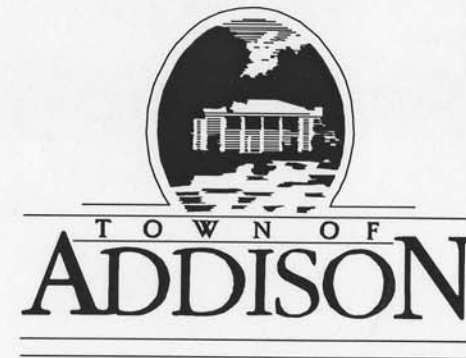


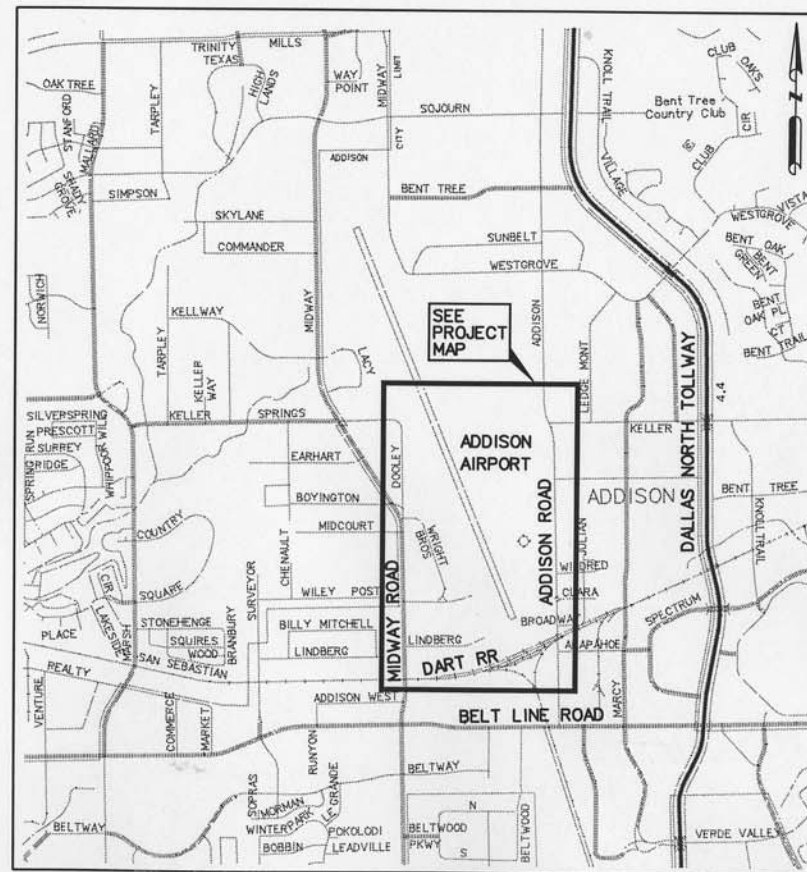
PLANS FOR THE CONSTRUCTION OF
GRADING, PAVING AND STORM SEWER
IMPROVEMENTS FOR

AIRPORT PARKWAY REALIGNMENT

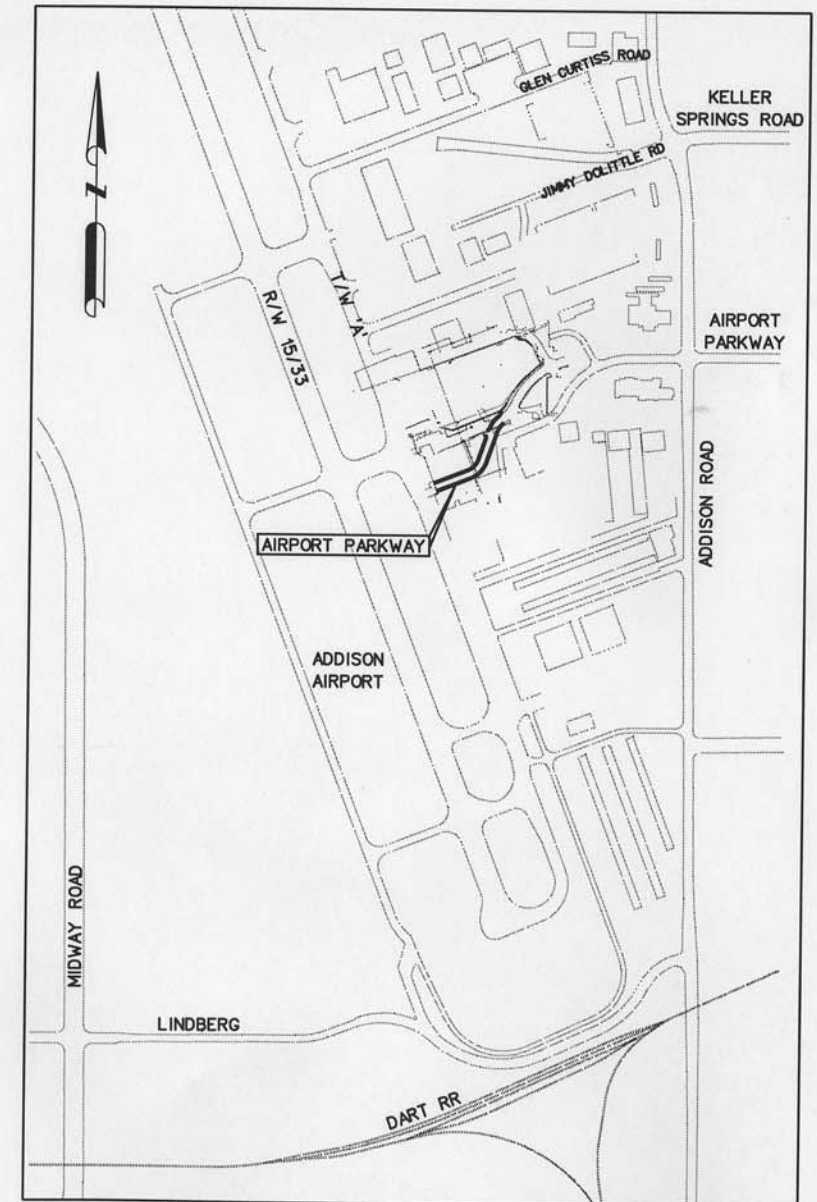
ADDISON AIRPORT



SHEET NO.	INDEX OF SHEETS
PL-1	COVER SHEET
N-1 TO N3	PROJECT LAYOUT
Q-1 TO Q-3	GENERAL NOTES
R-1	QUANTITY SHEETS
RM-1	ROW PLAN
TY-1	REMOVAL PLAN
PP-1 TO PP-2	PAVING TYPICAL SECTIONS
JL-1	PAVING PLAN AND PROFILES
PD-1	JOINT LAYOUT
EC-1	PAVING DETAILS
D-1 TO D-2	EROSION CONTROL PLANS
MD-1	STORM SEWER PLANS, PROFILES & DETAILS
	MISC DETAILS



LOCATION MAP
NOT TO SCALE



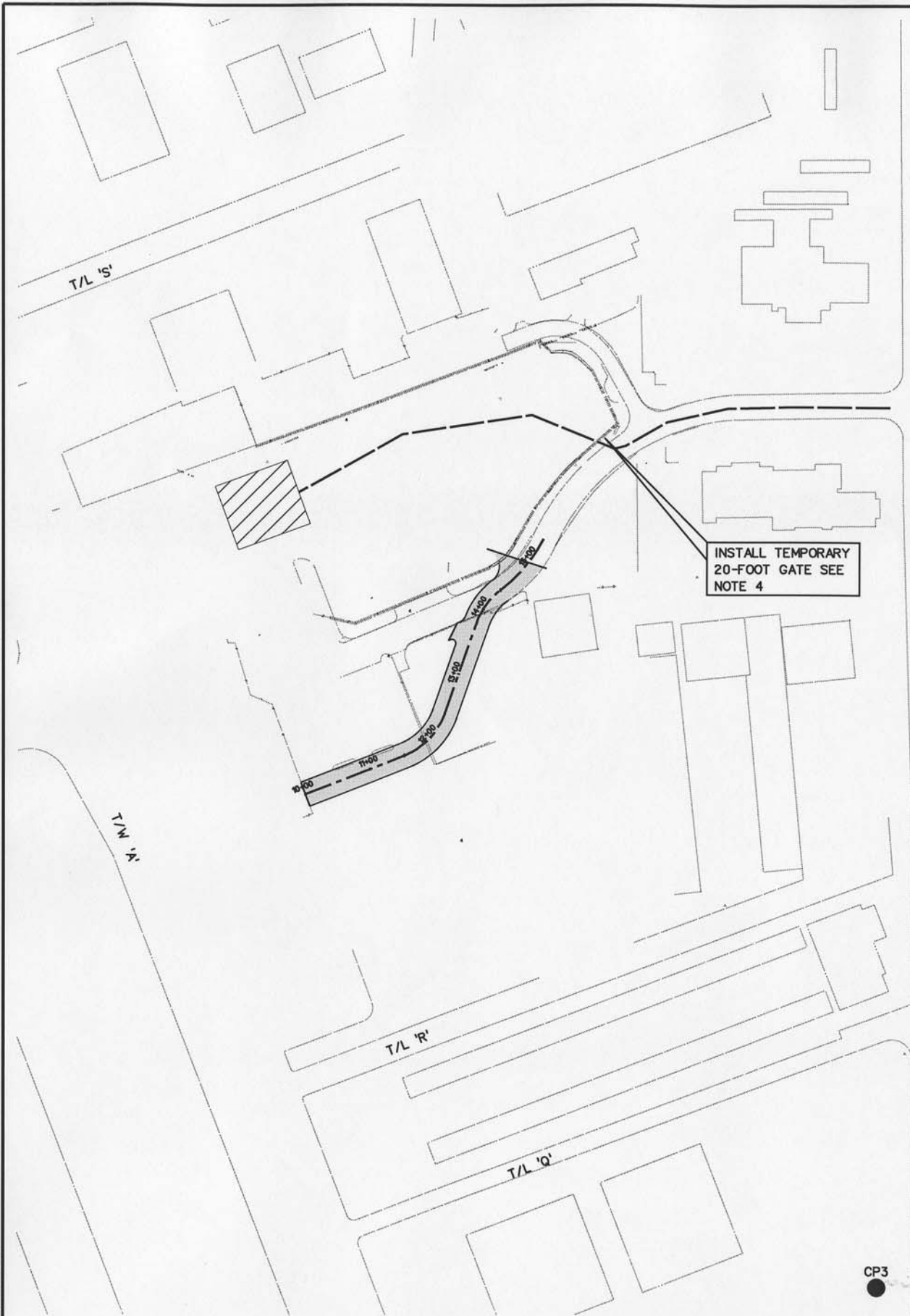
PROJECT MAP
NOT TO SCALE

OWNER:

TOWN OF ADDISON
DEPARTMENT OF PUBLIC WORKS
16801 WESTGROVE
P.O. BOX 144
ADDISON, TEXAS 75001
(972) 450-2886

ENGINEER:

HNTB CORPORATION
5910 WEST PLANO PARKWAY, SUITE 200
DALLAS, TEXAS 75093
(972) 661-5626



NOTES:

1. THE CONTRACTOR WILL NEED TO COORDINATE WITH ADDISON AIRPORT OPERATIONS THROUGHOUT THE CONSTRUCTION PROCESS. THE CONTRACTOR IS TO KEEP AIRPORT OPERATIONS UPDATED REGARDING THE SCHEDULE ON A WEEKLY BASIS. THE AIRPORT OPERATIONS WILL NEED 14 DAYS NOTICE BEFORE WORK CAN BEGIN ON THE NEXT PHASE OF THE PROJECT.
2. CONTRACTOR TO VERIFY THE LIMITS OF THE STAGING AREA WITH THE ADDISON AIRPORT PRIOR TO BEGINNING WORK.
3. CONTROL MONUMENTS (NAD83, TEXAS NORTH CENTRAL - 4202)
4. CONTRACTOR TO COORDINATE WITH THE TOWN OF ADDISON PRIOR TO INSTALLATION OF TEMPORARY GATE. EXISTING IRRIGATION SYSTEM TO BE CUT AND PLUGGED BY THE TOWN OF ADDISON WITHIN THE LIMITS OF THE GATE. IRRIGATION SYSTEM WILL BE RECONNECTED AFTER CONSTRUCTION IS COMPLETED. CONTRACTOR TO PROVIDE ASPHALT TRANSITION AGAINST THE BACK OF CURBS FOR TEMPORARY CONSTRUCTION ACCESS. ASPHALT WILL BE REMOVED AFTER CONSTRUCTION IS COMPLETE

HORIZONTAL AND VERTICAL CONTROL MONUMENTATION IS BASED ON A DAL-TECH, 2002, SURVEY PROVIDED BY THE ADDISON AIRPORT

CP#	NORTH	EAST	ELEV	DESC.
1	7,039,689.078	2,480,245.682	638.30	AA-1
2	7,041,501.126	2,480,144.730	641.92	AA-2
3	7,037,202.352	2,480,632.193	638.03	AA-3
4	7,035,957.986	2,479,444.822	632.41	AA-4
5	7,039,506.863	2,478,102.163	637.40	AA-5
6	7,043,849.691	2,477,497.499	652.26	AA-6
7	7,039,305.132	2,478,531.657	640.77	AB2786
8	7,040,772.156	2,478,253.153	633.02	AB7648
9	7,043,042.492	2,477,532.021	637.40	AH7852
10	7,038,354.486	2,478,875.925	642.02	CS1308

LEGEND

- CONTRACTOR HAUL ROUTE
- CONTRACTOR STAGING AREA
- CP# CONTROL POINT (SEE GENERAL NOTES FOR DETAILS)

1-SEP-2003 12:21
 \\39020\CCo\Sheets\app\01.dgn

NO.	DATE	REVISION	APPROV.	NO.	DATE	REVISION	APPROV.



HNTB ARCHITECTS ENGINEERS PLANNERS
 The HNTB Companies
 PROJECT NO. 39020
 DESIGN: JSN, CHECK: JDH
 DRAWN: GFS, CHECK: JSN
 DATE: SEPTEMBER 2003



TOWN OF ADDISON
 AIRPORT PARKWAY REALIGNMENT
 PROJECT LAYOUT
 SHEET

CONSTRUCTION CONTROL NOTES

I. GENERAL REQUIREMENTS

- A. IN ACCEPTING THE CONTRACTOR'S BID ON THIS PROJECT, THE TOWN OF ADDISON ASSUMES THE CONTRACTOR IS, OR HAS BECOME PRIOR TO SUBMITTING HIS/HER BID, KNOWLEDGEABLE OF THE CONSTRUCTION REQUIREMENTS, RESTRICTIONS, METHODS, MEANS, AND GENERAL CONSIDERATIONS OF DOING CONSTRUCTION WORK ON AN ACTIVE AIRPORT. WHILE A CONSCIENTIOUS AND GOOD FAITH EFFORT HAS BEEN MADE TO INCLUDE ALL APPROPRIATE AND RELEVANT REQUIREMENTS IN THESE PLANS AND SPECIFICATIONS, THE CONTRACTOR, AS A CONSIDERATION OF THIS CONTRACT, SHALL NOT USE A CLAIM OF LACK OF UNDERSTANDING OF THE COMPLEXITIES OF AIRPORT WORK AS A REASON TO CLAIM AGAINST THE TOWN OF ADDISON FOR ADDITIONAL TIME AND/OR COMPENSATION.
- B. THE GENERAL REQUIREMENTS PRESENTED IN THESE NOTES ARE SUPPLEMENTARY TO THOSE GIVEN IN THE VARIOUS SECTIONS OF THE SPECIFICATIONS. PARTICULAR ATTENTION IS DIRECTED TO THE TECHNICAL SPECIFICATIONS AND TO APPENDIX 'A'
- C. THE CONTRACTOR AND HIS/HER STAFF, INCLUDING SUBCONSULTANTS, WHO WILL BE DRIVING IN THE AIRPORT OPERATIONS AREA (AOA) WILL BE REQUIRED TO ATTEND THE ADDISON AIRPORT DRIVING SCHOOL PRIOR TO CONSTRUCTION. THEY WILL ALSO BE REQUIRED TO ADHERE TO ALL RULES AND REGULATIONS PRESENTED IN THE DRIVING SCHOOL.
- D. THE ENGINEER SHALL BE THE INDIVIDUAL, PARTNERSHIP, FIRM OR CORPORATION AUTHORIZED BY THE OWNER (SPONSOR) TO BE RESPONSIBLE FOR ENGINEERING SUPERVISION OF THE CONTRACT WORK AND ACTING DIRECTLY OR THROUGH THE AUTHORIZED REPRESENTATIVE.

