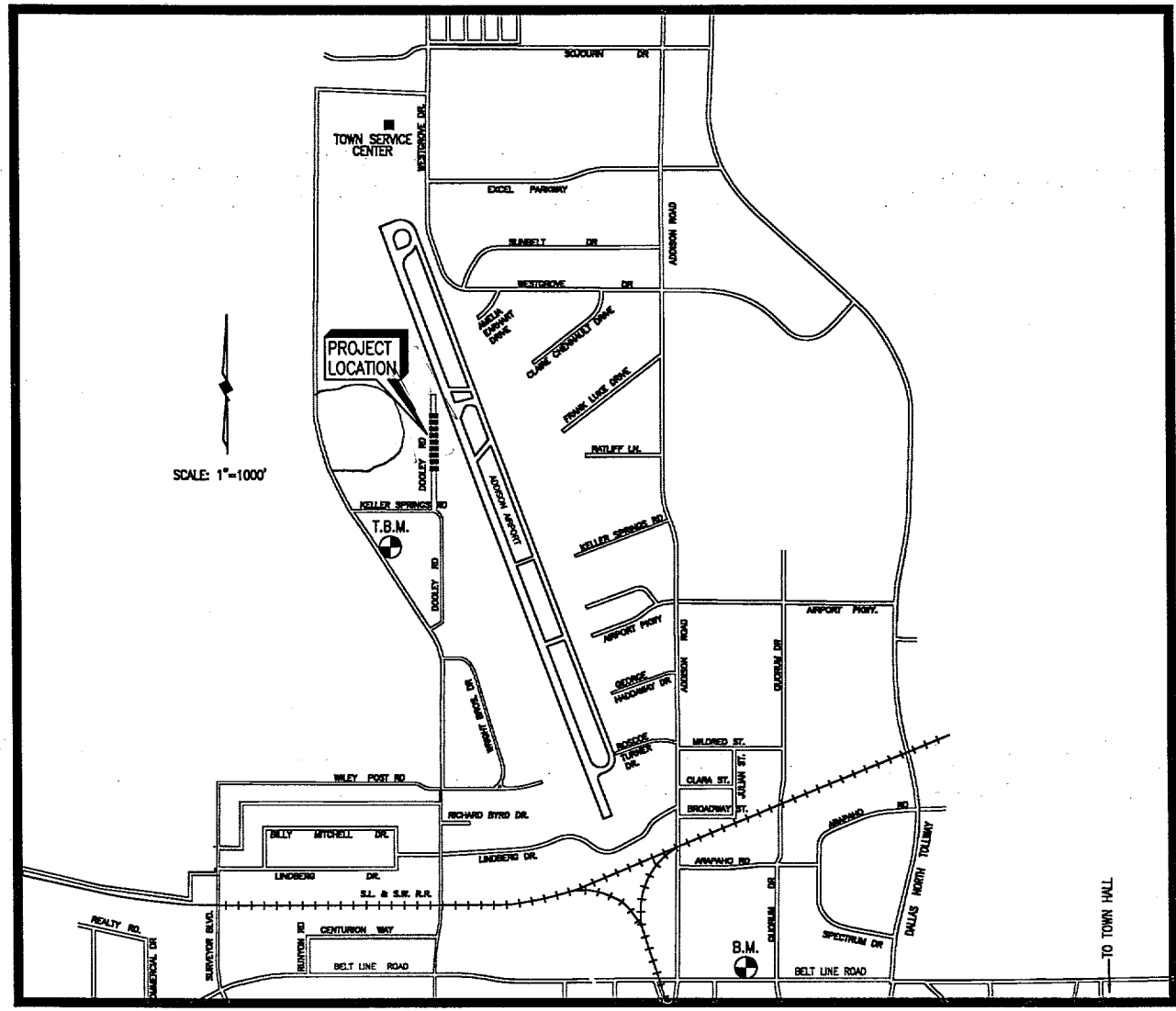


Addison!

CONSTRUCTION PLANS FOR ~~PROPERTY~~ ADDISON AIRPORT RECLAMATION LAND



AS-Built Essentially as planned, except additional fill was placed on former Chester Property.

SHIMEK, JACOBS & FINKLEA, L.L.P.
 CONSULTING ENGINEERS
 Dallas, Texas

NOT TO SCALE

FEBRUARY 1998

SHEET INDEX

SHEET DESCRIPTION	SHEET NO.
CONSTRUCTION PLAN	1
EXISTING GRADING PLAN	2
PROPOSED GRADING PLAN	3
CROSS SECTIONS	4 & 5
DETAILS	6 & 7

BID
 SET



THESE DOCUMENTS ARE FOR BIDDING, CONSTRUCTION, AND PERMIT PURPOSES.
John W. Brinkhoff
 DATE: 3/17/98

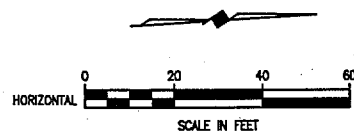
FF-16

REVISED 03/16/98 B.L. 98100-00.DWG SCALE: 1=1 BLOCKS: LOC-NIT IMAGE: ADDISON.TIF

GEORGE SYMS SURVEY
ABSTRACT NO. 1344
DALLAS COUNTY

BENCHMARK:
 U.S.G.S. DISK IN BRICK WALL OF
 OLD ADDISON SCHOOL BUILDING
 (4.7 FEET ABOVE GROUND)
 ELEV. = 650.61

T.B.M. 17:
 "C" CUT CENTER OF INLET 275 FT.
 OF KELLER SPRING ROAD, EAST
 SIDE OF MIDWAY ROAD
 ELEV. = 639.05



ESTIMATED QUANTITIES

DESCRIPTION	UNIT	QTY.
REMOVE & SALVAGE EXISTING CHAIN LINK FENCE	L.F.	658
REMOVE EXISTING MONOLITHIC CONCRETE CURB	L.F.	61
REMOVE EXISTING REINFORCED CONCRETE PAVEMENT	S.Y.	41.6
REMOVE & SALVAGE UTILITY POLE & GUYWIRE	EA.	1
INSTALL SALVAGED 6" CHAIN LINK FENCE INCLUDING MOW STRIP	L.F.	354.1
INSTALL SALVAGED 6" CHAIN LINK FENCE IN CONCRETE PAVEMENT	L.F.	28
FURNISH & INSTALL DOUBLE SWING GATE	EA.	1
FURNISH & INSTALL 5" STANDARD INLET	EA.	1
FURNISH & INSTALL 10" STANDARD INLET	EA.	1
FURNISH & INSTALL 18" CLASS W R.C.P.	L.F.	35
CONNECT TO EXISTING R.C.P. STORM SEWER PIPE	S.Y.	8
CONST. 8" REIN. CONC. PMNT. @ PROP. LATERALS	L.F.	37.3
PLUG EXISTING STORM SEWER LATERAL/ REMOVE INLET	EA.	3
REMOVE EXISTING FIRE HYDRANT & VALVE	EA.	1
RELOCATE EXISTING FIRE HYDRANT & VALVE	EA.	1
FURNISH & INSTALL 8" WATER LINE	L.S.	1
CONNECT TO EXISTING 8" WATER LINE	EA.	1
CUT & PLUG EXISTING 8" WATER LINE	EA.	1
FURNISH & INSTALL EROSION CONTROL DEVICES	L.F.	175
ESTABLISH GRASS	ACRES	1.2
COMPACTED FILL	C.Y.	3,250 #
ADJUST MANHOLE	EA.	1
VALVE RISERS	EA.	2

COMPACTED IN PLACE QUANTITY

TOWN OF ADDISON
 FORMERLY
PROTECTA-CAB, INC.
 VOL. 90160, PG. 1555
 65,235.02 S.F.

TOWN OF ADDISON
 FORMERLY
WILLIAM F. CHESTER
 VOL. 84216, PG. 3066
 65,152.15 S.F.

ROFA = APPROXIMATE LOCATION OF RUNWAY
 OBJECT FREE AREA BOUNDARY

NOTES:

- ALL EXISTING FENCE WHICH IS REMOVED SHALL BE SALVAGED AND PROVIDED TO THE TOWN OF ADDISON. FABRIC SHALL BE ROLLED. BARBED WIRE SHALL BE ROLLED. POSTS SHALL HAVE CONCRETE REMOVED AND STACKED. RAILS SHALL BE STACKED. TIES SHALL BE DISPOSED OF OFF-SITE. SALVAGED MATERIAL SHALL BE DELIVERED TO AIRPORT BASE OPERATOR. BOTTOM RAILS SHALL BE PROVIDED. UTILIZE SALVAGED TOP RAIL AND FURNISH AND INSTALL POST TO RAIL HARDWARE.
- CONNECTION TO EXISTING STORM SEWER SHALL BE COMPLETED BY DRILLING EXISTING STORM SEWER PIPE TO THE REQUIRED RADIIUS, THEN KNOCKING OUT THE WALL OF THE 48" R.C.P. INSERT PROPOSED LATERAL PIPE AND GROUT CONNECTION. EXISTING 48" R.C.P. SHALL BE CLEANED PRIOR TO APPLYING GROUT.
- ALL PROPOSED STORM SEWER LATERAL PIPE SHALL BE CLASS III.
- WHERE EXISTING 18" R.C.P. STORM SEWER LATERALS ARE TO BE PLUGGED, INSTALL 18" R.C.P. PLUGS. FILL EXISTING INLET BOXES WITH CONCRETE TO 12" ABOVE THE TOP OF THE LATERAL PIPES. THEN, FILL INLET BOXES WITH NATIVE FILL MATERIAL, COMPACTED IN 12" LIFTS.
- PAVEMENT AND CURB REMOVAL AT LOCATIONS FOR REMOVAL OF EXISTING INLETS SHALL NOT BE RECONSTRUCTED.
- EROSION CONTROL DEVICES (FABRIC FENCE) SHALL BE ERECTED ALONG THE TOP OF BANKS (NORTH & WEST).
- AT CONTRACTORS EXPENSE, NEW FIRE HYDRANT AND/OR VALVE MAY BE SUBSTITUTED FOR RELOCATING FIRE HYDRANT.
- REMOVE EXISTING UTILITY POLE AND DELIVER TO SERVICE CENTER LOCATED NORTH END OF AIRPORT.
- CONTRACTOR MAY AT HIS COST FURNISH AND INSTALL NEW FENCE IN PLACE OF RECONSTRUCTING EXISTING FENCE. NEW FENCE SHALL BE AS CALLED FOR IN FENCE DETAILS.
- SANITARY SEWER MANHOLE SHALL BE RAISED BY:
 - REMOVING EXISTING CONE, RING & COVER
 - DRILL 18 INCH HD. 4 SHROUD BAR INTO EXISTING MANHOLE AT 1/4 POINTS. EPOXY TO A DEPTH OF 18 INCHES.
 - FORM AND CAST IN PLACE MANHOLE BARREL EXTENSION WITH EPOXY COATED JOINT.
 - JOINTS SHALL BE SEALED ON THE EXTERIOR WITH AN 18 INCH WIDE HEAT SHRINKABLE MANHOLE SEAL, AQUAGARD (I.C. UTILITY) OR EQUAL.
 - FURNISHING & INSTALLING FLAT TOP WITH 30 INCH RING & COVER.

NOT TO SCALE

CONST. 5" STANDARD INLET A-1
 SMCUT EXIST. PMNT. 18" FROM
 BACK OF AND PARALLEL TO CURB
 & ON EA. SIDE OF PROP. LATERAL.
 REMOVE EXIST. CURB & PMNT. AS SHOWN
 CONST. 5" STANDARD INLET A-1
 T.C. 627.53, F.L. 623.01
 FURNISH & INSTALL 6 L.F. 18" R.C.P.
 CONNECT TO EXIST. ST. SEW. (SEE NOTE 2)

FURNISH & INSTALL
 2-VALVE RISERS SET
 TO FINAL GRADE = 628.25

REINSTALL EXIST. FIRE HYDRANT
 REINSTALL EXIST. FIRE HYDRANT & VALVE
 SET AT FINAL GRADE
 FURNISH & INSTALL:
 1 - 8" 90° BEND
 7 L.F. 8" WATER LINE
 CONNECT TO EXIST. 8" WATER LINE
 CUT, PLUG WITH THRUST BLOCKING
 ABANDON EXIST. 8" WATER LINE
 SEE NOTE 7

REMOVE & RELOCATE
 EXIST. FIRE HYDRANT & VALVE
 ABANDON EXIST. 8"
 WATER MAIN (SOUTH)

PLUG EXIST. 5" RECESSED INLET
 REMOVE INLET TOP AND 6" MIN.
 OF INLET BOX. PLUG EXIST.
 ST. SEW. LATERAL. FILL INLET
 WITH CONCRETE TO 12" ABOVE
 EXIST. LATERAL TOP OF PIPE.

RAISE MANHOLE TO
 EXISTING FIN EL. = 623.3
 FINISHED GRADE = 626.8

CONST. DOWELED CONC. CURB
 CONST. 37.3 L.F. DOWELED CONC. CURB
 CONSTRUCT CURB ACROSS ENTIRE WIDTH
 OF ROADWAY (FROM EAST BACK OF CURB
 TO WEST BACK OF CURB).

CONST. NEW GATE & INSTALL SALVAGED FENCE
 INSTALL SALVAGED 28 L.F. 6" CHAIN LINK
 FENCE W/3 STRAND BARBED WIRE TOPPING
 IN CONCRETE PAVEMENT
 INSTALL SALVAGED 36.3 L.F. 6" CHAIN LINK
 FENCE W/3 STRAND BARBED WIRE TOPPING
 WITH MOW STRIP
 FURNISH & INSTALL DOUBLE SWING GATE
 W/WHEELS (12" OVERALL LENGTH)
 NEW MATERIAL SHALL MATCH EXIST.
 POST, FABRIC & HARDWARE.

REMOVE POWER POLE
 AND GUYWIRE

CONST. 10" STANDARD INLET A-2
 SMCUT EXIST. PMNT. 18" FROM
 BACK OF AND PARALLEL TO CURB
 & ON EA. SIDE OF PROP. LATERAL.
 REMOVE EXIST. CURB & PMNT. AS SHOWN
 CONST. 10" STANDARD INLET A-2
 T.C. 627.53, F.L. 623.01
 FURNISH & INSTALL 29 L.F. 18" R.C.P.
 CONNECT TO EXIST. ST. SEW. (SEE NOTE 2)

PLUG EXIST. 5" RECESSED INLET
 REMOVE INLET TOP AND 6" MIN.
 OF INLET BOX. PLUG EXIST.
 ST. SEW. LATERAL. FILL INLET
 WITH CONCRETE TO 12" ABOVE
 EXIST. LATERAL TOP OF PIPE.

REMOVE 388 L.F. OF
 6" CHAIN LINK FENCE
 W/BARBED WIRE TOP
 (SHADED AREA, TYP.)

THESE DOCUMENTS ARE FOR
 BIDDING, CONSTRUCTION,
 AND PERMITS PURPOSES.
 DATE: 3/19/98



TOWN OF ADDISON, TEXAS
 ADDISON AIRPORT PROPERTY RECLAMATION
DOOLEY ROAD
CONSTRUCTION PLAN
 SHIMEK, JACOBS & FINKLEA, L.L.P.
 CONSULTING ENGINEERS
 Dallas, Texas

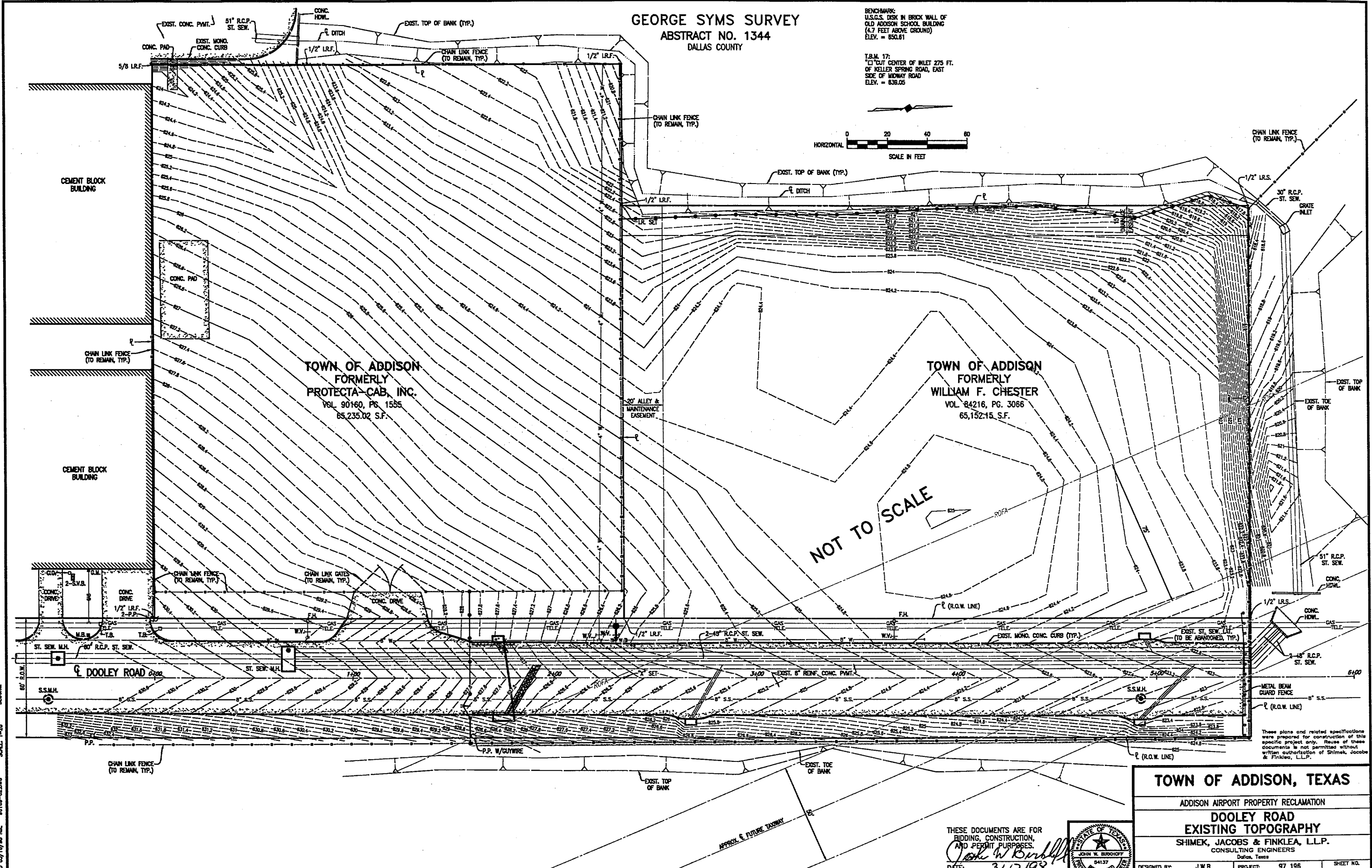
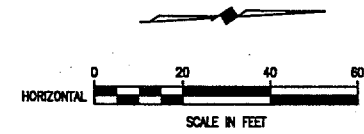
DESIGNED BY: J.W.B. PROJECT: 97 196 SHEET NO. 1
 DRAWN BY: R.J.L. DATE: FEBRUARY 1998 OF 7 SHEETS

REVISION 03/20/98 R.J.L. 98109-01.DWG SCALE: 1"=20'
 36"x24" FULL PLOT: DMS256.PCP HALF PLOT: 11x17.71.PCP

GEORGE SYMS SURVEY
ABSTRACT NO. 1344
DALLAS COUNTY

BENCHMARK:
 U.S.G.S. DISK IN BRICK WALL OF
 OLD ADDISON SCHOOL BUILDING
 (4.7 FEET ABOVE GROUND)
 ELEV. = 850.81

T.B.M. 17:
 "C" CUT CENTER OF INLET 275 FT.
 OF KELLER SPRING ROAD, EAST
 SIDE OF MIDWAY ROAD
 ELEV. = 839.05



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REVISION 02/16/98 R.L. 00189-02.DWG SCALE 1"=20' BLOCKS

