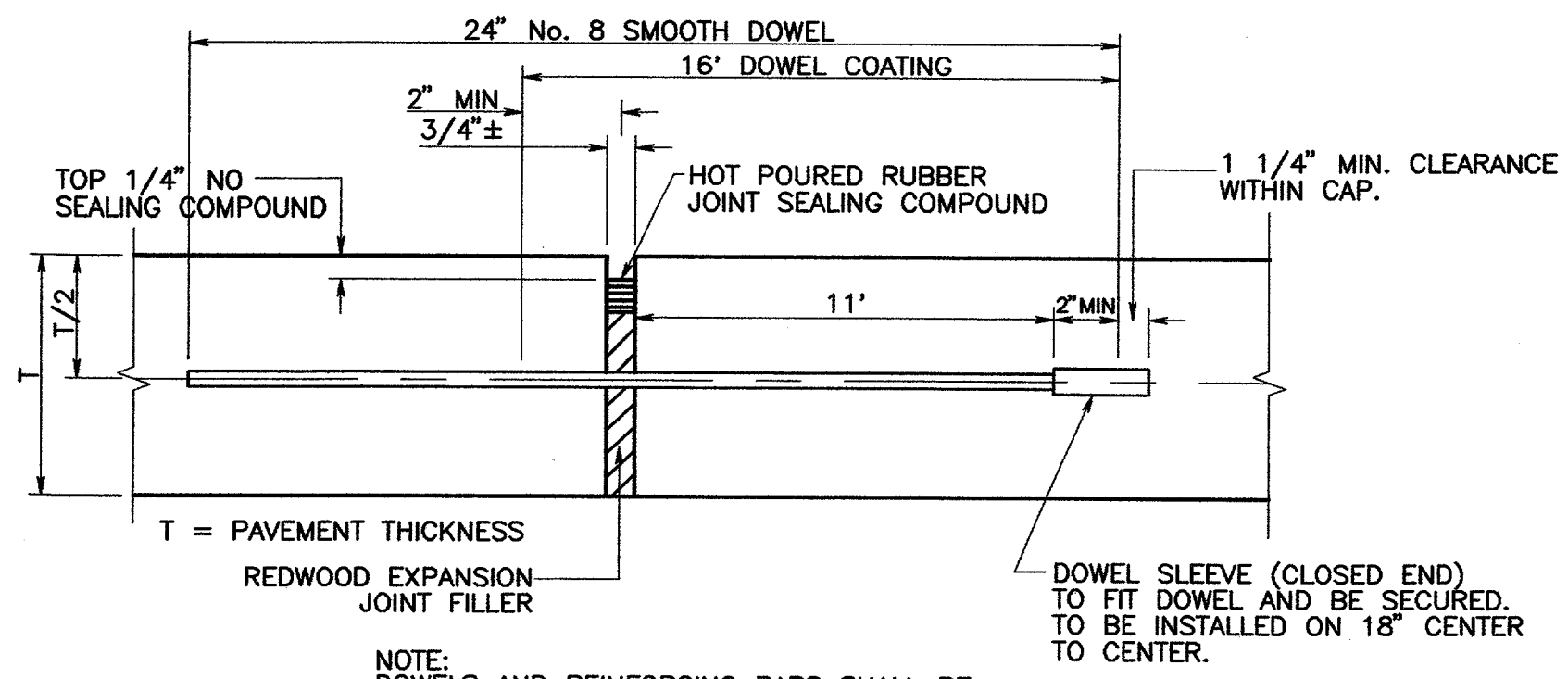


- STANDARD GENERAL NOTES:**
1. THE STORM WATER POLLUTION PREVENTION PLAN MUST BE IMPLEMENTED BY THE CONTRACTOR.
 2. THE CITY AND THE CONTRACTOR ARE EACH REQUIRED TO COMPLETE EROSION CONTROL INSPECTION REPORTS EVERY 2 WEEKS AND WITHIN 24 HOURS OF RAINFALL EVENTS. THE CONTRACTOR'S REPORTS MUST BE KEPT BY THE CONTRACTOR ON SITE. THE CONTRACTOR MUST AUTHORIZE AN APPROPRIATE PERSON IN THEIR FIRM TO CERTIFY THESE REPORTS.
 3. EROSION CONTROL DEVICES AS SHOWN ON THE EROSION CONTROL PLAN FOR THE PROJECT SHALL BE INSTALLED PRIOR TO THE START OF LAND DISTURBING ACTIVITIES ON THE PROJECT.
 4. ALL EROSION CONTROL DEVICES ARE TO BE INSTALLED IN ACCORDANCE WITH THE APPROVED PLANS AND SPECIFICATIONS FOR THE PROJECT. CHANGES ARE TO BE APPROVED BEFORE CONSTRUCTION BY THE TOWN OF ADDISON ENGINEERING DIVISION.
 5. IF THE EROSION & SEDIMENTATION CONTROL PLAN CANNOT CONTROL EROSION AND OFF-SITE SEDIMENTATION FROM THE TO BE REVISED AND/OR ADDITIONAL EROSION CONTROL DEVICES WILL BE REQUIRED ON SITE.