II. SITE AVAILABILITY

- A. ALL WORK AREAS WILL BE AVAILABLE TO THE CONTRACTOR AT THE TIME OF NOTICE TO PROCEED (NTP) AS DETAILED ON THE PROPOSED CONSTRUCTION SCHEDULE CONTAINED IN THESE DOCUMENTS AND WITHIN THE AVAILABLE CLOSURE PERIODS.
- B. RUNWAY 15/33 AND TAXIWAY 'B' MUST BE OPENED FOR AIR TRAFFIC ARRIVALS/DEPARTURES EACH WEEK DAY MORNING AND EVENING. THE AVAILABLE TIME PERIOD EACH DAY FOR CONSTRUCTION ACTIVITIES DURING A TAXIWAY CLOSURE SHALL BE SET FROM 10:00 P.M. UNTIL 6:00 A.M. TYPICALLY THE OTHER TAXIWAYS WILL BE AVAILABLE TO THE CONTRACTOR DURING NORMAL BUSINESS HOURS.
- C. DUE TO WEATHER, WIND DIRECTION OR EMERGENCIES THE CONTRACTOR MAY BE DELAYED, RE-DIRECTED TO ANOTHER AREA OR SHUT DOWN.
- D. ALTHOUGH NOT ANTICIPATED FOR THIS PROJECT, FOR THE CONTRACTOR TO OPERATE WITHIN 250 FEET OF THE CENTERLINE OF ANY RUNWAY, THAT RUNWAY MUST BE CLOSED BY AIR TRAFFIC CONTROL TOWER (ATCT). ALL RUNWAY CLOSURES SHALL BE CONDUCTED BY AIRPORT PERSONNEL. A RUNWAY CLOSURE REQUIRES A 24-HOUR NOTICE TO AIRPORT OPERATIONS.
- E. THE CONTRACTOR SHALL NOT ENTER THE AIRPORT OPERATIONS AREA (AOA) NOR LEAVE HIS DESIGNATED AOA WORK AREA WITHOUT AN ESCORT FROM AIRPORT OPERATIONS OR PERMISSION FROM OPERATIONS TO PERFORM HIS OWN ESCORT.

III. MAINTENANCE OF VEHICULAR TRAFFIC

A. VEHICULAR TRAFFIC

- 1. THE CONTRACTOR IS ADVISED THAT AIRPORT RUNWAY 15/33, AND ALL TAXIWAYS WILL BE ACTIVE DURING THIS CONTRACT. THE CONTRACTOR WILL BE EXPECTED TO CONDUCT THE WORK SUCH THAT THE SAFETY OF OPERATIONS IS NOT DIMINISHED AND THAT VEHICULAR FLOW IS MAINTAINED AT ALL TIMES. THE CONTRACTOR IS ADVISED, AND SHALL ACCEPT AS AN IMPORTANT CONSIDERATION OF THE WORK, THAT THE MAINTENANCE OF SAFE AND EFFICIENT ACCESS TO THE AIRPORT VIA THE ROADWAY SYSTEM IS AN INTEGRAL PART OF THE WORK. ALL TRAFFIC MAINTENANCE SHALL BE DONE IN CONFORMANCE TO THE LATEST EDITION OF THE MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES.
- 2. ALL EXISTING ROADS OR PAVEMENTS USED AS ACCESS/HAUL ROADS SHALL BE MAINTAINED AND IMPROVED AS REQUIRED. THE ROAD OR PAVEMENT CONDITIONS WILL BE INSPECTED JOINTLY BY THE ENGINEER AND CONTRACTOR PRIOR TO AND AT THE END OF THE PROJECT. THE CONDITION OF THESE ROADS AND PAVEMENTS SHALL BE EQUAL OR BETTER THAN AT THE START OF THE PROJECT AS DETERMINED BY THE ENGINEER. MECHANICAL BROOMS WILL ALSO BE REQUIRED FOR THESE EXISTING PAVEMENT ACCESS ROADS AT ALL TIMES.

B. AIRCRAFT TRAFFIC

- 1. THE CONTRACTOR IS ADVISED THAT ALL AIRPORT TAXIWAYS, RUNWAYS AND RAMPS WILL BE ACTIVE DURING THIS CONTRACT WITH THE EXCEPTION OF THOSE PAVEMENTS CLOSED EACH DAY DURING THE DEFINED CLOSURE PERIODS. THE CONTRACTOR WILL BE EXPECTED TO CONDUCT THE WORK SUCH THAT THE SAFETY OF OPERATIONS IS NOT DIMINISHED AND THAT AIRCRAFT FLOW IS MAINTAINED AT ALL TIMES. THE CONTRACTOR IS FURTHER ADVISED, AND SHALL ACCEPT AS AN IMPORTANT CONSIDERATION OF THE WORK, THAT THE MAINTENANCE OF SAFE AND EFFICIENT OPERATION OF THE AIRPORT OPERATIONS AREA (AOA) IS AN INTEGRAL PART OF THE WORK. ALL CONSTRUCTION INTERFACE WITH AIRCRAFT PAVEMENTS, TAXIWAY CROSSINGS, AND SECURITY REQUIREMENTS AS CONTAINED IN FAA ADVISORY CIRCULAR 150/5370-2E WILL APPLY.
- 2. AOA NIGHT WORK SHALL BE DEFINED AS 10 P.M. TO 6 A.M. CONTRACTOR SHALL BEGIN CLEAN UP OPERATIONS NO LATER THAN 5:30 A.M. TO HAVE ALL AOA PAVEMENT OPEN NO LATER THAN 6:00 A.M.
- 3. AOA WEEKEND WORK SHALL BE DEFINED AS BEGINNING AT 9:00 A.M. SATURDAY AND MAY CONTINUE UNTIL 4:30 A.M. MONDAY.

IV. WORK WITHIN AIRPORT OPERATIONS AREA (AOA)

A. GENERAL REQUIREMENTS

ALL WORK TO BE PERFORMED WILL BE INSIDE OR ADJACENT TO THE AIRPORT OPERATIONS AREA (AOA). CONSEQUENTLY ALL WORK MUST BE PERFORMED SUCH THAT THE SECURITY OF THE AOA IS MAINTAINED.

THE CONTRACTOR WILL BE REQUIRED TO OBTAIN ALL VEHICLE PASSES AND TEMPORARY PERSONNEL SECURITY BADGES TO SATISFY ALL AIRPORT SECURITY REQUIREMENTS PRIOR TO BEGINNING WORK WITHIN THE AOA.

IN ADDITION, THE CONTRACTOR IS ADVISED THAT CERTAIN RULES AND RESTRICTIONS, AS CONTAINED IN FAA ADVISORY CIRCULAR 150/5370-2D AND AUGMENTED BY THESE PLANS AND SPECIFICATIONS, WILL APPLY TO THE WORK. THE CONTRACTOR SHALL BECOME FAMILIAR WITH ALL REQUIREMENTS APPLICABLE TO AIRPORT CONSTRUCTION AND COOPERATE WITH THE ENGINEER IN MAINTAINING A SAFE CONSTRUCTION SITE WHICH IS COMPATIBLE WITH AIRCRAFT AND AIRPORT OPERATIONS.

THE CONTRACTOR'S ACCESS TO THE JOB SITE AND WITHIN THE AIRCRAFT OPERATIONS AREA (AOA), SHALL BE LIMITED THROUGH THE EXISTING SECURITY GATES. THE CONTRACTOR WILL BE REQUIRED TO MAINTAIN SECURITY AT THE GATES THROUGHOUT THE DURATION OF THE PROJECT. GATES SHALL BE MANNED BY A SECURITY GUARD AT ALL TIMES WHEN THE SECURITY OF THE AIRCRAFT OPERATIONS AREA (AOA) IS BREACHED. THE GUARD WILL BE TRAINED BY THE CONTRACTOR IN THE USE OF THE RADIOS AND THE SECURITY REQUIREMENTS OF THE AOA. WHEN THE GATE IS UNGUARDED FOR WHATEVER REASON OR LENGTH OF TIME, IT SHALL BE LOCKED WITH LOCKS PROVIDED BY THE CONTRACTOR AND APPROVED BY THE ENGINEER. THE CONTRACTOR WILL BE LIABLE FOR ANY FINES ISSUED BY THE FAA FOR SECURITY (OR OTHER) VIOLATIONS FOR WHICH IT IS CITED. AN ASSESSMENT OF \$1,000 MAY BE ASSESSED FOR EACH AND EVERY OCCURRENCE WHERE THE GATE IS LEFT UNGUARDED AND UNLOCKED.

CONTRACTOR SHALL SUBMIT HIS 3 WEEK ROLLING SCHEDULE FOR DAILY WORK AND CLOSURE OF THE AIRFIELD PAVEMENTS. THE SCHEDULE SHALL BE APPROVED BY THE ENGINEER AND AIRPORT OPERATIONS PRIOR TO START OF WORK. DUE TO WEATHER, WIND DIRECTION OR UNFORESEEN EMERGENCIES THE CONTRACTOR MAY BE REQUIRED TO PULL OFF ANY GIVEN AREA AT SHORT NOTICE. EFFORTS WILL BE MADE TO ALLOW WORK IN OTHER AREAS BUT WILL NOT BE GUARANTEED. IF THE CONTRACTOR IS NOT ALLOWED TO WORK IN ANOTHER AREA IT MAY BE COUNTED AS A WEATHER DAY WITH NO OTHER COMPENSATION, ACCORDING TO THE ENGINEERS APPROVAL.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROVIDING HIS/HER OWN PROJECT OFFICE, TOILET FACILITIES AND OTHER NECESSARY BUILDINGS OR SHELTERS. THE AIRPORT SPONSOR WILL NOT PROVIDE ANY FACILITIES TO THE CONTRACTOR DURING CONSTRUCTION.

THE CONTRACTOR SHALL SUBMIT HIS/HER CONSTRUCTION WORK SCHEDULE TO THE ENGINEER PROJECTING HIS UPCOMING WORK FOR THE NEXT THREE WEEKS. THE ENGINEER AND AIRPORT MANAGER SHALL REVIEW THIS PLAN WEEKLY WITH THE CONTRACTOR SO THAT EVERYONE IS AWARE OF UPCOMING CONSTRUCTION EVENTS.

THE CONTRACTOR SHALL MAINTAIN A CLEAN AND SAFE CONSTRUCTION WORK AREA. THE CONTRACTOR SHALL PERFORM CLEAN-UP OPERATIONS ON A DAILY BASIS.

THE CONTRACTOR SHALL NOT DEVIATE FROM THE APPROVED CONSTRUCTION SEQUENCE WITHOUT FIRST OBTAINING APPROVAL FROM THE ENGINEER. THE CONTRACTOR SHALL BRING IN HIS/HER EQUIPMENT AND SHALL KEEP THAT EQUIPMENT ON SITE FOR THE DURATION OF ITS FUNCTION.

THE CONTRACTOR SHALL HAVE SUFFICIENT EQUIPMENT AND PERSONNEL ON SITE TO ACCOMPLISH EFFICIENT AND PROMPT CONSTRUCTION OF THE VARIOUS WORK ITEMS, INCLUDING WORK ON MORE THAN ONE WORK ITEM SIMULTANEOUSLY.

NO TRENCHES IN OR DIRECTLY ADJACENT TO OPERATIONAL PAVEMENT SHALL REMAIN OPEN OVERNIGHT OR WHEN THE CONTRACTOR FINISHES WORK FOR THE DAY IN THE AREA. TRENCHES NOT BACKFILLED SHALL BE COVERED WITH STEEL PLATES TO ALLOW FOR SAFE PASSAGES BY AIRCRAFT ACROSS THE TRENCH, IF APPROVED BY THE AIRPORT MANAGER.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR REPAIR OF ANY DAMAGE TO EXISTING FACILITIES NOT DESIGNATED FOR RECONSTRUCTION OR REPLACEMENT AT HIS/HER EXPENSE DAMAGE TO EXISTING PAVEMENTS DUE TO MOVING OR USAGE OF HEAVY EQUIPMENT OR THE TRANSPORT OF MATERIALS TO OR ON THE SITE SHALL BE REPAIRED TO EQUAL OR BETTER QUALITY BY THE CONTRACTOR AT HIS/HER OWN EXPENSE.

THE CONTRACTOR SHALL VIDEO TAPE THE ENTIRE WORK AREA AFTER THE PRE-CONSTRUCTION MEETING AND PRIOR TO THE MOBILIZATION OF PERSONNEL AND EQUIPMENT. THE CONTRACTOR SHALL PROVIDE ONE (1) COPY OF THIS VIDEO TAPE(S) TO BOTH THE AIRPORT MANAGER AND THE ENGINEER. IN AREAS WHERE CONSTRUCTION EQUIPMENT CROSSES EXISTING PAVEMENTS, THE CONTRACTOR SHALL TAKE PICTURES OR VIDEO OF THE PAVEMENT PRIOR TO COMMENCING OPERATIONS. THE CONTRACTOR SHALL PROVIDE THE ENGINEER AND AIRPORT MANAGER WITH ONE COPY EACH OF THE PHOTOGRAPH OR VIDEO TAPE(S) TAKEN. THIS DOCUMENTATION SHALL BE USED TO DETERMINE THE AMOUNT OF DAMAGE, IF ANY, CAUSED TO THE PAVEMENTS AND OTHER STRUCTURES BY THE CONSTRUCTION EQUIPMENT CROSSINGS AND THE QUALITY OF CONSTRUCTION WHICH SHALL BE REQUIRED FOR THE REPAIRS. NO SEPARATE BID ITEM WILL BE SET UP FOR THIS ACTIVITY, IT SHALL BE CONSIDERED SUBSIDIARY TO OTHER BID ITEMS.

CONSTRUCTION EQUIPMENT AND VEHICLES SHALL TRAVEL A MINIMUM AMOUNT ON NEWLY CONSTRUCTED PAVEMENTS SO THAT THE NEWLY CONSTRUCTED AREAS WILL NOT BE DAMAGED.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR REPAIR OF ANY DAMAGE TO UNDERGROUND CABLES ENCOUNTERED. NO DIGGING SHALL BE PERFORMED WITHOUT FIRST CONTACTING THE AIRPORT MANAGER. ANY DAMAGE TO FAA CABLE OR OTHER UNDERGROUND CABLING OR FACILITIES SHALL BE REPAIRED IN ACCORDANCE WITH THE APPLICABLE FAA SPECIFICATIONS AND IN A MANNER ACCEPTABLE TO AIRPORT MANAGER, AND THE ENGINEER.

ALL ABOVE GROUND AND GROUND LEVEL ELECTRICAL RELATED APPURTENANCES (I.E., RUNWAY LIGHTS, CABLE BOXES, CABLE AND/OR DUCT MARKERS, CONDUIT, ETC.) SHALL BE PROTECTED AT ALL TIMES. ANY DAMAGE DONE TO SAID APPURTENANCES BY THE CONTRACTOR SHALL BE REPAIRED IMMEDIATELY TO LIKE QUALITY AT THE CONTRACTOR'S EXPENSE. THE REPAIRS SHALL BE PERFORMED TO THE SATISFACTION OF THE AIRPORT MANAGER, AND THE ENGINEER.

1-SEP-2003 12:23 \\39020\cadd\Sheets\opnt01.dgn

NO.	DATE	REVISION	APPROV.	NO.	DATE	REVISION	APPROV.



HNTB ARCHITECTS ENGINEERS PLANNERS
The HNTB Companies

DESIGN CHECK	JSN	PROJECT NO.	39020
CHECK	JDH		
DRAWN CHECK	GFS		
	JSN		
DATE	SEPTEMBER 2003		
SCALE			



TOWN OF ADDISON		SHEET
AIRPORT PARKWAY REALIGNMENT		
GENERAL NOTES		
TOWN OF ADDISON, TEXAS		N-1

CONTRACTOR SHALL PROVIDE A MOTORIZED MECHANICAL SWEEPER, ALONG WITH A FOREIGN OBJECTS DAMAGE (FOD) PLAN, PRIOR TO BEGINNING WORK. THE FOD PLAN SHALL BE PRESENTED BY THE CONTRACTOR AT THE PRE-CONSTRUCTION MEETING. THE FOD PLAN IS SUBJECT TO APPROVAL BY THE AIRPORT MANAGER.

CONSTRUCTION WORKERS WILL NOT BE ALLOWED TO ESTABLISH OVERNIGHT RESIDENCE ON THE PREMISES. ALL CONSTRUCTION WORKERS SHALL LEAVE THE CONSTRUCTION SITE AND AIRPORT PROPERTY AT THE END OF THEIR WORK PERIOD.

ALL SAWCUTTING ON THIS PROJECT SHALL BE SUBSIDIARY TO THE VARIOUS BID ITEMS ON THE PROJECT.

WORK CANNOT COMMENCE UNTIL:

- A). SUFFICIENT BARRICADES ARE IN PLACE TO CONFINE THE AREA AND CREATE A BARRIER BETWEEN AIRCRAFT AND VEHICLE MOVEMENT AREAS AND THE CONSTRUCTION AREA.
- B). ALL SAFETY EQUIPMENT FOR PERSONNEL AND CONSTRUCTION EQUIPMENT IS IN PLACE AND OPERABLE.
- C). A NOTICE TO PROCEED HAS BEEN ISSUED BY THE TOWN OF ADDISON TO THE CONTRACTOR.