TOWN OF ADDISON, TEXAS

ADDISON AIRPORT PROPERTY RECLAMATION

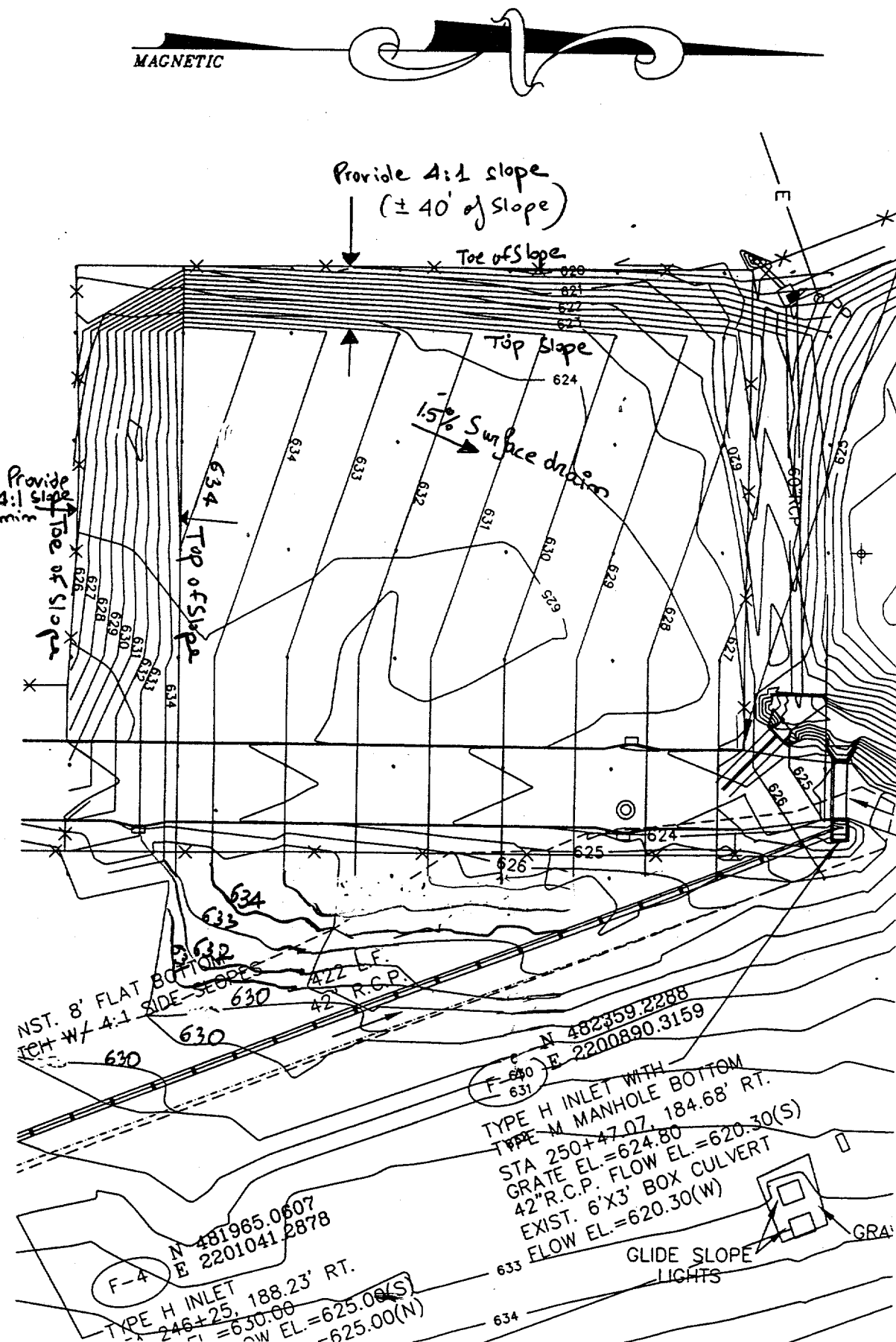
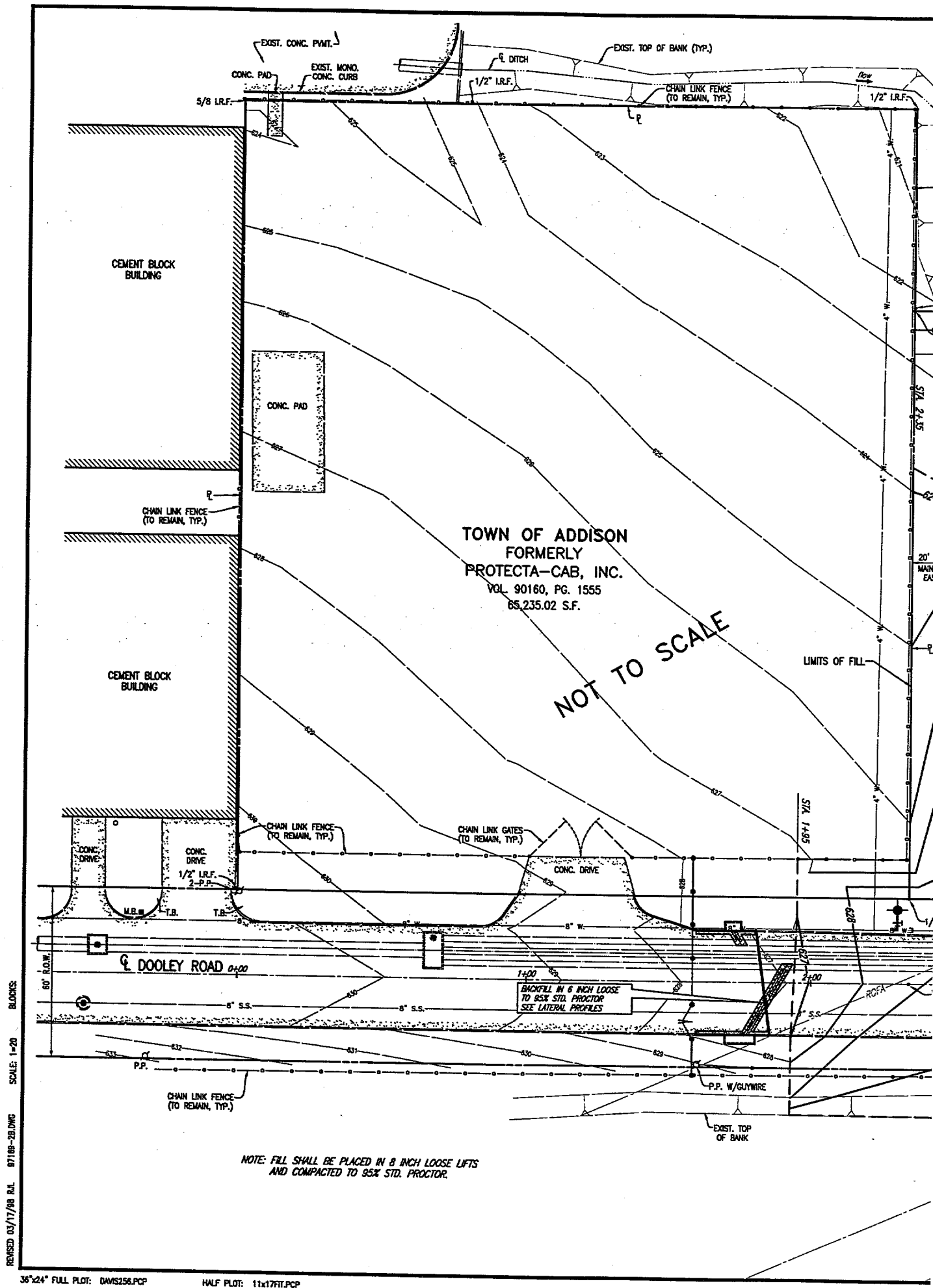
DOOLEY ROAD
EXISTING TOPOGRAPHY

SHIMEK, JACOBS & FINKLEA, L.L.P.
 CONSULTING ENGINEERS
 Dallas, Texas

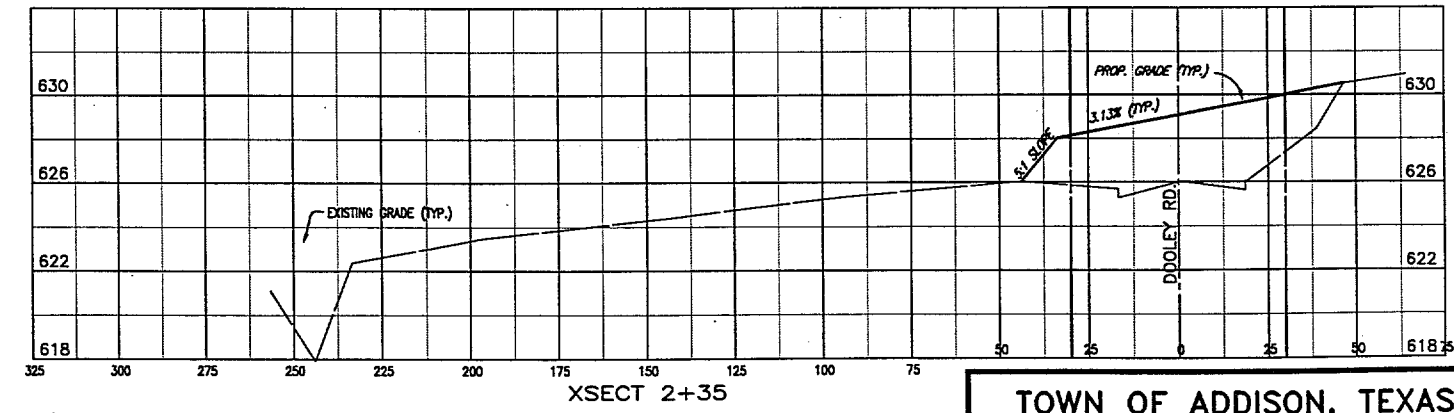
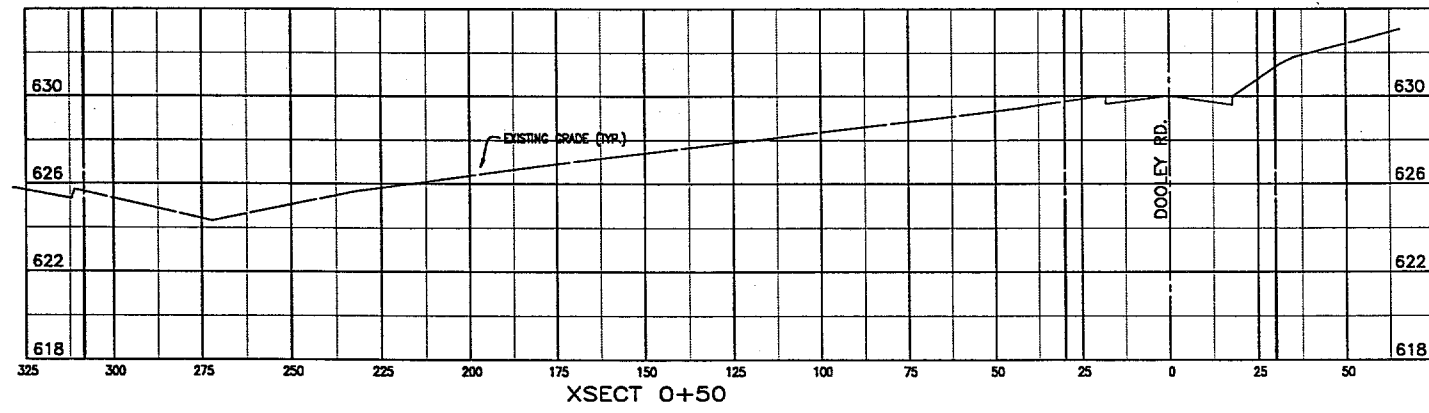
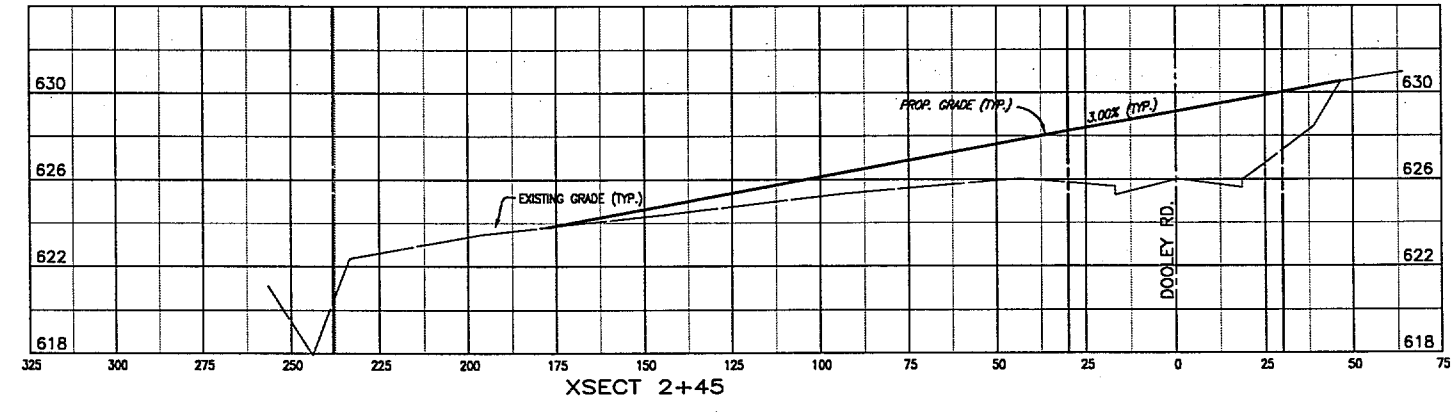
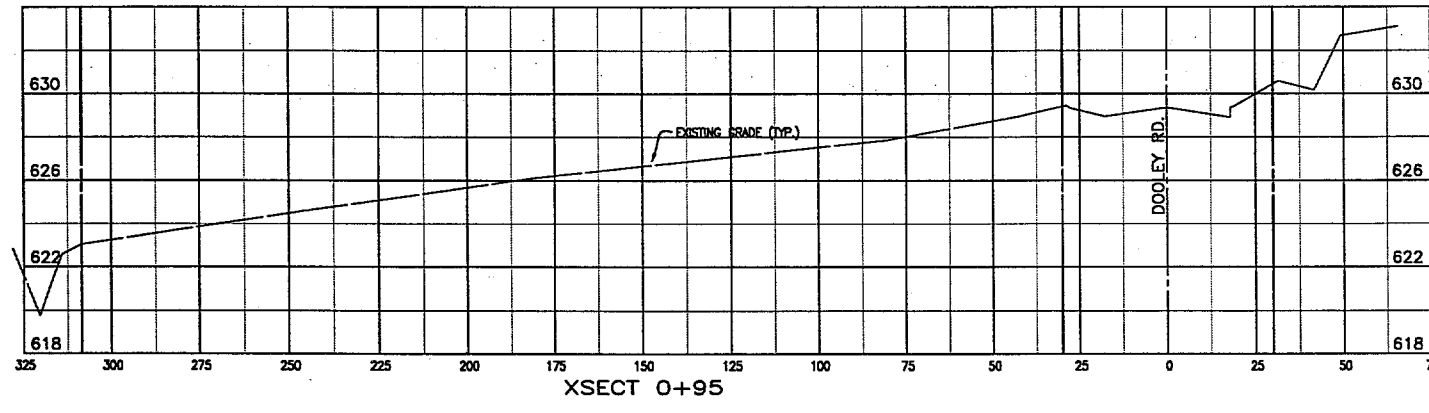
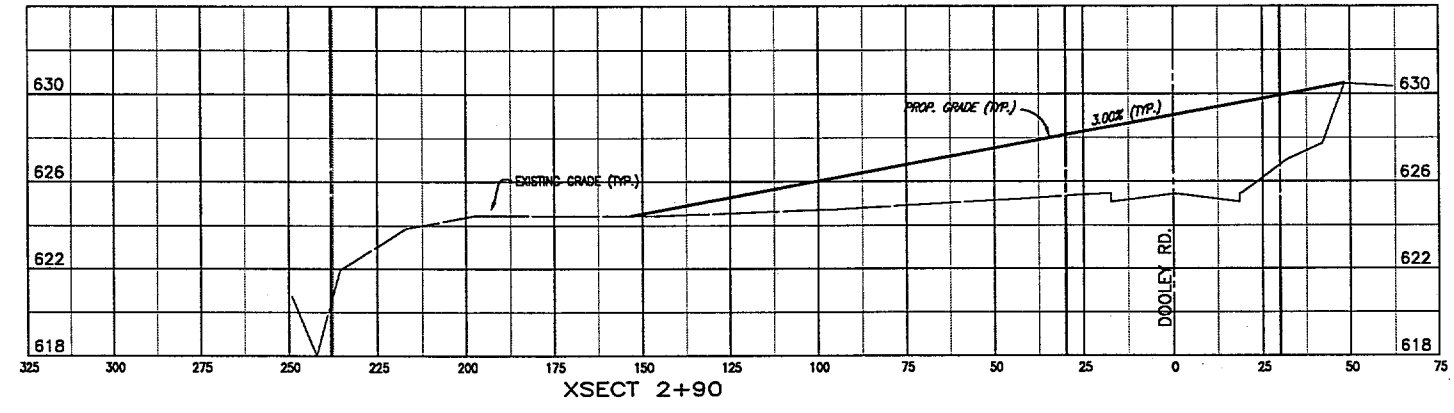
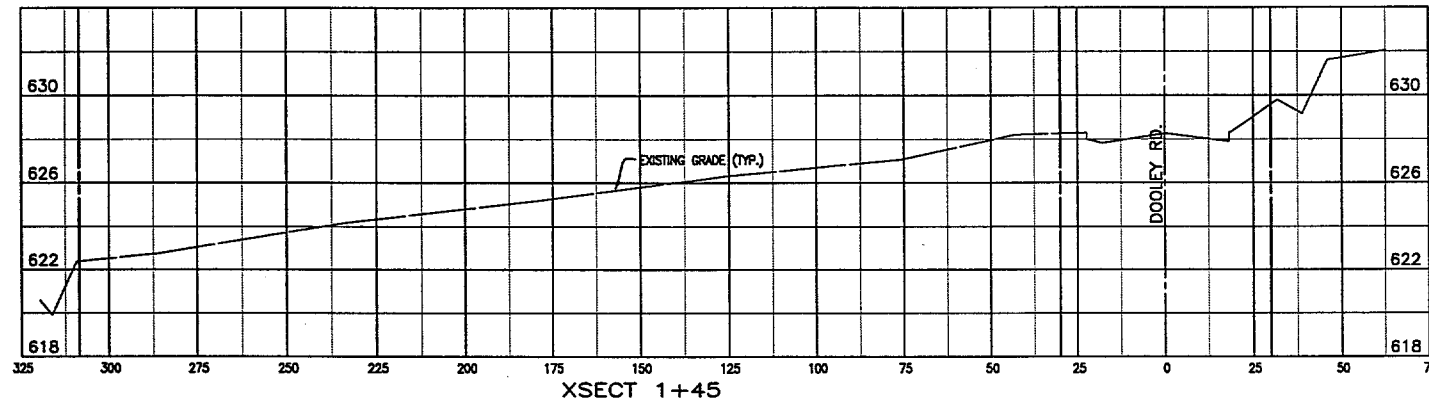
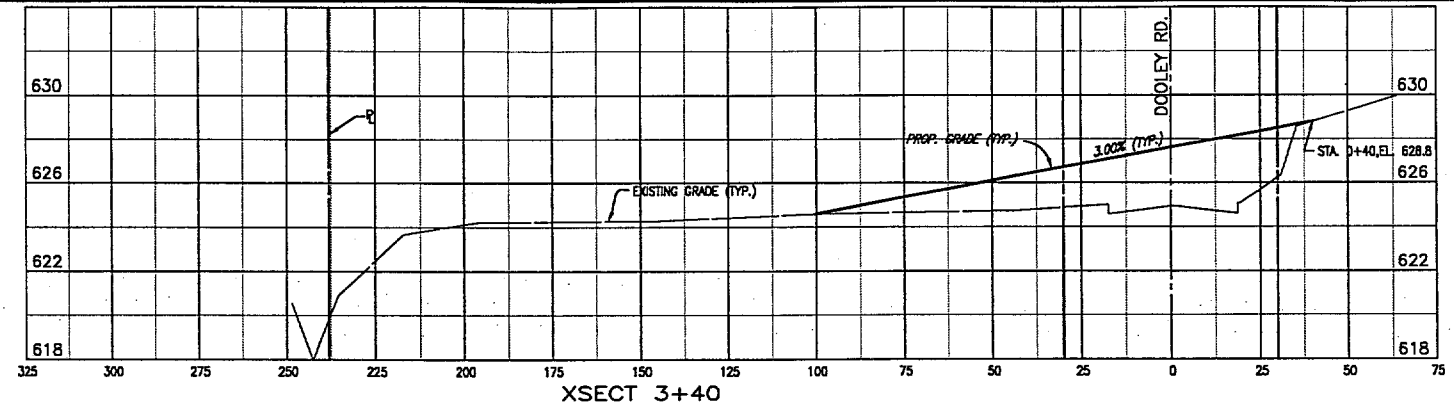
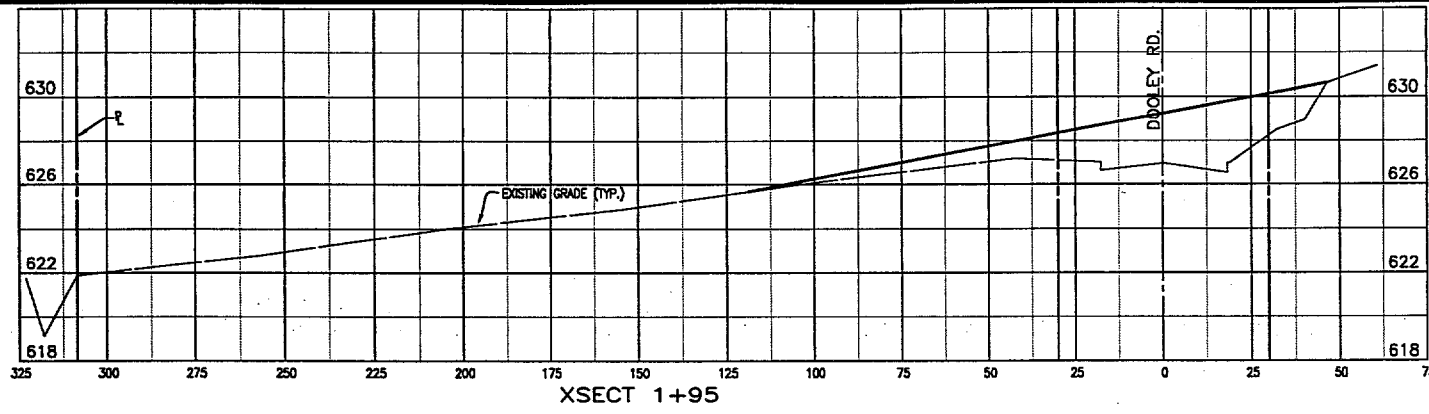
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 AND PERMIT PURPOSES.
John W. Burdette
 DATE: 3/17/98