 6. THE CONTRACTOR IS RESPONSIBLE FOR MAINTAINING SURFACE DRAINAGE AND EROSION CONTROL FACILITIES ON SITE DURING CONSTRUCTION.
 7. THE CONTRACTOR IS RESPONSIBLE FOR KEEPING SIDEWALKS ADJACENT TO THE PROJECT FREE OF DIRT, MUD AND DEBRIS FROM THE CONSTRUCTION AT ALL TIMES.
 8. THE CONTRACTOR SHALL SOD ALL DISTURBED AREAS PRIOR TO FINAL ACCEPTANCE.



NO.	DATE	REVISION DESCRIPTION				
EROSION & SEDIMENTATION CONTROL DETAILS						
ADDISON AIRPORT ACCESS ROAD REALIGNMENT						
TOWN OF ADDISON DALLAS COUNTY, TEXAS						
TOWN OF ADDISON PUBLIC WORKS DEPARTMENT						
DESIGN	DRAWN	DATE	SCALE	NOTES	JOB NUMBER	SHEET
TCB	TCB	06/19/2007	NTS	TCB		C5

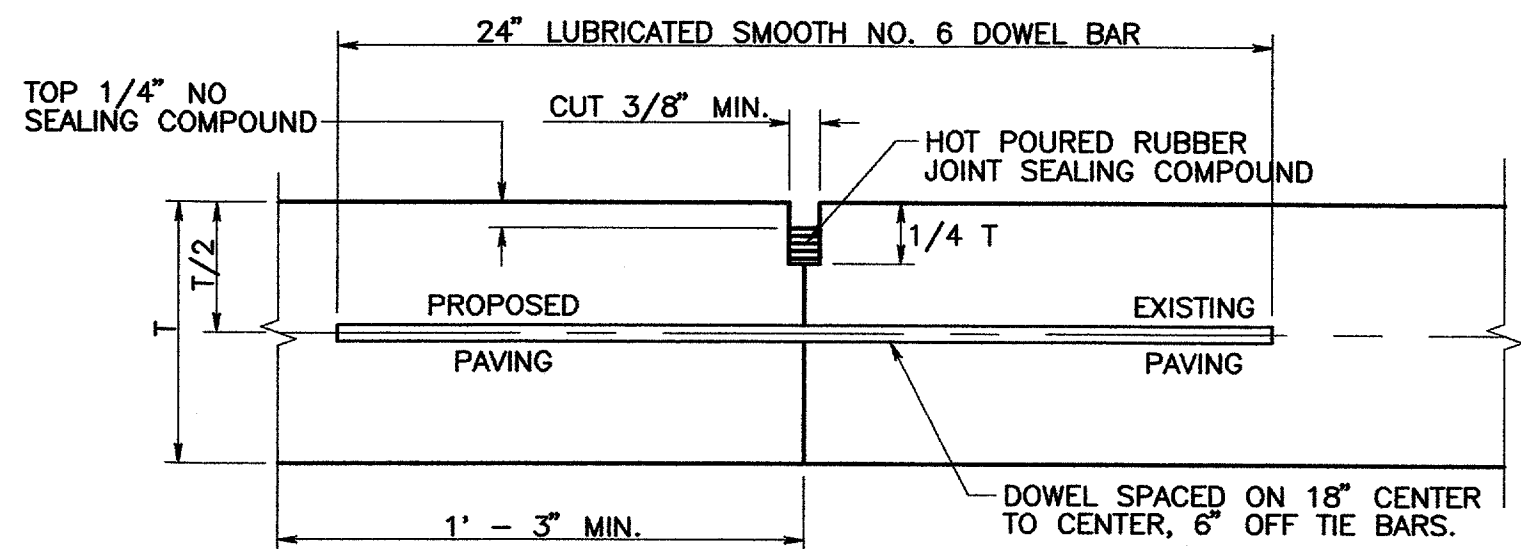
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NOTE:
DOWELS AND REINFORCING BARS SHALL BE SUPPORTED BY AN APPROVED DEVICE & MUST BE TIED OR CHAIRED ON EACH SIDE.

