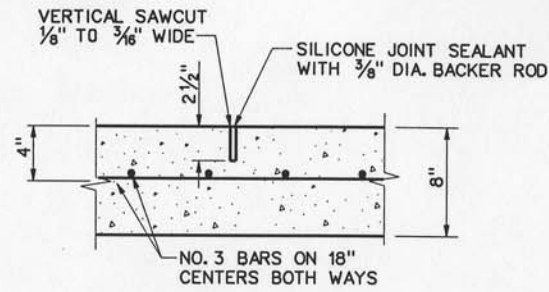
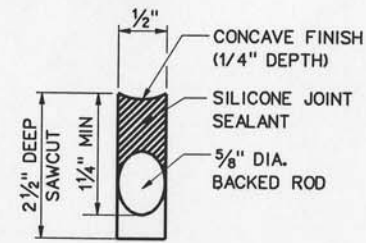


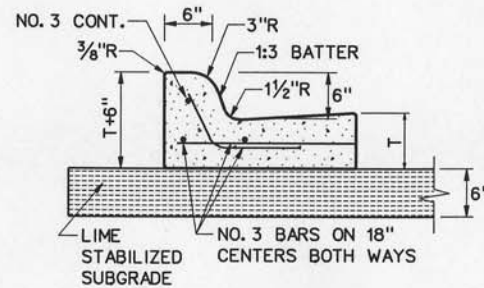
CONSTRUCTION JOINT
NOT TO SCALE



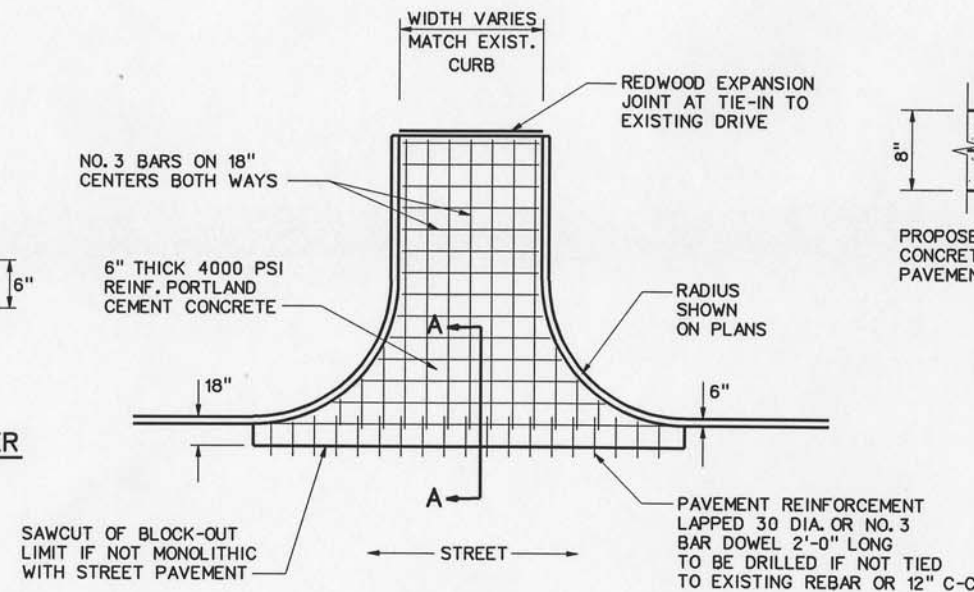
SAWED DUMMY JOINT
NOT TO SCALE



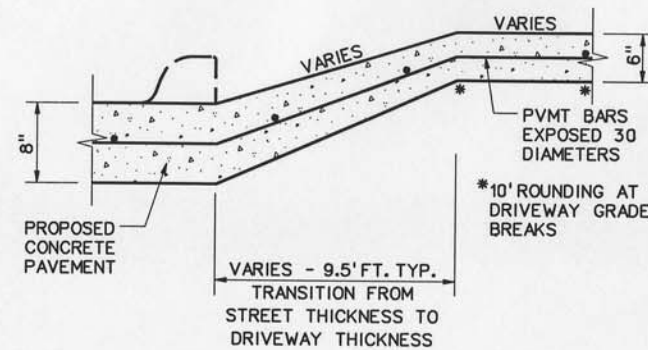
TYPICAL JOINT DETAIL
NOT TO SCALE



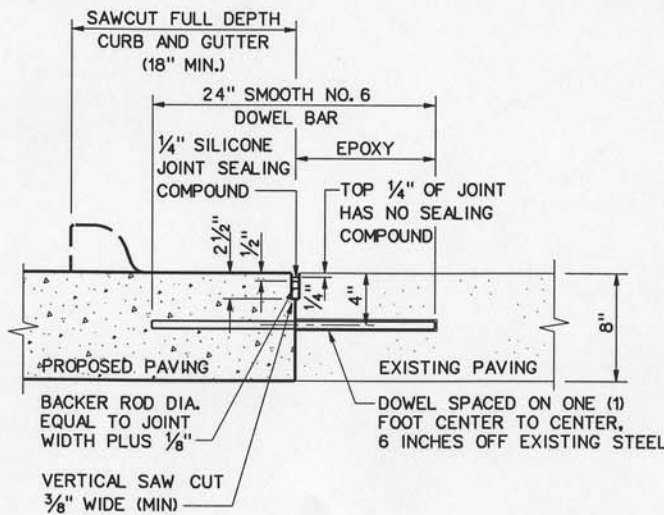
INTEGRAL CURB AND GUTTER
NOT TO SCALE



DRIVE APPROACH
NOT TO SCALE

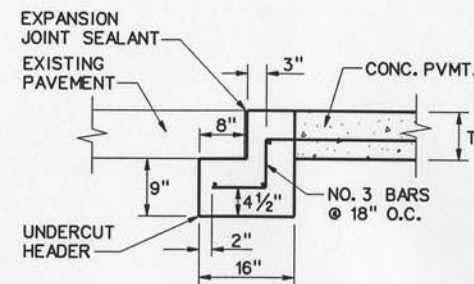


SECTION A-A
NOT TO SCALE



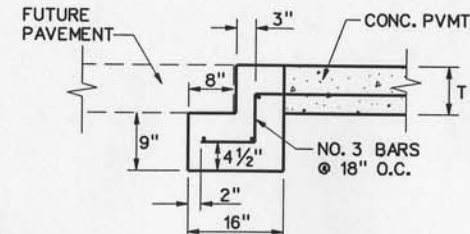
LONGITUDINAL BUTT JOINT
NOT TO SCALE

NOTE:
DOWEL BARS SHALL BE DRILLED INTO EXIST. PAVEMENT HORIZONTALLY BY USE OF A MECHANICAL RIG. DRILLING BY HAND IS NOT ACCEPTABLE, PUSHING DOWEL BARS INTO GREEN CONCRETE IS NOT ACCEPTABLE.



PAVEMENT BARS TO BE BENT DOWN INTO HEADER. HEADER AND PAVEMENT TO BE MONOLITHIC.

STREET HEADER
NOT TO SCALE



NOTE:

STEEL DOWELS WILL BE USED FOR LOAD TRANSFER AT ALL TRAVERSE CONTRACTION JOINTS (SAWED DUMMY JOINTS). THE DOWELS WILL BE 24 INCHES LONG, PLACED 12 O-C AND LOCATED MID-HEIGHT IN THE SLAB. NO. 6 SMOOTH DOWELS WILL BE USED. NO SEPERATE PAY ITEM WILL BE ESTABLISHED FOR THIS ITEM.

NOTES:

- UNLESS SPECIFICALLY STATED OTHERWISE IN PLANS OR CONTRACT DOCUMENTS, THE CONTROLLING SPECIFICATIONS FOR ALL WORK WITHIN PUBLIC RIGHT-OF-WAY AND EASEMENTS SHALL BE THE STANDARD SPECIFICATIONS FOR PUBLIC WORKS CONSTRUCTION, THIRD EDITION 1998, AS AMENDED BY THE NORTH CENTRAL TEXAS COUNCIL OF GOVERNMENTS (THE "STANDARD SPECIFICATIONS"). COPIES OF THE STANDARD SPECIFICATIONS MAY BE PURCHASED BY MAIL OR OVER THE COUNTER FROM THE NORTH CENTRAL TEXAS COUNCIL OF GOVERNMENTS, 616 SIX FLAGS DRIVE, P.O. BOX DRAWER COG, ARLINGTON, TEXAS 76005-5888. PHONE METRO 817/640-3300. BULK DISCOUNTS ARE AVAILABLE. THIS DOCUMENT IS COPYRIGHTED.
- ROUGH GRADING SHALL BE ACCOMPLISHED TO WITHIN +/- 0.10 FEET OF PLAN ELEVATION.
- ALL TRAFFIC CONTROL NECESSARY FOR THE WORK SHALL BE PROVIDED BY THE CONTRACTOR. ALL BARRICADES, WARNING SIGNS, LIGHTS, DEVICES, ETC. FOR THE GUIDANCE AND PROTECTION OF TRAFFIC AND PEDESTRIANS MUST CONFORM TO THE INSTALLATIONS SHOWN IN THE LATEST ISSUE OF THE "TEXAS MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES", AS CURRENTLY AMENDED, TEXAS DEPARTMENT OF TRANSPORTATION.
- ALL FILL SHALL BE PLACED IN MAXIMUM 8-INCH LIFTS COMPACTED TO 95% OF STANDARD PROCTOR DENSITY BETWEEN 0% AND +3% OF OPTIMUM MOISTURE CONTENT.
- ALL TREES, STUMPS, BRUSH, GRASSES AND SURFACE ORGANICS WITHIN PROPOSED RIGHT-OF-WAY ARE TO BE REMOVED AND PROPERLY DISPOSED OF OFF-SITE. TREE REMOVAL PERMITS, IF REQUIRED, WILL BE OBTAINED BY THE OWNER.
- EXISTING UTILITY POLES, IF ANY, WILL BE REMOVED OR RELOCATED BY THE UTILITY COMPANIES THROUGH COORDINATION BY THE OWNER. CONTRACTOR SHALL BRING TO THE OWNER'S ATTENTION ANY FACILITIES THAT APPEAR TO BE IN CONFLICT SO THAT THE OWNER HAS SUFFICIENT TIME TO ACCOMPLISH THE NECESSARY RELOCATIONS.
- WHERE DEEP VERTICAL EXCAVATIONS (IN EXCESS OF 3 FEET) ARE INDICATED, CUTS SHALL BE LAID BACK AT A STABLE SLOPE (ON OWNER'S PROPERTY) UNTIL WALLS ARE CONSTRUCTED. BACKFILL MATERIAL SHALL BE STOCKPILED ON-SITE AT THE DIRECTION OF THE OWNER.
- ALL REINFORCING STEEL AND DOWEL BARS IN PAVEMENT SHALL BE SUPPORTED AND MAINTAINED AT THE CORRECT CLEARANCES BY THE USE OF BAR CHAIRS OR OTHER APPROVED SUPPORT.
- THE USE OF WOOD FORMS FOR PAVEMENT CONSTRUCTION WILL BE PERMITTED.
- DUMMY JOINTS SHALL BE SAWED IN THE PAVEMENT IN THIS PROJECT. SPACING VARIATIONS SHALL BE MADE AT BLOCKOUTS, CONSTRUCTION JOINTS, AND STREET INTERSECTIONS TO LINE UP WITH EXISTING PAVING JOINTS AS SHOWN ON THE PLANS. THE SPACING BETWEEN ANY JOINT SHALL NOT BE LESS THAN 8 FEET NOR MORE THAN 12 FEET. ALL DUMMY JOINTS SHALL BE SAWED NO SOONER THAN 4 HOURS AND NO LATER THAN 12 HOURS AFTER THE PLACEMENT OF THE PAVEMENT.
- TWO-WAY TRAFFIC MUST BE MAINTAINED AT ALL TIMES ON PUBLIC STREETS ADJACENT TO THIS PROJECT UNLESS SPECIFIED OTHERWISE IN THE PLANS AND SPECIFICATIONS, OR DIRECTED BY THE ENGINEER. THE TRAVELWAY WIDTH SHALL NOT BE LESS THAN 10 FEET.
- SEE TYPICAL SECTIONS SHEET FOR ADDITIONAL DETAILS AND NOTES.
- ALL CURBS SHALL BE PLACED INTEGRAL WITH PAVEMENT.
- CURBS SHALL MEET THE SAME COMPRESSIVE STRENGTH AS SPECIFIED FOR THE CONCRETE PAVEMENT.
- BAR LAPS SHALL BE 30 DIAMETERS.

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NO.	DATE	REVISION	APPROV.	NO.	DATE	REVISION	APPROV.



HNTB ARCHITECTS ENGINEERS PLANNERS The HNTB Company	
DESIGN CHECK	JSN
CHECK	JDH
DRAWN CHECK	GFS
CHECK	JSN
DATE	SEPTEMBER 2003
SCALE	



TOWN OF ADDISON AIRPORT PARKWAY REALIGNMENT	SHEET
PAVING DETAILS	
TOWN OF ADDISON TEXAS	