DESIGNED BY: J.W.B. PROJECT: 97 196 SHEET NO. 2
 DRAWN BY: R.J.L. DATE: FEBRUARY 1998 OF 7 SHEETS



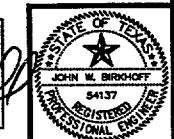
Contours of additional fill



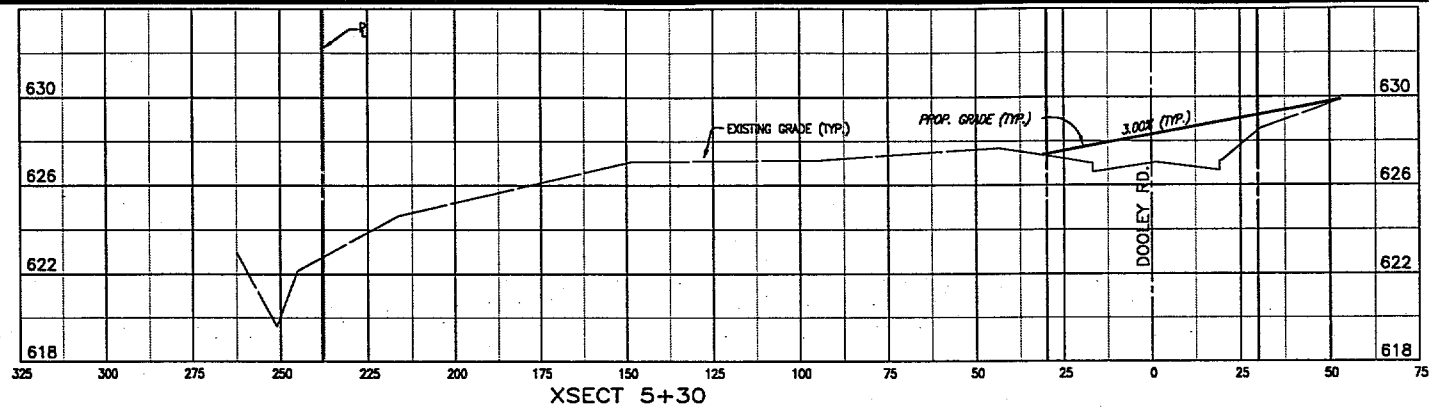
NOT TO SCALE

SCALES:
HORIZONTAL 1" = 25'
VERTICAL 1" = 4'

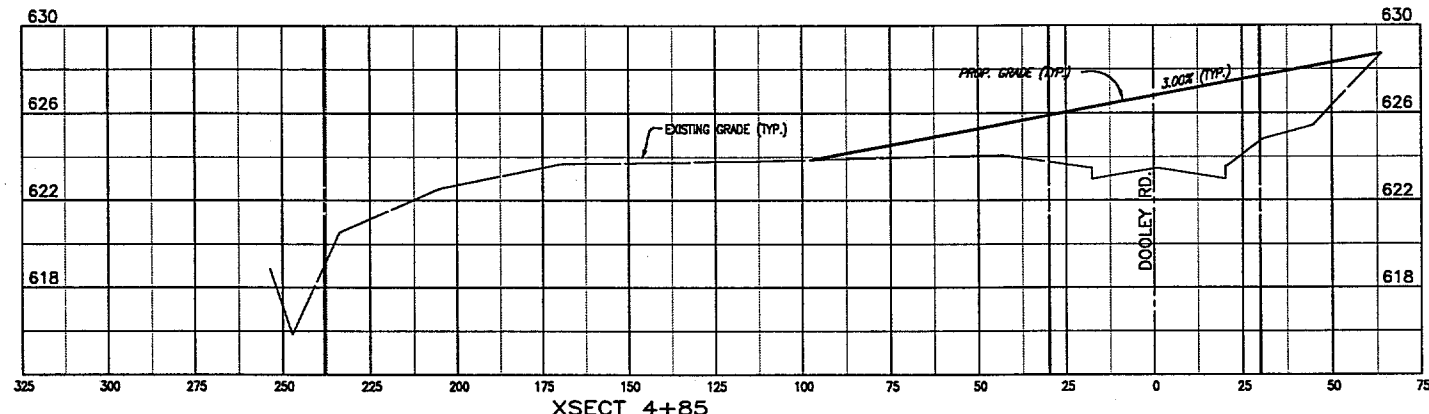
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DATE: 3/17/98



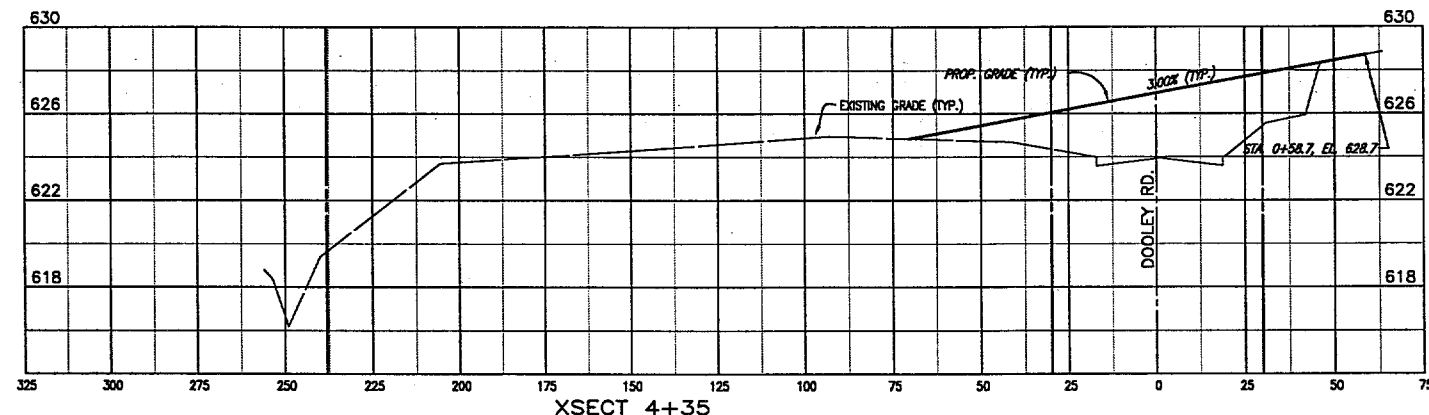
TOWN OF ADDISON, TEXAS		
ADDISON AIRPORT PROPERTY RECLAMATION		
DOOLEY ROAD CROSS SECTIONS		
SHIMEK, JACOBS & FINKLEA, L.L.P. CONSULTING ENGINEERS Dallas, Texas		
DESIGNED BY: J.W.B.	PROJECT: 97 196	SHEET NO. 4
DRAWN BY: R.J.L.	DATE: FEBRUARY 1998	OF 7 SHEETS



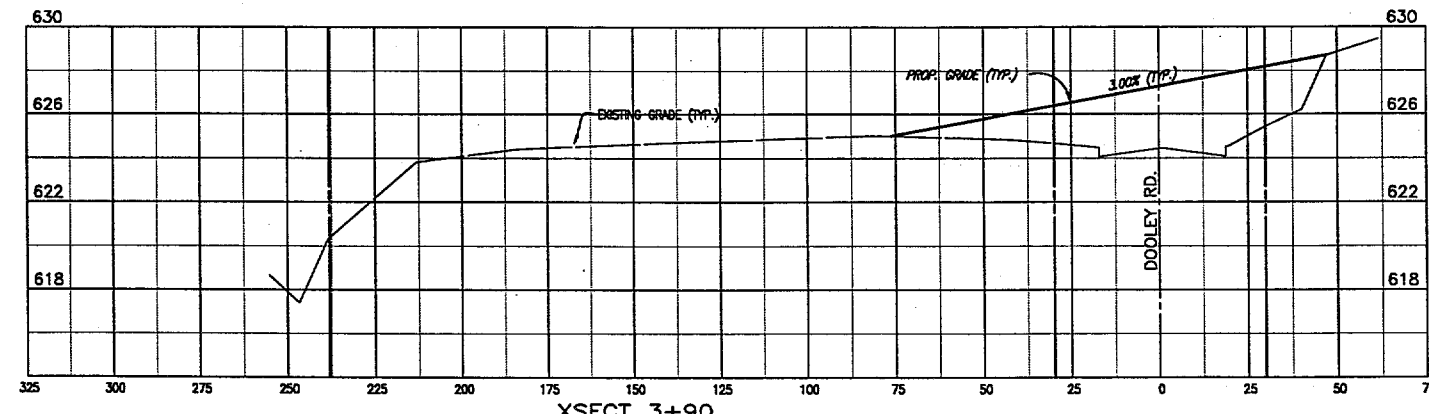
XSECT 5+30



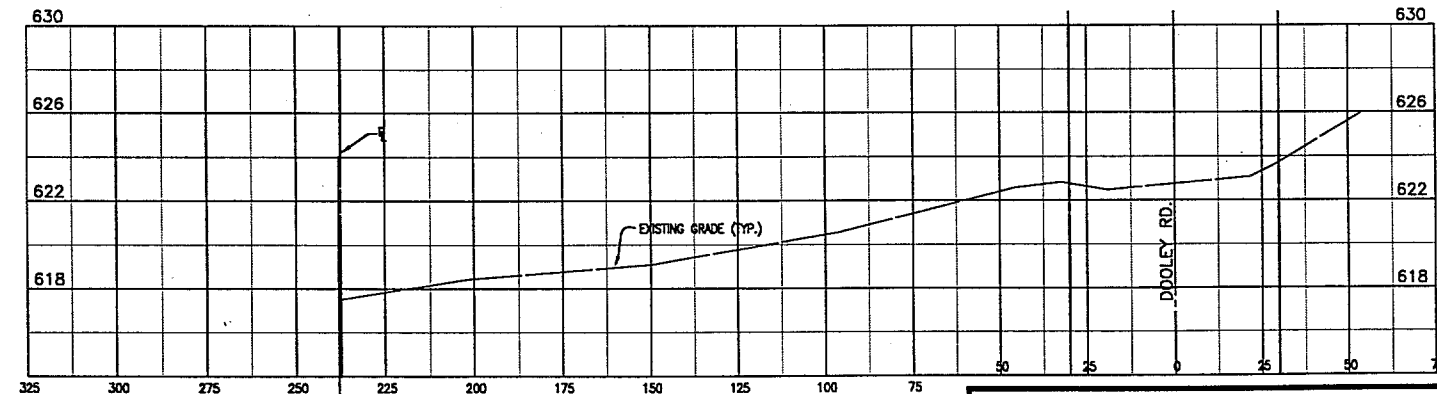
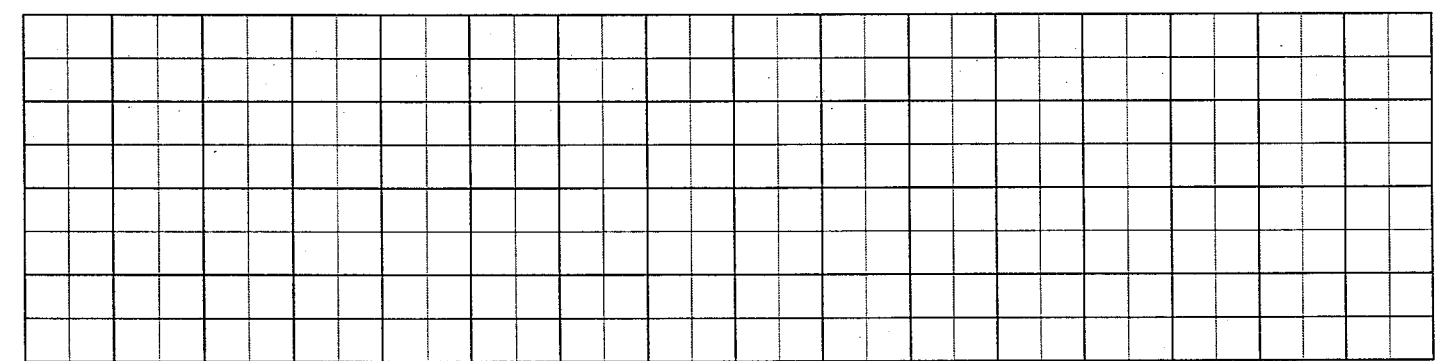
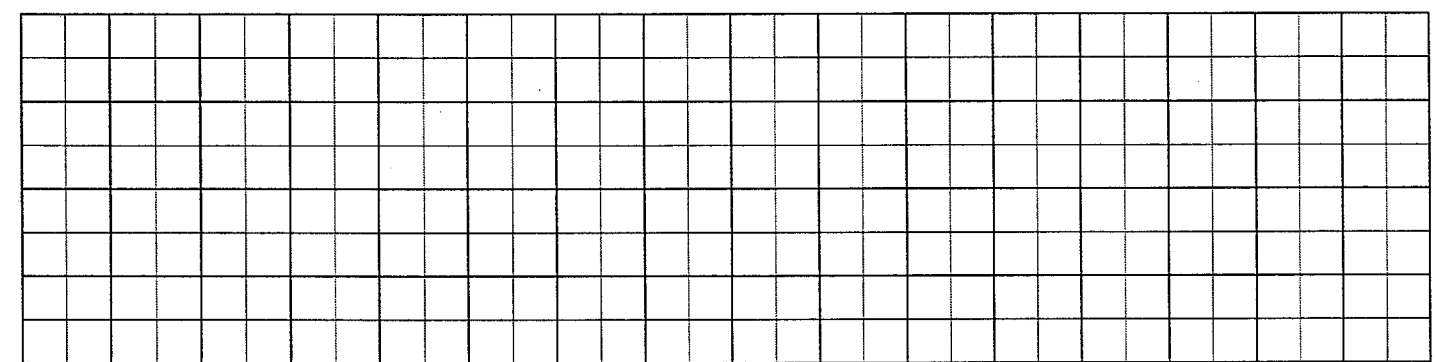
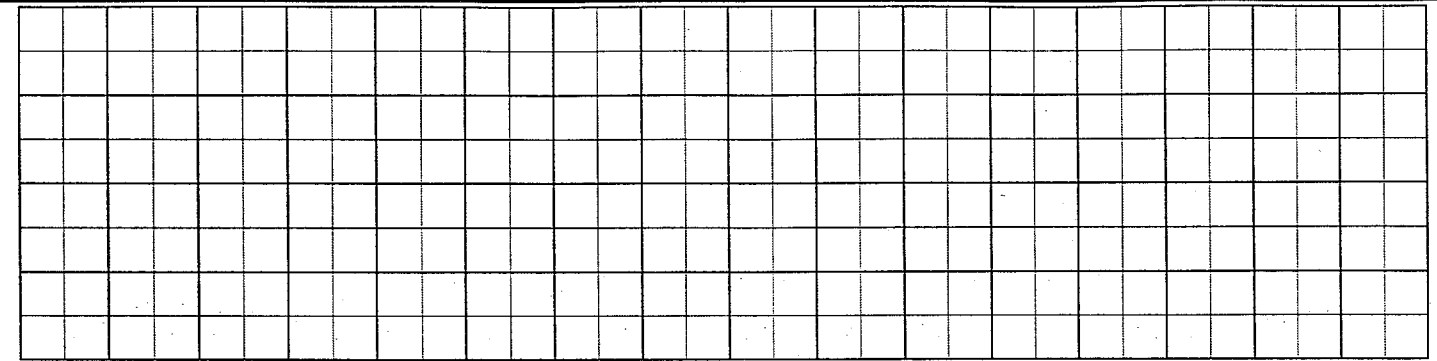
XSECT 4+85



XSECT 4+35



XSECT 3+90

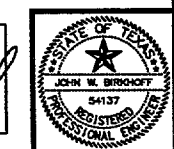


XSECT 5+47.4

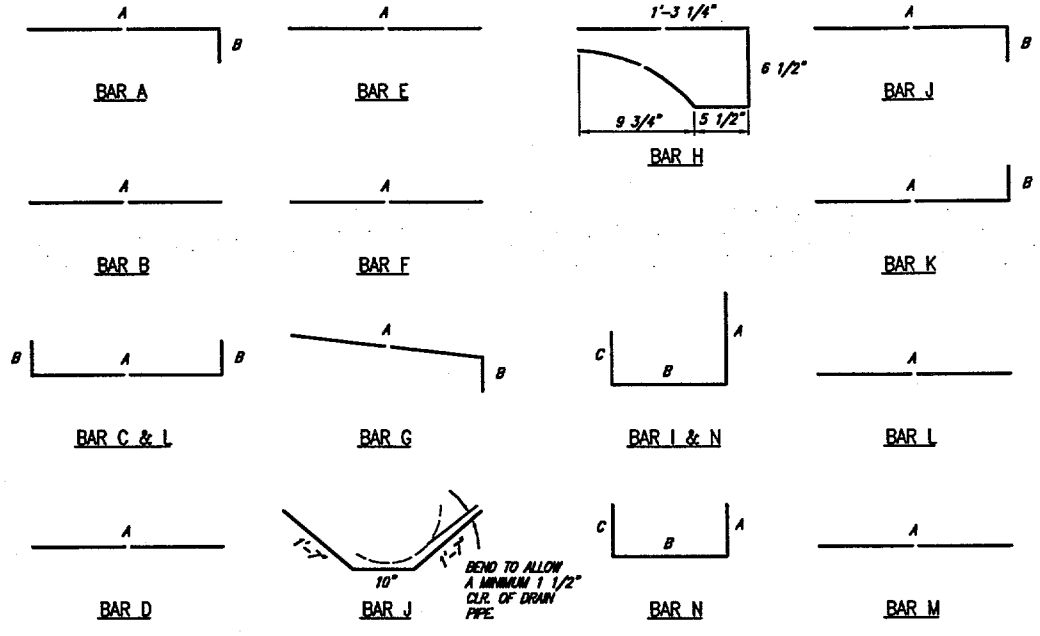
NOT TO SCALE

SCALES:
HORIZONTAL 1" = 25'
VERTICAL 1" = 4'

THESE DOCUMENTS ARE FOR
BIDDING, CONSTRUCTION
AND PERMIT PURPOSES
DATE: 3/17/98



TOWN OF ADDISON, TEXAS		
ADDISON AIRPORT PROPERTY RECLAMATION		
DOOLEY ROAD CROSS SECTIONS		
SHIMEK, JACOBS & FINKLEA, L.L.P. CONSULTING ENGINEERS Dallas, Texas		
DESIGNED BY: J.W.B.	PROJECT: 97 196	SHEET NO. 5
DRAWN BY: R.J.L.	DATE: FEBRUARY 1998	OF 7 SHEETS