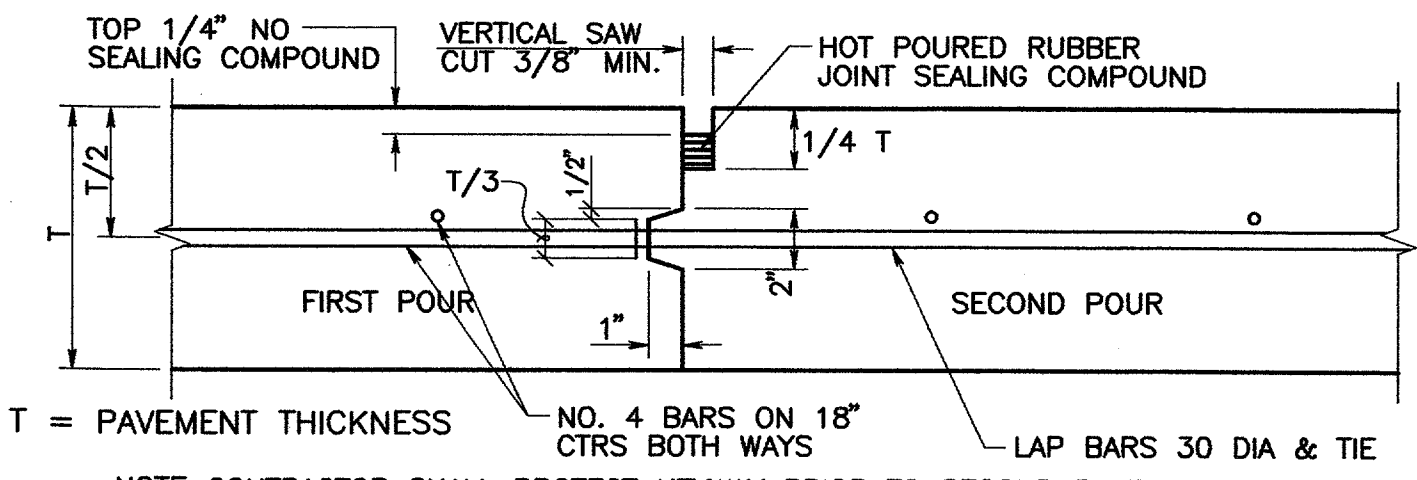
TRANSVERSE EXPANSION JOINT
(SPACED 600 FT. MAXIMUM, LOCATE AT INTERSECTIONS)
NTS

- PAVING GENERAL NOTES:**
- REPAIR AREAS SHALL BE LEVELLED TO MATCH EXISTING GRADE OF ADJACENT CONCRETE OR ASPHALT PAVEMENT.
 - IF ADDITIONAL FILL MATERIAL IS NEEDED, FLEXIBLE BASE MEETING THE REQUIREMENTS OF NCTCOG STANDARD SPECIFICATIONS 301.5 SHALL BE USED.
 - REMOVED PAVEMENT SHALL BE LEGALLY DISPOSED OF BY THE CONTRACTOR.
 - BAR CHAIRS OR AN APPROVED DEVICE SHALL BE FURNISHED.