ALL MATERIAL SUBMITTALS FOR ITEMS TO BE USED IN CONSTRUCTION OF THE PROJECT SHALL BE SUBMITTED TO THE ENGINEER FOR REVIEW AND APPROVAL 21-DAYS PRIOR TO COMMENCEMENT OF WORK. THREE-WEEK LOOK AHEAD SCHEDULES TO BE PROVIDED ON A WEEKLY BASIS AND BE EXPANDED AND ENHANCED VERSIONS OF THE PROJECT SCHEDULE. WEEKLY SCHEDULES TO BE PRESENTED BY THE CONTRACTOR AT THE WEEKLY CONSTRUCTION MEETINGS.

A COMPLETE PROJECT SCHEDULE SHALL BE PROVIDED AT THE PRE-CONSTRUCTION MEETING AND PRESENTED BY THE CONTRACTOR TO THE MEETING ATTENDEES. RUNWAY SHUT-DOWN DATES MAY BE DETERMINED AT THE PRE-CONSTRUCTION MEETING, OR AT A LATER TIME, AS APPROVED BY THE AIRPORT MANAGER.

INTERIM PROJECT SCHEDULES TO BE PROVIDED ON THE FIRST OF EACH MONTH AND INCLUDE ORIGINAL BASELINE, UPDATED TO CURRENT CONSTRUCTION ACTIVITY.

B. WORK ADJACENT TO AND ON RUNWAYS

THE CONTRACTOR SHALL:

- 1. NOT ALLOW ANY WORK TO BE UNDERTAKEN INSIDE OF, OR ANY PERSONNEL, EQUIPMENT, OR VEHICLES TO ENTER THE TAXIWAY OR RUNWAY RESTRICTED ZONE (OBJECT FREE AREA) WHILE ANY TAXIWAY OR RUNWAY IS "OPEN". THE RESTRICTED ZONE FOR TAXIWAY 'B' IS DEFINED AS THE AREA WITHIN 93- FEET OF THE TAXIWAY CENTERLINE. THE ENGINEER SHALL, WHEN REQUESTED BY THE CONTRACTOR AND WHEN IN CONFORMANCE TO THE APPROVED CONSTRUCTION SCHEDULE, ARRANGE FOR THE CLOSURE OF AFFECTED TAXIWAYS AND RUNWAYS. A MINIMUM OF 24 HOURS ADVANCE NOTICE IS REQUIRED TO SCHEDULE ANY AIRFIELD PAVEMENT CLOSURES.
- 2. BE RESPONSIBLE FOR PROVIDING ALL TEMPORARY LIGHTING AND OTHER SPECIAL EQUIPMENT THAT MAY BE NEEDED FOR NIGHTTIME CONSTRUCTION IF REQUESTED BY CONTRACTOR AND APPROVED BY ENGINEER. THE COST OF THIS EQUIPMENT SHALL BE INCLUDED IN THE GENERAL COST OF THE WORK, NO SEPARATE PAYMENT WILL BE MADE.

REQUEST THROUGH THE ENGINEER AND IN CONFORMANCE WITH THE PHASING PLANS THE CLOSURES OF ANY AIRFIELD PAVEMENTS. THE AIRPORT MANAGER, IN COOPERATION WITH THE FAA, WILL CLOSE THE TAXIWAYS AND RUNWAYS TO AIRCRAFT ACTIVITY TO ALLOW CONSTRUCTION ACTIVITY WITHIN THE RUNWAY/TAXIWAY RESTRICTION ZONE. THESE ZONES ARE DEFINED AS AN AREA WITHIN 250- FEET OF A RUNWAY CENTERLINE AND 93- FEET OF A TAXIWAY 'A' CENTERLINE. THE CLOSURE PERIOD WILL BE SUBJECT TO THE FOLLOWING CRITERIA:

THE CLOSURE PERIODS WILL BE SCHEDULED IN GENERAL CONFORMANCE WITH THE PHASING PLANS. ADDITIONALLY, THE RUNWAY CLOSURE WILL BE DEPENDENT UPON THE WEATHER FORECAST; THE CONTRACTOR BEING FULLY MOBILIZED TO PURSUE THE WORK AT MAXIMUM EFFICIENCY (IN THE ENGINEER'S OPINION); AND ANY UNFORESEEN EMERGENCY WHICH, IN THE ENGINEER'S AND TOWN'S OPINIONS, MAKES THE CLOSURE UNFEASIBLE.

THE CONTRACTOR WILL BE REQUIRED TO FURNISH ALL BARRIERS, BARRICADES, AND TAXIWAY/RUNWAY CLOSED SYMBOLS AS NECESSARY. THESE ITEMS WILL NOT BE PAID FOR SEPARATELY, BUT WILL BE INCLUDED IN THE PRICE BID FOR M-101, MOBILIZATION.

AT THE CONCLUSION OF ANY TAXIWAY CLOSURE, THE TAXIWAY SAFETY AREA AND ASSOCIATED NAVAID CRITICAL AREA SHALL BE RESTORED TO ORIGINAL ELEVATIONS AND GRADES. THE CONTRACTOR MAY NOT CONCLUDE A DAYS WORK AND REMOVE HIS FORCES FROM A WORK AREA WITHOUT RESTORING THE TAXIWAY OR EXISTING TAXIWAY TO OPERATIONAL STATUS.

V. STAGING AREAS - GENERAL REQUIREMENTS

- A. THE LOCATION AND SIZE OF THE CONTRACTOR'S STAGING AREA IS SHOWN FOR REFERENCE ONLY. THE ACTUAL SIZE AND EXACT LOCATION WILL BE ESTABLISHED PRIOR TO CONSTRUCTION. THE FOLLOWING REQUIREMENTS WILL APPLY, HOWEVER:

ACCESS TO THE STAGING AREA SHALL BE OFF RICHARD BYRD DRIVE. ALL CONSTRUCTION SITE ACCESS SHALL BE VIA THE SECURITY GATE ADJACENT TO THE CUSTOMS FACILITY AND THROUGH THE STAGING AREA OR AS DIRECTED BY AIRPORT OPERATIONS. THE CONTRACTOR SHALL NOT PREVENT ACCESS TO THE SECURITY GATE BY AIRPORT STAFF, FAA PERSONNEL, THE ENGINEER OR THE ENGINEER'S SUBCONSULTANTS. ALL OF THE CONTRACTOR'S ACTIVITY WILL BE STAGED FROM THIS AREA. ALL FIELD OFFICE AND EMPLOYEE PARKING WILL BE LIMITED TO THE AREA ALONG RICHARD BYRD DRIVE. ONLY VEHICLES REQUIRED TO PERFORM LEGITIMATE FUNCTIONS BY THE CONTRACTOR ON THIS PROJECT AND FULLY COVERED UNDER THE CONTRACTOR'S INSURANCE AS SPECIFIED WITHIN THE SPECIAL PROVISIONS, WILL BE ALLOWED WITHIN THE AOA. THE CONTRACTOR WILL BE RESPONSIBLE FOR TRANSPORTING EMPLOYEES FROM THE STAGING AREA TO AND FROM THE WORK AREAS.

IT IS NOT ANTICIPATED THAT THE AIRPORT'S OBSTRUCTION HEIGHT REQUIREMENTS, AS DEFINED IN FAA'S FAR PART 77, WILL RESTRICT THE CONTRACTOR'S ACTIVITIES IN THE STAGING AREA. IT IS REQUIRED, HOWEVER, THAT THIS ASSUMPTION BE VERIFIED BY THE CONTRACTOR THROUGH THE ENGINEER PRIOR TO BEGINNING WORK.

THE CONTRACTOR WILL BE REQUIRED TO OBSERVE ALL EXISTING TRAFFIC FLOW DIRECTIONS WHEN ENTERING AND LEAVING THE STAGING AREA. NO COUNTER FLOW WILL BE ALLOWED ANY TIME.

THE CONTRACTOR SHALL PROVIDE PROFESSIONALLY-MADE SIGNS INDICATING THE NAME OF THE CONTRACTOR AND A MESSAGE DIRECTING ALL MATERIALS DELIVERIES TO THE STAGING AREA.

VI. SCHEDULES

- A. THE WORK IN THIS CONTRACT HAS BEEN SEQUENCED IN A MANNER WHICH WILL MINIMIZE DISRUPTION TO NORMAL AIRPORT OPERATION AND COMPLY WITH APPROPRIATE FAA SAFETY CRITERIA. THE CONTRACTOR'S DETAILED SCHEDULING OF HIS WORK MUST BE DONE WITHIN THE FRAMEWORK OF THE SPECIFIED SEQUENCE OF CONSTRUCTION AND THESE CONSTRUCTION CONTROL NOTES. FAILURE BY THE CONTRACTOR TO APPRECIATE AND UNDERSTAND THE COMPLEXITY OF THE WORK IN HIS SCHEDULING WILL NOT BE REASON FOR HIM/HER TO CLAIM FOR ADDITIONAL TIME AND/OR COMPENSATION.
- B. BECAUSE OF THE CIRCUMSTANCES OF THIS WORK, THE CONTRACTOR IS ADVISED THAT THE CONTRACT REQUIREMENTS FOR SCHEDULING OF THE WORK AND THE PENALTIES FOR FAILURE TO MAINTAIN AN APPROVED REALISTIC CONSTRUCTION SCHEDULE WILL BE STRICTLY ENFORCED. SHOULD THE CONTRACTOR FAIL TO MAINTAIN A CONSTRUCTION SCHEDULE THAT REASONABLY REFLECTS ACTUAL AND ANTICIPATED PROGRESS, ANY ADDITIONAL COSTS NECESSARY TO RESOLVE CONFLICTS WITH THE WORK THAT, IN THE ENGINEER'S OPINION, COULD HAVE OTHERWISE BEEN FORESEEN AND AVOIDED, WILL BE BORNE BY THE CONTRACTOR.

VII. DUST CONTROL

- A. THE CONTRACTOR SHALL PAY PARTICULAR ATTENTION TO THE DUST CONTROL REQUIREMENTS OF THIS CONTRACT. THE OPERATION OF RUNWAYS, TAXIWAYS, AND ASSOCIATED NAVAIDS ARE ESPECIALLY SENSITIVE TO DUST. THE ENGINEER RESERVES THE RIGHT TO STOP CONTRACTOR OPERATIONS, IF NECESSARY TO BRING DUST UNDER CONTROL. THE CONTRACTOR SHALL BE RESPONSIBLE FOR DUST CONTROL WITHIN THE CONSTRUCTION LIMITS AS WELL AS ALONG ANY ROADWAYS USED BY THE EQUIPMENT AND VEHICLES.
- B. WATER TRUCKS SHALL BE MAINTAINED AT ALL TIMES SUCH THAT THE ACCESS ROADS AND CONSTRUCTION AREAS CAN BE WETTED AS NECESSARY. THE CONTRACTOR SHALL BE PREPARED, AT NO EXTRA COST TO THE OWNER, TO USE ADDITIONAL WATER TRUCKS OR OTHER MEANS SHOULD IT BE NECESSARY TO MAINTAIN DUST TO AN ACCEPTABLE LEVEL. ALL WATER TRUCKS SHALL BE SUBSIDIARY TO THE VARIOUS BID ITEMS ON THE PROJECT.

VIII. LIQUIDATED DAMAGES

FAILURE TO REOPEN A RUNWAY AT THE DESIGNATED TIME WILL RESULT IN LIQUIDATED DAMAGES OF \$500 PER 15 MINUTE INCREMENT BEYOND THE SCHEDULED REOPENING TIME.

\$100 PER 30-MINUTE INCREMENT FOR EACH 30-MINUTE INTERVAL A TAXIWAY IS DELAYED FROM ITS SCHEDULED REOPENING.

\$1,000 PER DAY FOR EACH DAY THE PROJECT COMPLETION IS DELAYED FROM ITS SCHEDULED COMPLETION DATE.

IX. DEMOBILIZATION

CONDITIONS OF THE PROJECT AREA UPON COMPLETION OF THE JOB SHALL BE GOOD AS OR BETTER THAN THE CONDITIONS PRIOR TO STARTING WORK, IN ADDITION TO THE WORK ITEMS LISTED.

THE PROJECT AREA SHALL BE FREE OF ANY CONTRACTOR STOCKPILE MATERIALS UPON COMPLETION OF THE JOB UNLESS OTHERWISE DIRECTED BY THE ENGINEER.

UPON COMPLETION OF THE PROJECT, ALL OF THE HAUL ROUTES SHALL BE PROPERLY CLEANED TO PREVENT OBSTRUCTION AND/OR CAUSE INCONVENIENCE TO NORMAL REGULAR TRAFFIC. ALL TEMPORARY HAUL ROUTES SHALL BE REMOVED AND BROUGHT BACK TO ORIGINAL CONDITION OR BETTER.

ANY PROPERTIES BELONGING TO THE AIRPORT SHALL BE RETURNED TO THE AIRPORT OWNER.

PROPER DRAINAGE (NO LOCALIZED PONDING) SHALL BE MAINTAINED, PRIOR TO, DURING AND AFTER MOBILIZATION.

DEMOBILIZATION SHALL BE DONE TO THE SATISFACTION OF THE ENGINEER.

DEMOBILIZATION SHALL BE DONE IN A MANNER THAT WILL NOT CAUSE ANY INCONVENIENCE TO AIRPORT OPERATIONS.

ANY DAMAGE TO THE AIRPORT PROPERTIES DURING DEMOBILIZATION SHALL BE REPAIRED AND PAID FOR AT THE CONTRACTOR'S OWN EXPENSE.

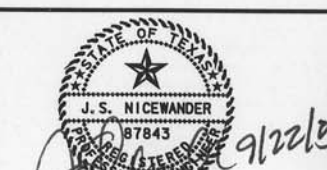
SAFETY REGULATIONS SHALL BE OBSERVED AT ALL TIMES DURING DEMOBILIZATION.

THE COST FOR DEMOBILIZATION SHALL BE CONSIDERED SUBSIDIARY TO ITEM 1, M-101, MOBILIZATION.

THE CONTRACTOR SHALL VIDEO TAPE THE ENTIRE WORK AREA AFTER THE CONSTRUCTION ACTIVITIES HAVE BEEN COMPLETED. THE CONTRACTOR SHALL PROVIDE ONE (1) COPY OF THIS VIDEO TAPE(S) TO BOTH THE AIRPORT MANAGER AND THE ENGINEER. THE COST OF PREPARING VIDEO SHALL BE SUBSIDIARY TO THE OTHER BID ITEMS.

1-SEP-2003 12:23 39020\cadd\Sheets\opnt02.dgn

NO.	DATE	REVISION	APPROV.	NO.	DATE	REVISION	APPROV.