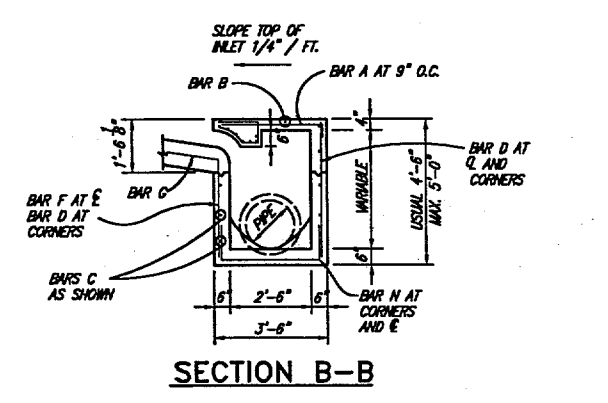
BAR DIAGRAMS

DIMENSIONS SHOWN ARE FOR MAXIMUM SIZE INLETS

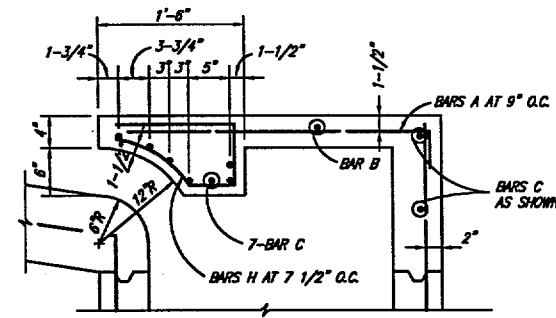
INLET LENGTH	BAR TYPE	BAR DIA. (1/8 IN.)	NO. REQ'D.	BAR DIMENSIONS		
				A	B	C
4 FT.	A	3	6	3'-2"	0'-3"	-
	B	3	1	2'-10"	-	-
	C	4	15	4'-8"	0'-6"	-
	D	4	5	4'-8"	-	-
	F	4	1	3'-2"	-	-
	G	3	5	2'-0"	1'-3"	-
	H	3	6	"	"	"
	H	3	3	3'-2"	3'-2"	3'-2"
5 FT.	A	3	6	3'-2"	0'-3"	-
	B	3	1	3'-10"	-	-
	C	4	15	5'-8"	0'-6"	-
	D	4	5	5'-8"	-	-
	F	4	1	3'-2"	-	-
	G	3	5	2'-0"	1'-3"	-
	H	3	8	"	"	"
	H	3	3	3'-2"	3'-2"	3'-2"
6 FT.	A	3	9	3'-2"	0'-3"	-
	B	3	1	4'-10"	-	-
	C	4	15	6'-8"	0'-6"	-
	D	4	5	4'-8"	-	-
	F	4	1	3'-2"	-	-
	G	3	5	2'-0"	1'-3"	-
	H	3	9	"	"	"
	H	3	3	3'-2"	3'-2"	3'-2"
8 FT.	A	3	12	3'-2"	0'-3"	-
	B	4	1	6'-10"	-	-
	C	4	15	8'-8"	0'-6"	-
	D	4	5	4'-8"	-	-
	F	4	1	3'-2"	-	-
	G	3	5	2'-0"	1'-3"	-
	H	3	12	"	"	"
	H	3	3	3'-2"	3'-2"	3'-2"
10 FT.	A	3	10	3'-2"	0'-3"	-
	B	3	2	8'-10"	-	-
	C	4	16	10'-8"	0'-6"	-
	D	4	4	4'-8"	-	-
	E	5	6	10'-8"	-	-
	G	3	5	2'-0"	1'-3"	-
	H	3	14	"	"	"
	L	4	5	4'-3"	-	-

* SEE DIAGRAM FOR DIMENSIONS: 4', 5', 6', 8' AND 10' INLETS

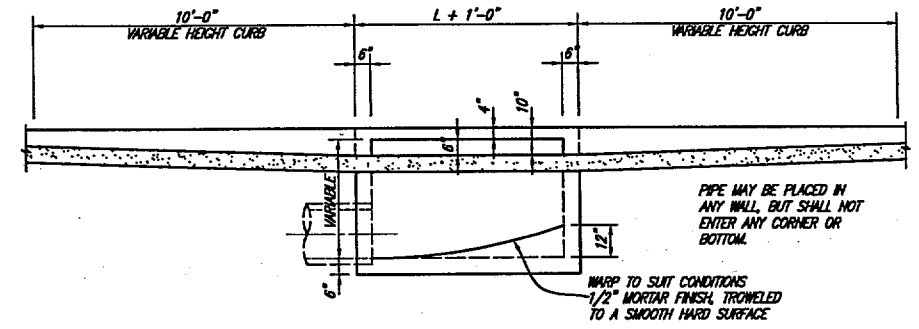
REINFORCING STEEL SCHEDULE



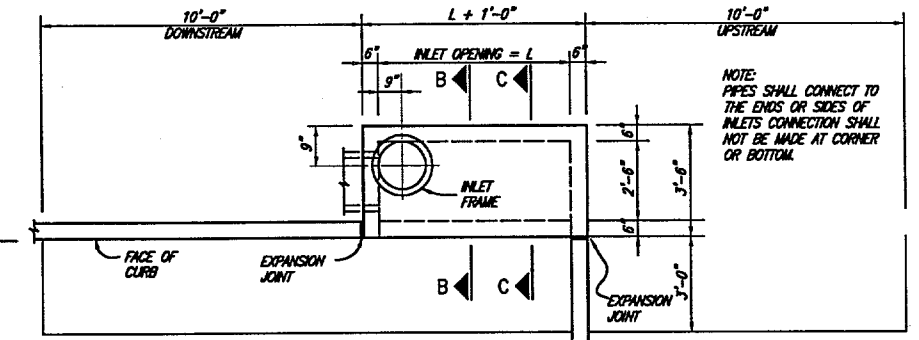
SECTION B-B



SECTION C-C

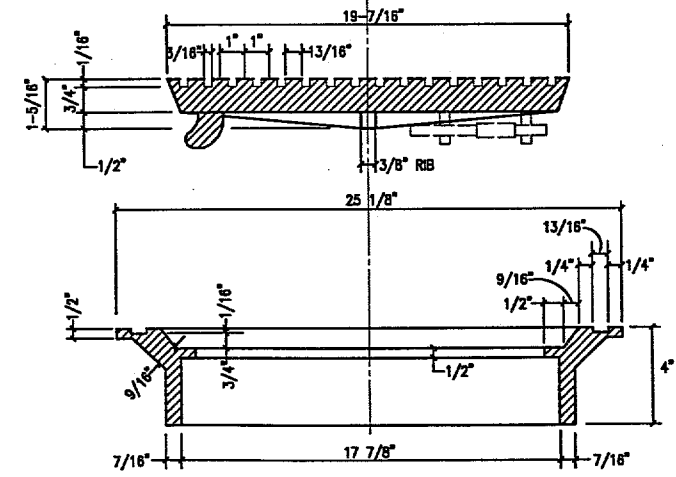
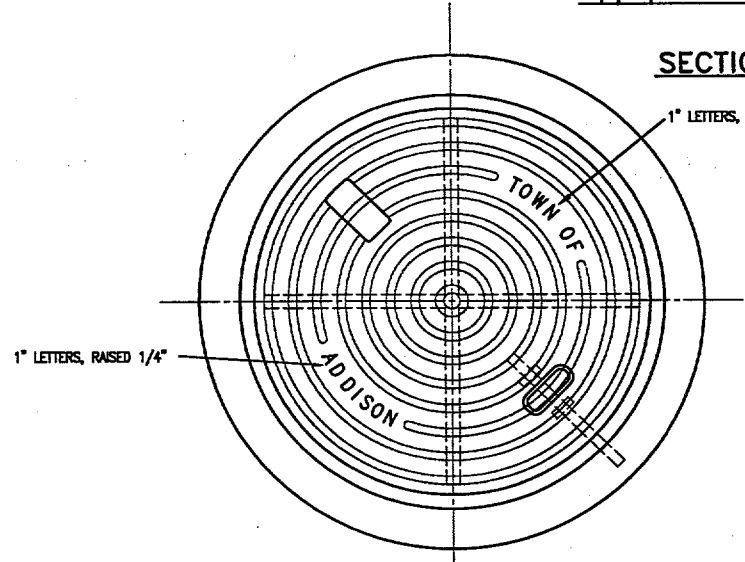


SECTION A-A



PLAN

5 AND 10 FOOT STANDARD INLETS



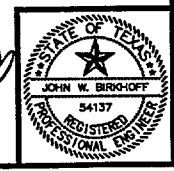
INLET FRAME AND COVER
COVER 55#, FRAME 45#

NOT TO SCALE

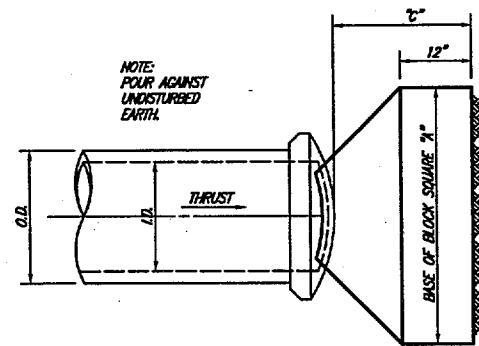
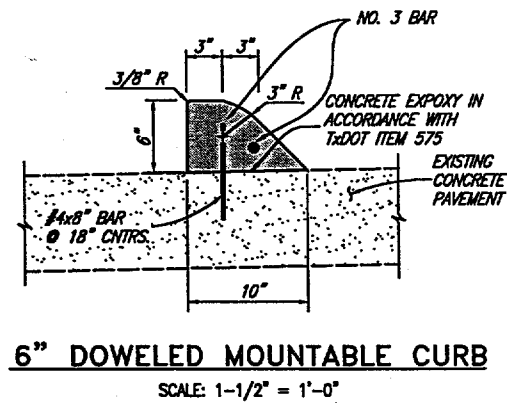
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TOWN OF ADDISON, TEXAS			
ADDISON AIRPORT PROPERTY RECLAMATION			
DOOLEY ROAD			
DETAILS			
SHIMEK, JACOBS & FINKLER, L.L.P. CONSULTING ENGINEERS Dallas, Texas			
DESIGNED BY: J.W.B.	PROJECT: 97 196	SHEET NO. 6	
DRAWN BY: R.J.L.	DATE: FEBRUARY 1998	OF 7 SHEETS	

THESE DOCUMENTS ARE FOR BIDDING, CONSTRUCTION, AND PERMIT PURPOSES.
DATE: 3/17/98



REvised 02/19/88 R.L. 88189-c3.dwg SCALE: 1"=1' BLOCKS: MH-COV2, STDALTI, BARLIST



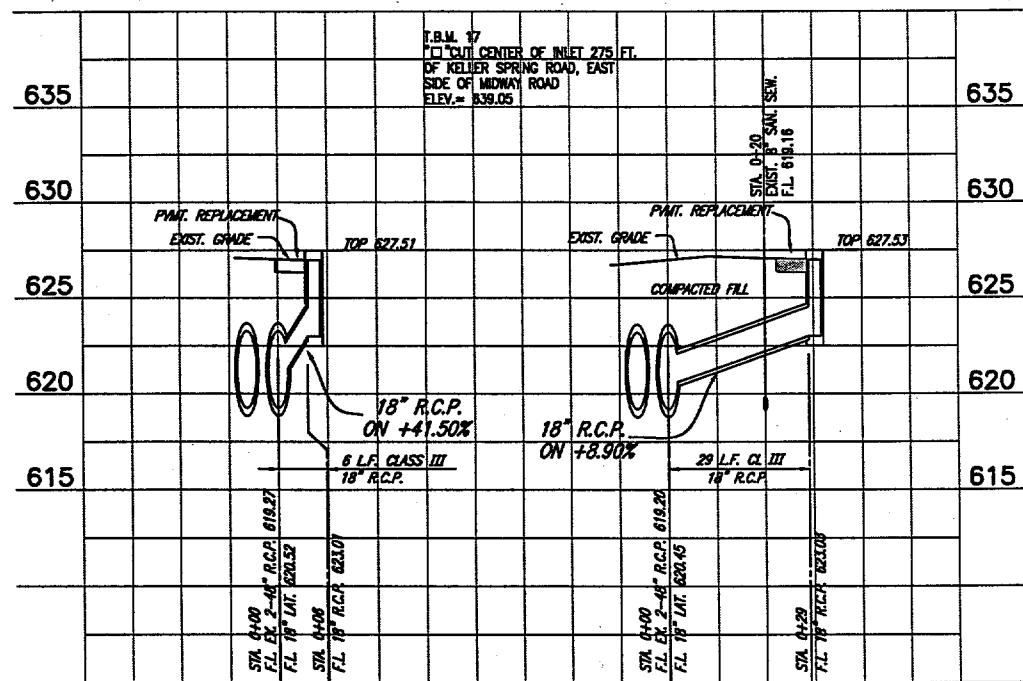
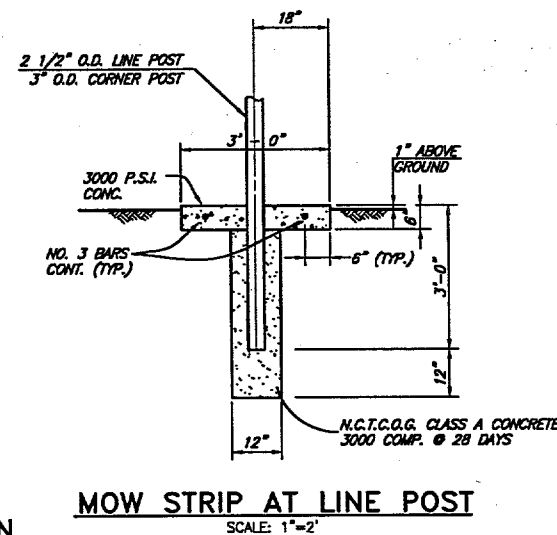
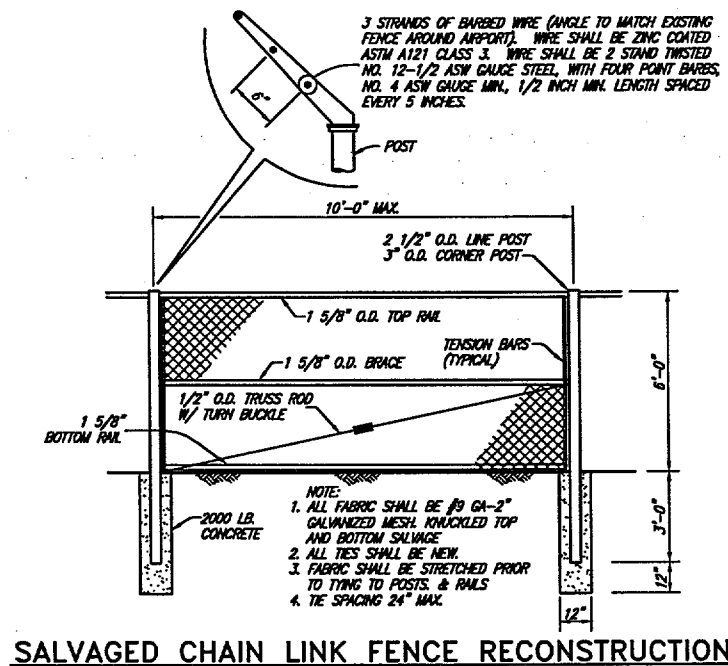
THRUST DATA - PLUGS

I.D. (INCHES)	THRUST (TONS)	C (FT.)	EARTH		ROCK	
			A (FT.)	VOL. (C.Y.)	A (FT.)	VOL. (C.Y.)
4.6.8	5.1	1.5	2.5	0.3	2.0	0.2
10.12	11.3	1.5	3.5	0.6	2.5	0.3
16.18	25.5	2.0	5.5	1.6	4.0	0.9
20	31.5	2.0	6.0	1.9	4.0	0.9
24	45.2	2.5	7.0	3.1	5.0	1.7
30	53.0	3.0	7.5	4.1	5.5	2.4

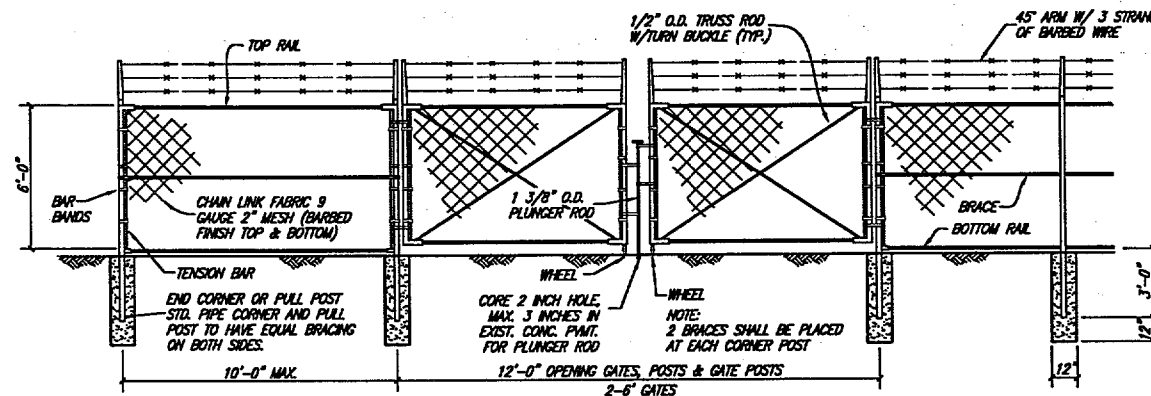
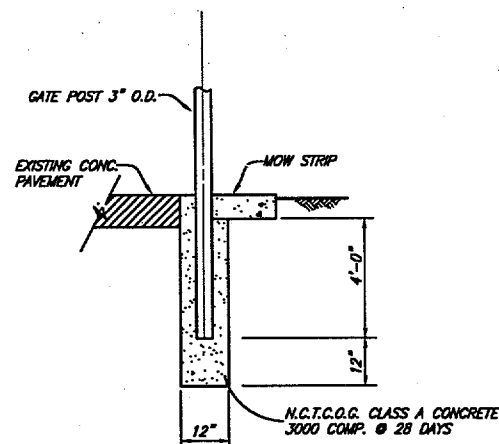
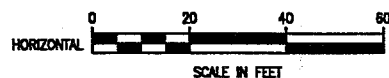
THRUST BLOCKS

NO SCALE

NOTE: CONTRACTOR SHALL POUR ADEQUATE BLOCKING TO OVERCOME WORKING PRESSURES IN WATER LINE.

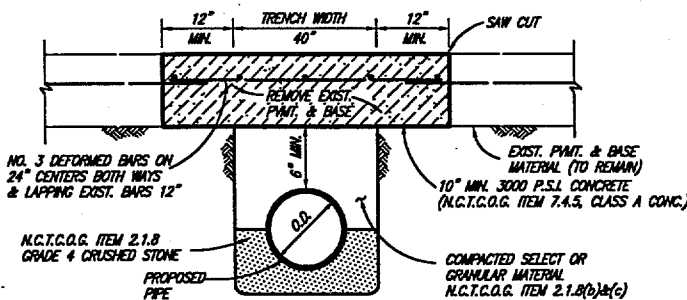


STORM SEWER PROFILES



GATE POST

NOT TO SCALE



TYPICAL CHAIN LINK GATES

NO SCALE
NOTE: GATE AND APPURTENANCES NOT DIPPED GALVANIZED.

N.C.T.C.O.G. GRADE 4 CRUSHED STONE:

SEIVE SIZE	PERCENT RETAINED
1 1/2"	0%
0-5X	0-5%
1/2"	40-75%
NO. 4	90-100%
NO. 8	95-100%

NOT TO SCALE

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TOWN OF ADDISON, TEXAS

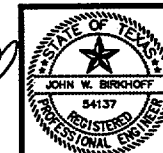
ADDISON AIRPORT PROPERTY RECLAMATION

DOOLEY ROAD DETAILS

SHIMEK, JACOBS & FINKLEA, L.L.P.
CONSULTING ENGINEERS
Dallas, Texas

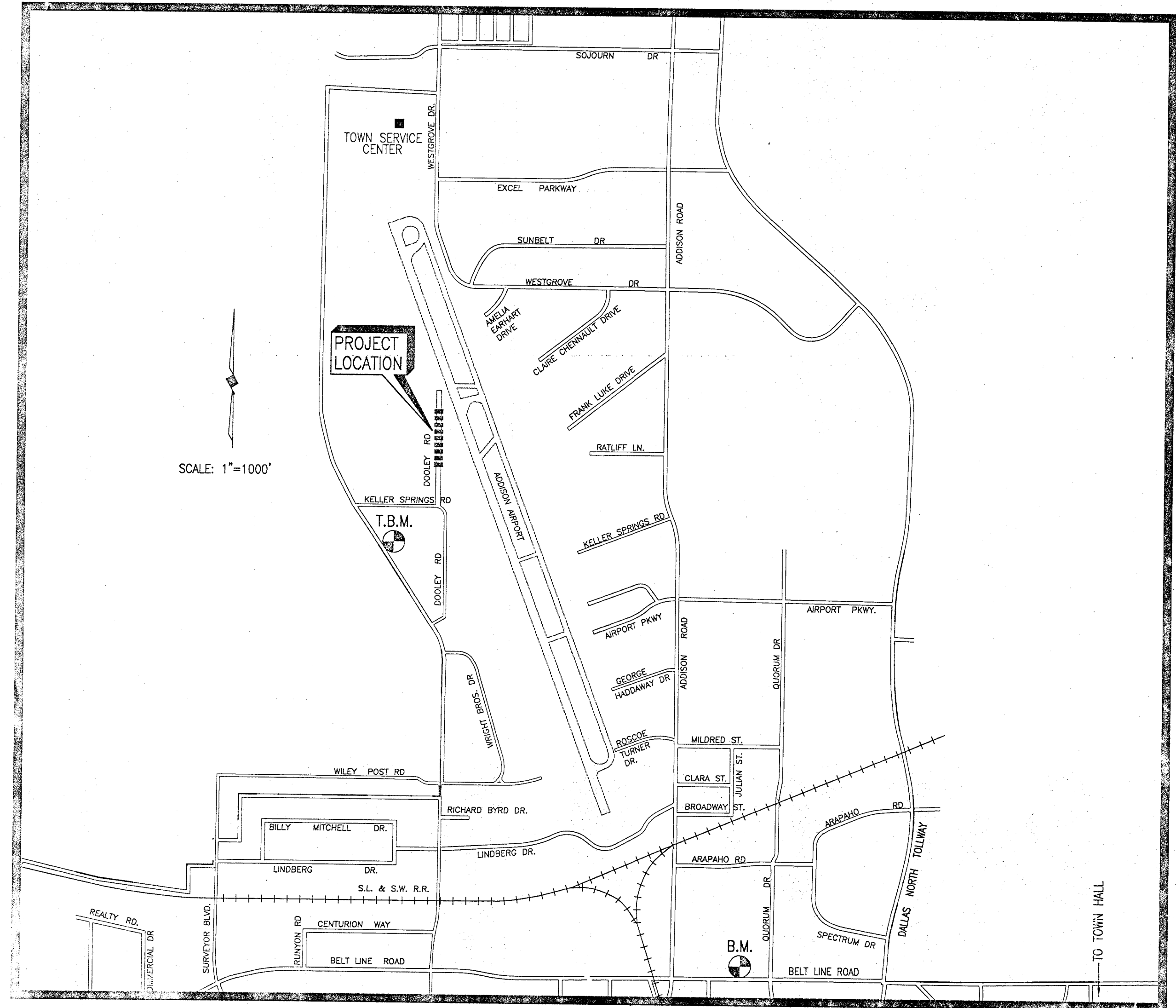
DESIGNED BY: J.W.B. PROJECT: 97 196 SHEET NO. 7
DRAWN BY: R.J.L. DATE: FEBRUARY 1998 OF 7 SHEETS

THESE DOCUMENTS ARE FOR BIDDING CONSTRUCTION AND PERMIT PURPOSES.
DATE: 3/12/98



Addison!