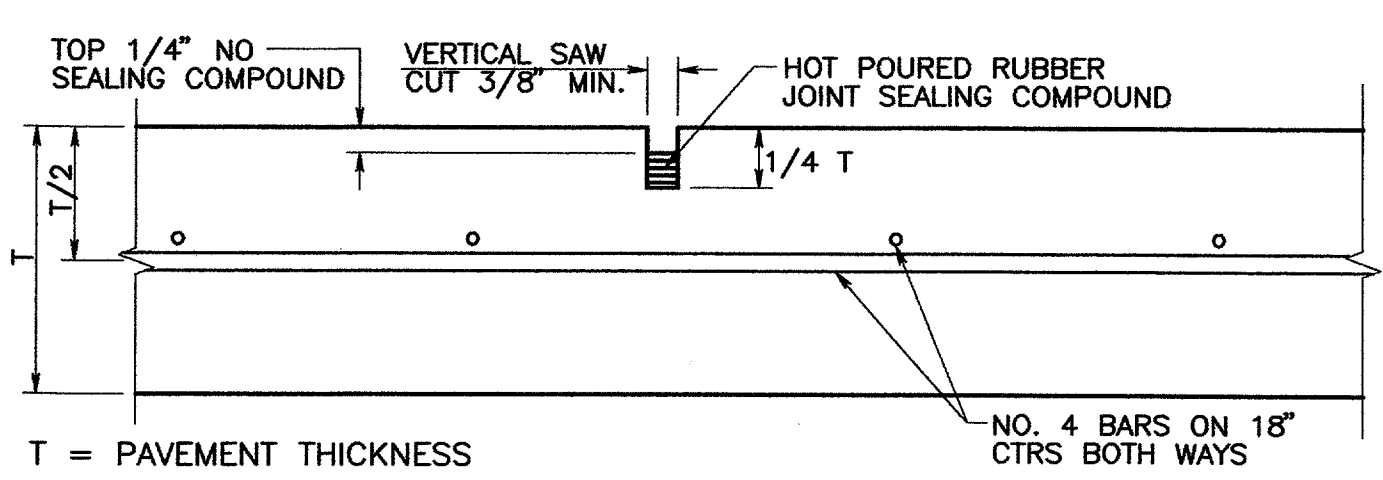
- T = PAVEMENT THICKNESS
- NOTES:
- NO. 5 SMOOTH DOWEL BAR MAY BE USED IN 6 INCH PAVEMENT.
 - LONGITUDINAL BUTT CONSTRUCTION MAY BE UTILIZED IN PLACE OF LONGITUDINAL HINGED (KEYWAY) JOINT AT CONTRACTOR'S OPTION.
 - DOWEL BARS SHALL BE DRILLED INTO PAVEMENT HORIZONTALLY BY USE OF A MECHANICAL RIC. DRILLING BY HAND IS NOT ACCEPTABLE. PUSHING DOWEL BARS INTO GREEN CONCRETE NOT ACCEPTABLE.

LONGITUDINAL BUTT JOINT
NTS

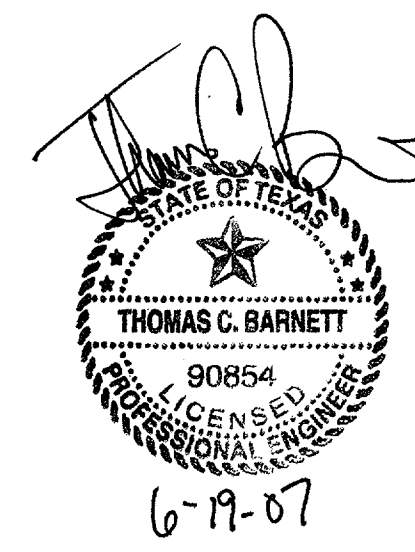


NOTE: CONTRACTOR SHALL PROTECT KEYWAY PRIOR TO SECOND POUR. IF LONGITUDINAL KEYWAY IS DAMAGED, CONTRACTOR SHALL REPAIR WITH THE USE OF LONGITUDINAL BUTT JOINT. DRILL AND GROUT DOWELS INTO FIRST POUR.

CONSTRUCTION JOINT FOR PAVEMENT
(TRANSVERSE AND LONGITUDINAL JOINTS)
NTS



SAWED DUMMY JOINT
NTS



NO.	DATE	REVISION DESCRIPTION				
PAVING DETAILS						
ADDISON AIRPORT ACCESS ROAD REALIGNMENT						
TOWN OF ADDISON DALLAS COUNTY, TEXAS						
TOWN OF ADDISON PUBLIC WORKS DEPARTMENT						
DESIGN	DRAWN	DATE	SCALE	NOTES	JOB NUMBER	SHEET
TCB	TCB	06/19/2007	NTS	TCB		C6

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