HNTB ARCHITECTS ENGINEERS PLANNERS
The HNTB Companies

DESIGN: JSN
CHECK: JDH
DRAWN: GFS
CHECK: JSN
DATE: SEPTEMBER 2003

PROJECT NO. 39020



TOWN OF ADDISON AIRPORT PARKWAY REALIGNMENT	SHEET
GENERAL NOTES	

ROADWAY CONSTRUCTION NOTES:

1. THE CONTRACTOR SHALL SCHEDULE OPERATIONS TO MINIMIZE POTENTIAL DROP-OFF HAZARDS. ANY DEVIATION TO THE CONSTRUCTION SEQUENCING PLANS CONTAINED IN THESE PLANS MUST BE APPROVED BY THE ENGINEER.
2. PRIOR TO FINAL ACCEPTANCE, ALL CURB SURFACES SHALL BE CLEARED OF ALL DISCOLORATION SUCH AS TIRE MARKS OR OTHER DISFIGUREMENT.
3. TREES OUTSIDE OF TOE OF SLOPES SHALL NOT BE DISTURBED EXCEPT WITH APPROVAL OF THE OWNER. ONLY A MINIMUM AMOUNT OF TREE REMOVAL SHALL BE DONE. CONTRACTOR SHALL FLAG ALL TREES TO BE REMOVED FOR OWNER'S APPROVAL BEFORE REMOVAL.
4. THE CONTRACTOR SHALL BEGIN CONSTRUCTION WITHIN FIVE (5) CALENDAR DAYS OF THE ISSUANCE OF THE NOTICE TO PROCEED AND GIVE NOTICE TO THE TOWN, IN WRITING, BEFORE WORK BEGINS ON THE PROJECT.
5. THE CONTRACTOR SHALL SEQUENCE CONSTRUCTION AROUND BUSINESS DRIVEWAYS SO THAT ACCESS TO THE BUSINESS CAN BE OBTAINED DURING BUSINESS HOURS. NO PARKING LOT WILL BE SHUT OFF FROM ACCESS WITHOUT APPROVAL OF THE TOWN OF ADDISON.
6. ALL UTILITY CONSTRUCTION (STORM WATER) SHALL BE COMPLETED PRIOR TO SUBGRADE PREPARATION.
7. SUBGRADES SHALL BE PROOF ROLLED. NO ADDITIONAL COMPENSATION SHALL BE PAID FOR PROOF ROLLING, IT SHALL BE CONSIDERED SUBSIDIARY TO OTHER BID ITEMS.
8. THE CONTRACTOR SHALL FURNISH, AT HIS OWN COST, THE FOLLOWING TESTING SERVICES BY A REPUTABLE INDEPENDENT TESTING LABORATORY APPROVED BY THE TOWN.
 - A. FIELD DENSITY TESTS OF EMBANKMENT, SUBGRADE, OR BASE, AT LOCATIONS SPECIFIED BY THE INSPECTOR.
 - B. PAVEMENT TESTING SHALL BE DONE ACCORDING TO SECTION 5.8.6, PAVEMENT TESTING, OF THE NCTCOG SPECS (COMPRESSIVE STRENGTH).
9. THE CONTRACTOR SHALL ADJUST ALL VALVE BOXES, METER BOXES, MANHOLES, AND OTHER UTILITY STRUCTURES TO GRADE AS NEEDED PRIOR TO AND AFTER STREET PAVING HAS BEEN COMPLETED.
10. EROSION CONTROL SHALL START WITH INITIAL CONSTRUCTION AND BE PRACTICED THROUGHOUT THE PROJECT.
11. IT IS THE INTENT OF THESE PLANS TO MAINTAIN TRAFFIC FLOW THROUGHOUT THE PROJECT LIMITS AT ALL TIMES DURING CONSTRUCTION. IF A SITUATION ARISES THAT WARRANTS A ROAD CLOSURE, THIS WILL ONLY BE PERMITTED WITH THE WRITTEN PERMISSION FROM THE PUBLIC WORKS DIRECTOR.
12. PROJECT SIGNS SHALL BE MAINTAINED AT INTERSECTIONS WITH ADDISON ROAD AND THROUGHOUT DURATION OF PROJECT.
13. MAINTAIN/REPAIR EXISTING IRRIGATION SYSTEMS TO INSURE WATERING OCCURS ON PRIVATE LANDSCAPING, NO EXTRA PAY.
14. THE CONTRACTOR SHALL PAY EXTRA ATTENTION TO ITEM 6.2.9 "BACKFILL" OF THE NCTCOG SPECIFICATIONS. THE OWNER WILL EXPECT ALL TRENCHES TO BE BACKFILLED WITH PROPER CARE AND ANY SETTLEMENT TO BE REMEDIED IN A TIMELY FASHION.
15. CONTRACTOR SHALL KEEP PROJECT SITE CLEAN AND ORDERLY. IT WILL BE UNACCEPTABLE TO ALLOW TRASH TO BLOW ONTO ADJACENT PROPERTIES. STRICT ATTENTION WILL BE PAID TO THIS ITEM. IF CONTRACTOR FAILS TO ADHERE TO THIS STIPULATION, OWNER RESERVES THE RIGHT TO STOP WORK UNTIL TRASH IS CLEANED UP. THIS STOPPAGE OF WORK WILL NOT EXTEND THE ORIGINAL CONTRACT TIME.
16. FLYASH MAY BE USED PER NCTCOG SPECIFICATIONS ITEM 2.2.2.(D)

UTILITY CONTACTS

THE INFORMATION SHOWN ON THESE DRAWINGS CONCERNING TYPE AND LOCATION OF UNDERGROUND UTILITIES IS NOT GUARANTEED TO BE ACCURATE OR ALL INCLUSIVE. THE CONTRACTOR IS RESPONSIBLE FOR MAKING HIS OR HER OWN DETERMINATION AS TO THE TYPE AND LOCATION OF UNDERGROUND UTILITIES AS MAY BE NECESSARY TO AVOID DAMAGE THERETO. THE CONTRACTOR SHALL MAINTAIN THE UTILITY LOCATION MARKINGS UNTIL THEY ARE NO LONGER NECESSARY.

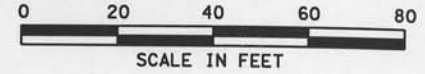
CONTRACTOR SHALL CONTACT 1-800-DIG-TESS (344-8377) PRIOR TO ANY EXCAVATION.

TYPICAL IRRIGATION NOTES:

1. PRIOR TO DEMOLITION OR CONSTRUCTION ACTIVITIES, EXISTING IRRIGATION SYSTEMS SHALL BE TURNED ON. THE LOCATION OF HEADS, VALVES, AND PIPE SHALL BE MARKED WHERE DEMOLITION AND CONSTRUCTION WILL IMPACT THE SYSTEM. DEFICIENCIES IN THE SYSTEM SHALL BE NOTED AND REPORTED TO THE SYSTEM OWNER. A PHOTOGRAPHIC OR VIDEO RECORD SHALL BE MADE IF APPROPRIATE.
2. PIPING SHALL BE CUT, CAPPED AND MARKED AT THE LIMITS OF CONSTRUCTION.

GENERAL NOTES:

1. ALL MATERIALS AND WORKMANSHIP SHALL CONFORM TO THE TOWN OF ADDISON STANDARDS AND SPECIFICATIONS AND THE NORTH CENTRAL TEXAS COUNCIL OF GOVERNMENT (NCTCOG) STANDARDS AND SPECIFICATIONS, EXCEPT AS NOTED. IN THE EVENT OF A CONFLICT, THE TOWN OF ADDISON STANDARDS AND SPECIFICATIONS SHALL GOVERN.
2. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PUBLIC SAFETY DURING CONSTRUCTION AND WILL PROVIDE THE NECESSARY TRAFFIC BARRICADES AND WARNING SIGNAGE TO PROTECT THE CONSTRUCTION SITE. ALL BARRICADES, WARNING SIGNS, LIGHTS, DEVICES, ETC. FOR THE GUIDANCE AND PROTECTION OF TRAFFIC AND PEDESTRIANS MUST CONFORM TO THE 1980 TEXAS MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES, AS CURRENTLY AMENDED, BY THE TEXAS DEPARTMENT OF TRANSPORTATION.
3. NO PERSON SHALL OPEN, TURN OFF, INTERFERE WITH, ATTACH ANY PIPE OR HOSE TO OR TAP ANY WATER MAIN BELONGING TO THE TOWN UNLESS DULY AUTHORIZED TO DO SO BY THE TOWN OF ADDISON PUBLIC WORKS DEPARTMENT. ARRANGEMENTS FOR CONSTRUCTION WATER SHALL BE MADE THROUGH CITY OF ADDISON PUBLIC WORKS DEPARTMENT.
4. CONTRACTOR SHALL MAINTAIN ADEQUATE SANITARY FACILITIES FOR USE BY WORKERS THROUGHOUT CONSTRUCTION.
5. NO TRAFFIC SIGNS ARE TO BE RELOCATED OR REMOVED WITHOUT PRIOR APPROVAL OF THE TOWN OF ADDISON PUBLIC WORKS DEPARTMENT.
6. THE CONTRACTOR SHALL TAKE ALL NECESSARY PRECAUTIONS TO PROTECT EXISTING FACILITIES FROM DAMAGE. ANY DAMAGE TO EXISTING FACILITIES RESULTING FROM CONSTRUCTION WORK SHALL BE REPAIRED AT THE CONTRACTOR'S EXPENSE.
7. THESE PLANS DO NOT EXTEND TO OR INCLUDE DESIGNS OR SYSTEMS PERTAINING TO THE SAFETY OF THE CONSTRUCTION CONTRACTOR OR ITS EMPLOYEES, AGENT OR REPRESENTATIVES IN PERFORMANCE OF THE WORK. THE SEAL OF HNTB CORPORATION, REGISTERED PROFESSIONAL ENGINEER(S) HEREON DOES NOT EXTEND TO ANY SUCH SAFETY SYSTEMS THAT MAY NOW OR HEREAFTER BE INCORPORATED IN THESE PLANS. THE CONSTRUCTION CONTRACTOR SHALL PREPARE OR OBTAIN THE APPROPRIATE SAFETY SYSTEMS, INCLUDING THE PLANS AND SPECIFICATIONS.
8. IT IS THE CONTRACTOR'S RESPONSIBILITY TO MAINTAIN NEAT AND ACCURATE PLANS OF RECORD.
9. THE CONTRACTOR IS RESPONSIBLE FOR MAINTAINING ADEQUATE SITE DRAINAGE THROUGHOUT THE DURATION OF THIS PROJECT.
10. THE CONTRACTOR IS RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND APPROVALS BEFORE CONSTRUCTION BEGINS.
11. THE CONTRACTOR SHALL TAKE ALL NECESSARY PRECAUTIONS TO ENSURE THAT ELECTRIC POWER AND TELEPHONE POLES ARE NOT DISTURBED DURING CONSTRUCTION.
12. THE CONTRACTOR SHALL KEEP EXCAVATED TRENCHES FREE OF GROUNDWATER DURING CONSTRUCTION. IF NECESSARY, THE CONTRACTOR SHALL UTILIZE DEWATERING IN ORDER TO CONTROL GROUNDWATER DURING CONSTRUCTION SUCH THAT IT DOES NOT AFFECT HIS CONSTRUCTION WORK. (NO SEPARATE PAY ITEM)
13. EXISTING FACILITIES ARE SHOWN IN APPROXIMATE LOCATIONS PER INFORMATION AND RECORDS AVAILABLE. CONTRACTOR SHALL UNCOVER AND VERIFY HORIZONTAL AND VERTICAL LOCATION OF EXISTING FACILITIES PRIOR TO CONSTRUCTION.
14. ALL STORM SEWERS SHALL BE RCP CLASS III UNLESS OTHERWISE NOTED.
15. INSTALL CONCRETE COLLAR AT CONNECTION OF NEW RCP'S TO EXIST. RCP'S.
16. CONTRACTOR SHALL FIELD VERIFY FLOWLINE OF EXIST. RCP'S AND CONTACT ENGINEER IF A DISCREPANCY IS FOUND.
17. CONTRACTOR TO BE ADVISED, THERE WILL BE OTHER CONSTRUCTION ACTIVITIES GOING ON ADJACENT TO THIS PROJECT. COORDINATION BETWEEN CONTRACTORS WILL BE ESSENTIAL THROUGHOUT THE CONSTRUCTION OF THE PROJECT.



2-SEP-2003 12:24 \\39020\Cad\Sheets\opnt03.dgn

NO.	DATE	REVISION	APPROV.	NO.	DATE	REVISION	APPROV.



HNTB ARCHITECTS ENGINEERS PLANNERS
The HNTB Companies

DESIGN CHECK	JSN	PROJECT NO.	39020
DRAWN	JDH		
CHECK	GFS		
	JSN		
DATE	SEPTEMBER 2003		
SCALE			

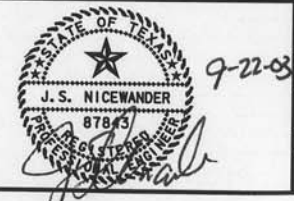


TOWN OF ADDISON AIRPORT PARKWAY REALIGNMENT	SHEET
GENERAL NOTES	

22-SEP-2003 13:21
 g:\39020\Cad\Sheets\apqt01.dgn

ITEM NO.	DESCRIPTION	UNIT	EST. QTY.
ROADWAY IMPROVEMENTS			
101	MOBILIZATION	L.S.	1
102	PREPARE RIGHT-OF-WAY	L.S.	1
103	FULL DEPTH SAWCUT EXISTING PAVEMENT	L.F.	940
104	REMOVE AND DISPOSE OF EXISTING PAVEMENT, INCLUDING CURB	S.Y.	1,550
105	UNCLASSIFIED STREET EXCAVATION	C.Y.	450
106	EMBANKMENT	C.Y.	100
107	10" THICK REINFORCED CONCRETE PAVEMENT, 4000 PSI @ 28 DAYS	S.Y.	2,250
108	10' CAST IN PLACE RECESSED INLET	EA	2
109	CLASS III 18" RCP STORM DRAIN PIPE	L.F.	112
110	REMOVE AND RELOCATE ROADSIDE SIGN	EA.	2
111	STOP SIGN (R1-1)	EA.	1
112	SPEED LIMIT SIGN (R2-1)	EA.	2
113	FIRE LANE STRIPING	L.F.	950
114	SOLID SODDING	S.Y.	1,000
115	FURNISH, INSTALL, MAINTAIN AND REMOVE SILT FENCE	L.F.	300
116	INLET PROTECTION (DROP)	EA	2
117	IRRIGATION CONDUIT	L.F.	114
118	FURNISH, INSTALL, SECURITY GATE WITH ACCESS CONTROL PANEL	LS	1

NO.	DATE	REVISION	APPROV.	NO.	DATE	REVISION	APPROV.

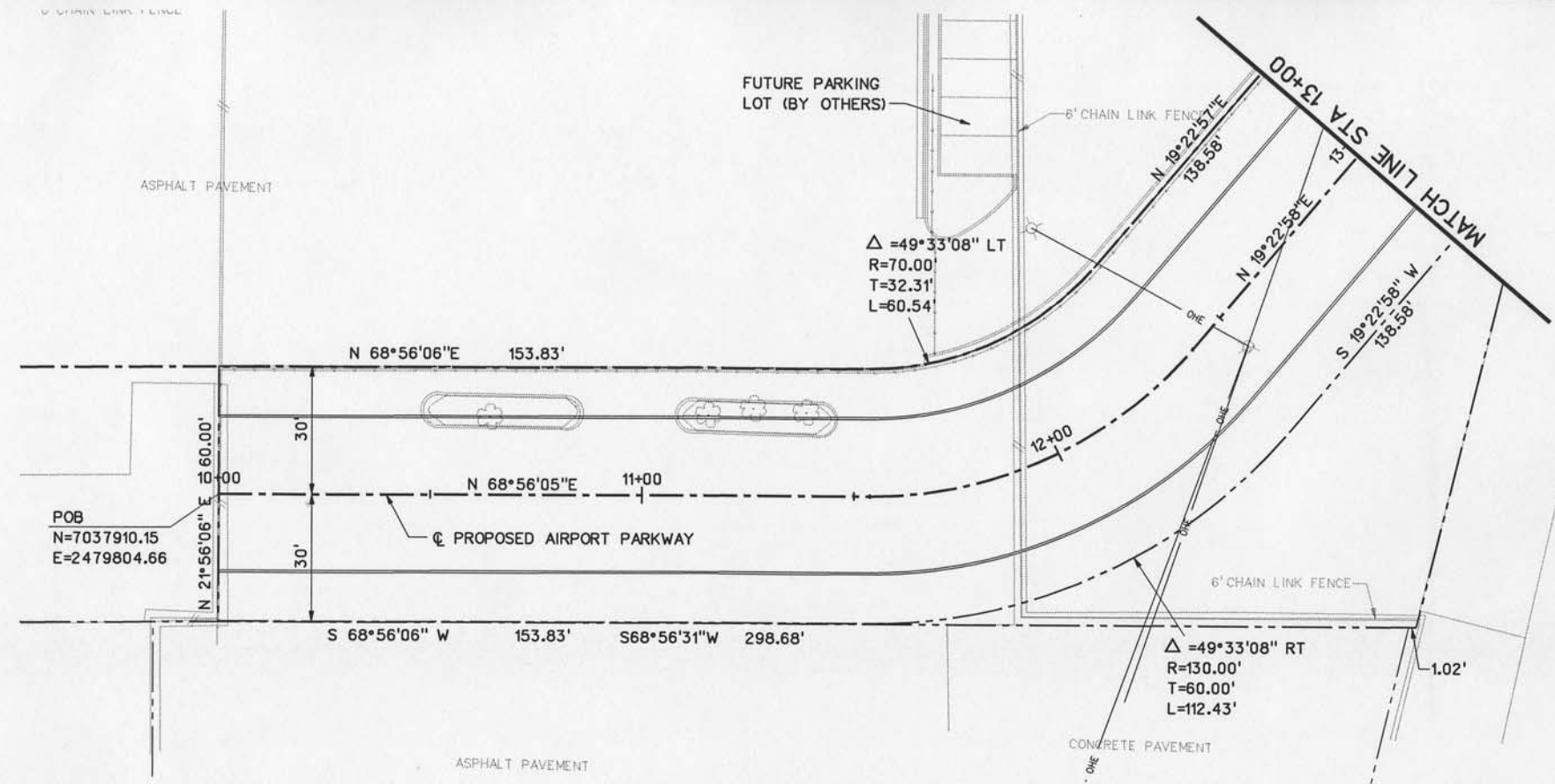
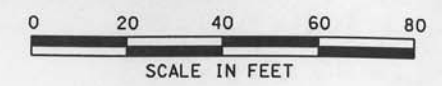