CONSTRUCTION PLANS FOR ADDISON AIRPORT PROPERTY RECLAMATION



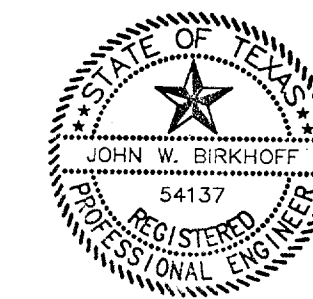
BID SET

SHEET INDEX

SHEET DESCRIPTION	SHEET NO.
CONSTRUCTION PLAN	1
EXISTING GRADING PLAN	2
PROPOSED GRADING PLAN	3
CROSS SECTIONS	4 & 5
DETAILS	6 & 7

SHIMEK, JACOBS & FINKLEA, L.L.P.
CONSULTING ENGINEERS
Dallas, Texas

FEBRUARY 1993



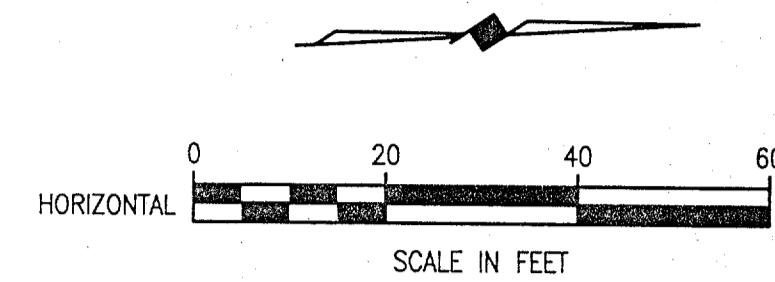
THESE DOCUMENTS ARE FOR
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AND PERMIT PURPOSES
John W. Birkhoff
DATE: 3/17/98

FF-16

GEORGE SYMS SURVEY
 ABSTRACT NO. 1344
 DALLAS COUNTY

BENCHMARK:
 U.S.G.S. DISK IN BRICK WALL OF
 OLD ADDISON SCHOOL BUILDING
 (4.7 FEET ABOVE GROUND)
 ELEV. = 650.61

T.B.M. 17:
 "C" CUT CENTER OF INLET 275 FT.
 OF KELLER SPRING ROAD, EAST
 SIDE OF MIDWAY ROAD
 ELEV. = 639.05



ESTIMATED QUANTITIES

DESCRIPTION	UNIT	QTY.
REMOVE & SALVAGE EXISTING CHAIN LINK FENCE	L.F.	656
REMOVE EXISTING MONOLITHIC CONCRETE CURB	L.F.	61
REMOVE EXISTING REINFORCED CONCRETE PAVEMENT	S.Y.	41.6
REMOVE & SALVAGE UTILITY POLE & GUYWIRE	EA.	1
INSTALL SALVAGED 6" CHAIN LINK FENCE, INCLUDING MOW STRIP	L.F.	354.1
INSTALL SALVAGED 6" CHAIN LINK FENCE IN CONCRETE PAVEMENT	L.F.	28
FURNISH & INSTALL DOUBLE SWING GATE	EA.	1
FURNISH & INSTALL 5" STANDARD INLET	EA.	1
FURNISH & INSTALL 10" STANDARD INLET	EA.	1
FURNISH & INSTALL 18" CLASS III R.C.P.	L.F.	35
CONNECT TO EXISTING R.C.P. STORM SEWER PIPE	S.Y.	8
CONST. 8" REIN. CONC. PAVT. @ PROP. LATERALS	S.Y.	8
CONSTRUCT 6" DOWELED CONCRETE CURB	L.F.	37.3
PLUG EXISTING STORM SEWER LATERAL/ REMOVE INLET	EA.	3
REMOVE EXISTING FIRE HYDRANT & VALVE	EA.	1
RELOCATE EXISTING FIRE HYDRANT & VALVE	EA.	1
FURNISH & INSTALL 8" WATER LINE	L.S.	1
CONNECT TO EXISTING 8" WATER LINE	EA.	1
CUT & PLUG EXISTING 8" WATER LINE	EA.	1
FURNISH & INSTALL EROSION CONTROL DEVICES	L.F.	175
ESTABLISH GRASS	ACRES	1.2
COMPACTED FILL	C.Y.	3,250 #
ADJUST MANHOLE	EA.	1
VALVE RISERS	EA.	2

*COMPACTED IN PLACE QUANTITY

TOWN OF ADDISON
 FORMERLY
 PROTECTA-CAB, INC.
 VOL. 90160, PG. 1555
 65,235.02 S.F.

NOTES:

- ALL EXISTING FENCE WHICH IS REMOVED SHALL BE SALVAGED AND PROVIDED TO THE TOWN OF ADDISON. FABRIC SHALL BE ROLLED. BARBED WIRE SHALL BE ROLLED. POSTS SHALL HAVE CONCRETE REMOVED AND STACKED. RAILS SHALL BE STACKED. TIES SHALL BE DISPOSED OF OFF-SITE. SALVAGED MATERIAL SHALL BE DELIVERED TO APPROPRIATE BASE OPERATOR. BOTTOM RAILS SHALL BE PROVIDED UTILIZE SALVAGED TOP RAIL AND FURNISH AND INSTALL POST TO RAIL HARDWARE.
- CONNECTION TO EXISTING STORM SEWER SHALL BE COMPLETED BY DRILLING EXISTING STORM SEWER PIPE TO THE REQUIRED RADIUS, THEN KNOCKING OUT THE WALL OF THE 48" R.C.P. INSERT PROPOSED LATERAL PIPE AND GROUT CONNECTION. EXISTING 48" R.C.P. SHALL BE CLEANED PRIOR TO APPLYING GROUT.
- ALL PROPOSED STORM SEWER LATERAL PIPE SHALL BE CLASS III.
- WHERE EXISTING 18" R.C.P. STORM SEWER LATERALS ARE TO BE PLUGGED, INSTALL 18" R.C.P. PLUGS. FILL EXISTING INLET BOXES WITH CONCRETE TO 12" ABOVE THE TOP OF THE LATERAL PIPES. THEN, FILL INLET BOXES WITH NATIVE FILL MATERIAL, COMPACTED IN 12" LIFTS.
- PAVEMENT AND CURB REMOVAL AT LOCATIONS FOR REMOVAL OF EXISTING INLETS SHALL NOT BE RECONSTRUCTED.
- EROSION CONTROL DEVICES (FABRIC FENCE) SHALL BE ERECTED ALONG THE TOP OF BANKS (NORTH & WEST).
- AT CONTRACTORS EXPENSE, NEW FIRE HYDRANT AND/OR VALVE MAY BE SUBSTITUTED FOR RELOCATING FIRE HYDRANT.
- REMOVE EXISTING UTILITY POLE AND DELIVER TO SERVICE CENTER LOCATED NORTH END OF AIRPORT.
- CONTRACTOR MAY AT HIS COST FURNISH AND INSTALL NEW FENCE IN PLACE OF RECONSTRUCTING EXISTING FENCE. NEW FENCE SHALL BE AS CALLED FOR IN FENCE DETAILS.
- SANITARY SEWER MANHOLE SHALL BE RAISED BY:
 - REMOVING EXISTING CONE, RING & COVER
 - DRILL 18 INCH NO. 4 SMOOTH BAR INTO EXISTING MANHOLE AT 1/4 POINTS. EPOXY TO A DEPTH OF 18 INCHES.
 - FORM AND CAST IN PLACE MANHOLE BARREL EXTENSION WITH EPOXY COATED JOINT.
 - JOINTS SHALL BE SEALED ON THE EXTERIOR WITH AN 18 INCH WIDE HEAT SHRINKABLE MANHOLE SEAL, AQUAGARD (I.C. UTILITY) OR EQUAL.
 - FURNISHING & INSTALLING FLAT TOP WITH 30 INCH RING & COVER.

TOWN OF ADDISON
 FORMERLY
 WILLIAM F. CHESTER
 VOL. 84216, PG. 3066
 65,152.15 S.F.

ROFA = APPROXIMATE LOCATION OF RUNWAY
 OBJECT FREE AREA BOUNDARY

CONST. 5" STANDARD INLET A-1
 SAWCUT EXIST. PAVT. 18" FROM
 BACK OF AND PARALLEL TO CURB
 & ON EA. SIDE OF PROP. LATERAL.
 REMOVE EXIST. CURB & PAVT. AS SHOWN
 CONST. 5" STANDARD INLET A-1
 T.C. 627.51, F.L. 623.01
 FURNISH & INSTALL 6 L.F. 18" R.C.P.
 CONNECT TO EXIST. ST. SEW. (SEE NOTE 2)

FURNISH & INSTALL
 2" VALVE RISERS SET
 TO FINAL GRADE = 628.25

REINSTALL EXIST. FIRE HYDRANT
 REINSTALL EXIST. FIRE HYDRANT & VALVE
 SET AT FINAL GRADE
 FURNISH & INSTALL:
 1 - 8" 90° BEND
 7 L.F. 8" WATER LINE
 CONNECT TO EXIST. 8" WATER LINE
 CUT, PLUG WITH THRUST BLOCKING
 ABANDON EXIST. 8" WATER LINE
 SEE NOTE 7

REMOVE & RELOCATE
 EXIST. FIRE HYDRANT & VALVE
 ABANDON EXIST. 8"
 WATER MAIN (SOUTH)

PLUG EXIST. 5" RECESSED INLET
 REMOVE INLET TOP AND 6" MIN.
 OF INLET BOX. PLUG EXIST.
 ST. SEW. LATERAL. FILL INLET
 WITH CONCRETE TO 12" ABOVE
 EXIST. LATERAL TOP OF PIPE.

CONST. DOWELED CONC. CURB
 CONST. 37.3 L.F. DOWELED CONC. CURB.
 CONSTRUCT CURB ACROSS ENTIRE WIDTH
 OF ROADWAY (FROM EAST BACK OF CURB
 TO WEST BACK OF CURB).

RAISE MANHOLE TO
 EXISTING FIN. EL. = 623.3
 FINISHED GRADE = 626.8

CONST. NEW GATE & INSTALL SALVAGED FENCE
 INSTALL SALVAGED 28 L.F. 6" CHAIN LINK
 FENCE W/3 STRAND BARBED WIRE TOPPING
 IN CONCRETE PAVEMENT
 INSTALL SALVAGED 36.3 L.F. 6" CHAIN LINK
 FENCE W/3 STRAND BARBED WIRE TOPPING
 WITH MOW STRIP
 FURNISH & INSTALL DOUBLE SWING GATE
 W/WHEELS (12' OVERALL LENGTH)
 NEW MATERIAL SHALL MATCH EXIST.
 POST, FABRIC & HARDWARE.

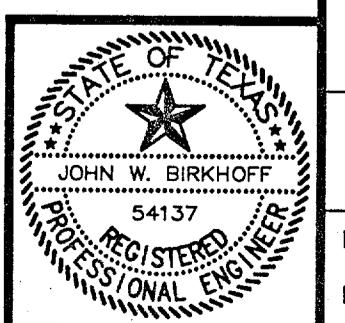
REMOVE POWER POLE
 AND GUYWIRE

CONST. 10" STANDARD INLET A-2
 SAWCUT EXIST. PAVT. 18" FROM
 BACK OF AND PARALLEL TO CURB
 & ON EA. SIDE OF PROP. LATERAL.
 REMOVE EXIST. CURB & PAVT. AS SHOWN
 CONST. 10" STANDARD INLET A-2
 T.C. 627.53, F.L. 623.03
 FURNISH & INSTALL 29 L.F. 18" R.C.P.
 CONNECT TO EXIST. ST. SEW. (SEE NOTE 2)

PLUG EXIST. 5" RECESSED INLET
 REMOVE INLET TOP AND 6" MIN.
 OF INLET BOX. PLUG EXIST.
 ST. SEW. LATERAL. FILL INLET
 WITH CONCRETE TO 12" ABOVE
 EXIST. LATERAL TOP OF PIPE.

REMOVE 388 L.F. OF
 6" CHAIN LINK FENCE
 W/BARBED WIRE TOP
 (SHADED AREA, TYP.)

THESE DOCUMENTS ARE FOR
 BIDDING, CONSTRUCTION,
 AND PERMIT PURPOSES.
 John W. Birkhoff
 DATE: 3/19/98



TOWN OF ADDISON, TEXAS
 ADDISON AIRPORT PROPERTY RECLAMATION
**DOOLEY ROAD
 CONSTRUCTION PLAN**
 SHIMEK, JACOBS & FINKLEA, L.L.P.
 CONSULTING ENGINEERS
 Dallas, Texas

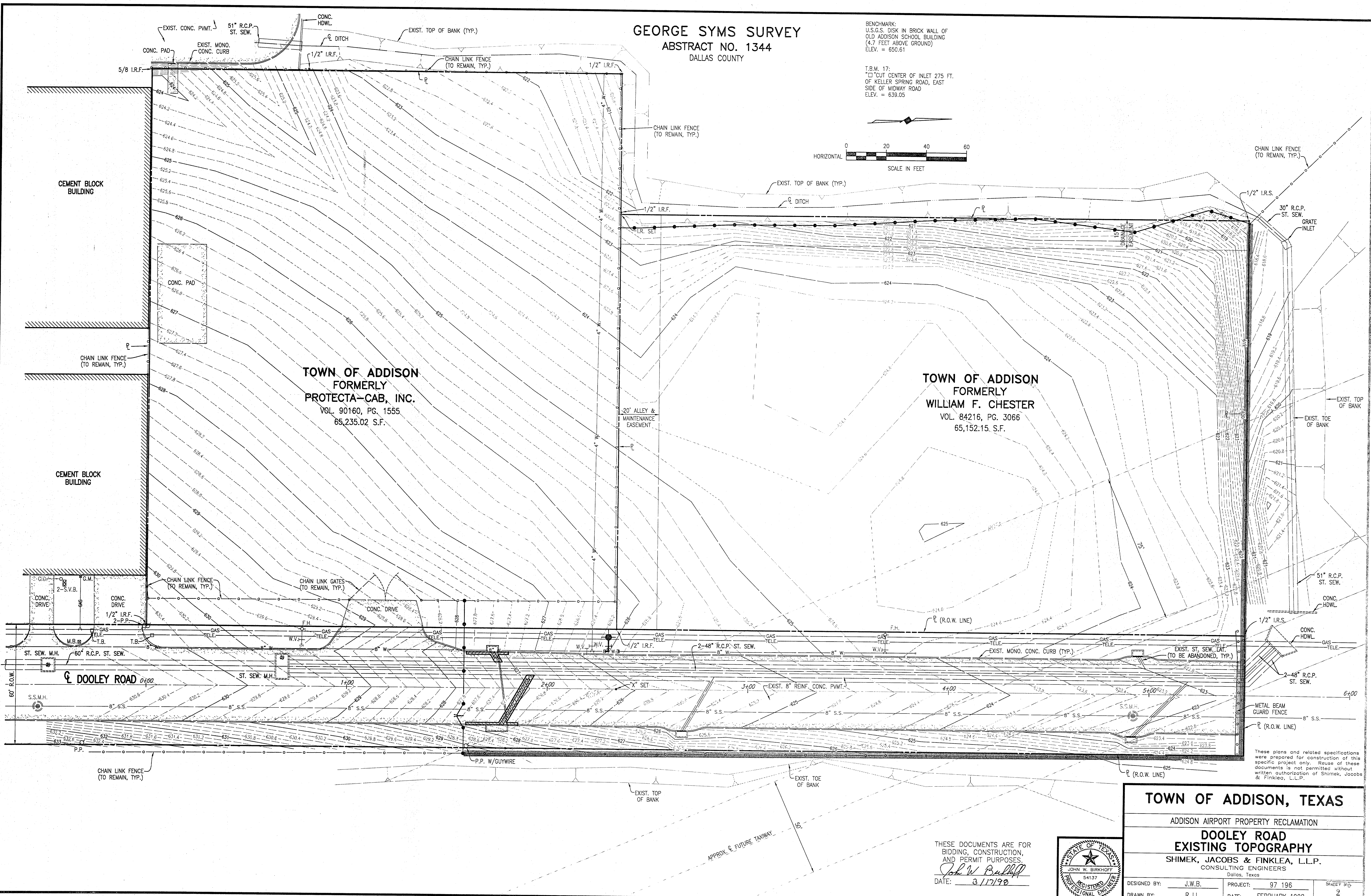
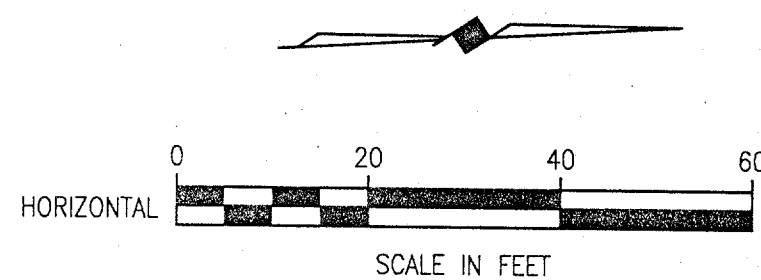
DESIGNED BY: J.W.B. PROJECT: 97 196 SHEET NO. 1
 DRAWN BY: R.J.L. DATE: FEBRUARY 1998 7

REVISIONS: 03/19/98 RLL 96169-01.DWG SCALE: 1"=20' BLOCKS

GEORGE SYMS SURVEY
 ABSTRACT NO. 1344
 DALLAS COUNTY

BENCHMARK:
 U.S.G.S. DISK IN BRICK WALL OF
 OLD ADDISON SCHOOL BUILDING
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 ELEV. = 650.61

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 SIDE OF MIDWAY ROAD
 ELEV. = 639.05



TOWN OF ADDISON
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 VOL. 90160, PG. 1555
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TOWN OF ADDISON
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 WILLIAM F. CHESTER
 VOL. 84216, PG. 3066
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TOWN OF ADDISON, TEXAS

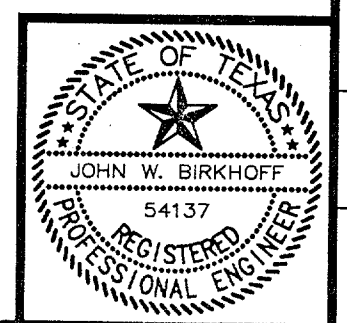
ADDISON AIRPORT PROPERTY RECLAMATION

DOOLEY ROAD
 EXISTING TOPOGRAPHY

SHIMEK, JACOBS & FINKLEA, L.L.P.
 CONSULTING ENGINEERS
 Dallas, Texas

DESIGNED BY: J.W.B. PROJECT: 97 196 SHEET NO. 2
 DRAWN BY: R.J.L. DATE: FEBRUARY 1998 OF 7 SHEETS

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John W. Bullard
 DATE: 3/17/98



