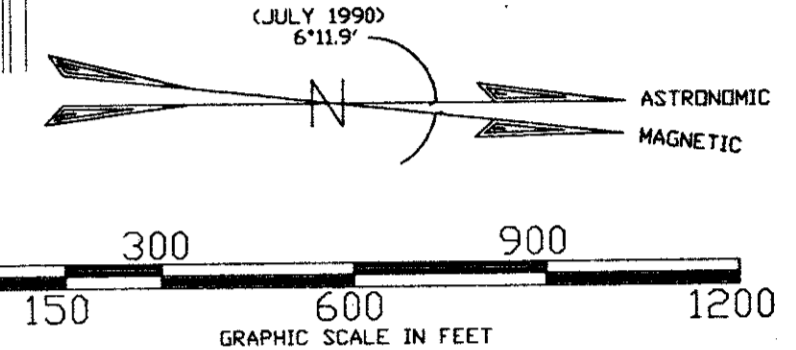
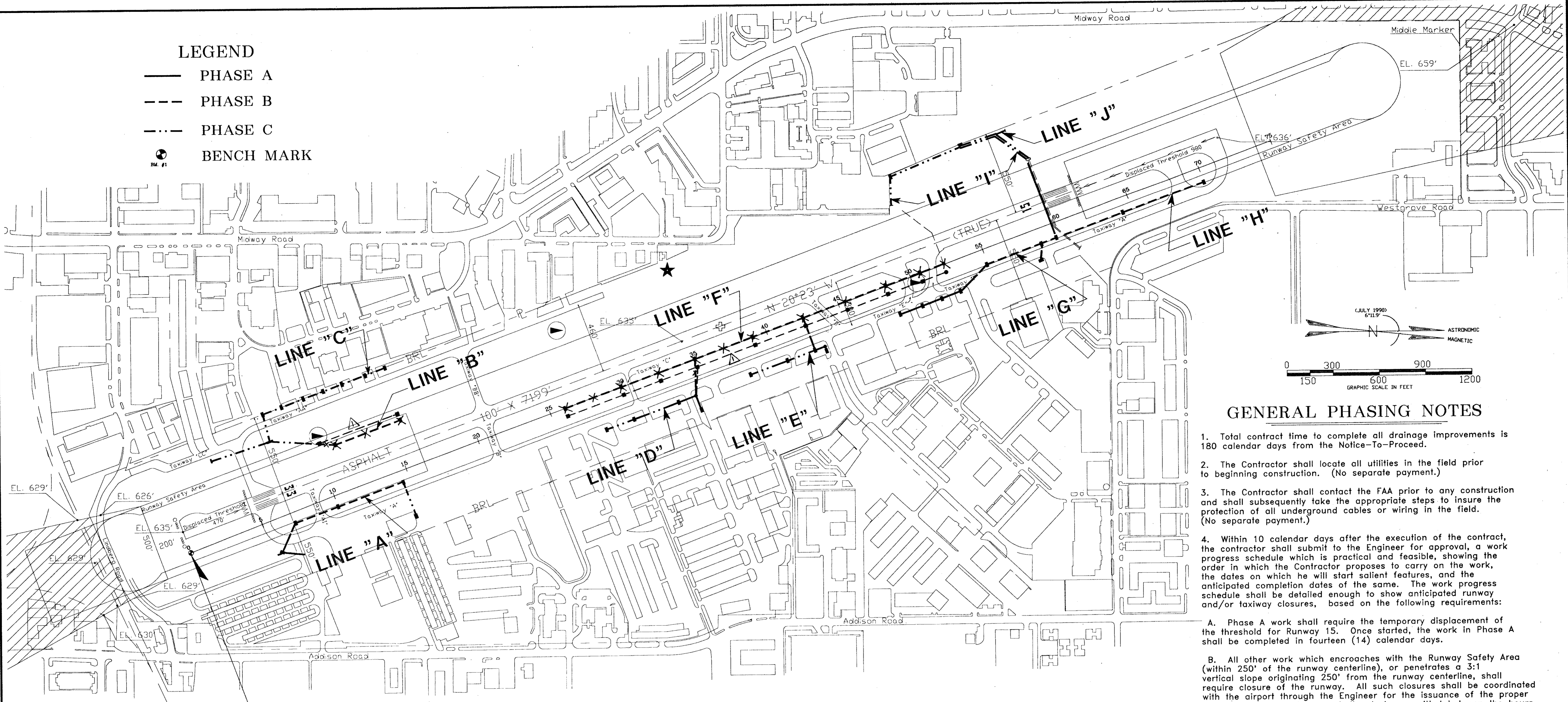