HNTB ARCHITECTS ENGINEERS PLANNERS
 The HNTB Companies

DESIGN CHECK	JSN	PROJECT NO.	39020
DRAWN	JDH		
CHECK	GFS		
	JSN		
DATE	SEPTEMBER 2003		
SCALE			

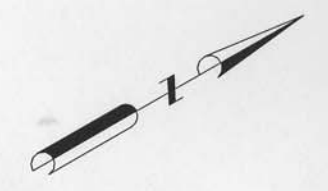
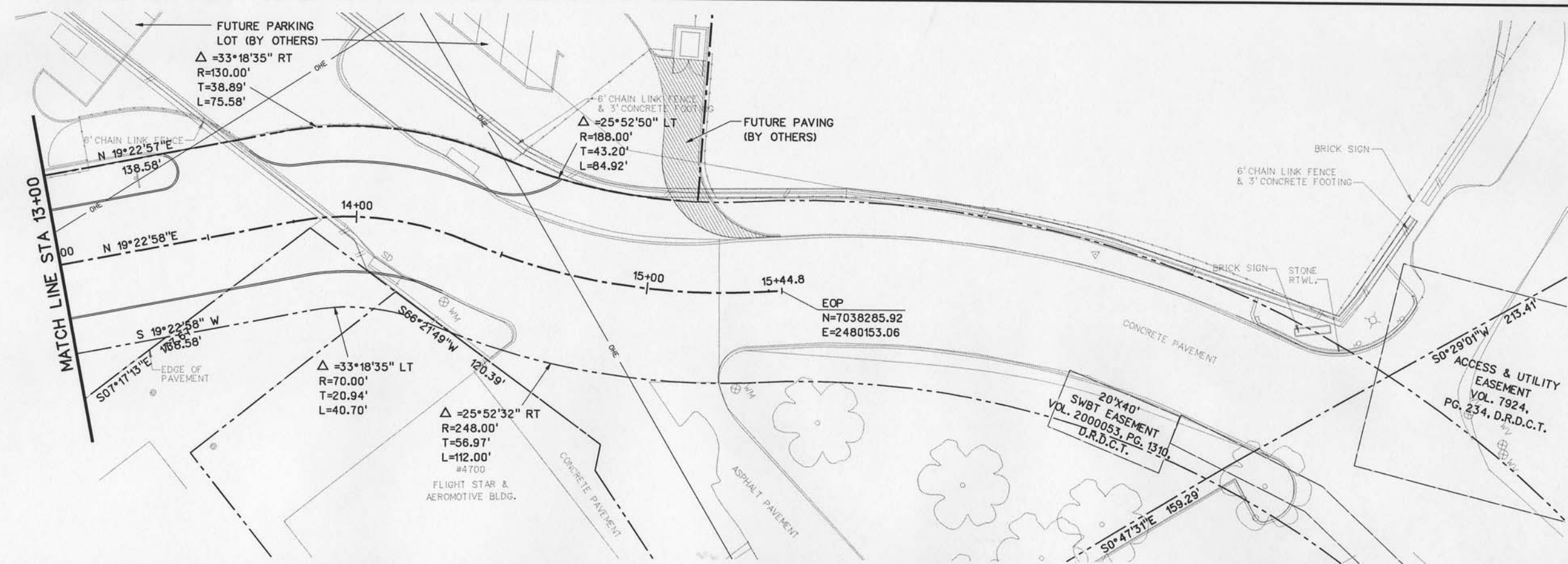


TOWN OF ADDISON	SHEET
AIRPORT PARKWAY REALIGNMENT	
QUANTITIES	Q-1
TOWN OF ADDISON, TEXAS	



LEGEND

- CENTERLINE OF PROPOSED AIRPORT PARKWAY
- RIGHT OF WAY



22-SEP-2003 12:16 G:\39020\Cad\Sheets\aprow01.dgn

NO.	DATE	REVISION	APPROV.	NO.	DATE	REVISION	APPROV.



HNTB ARCHITECTS ENGINEERS PLANNERS
The HNTB Companies

DESIGN CHECK JSN JDH
DRAWN GFS
CHECK JSN
DATE SEPTEMBER 2003
SCALE

PROJECT NO. 39020

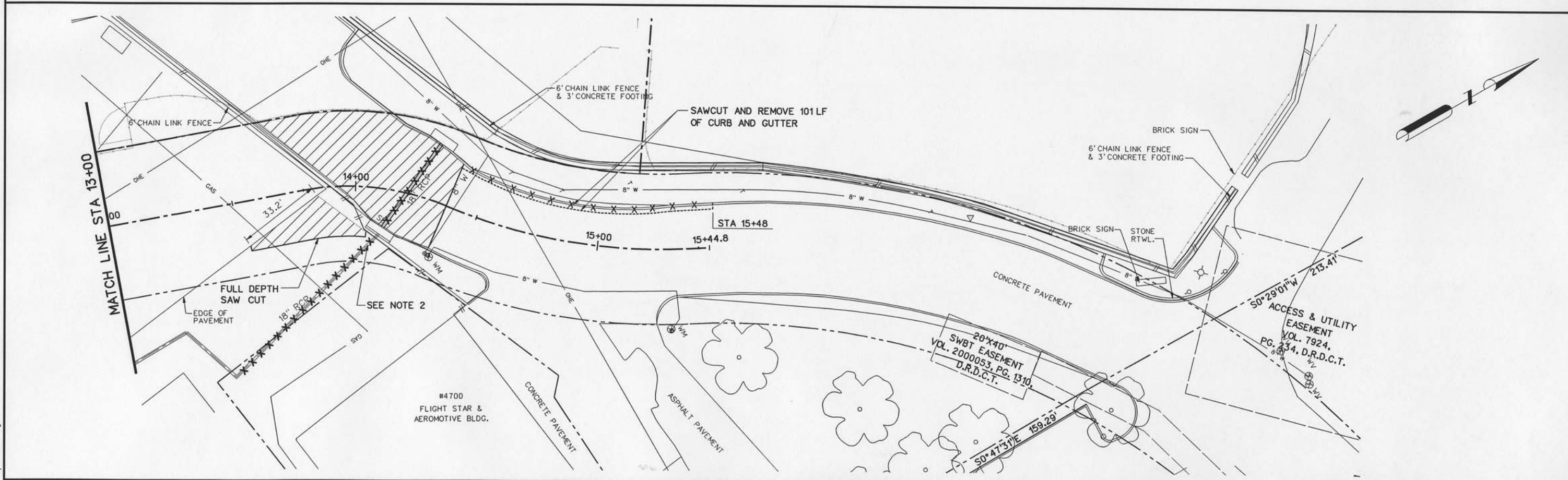
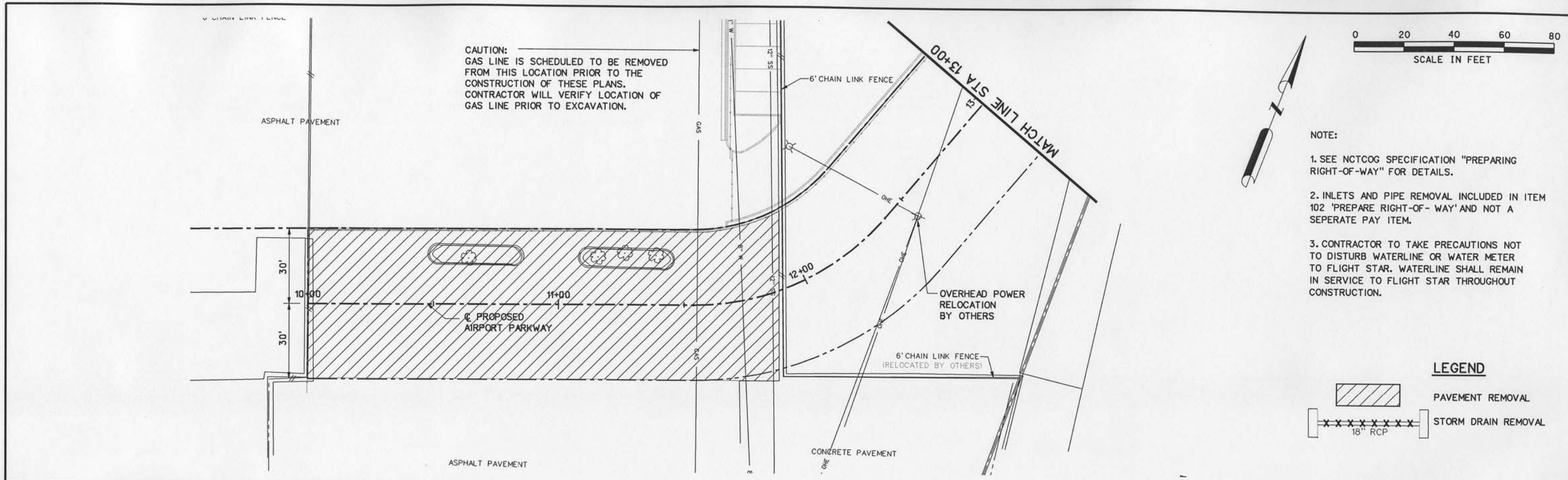


TOWN OF ADDISON
AIRPORT PARKWAY REALIGNMENT

ROW MAP
STA 10+00 TO STA 15+44.8

TOWN OF ADDISON, TEXAS

SHEET R-1



22-SEP-2003 12:16
G:\39020\Cad\Sheets\aprem01.dgn

NO.	DATE	REVISION	APPROV.	NO.	DATE	REVISION	APPROV.



HNTB ARCHITECTS ENGINEERS PLANNERS
The HNTB Companies

DESIGN CHECK JSN JDH
 DRAWN CHECK GFS JSN
 DATE SEPTEMBER 2003
 SCALE

PROJECT NO. 39020

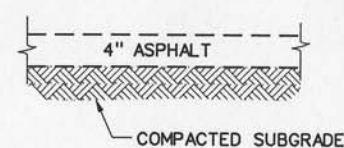
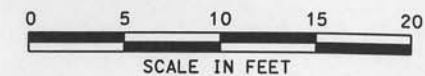


TOWN OF ADDISON
AIRPORT PARKWAY REALIGNMENT

REMOVAL PLAN
STA 10+00 TO STA 15+45

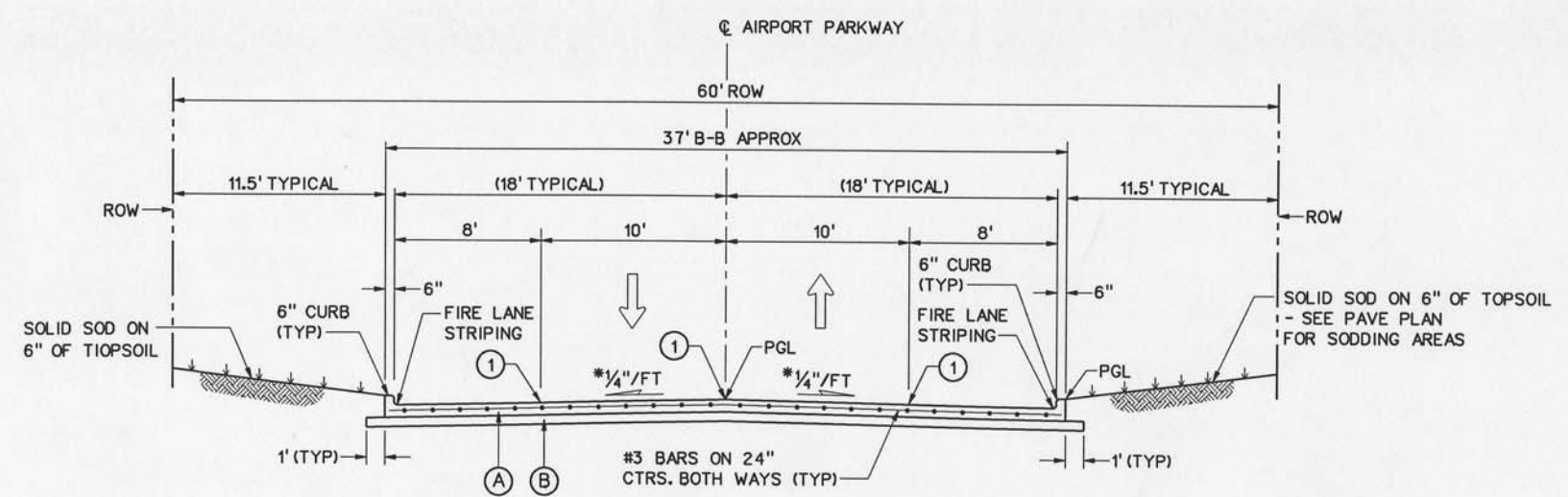
TOWN OF ADDISON, TEXAS

SHEET
RM-1



EXISTING ASPHALT SECTION
 (PER SOILS REPORT -
 SEE SPECIFICATIONS FOR BORINGS)
 NO SCALE

- NOTES:**
1. INTEGRAL CURB & GUTTER SHALL CONFORM TO NCTCOG STD DWG NO. 2120. THE INTEGRAL CURB AND GUTTER SHALL BE SUBSIDIARY TO THE 10" REINFORCED CONCRETE PAVEMENT PAY ITEM.
 2. ALL TRANSVERSE JOINTS SHALL BE ON 11' CENTERS MAX.
 3. PROFILE GRADES SHOWN ON THE PAVING PLAN & PROFILE SHEETS ARE TOP OF CURB AT BACK OF CURB UNLESS NOTED OTHERWISE ON THE PLANS.
 4. THE FINAL FINISH ON THE CONCRETE STREET PAVEMENT SHALL CONSIST OF A COMBINATION OF A LONGITUDINAL CARPET DRAG AND TRANSVERSE METAL TINE FINISH AS DESCRIBED IN TxDOT STANDARD SPECIFICATIONS FOR ITEM 360 - CONCRETE PAVEMENT UNLESS SPECIFIED OTHERWISE IN THE PLANS.



TYPICAL SECTION
AIRPORT PARKWAY
 STA 10+00.00 TO STA 15+44.8

*PAVEMENT CROSS SLOPES VARY FROM THE TYPICAL 1/4"/FT. SEE PAVING PLANS FOR PROPOSED GRADES.

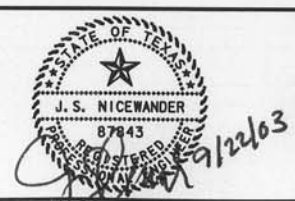
LEGEND

- (A) 10" 4000 p.s.i. @28 DAYS REINFORCED PORTLAND CEMENT CONCRETE PVMT.
- (B) SCARIFY 6" DEPTH AND RECOMPACT TO 95% STANDARD PROCTOR DENSITY. SUBSIDIARY TO 10" REINFORCED CONCRETE PAVEMENT PAY ITEM
- (1) CONSTRUCTION JOINT (FULL WIDTH PAVING IS ALLOWED WHERE APPROVED BY THE TOWN OF ADDISON). IF FULL WIDTH PAVING IS APPROVED, SAWED JOINT

NOTE:
 LIMITS OF REMOVAL/REPLACEMENT FOR AIRPORT PKWY. ARE SHOWN ON THE REMOVAL PLANS (RM-1) AND THE PLAN AND PROFILE SHEETS (PP-1 AND PP-2). SEE THESE SHEETS FOR SPECIFIC LIMITS REGARDING AIRPORT PKWY.

22-SEP-2003 12:16 G:\39020\Cad\Sheets\opt\up01.dgn

NO.	DATE	REVISION	APPROV.	NO.	DATE	REVISION	APPROV.