REVISIONS: 03/16/98 RLL 96169-02.DWG
 BLOCKS: SCALE: 1"=20'
 36"x24" FULL PLOT: DAVIS256.PCP HALF PLOT: 11x17FIT.PCP

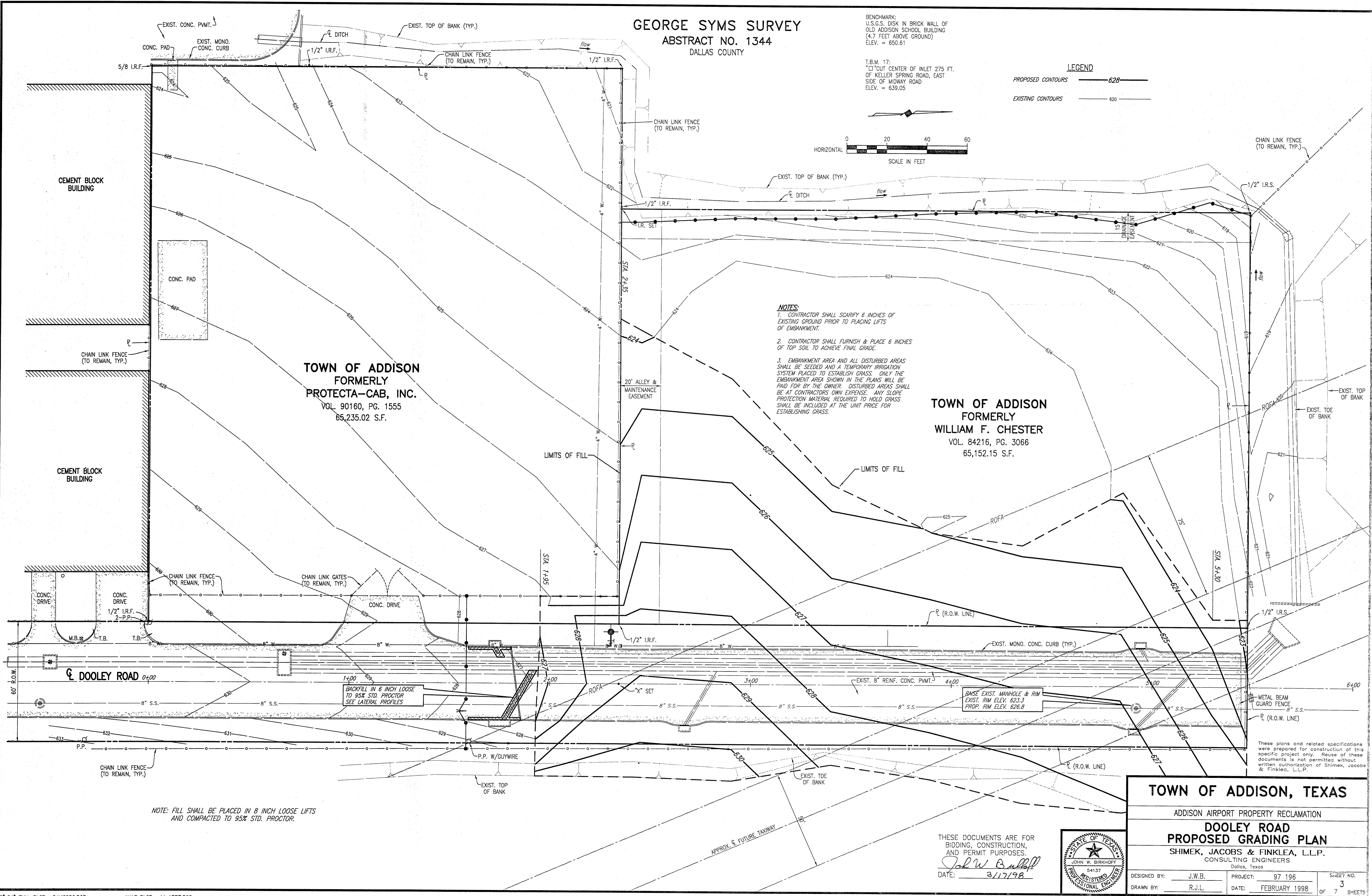
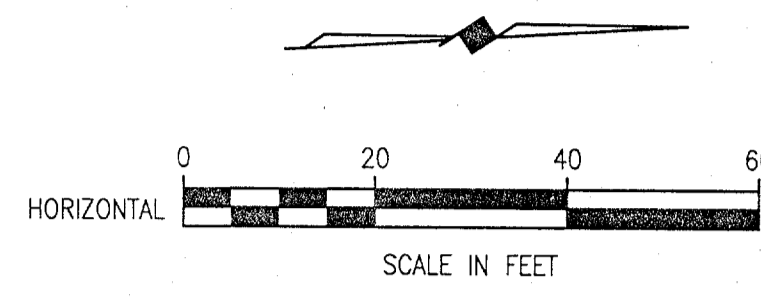
GEORGE SYMS SURVEY
 ABSTRACT NO. 1344
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T.B.M. 17:
 "CUT CENTER OF INLET 275 FT.
 OF KELLER SPRING ROAD, EAST
 SIDE OF MIDWAY ROAD
 ELEV. = 639.05

LEGEND

PROPOSED CONTOURS ——— 628 ———
 EXISTING CONTOURS ——— 620 ———



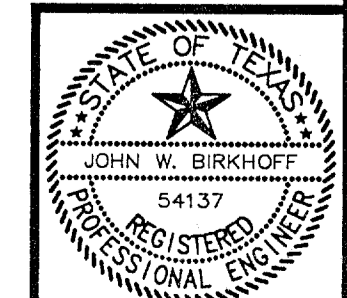
- NOTES:**
- CONTRACTOR SHALL SCARIFY 6 INCHES OF EXISTING GROUND PRIOR TO PLACING LIFTS OF EMBANKMENT.
 - CONTRACTOR SHALL FURNISH & PLACE 6 INCHES OF TOP SOIL TO ACHIEVE FINAL GRADE.
 - EMBANKMENT AREA AND ALL DISTURBED AREAS SHALL BE SEEDED AND A TEMPORARY IRRIGATION SYSTEM PLACED TO ESTABLISH GRASS. ONLY THE EMBANKMENT AREA SHOWN IN THE PLANS WILL BE PAID FOR BY THE OWNER. DISTURBED AREAS SHALL BE AT CONTRACTORS OWN EXPENSE. ANY SLOPE PROTECTION MATERIAL REQUIRED TO HOLD GRASS SHALL BE INCLUDED AT THE UNIT PRICE FOR ESTABLISHING GRASS.

TOWN OF ADDISON
 FORMERLY
 PROTECTA-CAB, INC.
 VOL. 90160, PG. 1555
 65,235.02 S.F.

TOWN OF ADDISON
 FORMERLY
 WILLIAM F. CHESTER
 VOL. 84216, PG. 3066
 65,152.15 S.F.

NOTE: FILL SHALL BE PLACED IN 8 INCH LOOSE LIFTS AND COMPACTED TO 95% STD. PROCTOR.

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 John W. Bullard
 DATE: 3/17/98



TOWN OF ADDISON, TEXAS

ADDISON AIRPORT PROPERTY RECLAMATION

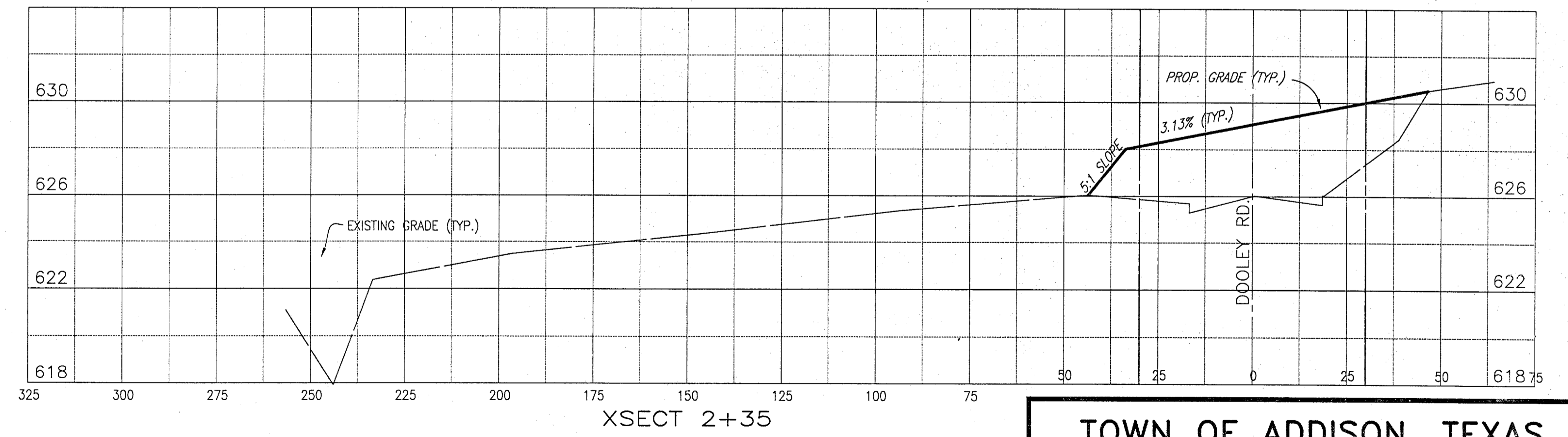
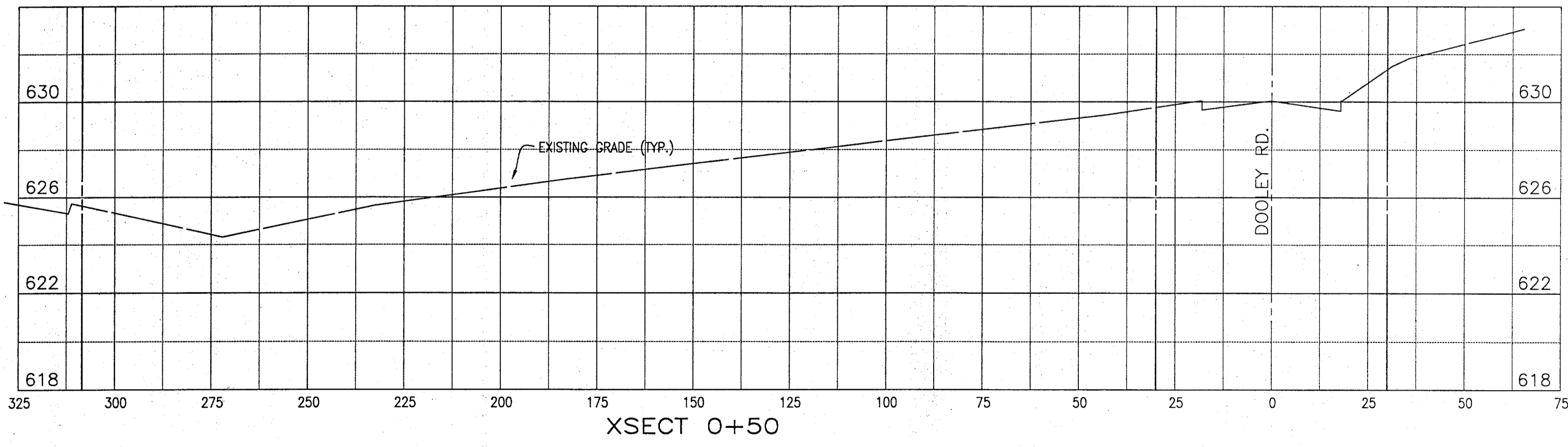
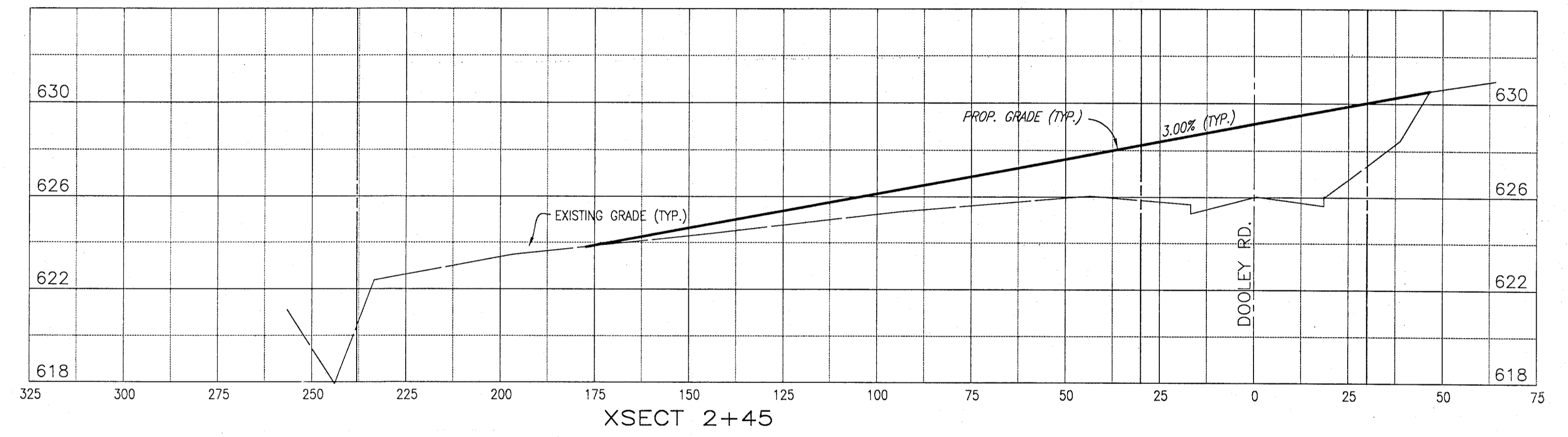
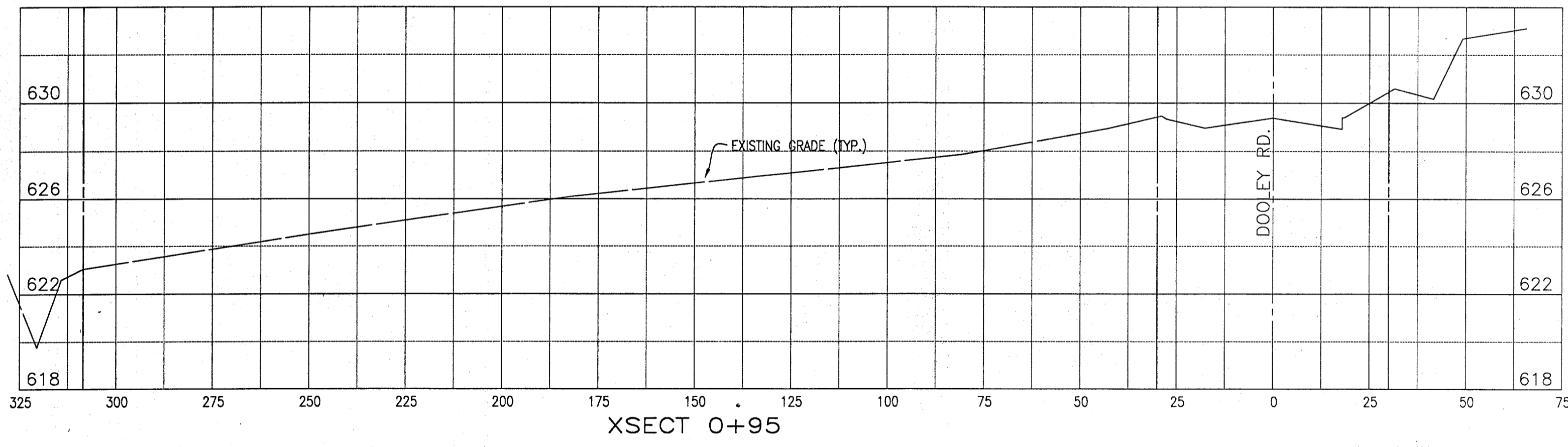
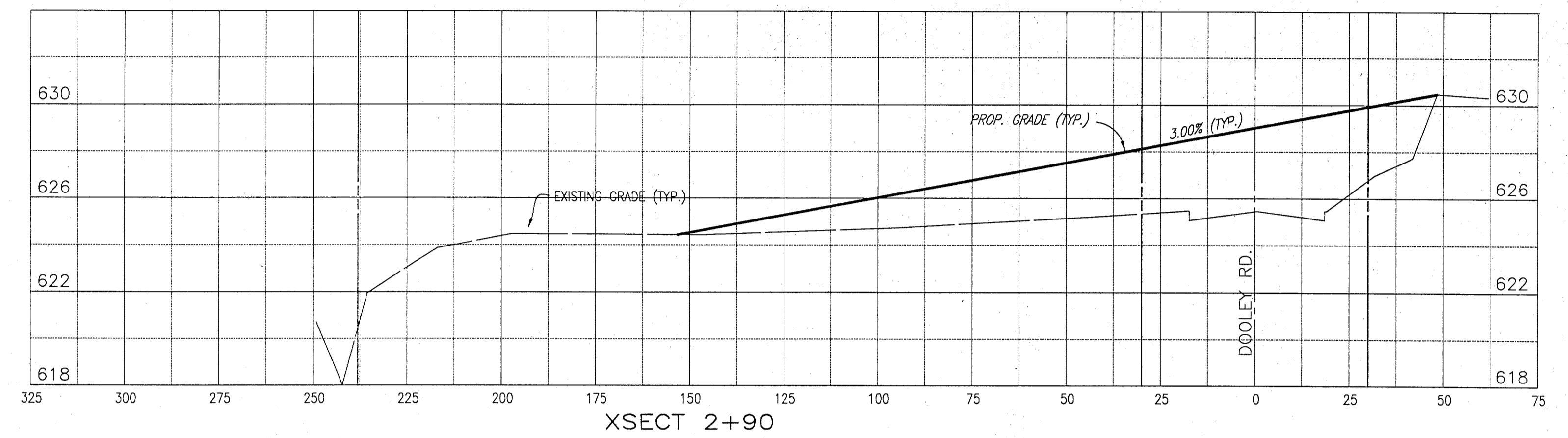
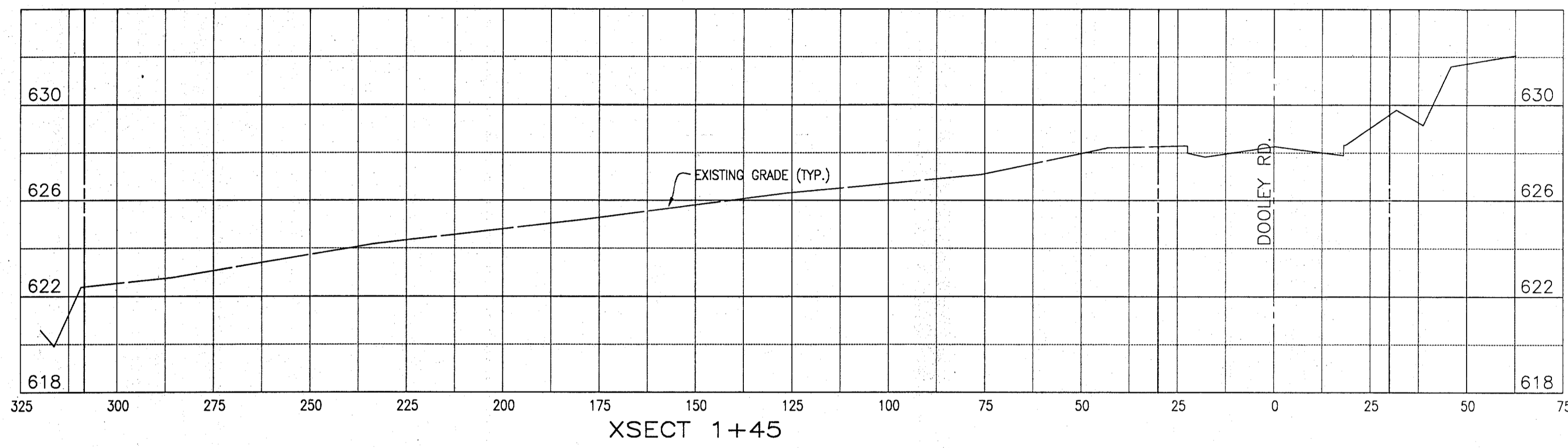
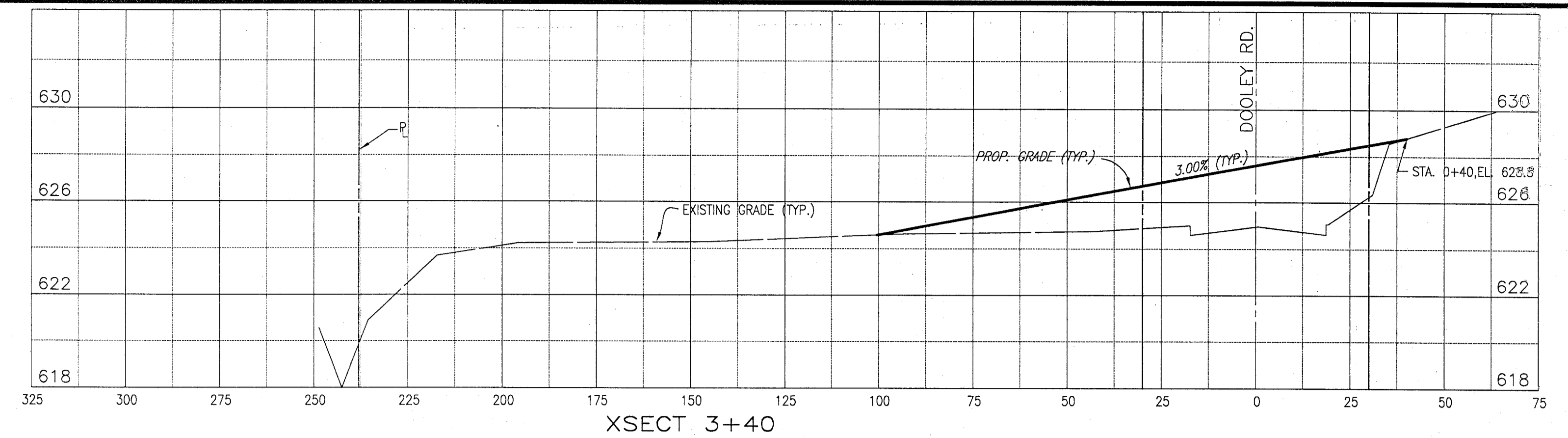
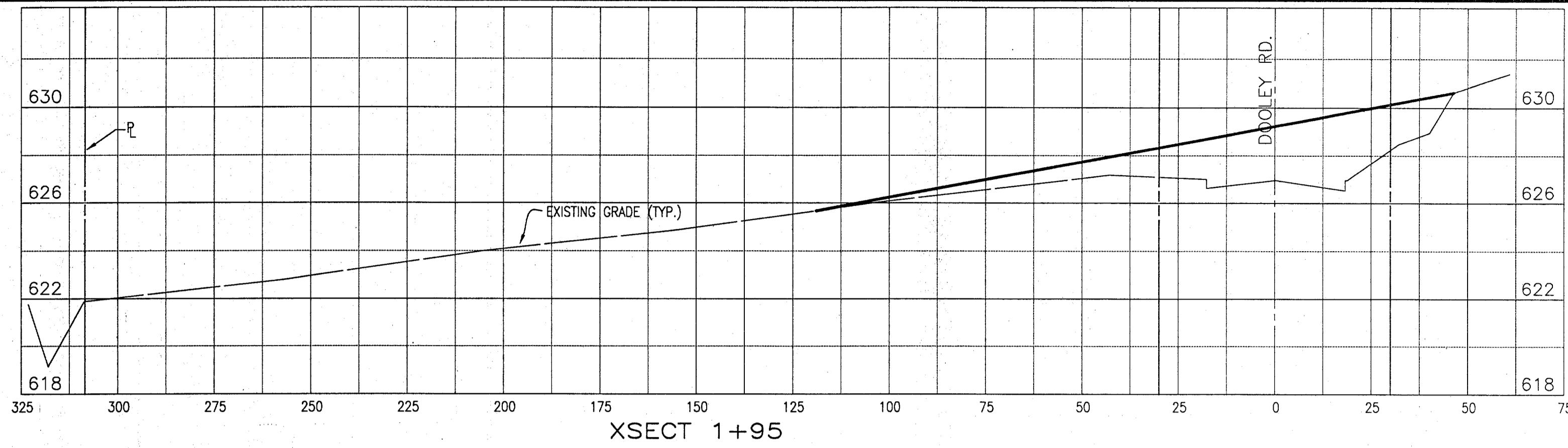
DOOLEY ROAD