LEGEND

- PHASE A
- - - PHASE B
- · · PHASE C
- ⊙ BENCH MARK



GENERAL PHASING NOTES

1. Total contract time to complete all drainage improvements is 180 calendar days from the Notice-To-Proceed.
2. The Contractor shall locate all utilities in the field prior to beginning construction. (No separate payment.)
3. The Contractor shall contact the FAA prior to any construction and shall subsequently take the appropriate steps to insure the protection of all underground cables or wiring in the field. (No separate payment.)
4. Within 10 calendar days after the execution of the contract, the contractor shall submit to the Engineer for approval, a work progress schedule which is practical and feasible, showing the order in which the Contractor proposes to carry on the work, the dates on which he will start salient features, and the anticipated completion dates of the same. The work progress schedule shall be detailed enough to show anticipated runway and/or taxiway closures, based on the following requirements:
 - A. Phase A work shall require the temporary displacement of the threshold for Runway 15. Once started, the work in Phase A shall be completed in fourteen (14) calendar days.
 - B. All other work which encroaches with the Runway Safety Area (within 250' of the runway centerline), or penetrates a 3:1 vertical slope originating 250' from the runway centerline, shall require closure of the runway. All such closures shall be coordinated with the airport through the Engineer for the issuance of the proper NOTAM's. Runway closures shall only be permitted between the hours of 10 p.m. and 6 a.m. See Safety Note 4 on Sheet 3.
 - C. All work which encroaches within the Taxiway Safety Area (within 40' of the taxiway centerline) shall require closure of that portion of the taxiway and shall be coordinated with the Airport through the Engineer for the issuance of the proper NOTAM's. No more than one (1) crossfield taxiway shall be closed during any one daytime period. No more than one (1) taxiway into an apron area shall be closed at any one time. Closures of crossfield and apron taxiways shall be coordinated with the Engineer on a case-by-case basis.
 - D. All trenches within the runway safety area, except those included in Phase A work, shall be backfilled nightly to the required densities, and all stockpiled or excavated material within the Runway Safety Area shall be removed daily to the acceptance of the Airport through the Engineer prior to re-opening the runway.
 - E. Access to all hangar and apron areas shall be maintained during daytime hours.
 5. Drainage is to be maintained by the Contractor at all times. (No separate payment.)
 6. Barricades shall be low level timber with flashers. See Detail Sheet No. 15. Barricades shall be placed at the discretion of the Engineer to indicate a closed taxiway or other hazardous condition adjacent to aircraft travel path. No separate payment will be made for this item.
 7. See Special Provisions in the Contract Specifications for additional requirements during construction.

PHASE A

- The Work of Phase A shall include the following:
1. Temporarily displace Runway 15 threshold 1220 feet southeast. This shall include removal of existing runway numeral, threshold and touchdown zone (TDZ) markings and placement of temporary runway numeral and threshold markings (See Detail Sheet 15), and temporary outboard threshold lights. Place barricades across the runway 500' from construction zone as shown on plans.
 2. Construct 500 l.f. 66" RCP as shown on plans.
 3. Replace runway pavement, see Detail Sheet No. 15
 4. Remove temporary displaced threshold markings, lights and replace original runway numeral, threshold and TDZ markings. Remove barricades.
 5. After inspection and acceptance by the Engineer, re-open runway.

PHASE B

- The Work of Phase B is within the Runway Safety Area, and therefore shall require closure of the runway. The runway shall only be closed between the hours of 10 p.m. and 6 a.m.
- Phase B work shall include:
1. Set up barricades across taxiways where the work is to progress that night.
 2. Construct drainage improvements as outlined on plans.
 3. Backfill all open trenches to required densities, and remove all stockpiled or excavated material, and equipment from the runway safety area prior to 6 a.m. daily.
 4. Construct pavement patch where required and remove barricades. No more than one (1) taxiway in the Phase B work area may remain closed (unpaved) during any given daytime period.
 5. After inspection and acceptance by the Engineer, re-open the runway by 6 a.m. daily.

PHASE C

- The Work of Phase C is outside the Runway Safety Area, and may progress at the Contractor's option and as approved by the Engineer. All taxiway closings shall be coordinated through the Engineer. No more than one (1) taxiway into an apron area may be closed at a time.
- Phase C work shall include:
1. Set up barricades where deemed necessary by the Engineer for that construction area.
 2. Construct drainage improvements as outlined on plans.
 3. Backfill all trenches to required densities, and remove all stockpiled or excavated materials.
 4. Construct pavement patch where required.
 5. After inspection and acceptance by the Engineer, re-open the taxiway.

B.M.
FOR EXACT LOCATION
SEE PLAN & PROFILE (LINE A)
SHEET NO. 5

AS BUILT
JUL 30 1992



DESIGN: F.C.R.	A.I.P. NO. 3-48-0063-03-91		
DRAWN: M.J.C.	S.P. NO.		
CHECKED: D.W.P.	JOB NO. Y8024.22		
SCALE: 1" = 300'	Date	Revisions	By

Greiner
Greiner, Inc.
Fort Worth, Texas

Engineers, Architects
and Planners



ADDISON AIRPORT

DRAINAGE IMPROVEMENTS
PHASING PLAN

SHEET 4
DATE: SEPT. 1991