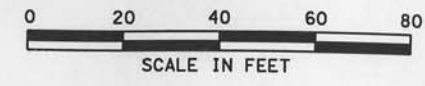


HNTB ARCHITECTS ENGINEERS PLANNERS
 The HNTB Companies

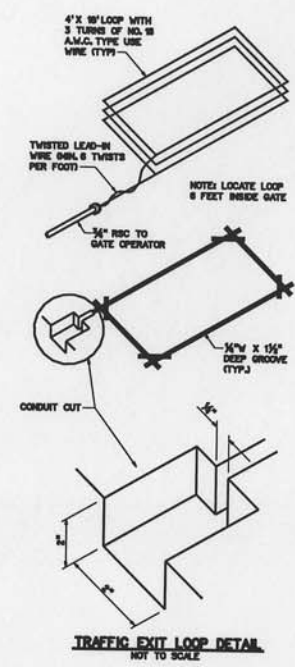
DESIGN CHECK	JSN	PROJECT NO.	39020
CHECK	JDH		
DRAWN CHECK	GFS		
CHECK	JSN		
DATE	SEPTEMBER 2003		
SCALE			



TOWN OF ADDISON	SHEET
AIRPORT PARKWAY REALIGNMENT	
TYPICAL SECTIONS	
TOWN OF ADDISON, TEXAS	TY-1

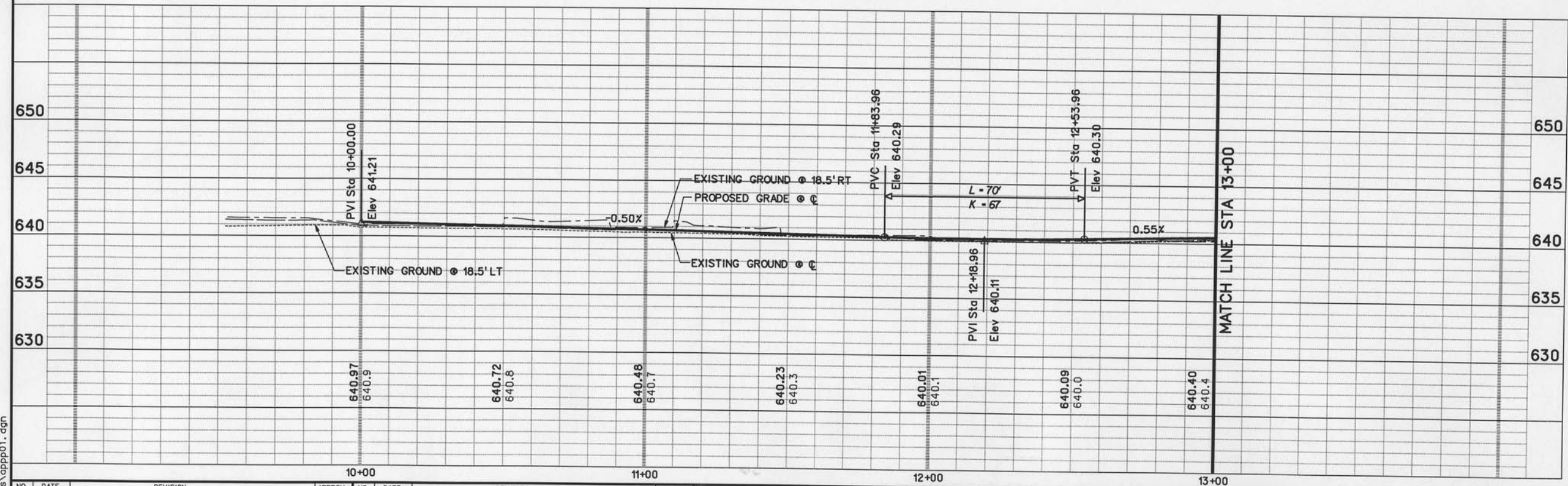
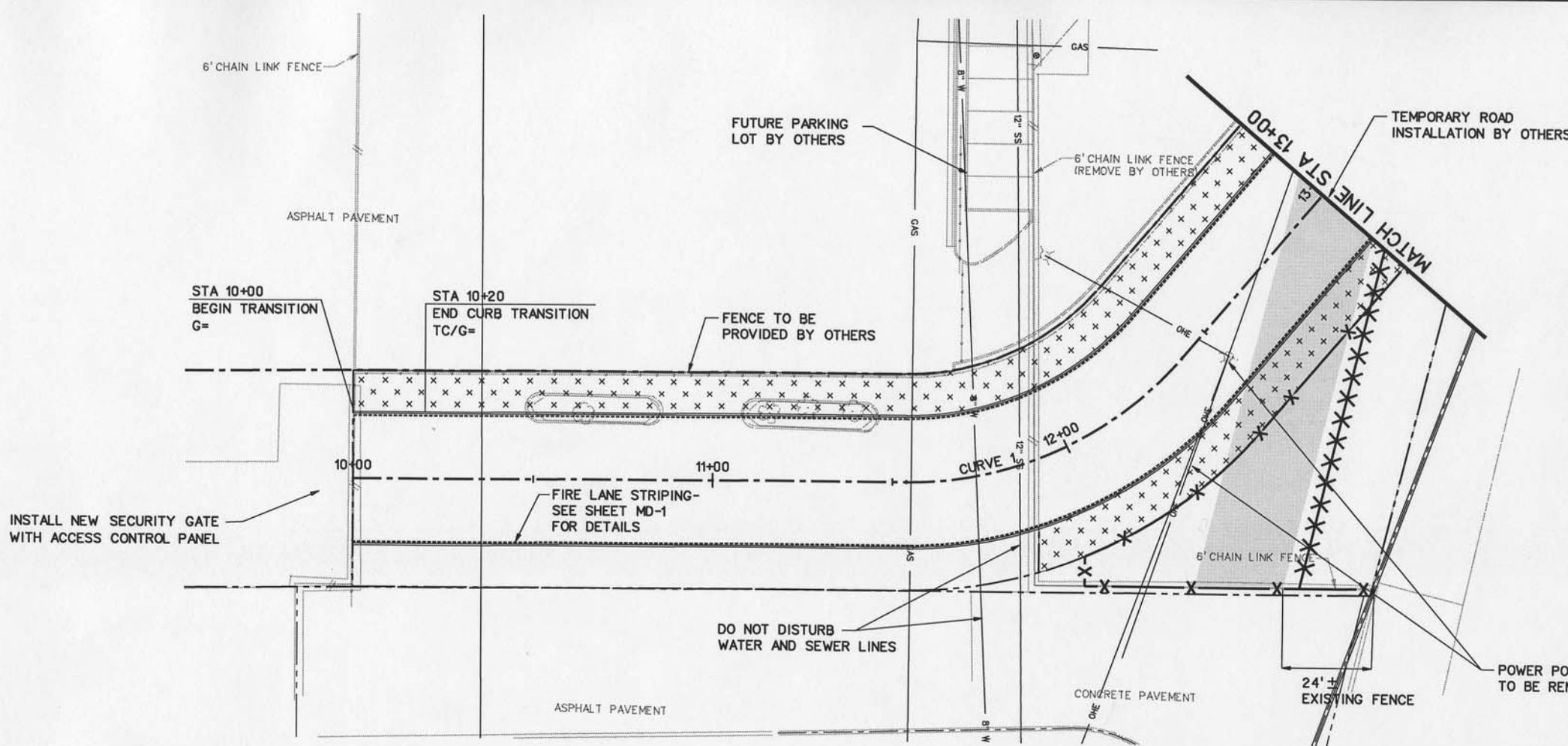


CURVE 1
 $\Delta = 49^{\circ}33'08.66''$ LT
 $R = 100.00'$
 $T = 46.16'$
 $L = 86.49'$
 $PC = STA 11+55.40$
 $PI = STA 12+01.55$
 $PT = STA 12+41.88$



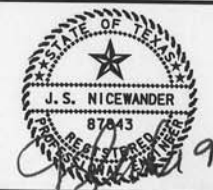
LEGEND

- SOLID SODDING
- AOA SECURITY FENCE
- FIRE LANE STRIPING



22-SEP-2003 12:27
 G:\39020\Cad\Sheets\app01.dgn

NO.	DATE	REVISION	APPROV.	NO.	DATE	REVISION	APPROV.

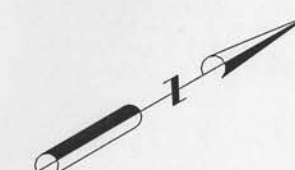
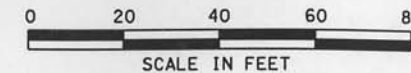


HNTB ARCHITECTS ENGINEERS PLANNERS
 The HNTB Companies

DESIGN CHECK	JSN	PROJECT NO.	39020
DRAWN CHECK	JSN	DATE	SEPTEMBER 2003
SCALE			

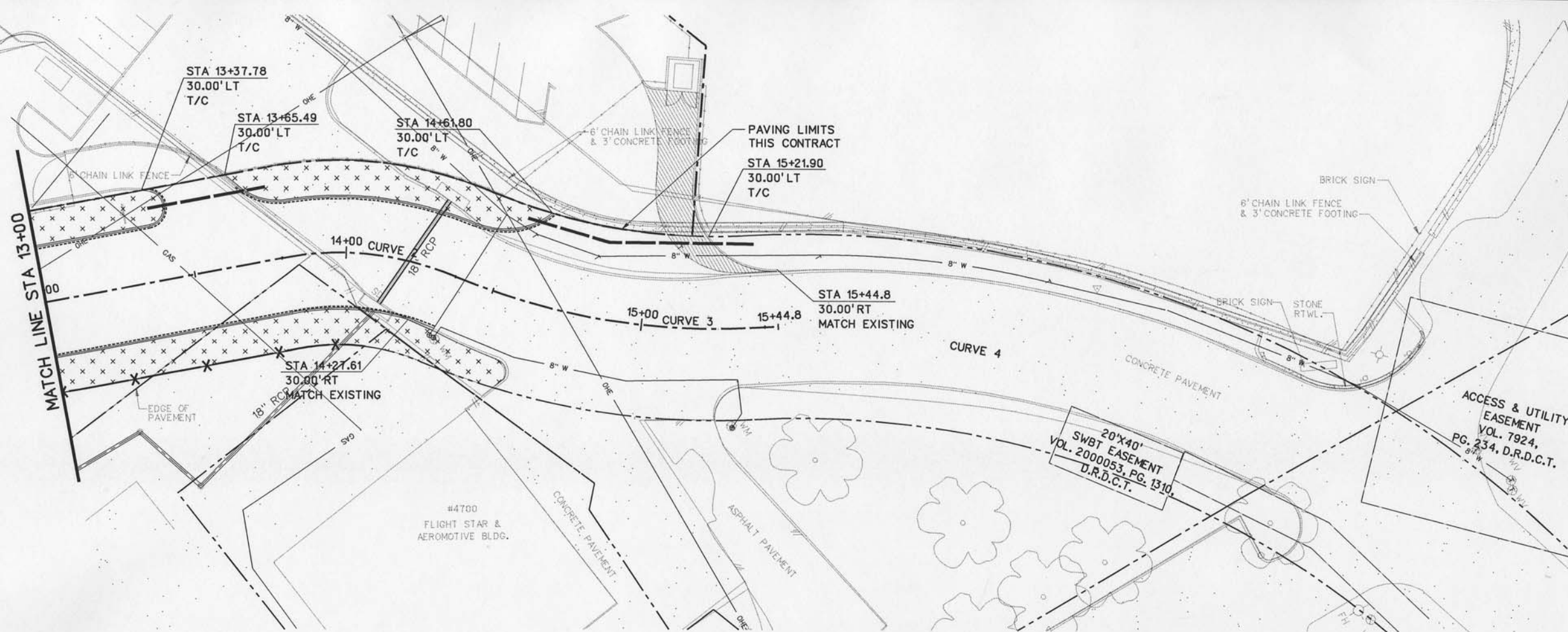


TOWN OF ADDISON AIRPORT PARKWAY REALIGNMENT		SHEET PP-1
PAVING PLAN/PROFILE STA 10+00 TO STA 13+00		
TOWN OF ADDISON, TEXAS		



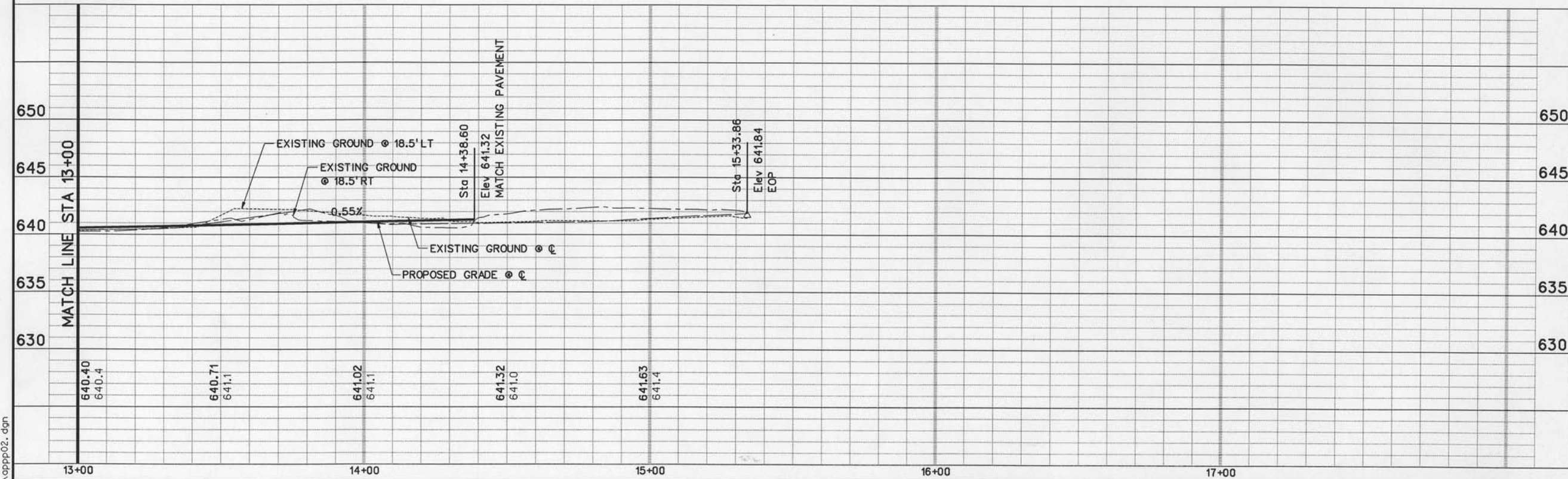
LEGEND

- SOLID SODDING
- AOA SECURITY FENCE
- FIRE LANE STRIPING
- 4" PVC CONDUIT



CURVE 2
 $\Delta = 33^\circ 18' 35.56''$ RT
 R=100.00'
 T=29.92'
 L=58.14'
 PC STA = 13+80.46
 PI STA = 14+10.38
 PT STA = 14+38.60

CURVE 3
 $\Delta = 25^\circ 49' 08.88''$ LT
 R=218.00'
 T=49.97'
 L=98.24'
 PRC STA = 14+38.60
 PI STA = 14+88.57
 PT STA = 15+36.84



22-SEP-2003 12:16 G:\33020\Cad\Sheet\is\app02.dgn

NO.	DATE	REVISION	APPROV.	NO.	DATE	REVISION	APPROV.

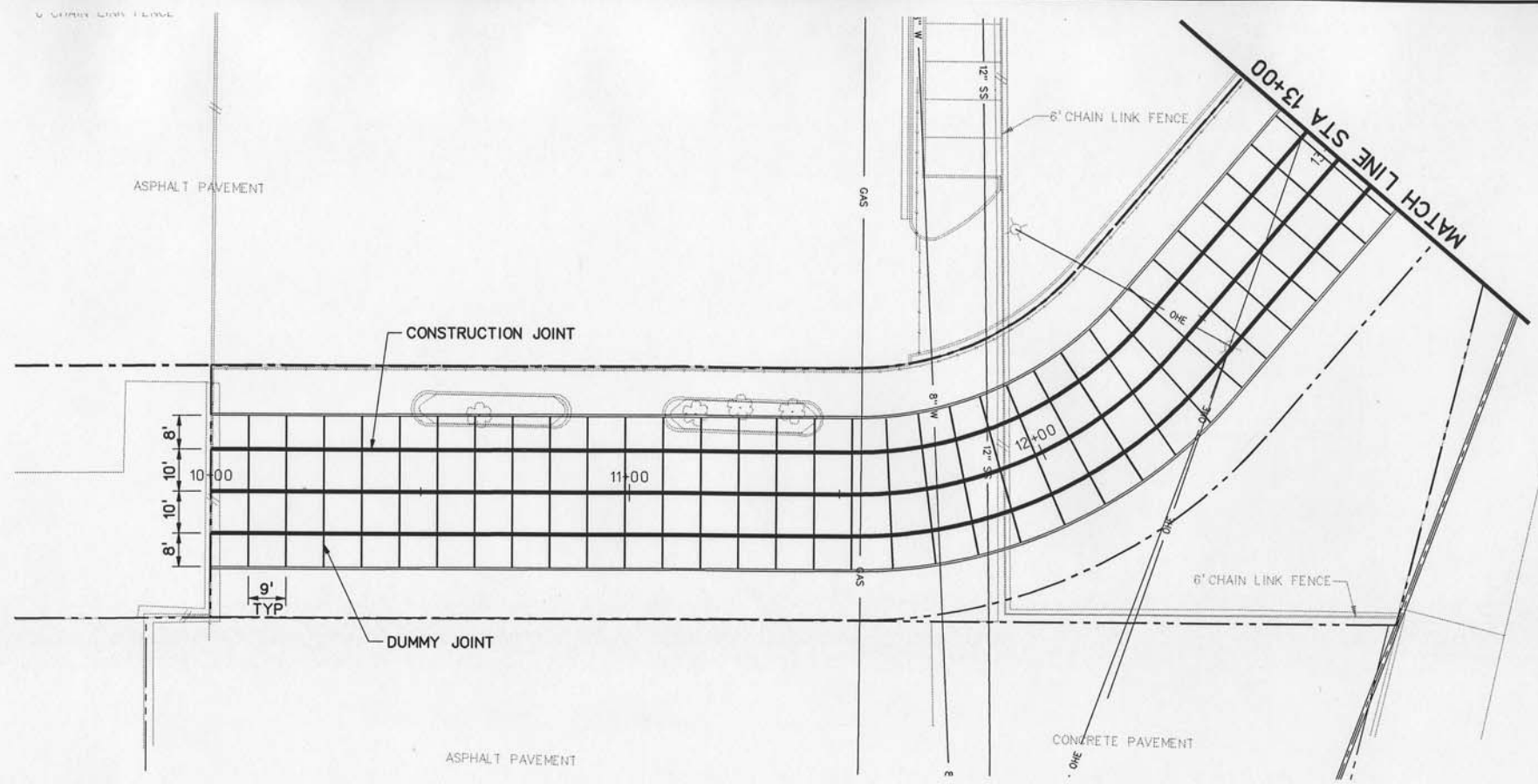
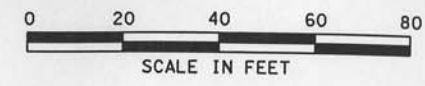


9/22/03

HNTB ARCHITECTS ENGINEERS PLANNERS
 The HNTB Companies
 PROJECT NO. 39020
 DESIGN CHECK JSN JDH
 DRAWN CHECK GFS JSN
 DATE SEPTEMBER 2003
 SCALE

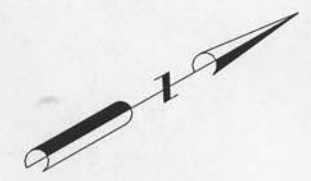


TOWN OF ADDISON
 AIRPORT PARKWAY REALIGNMENT
 PAVING PLAN/PROFILE
 STA 13+00 TO STA 16+36
 TOWN OF ADDISON, TEXAS



NOTE:
1. DUMMY JOINTS ON 9' CENTERS

LEGEND
 - - - LONGITUDINAL BUTT JOINT
 — CONSTRUCTION JOINT
 — DUMMY JOINT



22-SEP-2003 12:17 G:\39020\Cad\Sheets\op1\01.dgn

NO.	DATE	REVISION	APPROV.	NO.	DATE	REVISION	APPROV.