PROPOSED GRADING PLAN

SHIMEK, JACOBS & FINKLEA, L.L.P.
 CONSULTING ENGINEERS
 Dallas, Texas

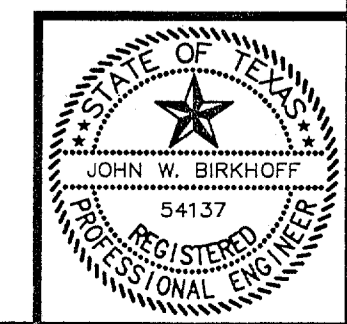
DESIGNED BY: J.W.B.	PROJECT: 97 196	SHEET NO. 3
DRAWN BY: R.J.L.	DATE: FEBRUARY 1998	OF 7 SHEETS

REVISIONS: 03/17/98 R.L. 97169-25.DWG
 SCALE: 1"=20'
 BLOCKS: 36"x24" FULL PLOT: DAVIS256.PCP HALF PLOT: 11x17FIT.PCP



SCALES:
HORIZONTAL 1" = 25'
VERTICAL 1" = 4'

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John W. Birkhoff
DATE: 3/17/98



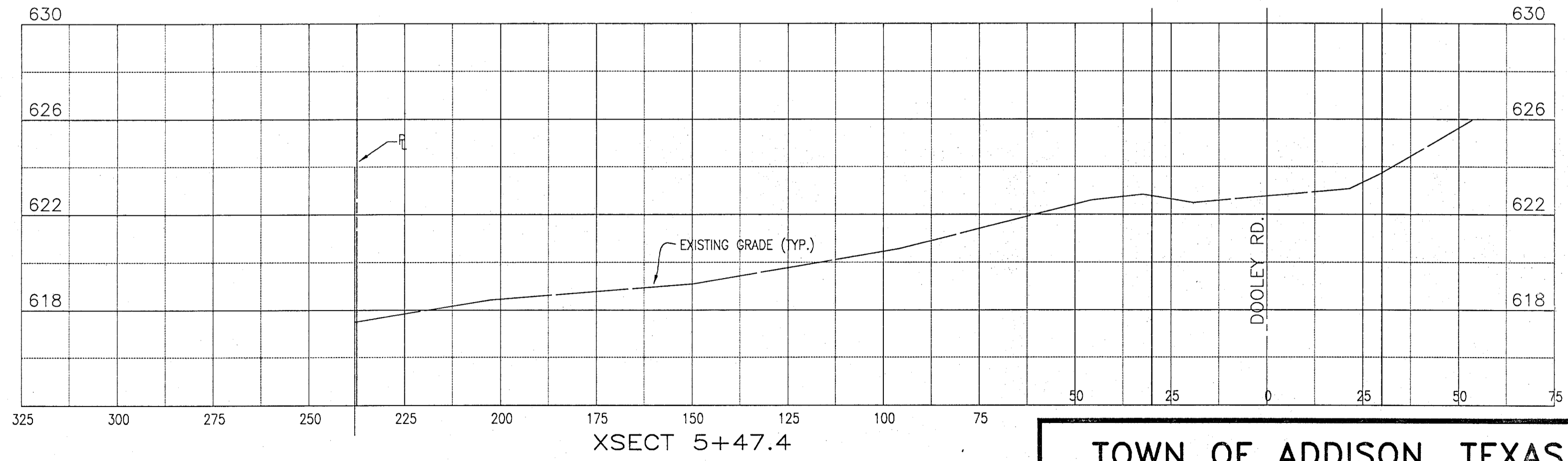
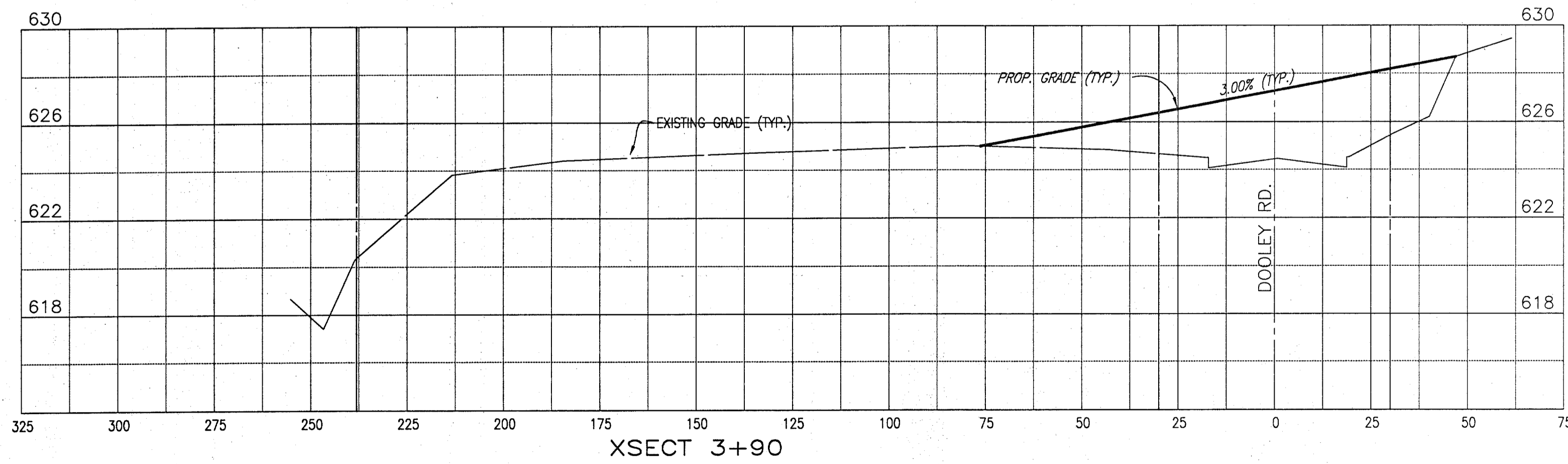
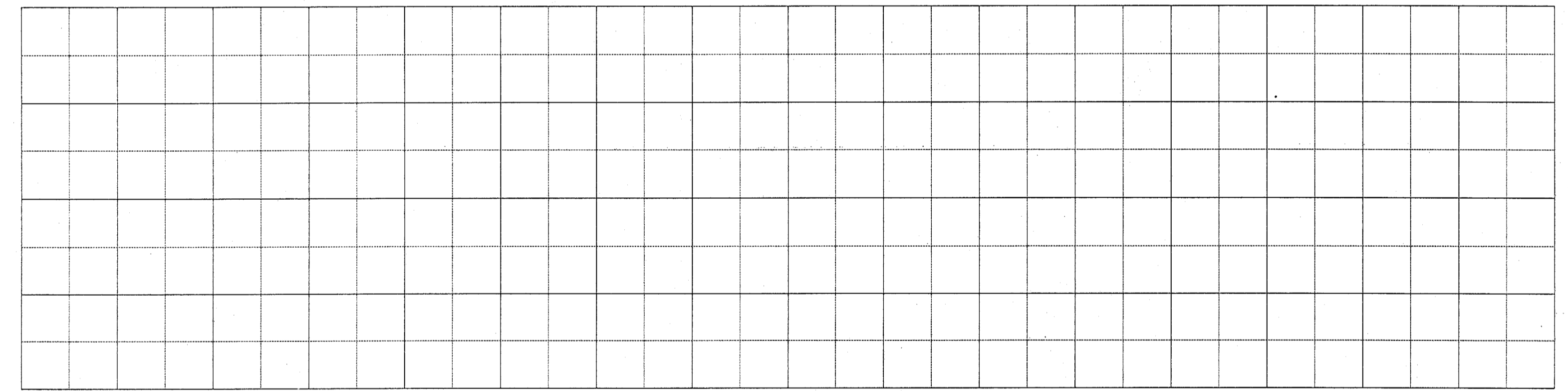
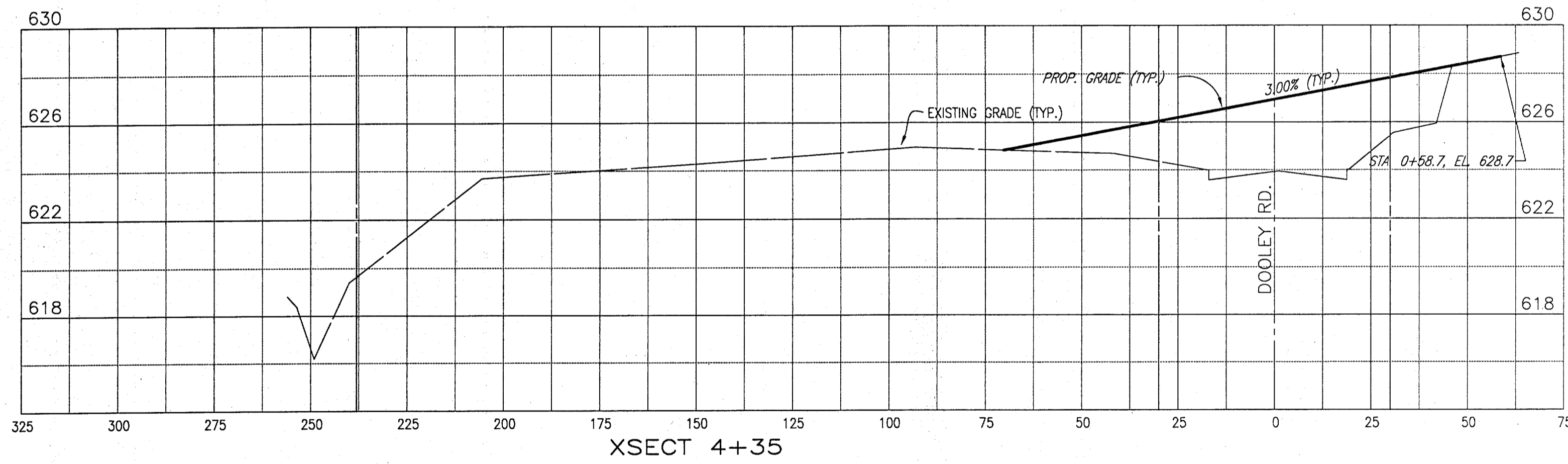
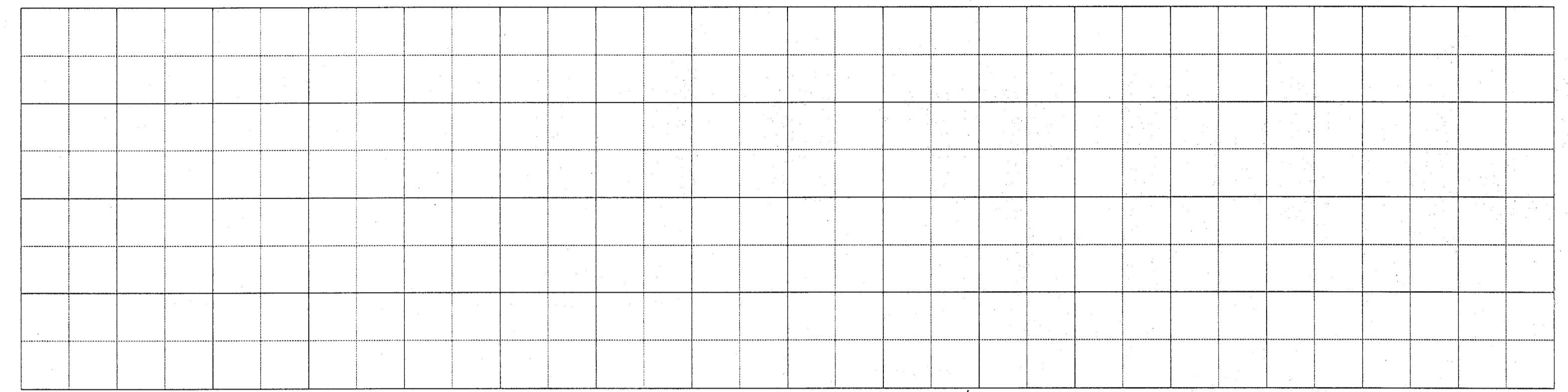
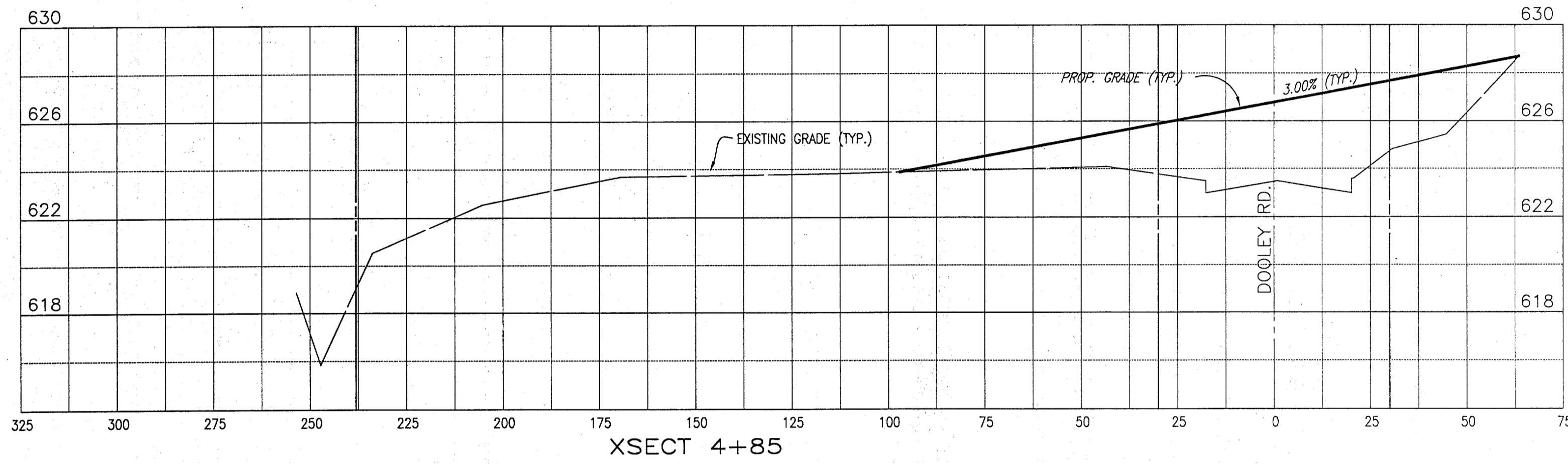
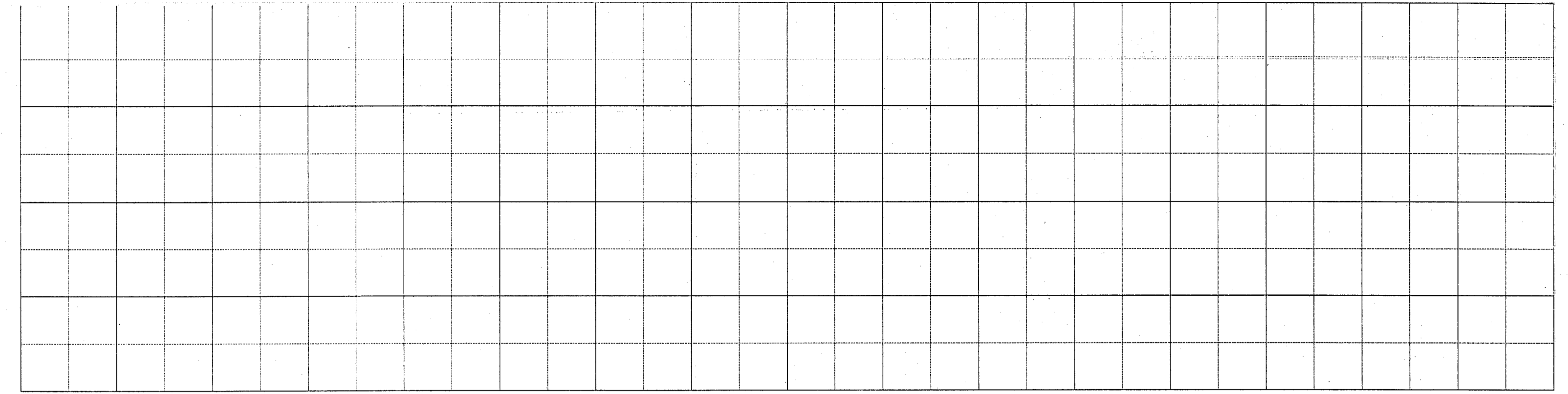
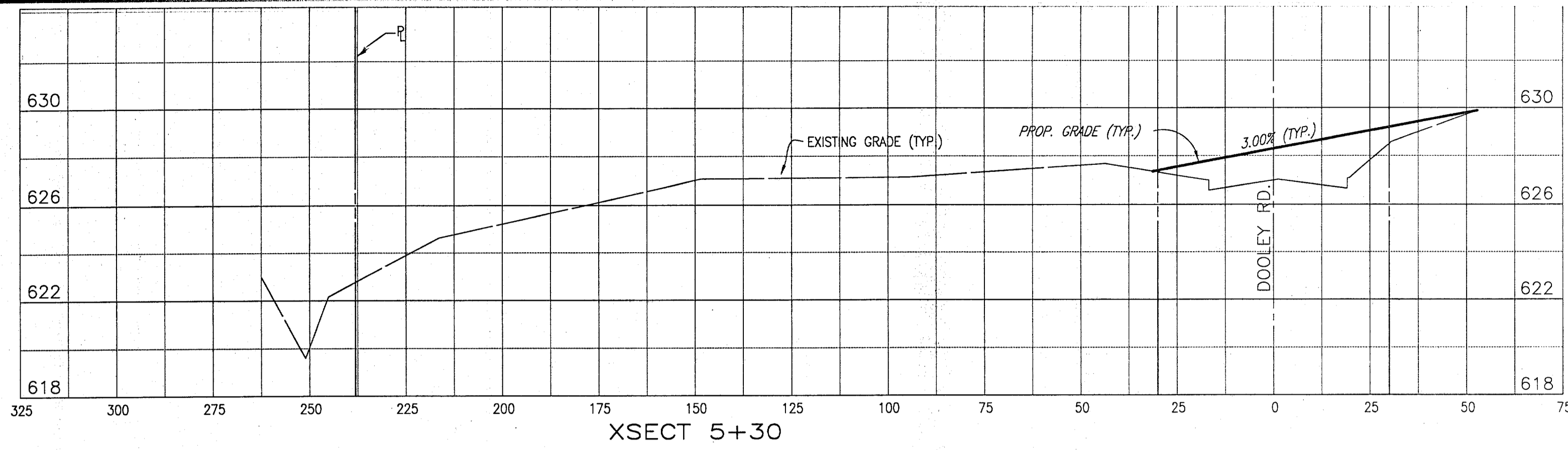
TOWN OF ADDISON, TEXAS