HNTB ARCHITECTS ENGINEERS PLANNERS
The HNTB Companies

DESIGN CHECK JSN
 CHECK JDH
 DRAWN CHECK GFS
 CHECK JSN
 DATE SEPTEMBER 2003
 SCALE

PROJECT NO. 39020



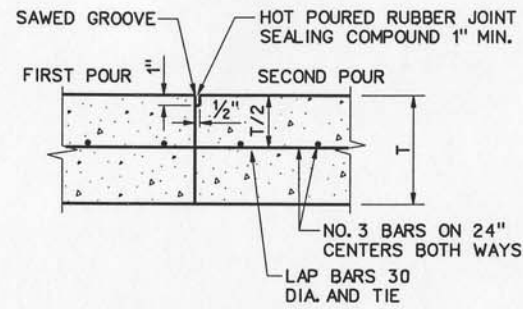
TOWN OF ADDISON
 AIRPORT PARKWAY REALIGNMENT

JOINT LAYOUT
 STA 10+00 TO STA 16+36

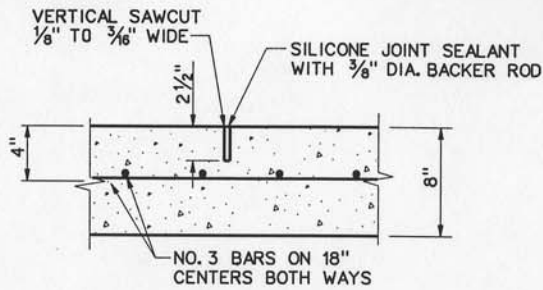
TOWN OF ADDISON, TEXAS

SHEET

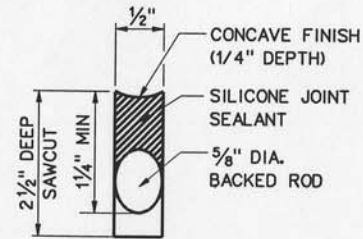
11-1



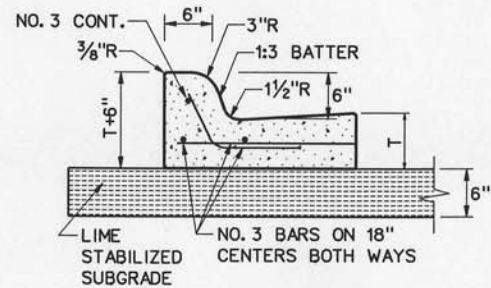
CONSTRUCTION JOINT
NOT TO SCALE



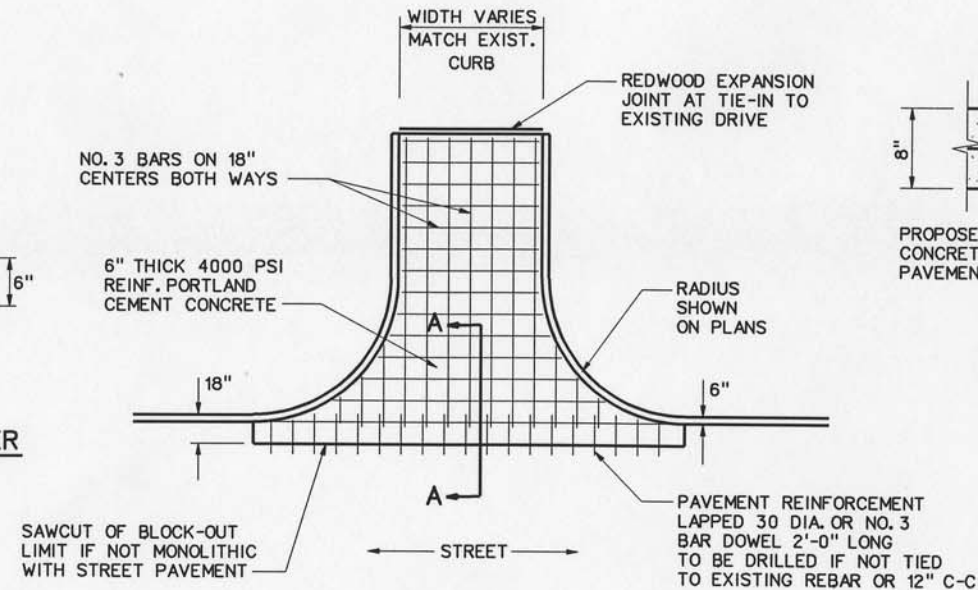
SAWED DUMMY JOINT
NOT TO SCALE



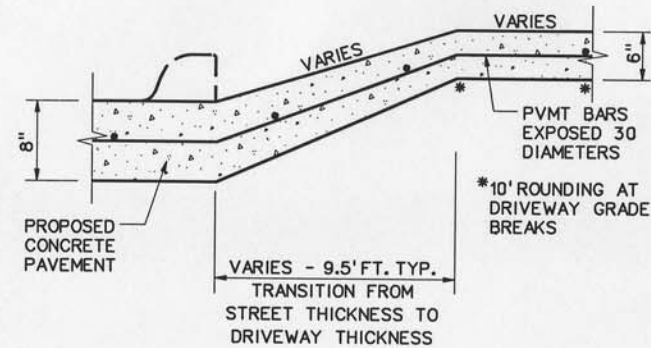
TYPICAL JOINT DETAIL
NOT TO SCALE



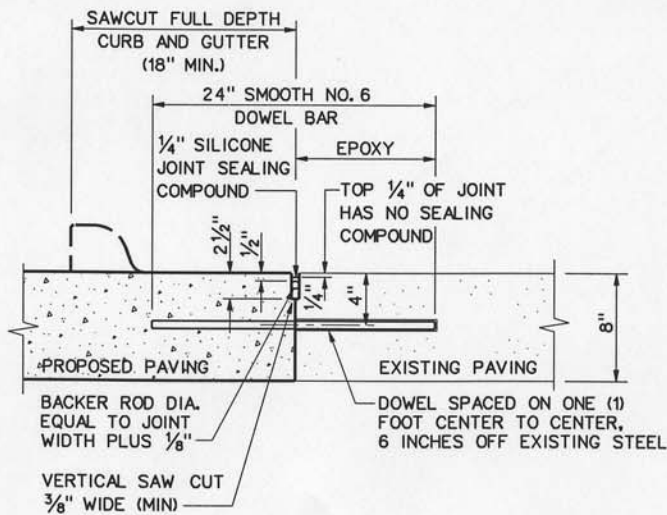
INTEGRAL CURB AND GUTTER
NOT TO SCALE



DRIVE APPROACH
NOT TO SCALE

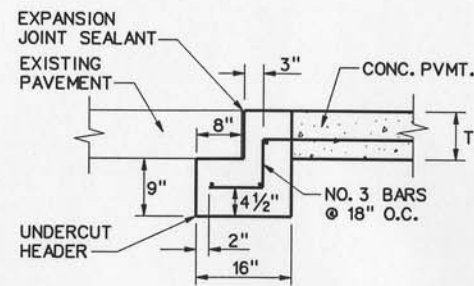


SECTION A-A
NOT TO SCALE



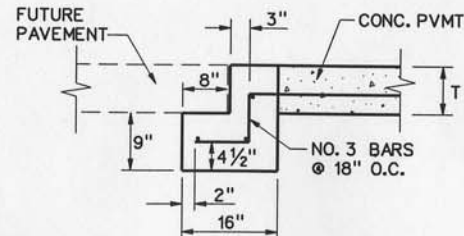
LONGITUDINAL BUTT JOINT
NOT TO SCALE

NOTE:
DOWEL BARS SHALL BE DRILLED INTO EXIST. PAVEMENT HORIZONTALLY BY USE OF A MECHANICAL RIG. DRILLING BY HAND IS NOT ACCEPTABLE, PUSHING DOWEL BARS INTO GREEN CONCRETE IS NOT ACCEPTABLE.



PAVEMENT BARS TO BE BENT DOWN INTO HEADER. HEADER AND PAVEMENT TO BE MONOLITHIC.

STREET HEADER
NOT TO SCALE



NOTE:

STEEL DOWELS WILL BE USED FOR LOAD TRANSFER AT ALL TRAVERSE CONTRACTION JOINTS (SAWED DUMMY JOINTS). THE DOWELS WILL BE 24 INCHES LONG, PLACED 12 O-C AND LOCATED MID-HEIGHT IN THE SLAB. NO. 6 SMOOTH DOWELS WILL BE USED. NO SEPERATE PAY ITEM WILL BE ESTABLISHED FOR THIS ITEM.

NOTES:

- UNLESS SPECIFICALLY STATED OTHERWISE IN PLANS OR CONTRACT DOCUMENTS, THE CONTROLLING SPECIFICATIONS FOR ALL WORK WITHIN PUBLIC RIGHT-OF-WAY AND EASEMENTS SHALL BE THE STANDARD SPECIFICATIONS FOR PUBLIC WORKS CONSTRUCTION, THIRD EDITION 1998, AS AMENDED BY THE NORTH CENTRAL TEXAS COUNCIL OF GOVERNMENTS (THE "STANDARD SPECIFICATIONS"). COPIES OF THE STANDARD SPECIFICATIONS MAY BE PURCHASED BY MAIL OR OVER THE COUNTER FROM THE NORTH CENTRAL TEXAS COUNCIL OF GOVERNMENTS, 616 SIX FLAGS DRIVE, P.O. BOX DRAWER COG, ARLINGTON, TEXAS 76005-5888. PHONE METRO 817/640-3300. BULK DISCOUNTS ARE AVAILABLE. THIS DOCUMENT IS COPYRIGHTED.
- ROUGH GRADING SHALL BE ACCOMPLISHED TO WITHIN +/- 0.10 FEET OF PLAN ELEVATION.
- ALL TRAFFIC CONTROL NECESSARY FOR THE WORK SHALL BE PROVIDED BY THE CONTRACTOR. ALL BARRICADES, WARNING SIGNS, LIGHTS, DEVICES, ETC. FOR THE GUIDANCE AND PROTECTION OF TRAFFIC AND PEDESTRIANS MUST CONFORM TO THE INSTALLATIONS SHOWN IN THE LATEST ISSUE OF THE "TEXAS MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES", AS CURRENTLY AMENDED, TEXAS DEPARTMENT OF TRANSPORTATION.
- ALL FILL SHALL BE PLACED IN MAXIMUM 8-INCH LIFTS COMPACTED TO 95% OF STANDARD PROCTOR DENSITY BETWEEN 0% AND +3% OF OPTIMUM MOISTURE CONTENT.
- ALL TREES, STUMPS, BRUSH, GRASSES AND SURFACE ORGANICS WITHIN PROPOSED RIGHT-OF-WAY ARE TO BE REMOVED AND PROPERLY DISPOSED OF OFF-SITE. TREE REMOVAL PERMITS, IF REQUIRED, WILL BE OBTAINED BY THE OWNER.
- EXISTING UTILITY POLES, IF ANY, WILL BE REMOVED OR RELOCATED BY THE UTILITY COMPANIES THROUGH COORDINATION BY THE OWNER. CONTRACTOR SHALL BRING TO THE OWNER'S ATTENTION ANY FACILITIES THAT APPEAR TO BE IN CONFLICT SO THAT THE OWNER HAS SUFFICIENT TIME TO ACCOMPLISH THE NECESSARY RELOCATIONS.
- WHERE DEEP VERTICAL EXCAVATIONS (IN EXCESS OF 3 FEET) ARE INDICATED, CUTS SHALL BE LAID BACK AT A STABLE SLOPE (ON OWNER'S PROPERTY) UNTIL WALLS ARE CONSTRUCTED. BACKFILL MATERIAL SHALL BE STOCKPILED ON-SITE AT THE DIRECTION OF THE OWNER.
- ALL REINFORCING STEEL AND DOWEL BARS IN PAVEMENT SHALL BE SUPPORTED AND MAINTAINED AT THE CORRECT CLEARANCES BY THE USE OF BAR CHAIRS OR OTHER APPROVED SUPPORT.
- THE USE OF WOOD FORMS FOR PAVEMENT CONSTRUCTION WILL BE PERMITTED.
- DUMMY JOINTS SHALL BE SAWED IN THE PAVEMENT IN THIS PROJECT. SPACING VARIATIONS SHALL BE MADE AT BLOCKOUTS, CONSTRUCTION JOINTS, AND STREET INTERSECTIONS TO LINE UP WITH EXISTING PAVING JOINTS AS SHOWN ON THE PLANS. THE SPACING BETWEEN ANY JOINT SHALL NOT BE LESS THAN 8 FEET NOR MORE THAN 12 FEET. ALL DUMMY JOINTS SHALL BE SAWED NO SOONER THAN 4 HOURS AND NO LATER THAN 12 HOURS AFTER THE PLACEMENT OF THE PAVEMENT.
- TWO-WAY TRAFFIC MUST BE MAINTAINED AT ALL TIMES ON PUBLIC STREETS ADJACENT TO THIS PROJECT UNLESS SPECIFIED OTHERWISE IN THE PLANS AND SPECIFICATIONS, OR DIRECTED BY THE ENGINEER. THE TRAVELWAY WIDTH SHALL NOT BE LESS THAN 10 FEET.
- SEE TYPICAL SECTIONS SHEET FOR ADDITIONAL DETAILS AND NOTES.
- ALL CURBS SHALL BE PLACED INTEGRAL WITH PAVEMENT.
- CURBS SHALL MEET THE SAME COMPRESSIVE STRENGTH AS SPECIFIED FOR THE CONCRETE PAVEMENT.
- BAR LAPS SHALL BE 30 DIAMETERS.

22-SEP-2003 12:17
G:\33020\cod\sheet\ts\vpdr01.dgn

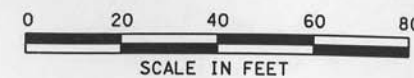
NO.	DATE	REVISION	APPROV.	NO.	DATE	REVISION	APPROV.



HNTB ARCHITECTS ENGINEERS PLANNERS The HNTB Company		PROJECT NO.	39020
DESIGN CHECK	JSN		
	JDH		
DRAWN CHECK	GFS		
	JSN		
DATE	SEPTEMBER 2003		
SCALE			

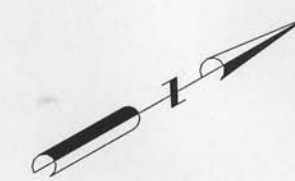
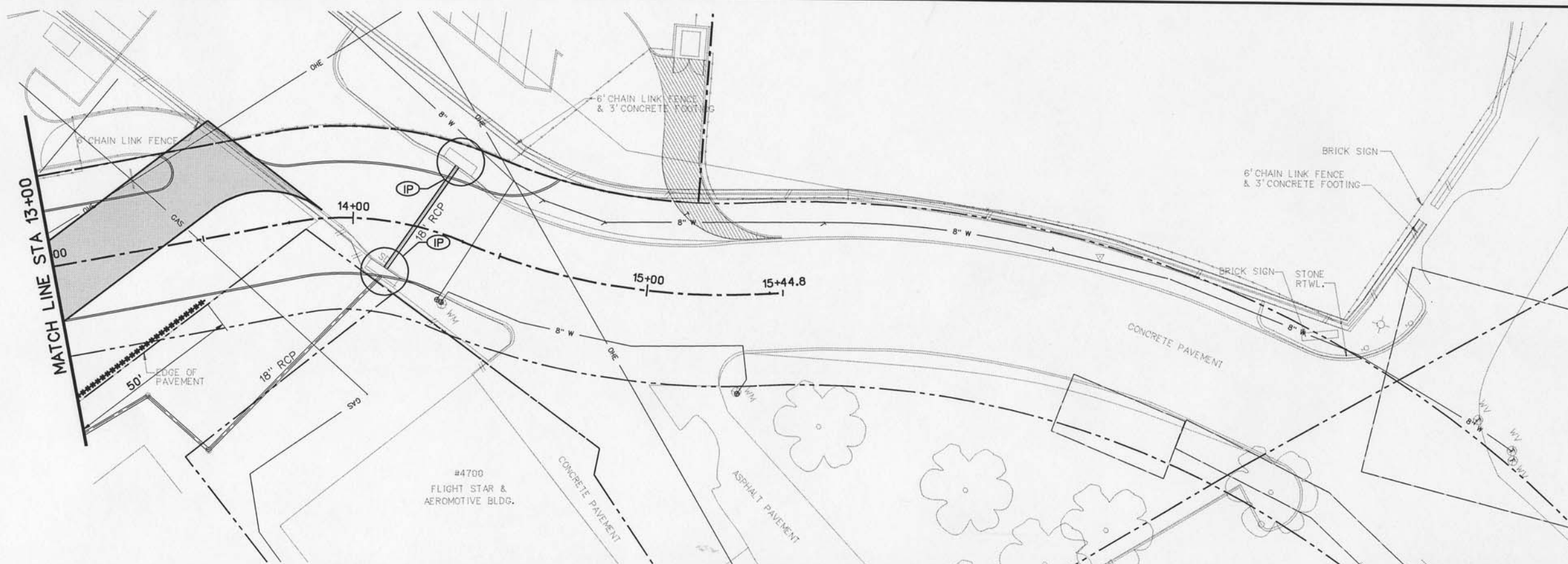
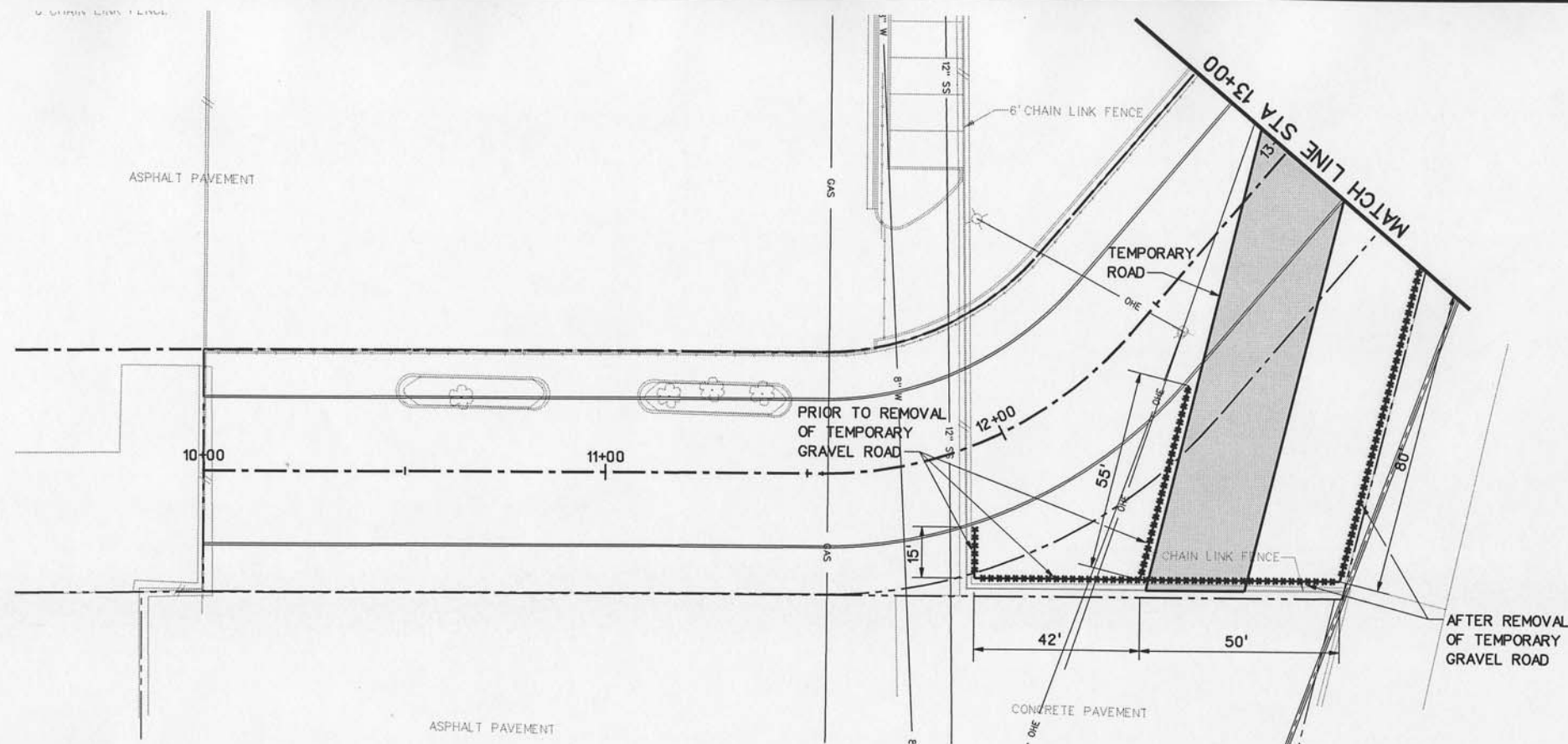


TOWN OF ADDISON AIRPORT PARKWAY REALIGNMENT	SHEET
PAVING DETAILS	
TOWN OF ADDISON TEXAS	



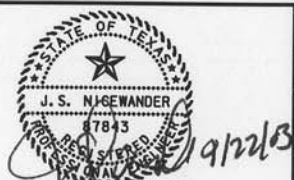
LEGEND

- SILT FENCE
- (IP) INLET PROTECTION



22-SEP-2003 12:17 G:\39020\Cad\Sheets\opsw3p01.dgn

NO.	DATE	REVISION	APPROV.	NO.	DATE	REVISION	APPROV.



HNTB ARCHITECTS ENGINEERS PLANNERS
The HNTB Companies

DESIGN CHECK JSN
CHECK JDH
DRAWN GFS
CHECK JSN
DATE SEPTEMBER 2003
SCALE

PROJECT NO. 39020

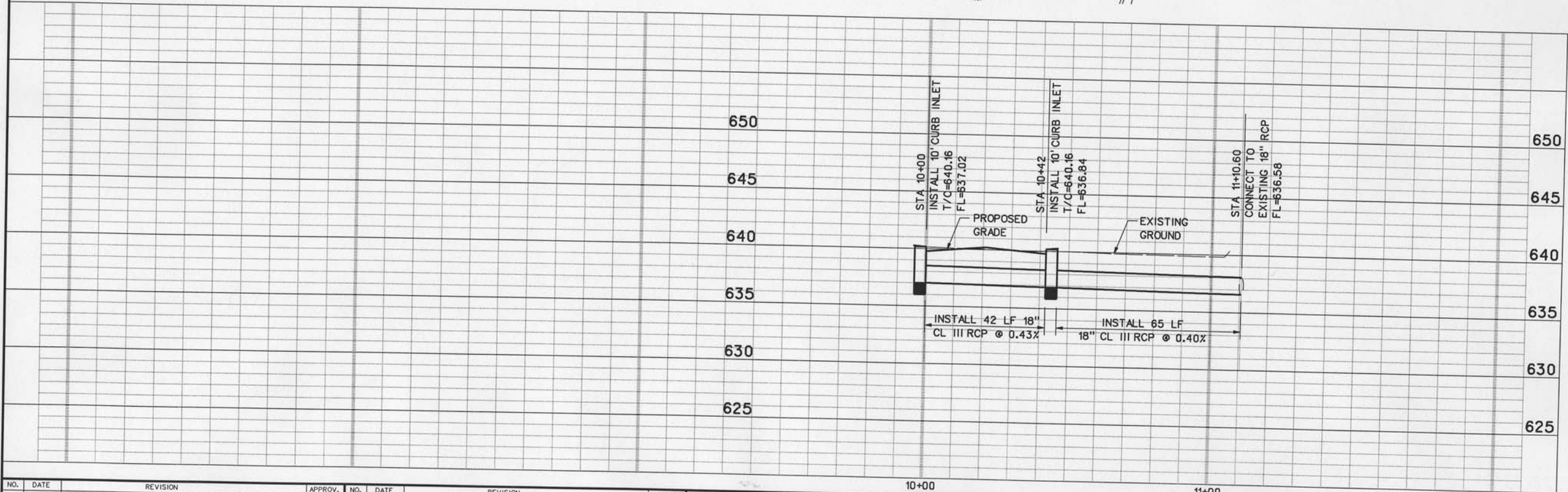
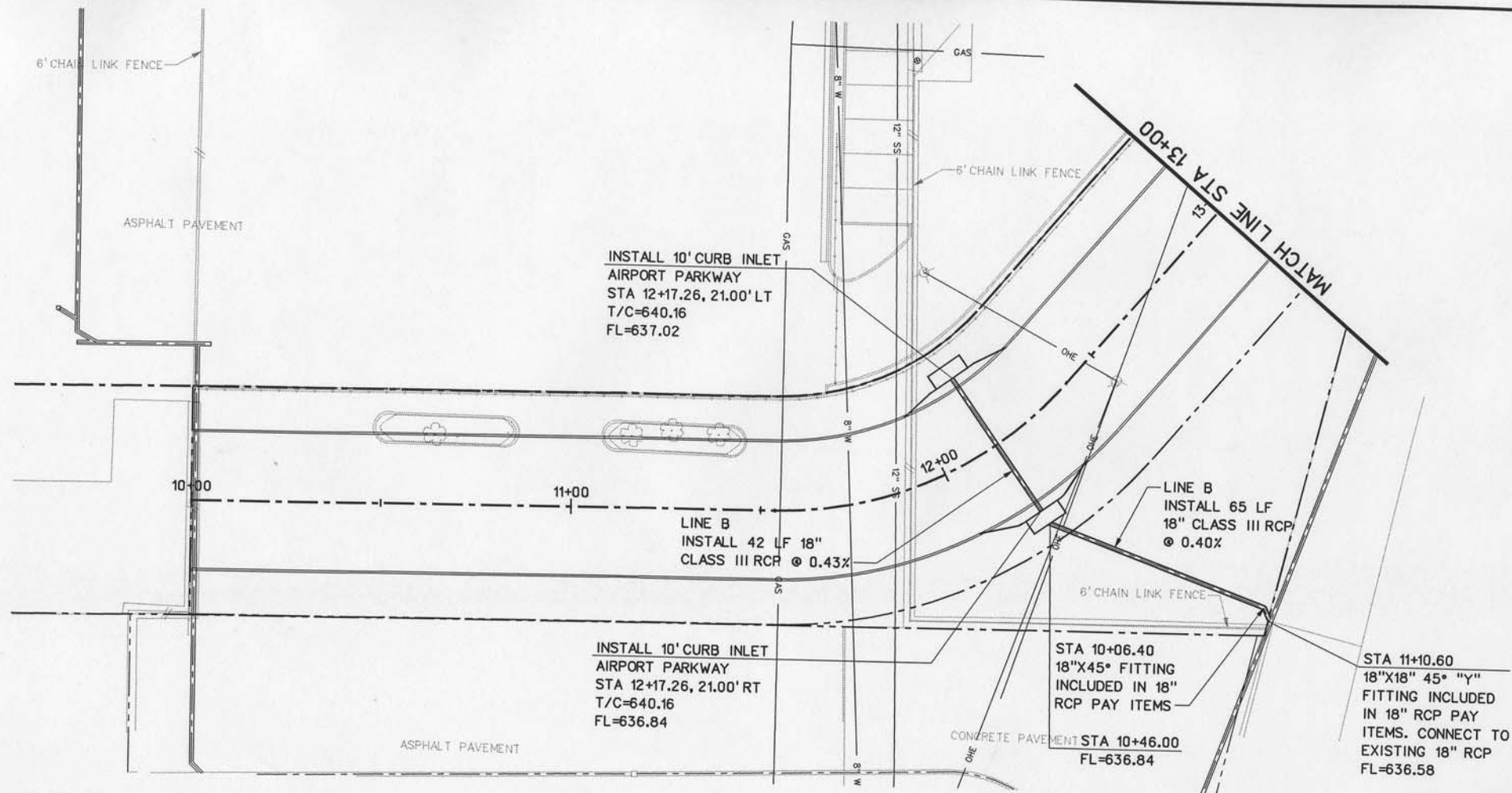
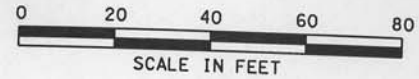


TOWN OF ADDISON
AIRPORT PARKWAY REALIGNMENT

EROSION CONTROL PLAN
STA 10+00 TO STA 15+44.8

TOWN OF ADDISON TEXAS

SHEET
EC-1



22-SEP-2003 13:03 G:\35020\Cad\Sheets\opdp01.dgn

NO.	DATE	REVISION	APPROV.	NO.	DATE	REVISION	APPROV.



HNTB ARCHITECTS ENGINEERS PLANNERS
The HNTB Companies

DESIGN CHECK: JSN, JDH
DRAWN CHECK: GFS, JSN
DATE: SEPTEMBER 2003
SCALE:

PROJECT NO. 39020



TOWN OF ADDISON
AIRPORT PARKWAY REALIGNMENT

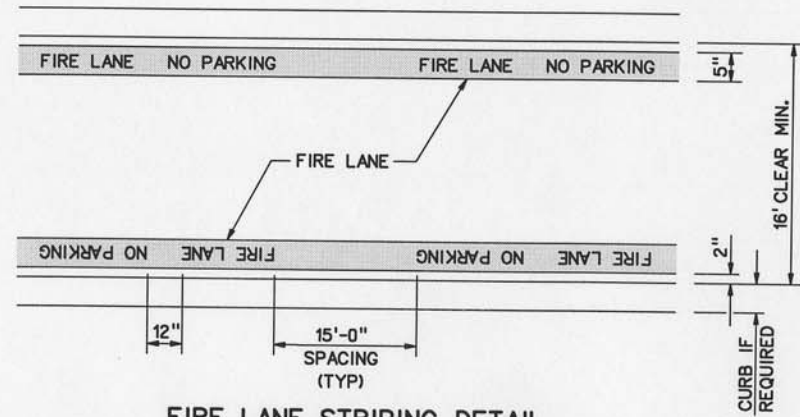
STORM SEWER PLAN/PROFILE
STA 10+00 TO STA 13+00

TOWN OF ADDISON, TEXAS

SHEET

D-1

22-SEP-2003 12:28
g:\39020\Cad\Sheets\opdt03.dgn



FIRE LANE STRIPING DETAIL
NOT TO SCALE

STRIPING DETAIL NOTES:

1. A. PAINT:
 1. STRIPE SHALL BE FIVE (5) INCHES WIDE PAINTED WITH AN EXTERIOR LATEX PAINT. COLOR SHALL BE "TRAFFIC RED" GLIDDEN NO. 63251 OR EQUAL.
 2. LETTERS SHALL BE FOUR (4) INCHES HIGH PAINTED WITH AN EXTERIOR ACRYLIC LATEX PAINT. COLOR SHALL BE "TRAFFIC WHITE" GLIDDEN NO. 563245 OR EQUAL.
- B. APPLICATION:
 1. STRIPE MAY BE BRUSHED OR SPRAYED, ONE COAT TO FINISH.
 2. LETTERS SHALL BE STENCIL FORMED, BRUSH APPLIED AND SPACED AS DETAILED ON THIS SHEET.
2. "FIRE LANE NO PARKING" TO BE PAINTED ENTIRE LENGTH OF DEDICATED FIRE LANE.
3. WHERE PRACTICAL ALL FIRE LANE MARKINGS SHALL BE REQUIRED TO BE PLACED ON THE CURB.
4. FOR ADDITIONAL FIRE LANE SPECIFICATIONS, PLEASE CONTACT THE FIRE DEPARTMENT.

NO.	DATE	REVISION	APPROV.	NO.	DATE	REVISION	APPROV.



HNTB ARCHITECTS ENGINEERS PLANNERS
The HNTB Companies

DESIGN CHECK	JSN	PROJECT NO.	39020
CHECK	JDH		
DRAWN CHECK	GFS		
CHECK	JSN		
DATE	SEPTEMBER 2003		
SCALE			



TOWN OF ADDISON	SHEET
AIRPORT PARKWAY REALIGNMENT	
MISCELLANEOUS DETAILS	
TOWN OF ADDISON, TEXAS	