ADDISON AIRPORT PROPERTY RECLAMATION

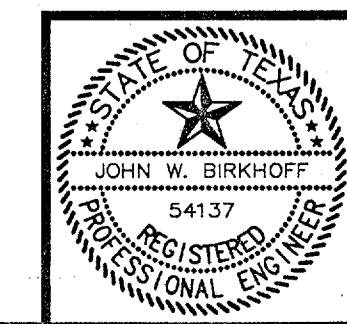
**DOOLEY ROAD
CROSS SECTIONS**

SHIMEK, JACOBS & FINKLEA, L.L.P.
CONSULTING ENGINEERS
Dallas, Texas

DESIGNED BY: J.W.B. PROJECT: 97 196 SHEET NO. 4
DRAWN BY: R.J.L. DATE: FEBRUARY 1998 OF 7 SHEETS



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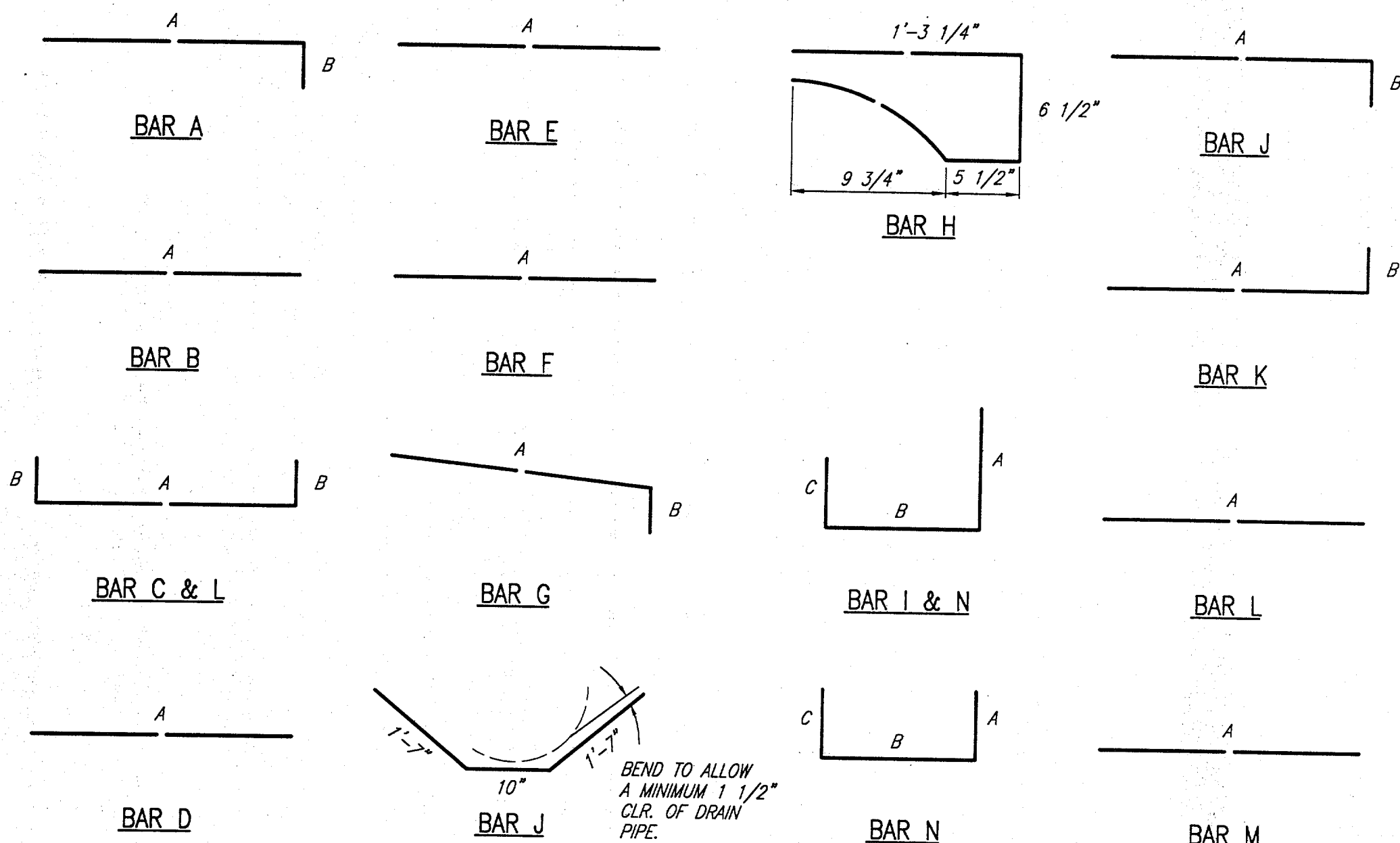
TOWN OF ADDISON, TEXAS

ADDISON AIRPORT PROPERTY RECLAMATION

**DOOLEY ROAD
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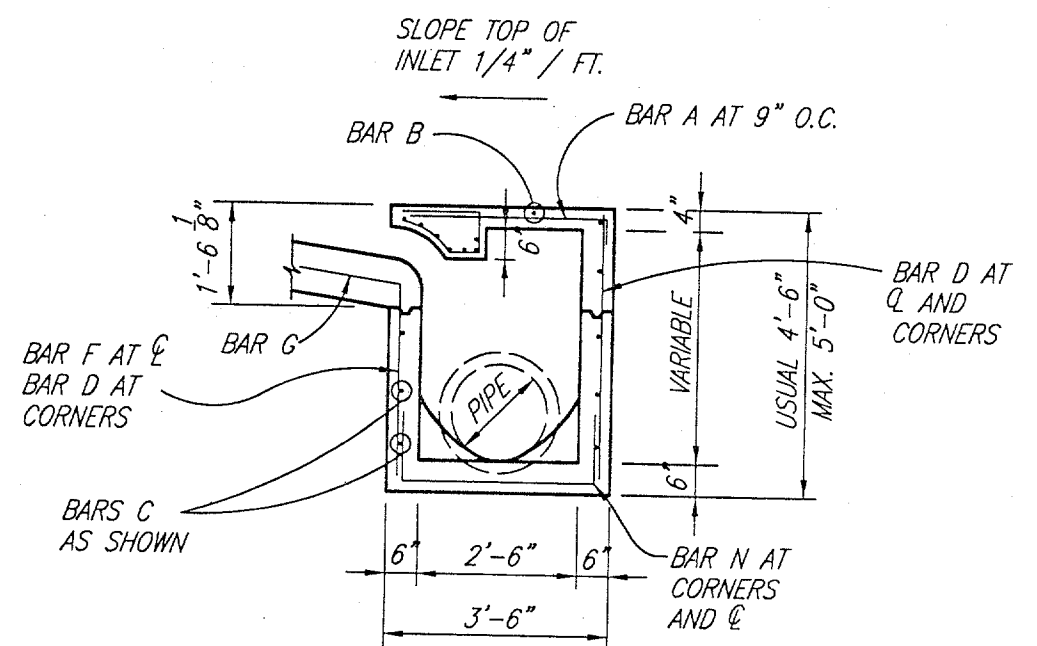
BAR DIAGRAMS

DIMENSIONS SHOWN ARE FOR MAXIMUM SIZE INLETS

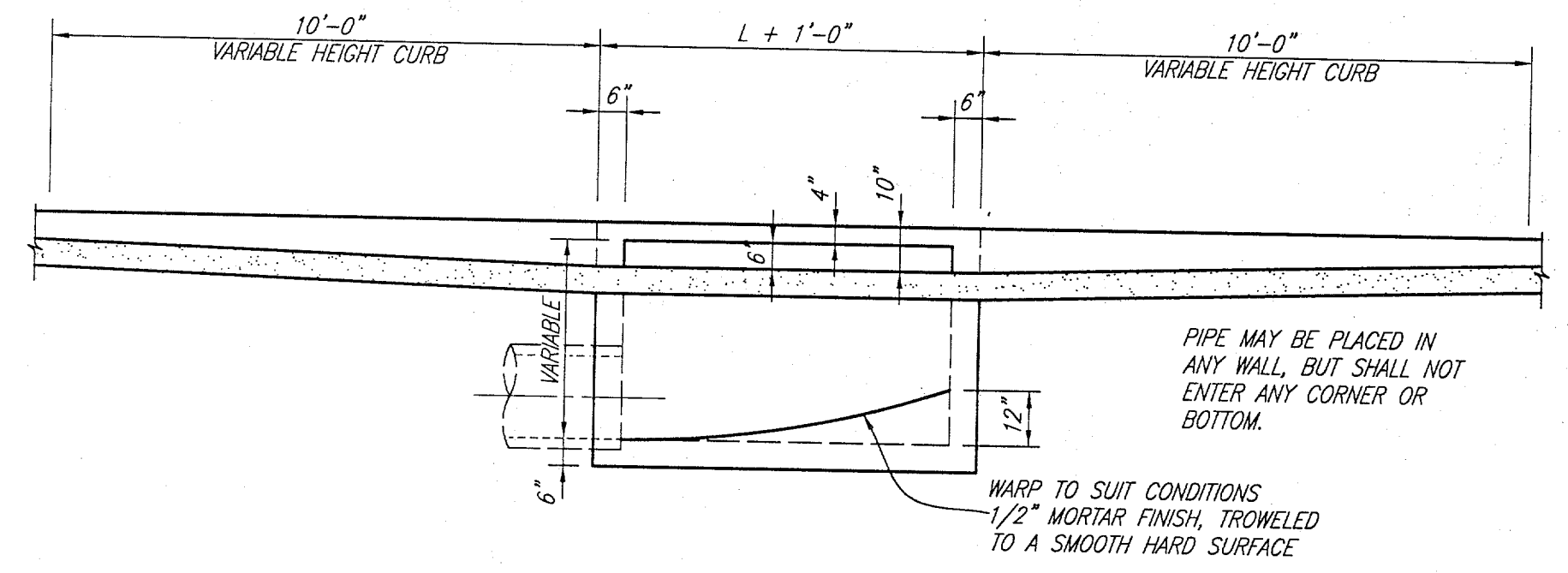
INLET LENGTH	BAR TYPE	BAR DIA. (1/8 IN.)	NO. REQ'D.	BAR DIMENSIONS		
				A	B	C
4 FT.	A	3	6	3'-2"	0'-3"	-
	B	3	1	2'-10"	-	-
	C	4	15	4'-8"	0'-6"	-
	D	4	5	4'-8"	-	-
	F	4	1	3'-2"	-	-
	G	3	5	2'-0"	1'-3"	-
5 FT.	A	3	6	3'-2"	0'-3"	-
	B	3	1	3'-10"	-	-
	C	4	15	5'-8"	0'-6"	-
	D	4	5	3'-8"	-	-
	F	4	1	3'-2"	-	-
	G	3	5	2'-0"	1'-3"	-
6 FT.	A	3	9	3'-2"	3'-2"	3'-2"
	B	3	1	4'-10"	-	-
	C	4	15	6'-8"	0'-6"	-
	D	4	5	4'-8"	-	-
	F	4	1	3'-2"	-	-
	G	3	5	2'-0"	1'-3"	-
8 FT.	A	3	12	3'-2"	0'-3"	-
	B	4	1	6'-10"	-	-
	C	4	15	8'-8"	0'-6"	-
	D	4	5	4'-8"	-	-
	F	4	1	3'-2"	-	-
	G	3	5	2'-0"	1'-3"	-
10 FT.	A	3	10	3'-2"	0'-3"	-
	B	3	2	8'-10"	-	-
	C	4	16	10'-8"	0'-6"	-
	D	4	4	4'-8"	-	-
	E	5	6	10'-8"	-	-
	G	3	5	2'-0"	1'-3"	-
H	3	14	*	*	*	
I	4	8	4'-8"	3'-2"	3'-2"	
L	4	5	4'-3"	-	-	

* SEE DIAGRAM FOR DIMENSIONS.
4', 5', 6', 8' AND 10' INLETS

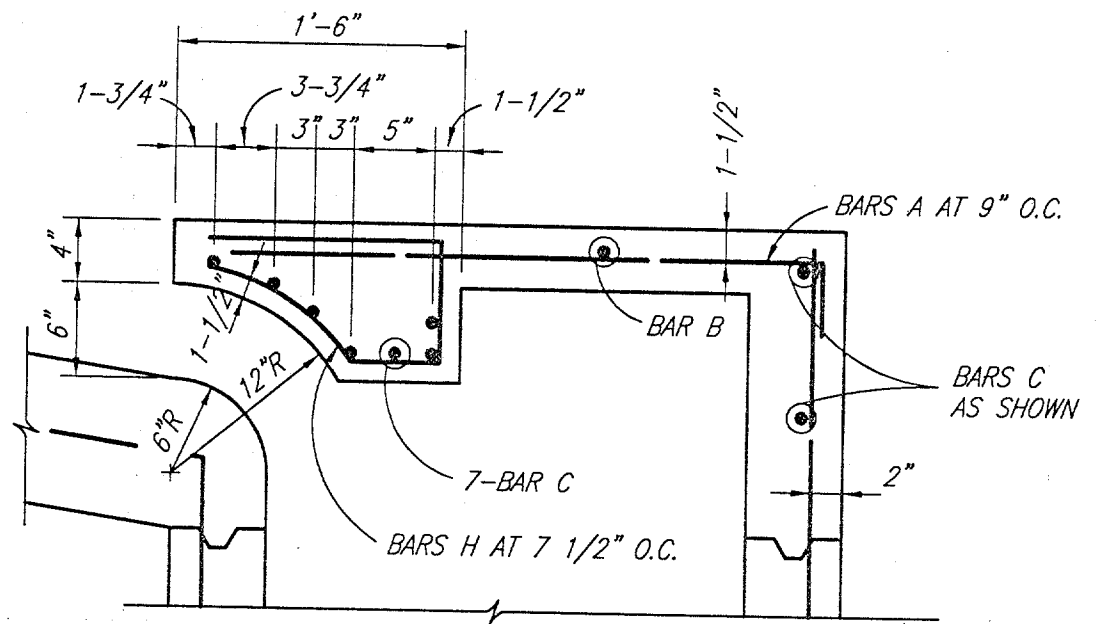
REINFORCING STEEL SCHEDULE



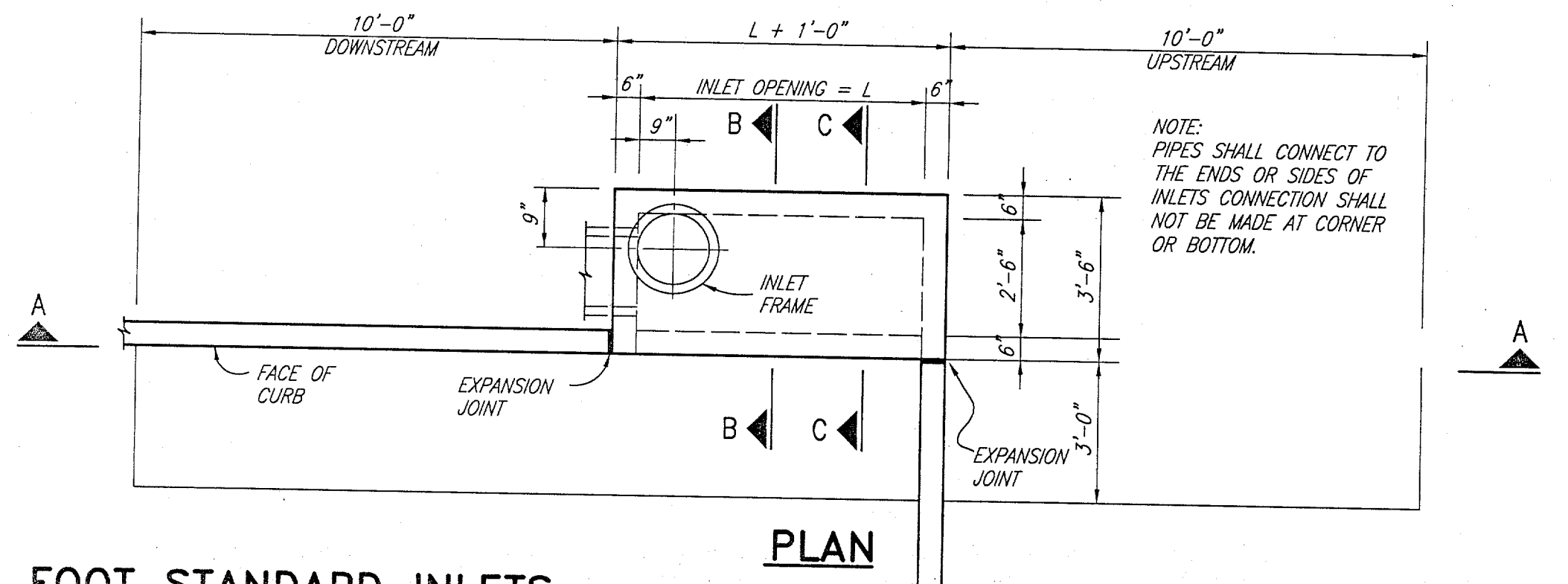
SECTION B-B



SECTION A-A

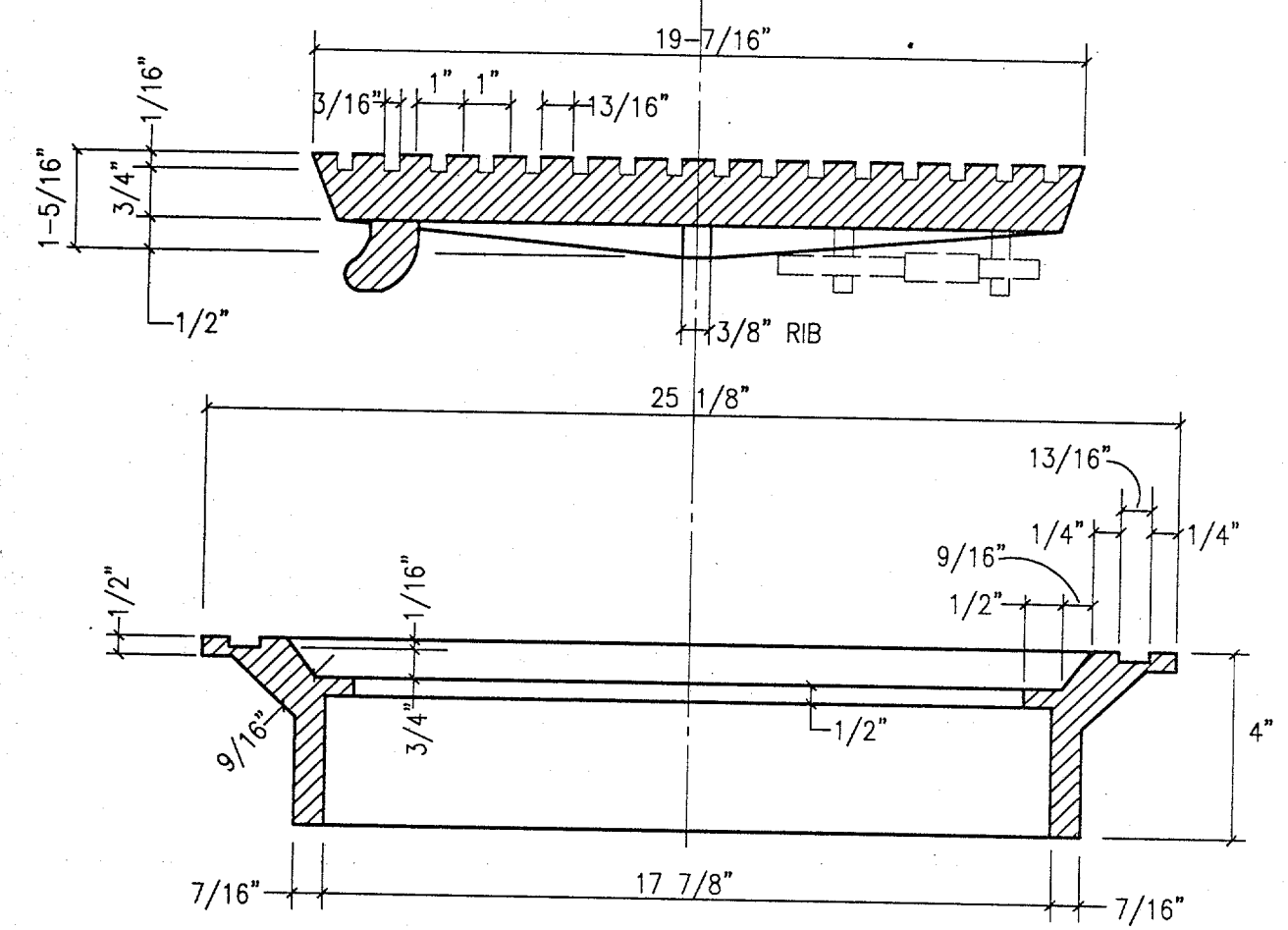
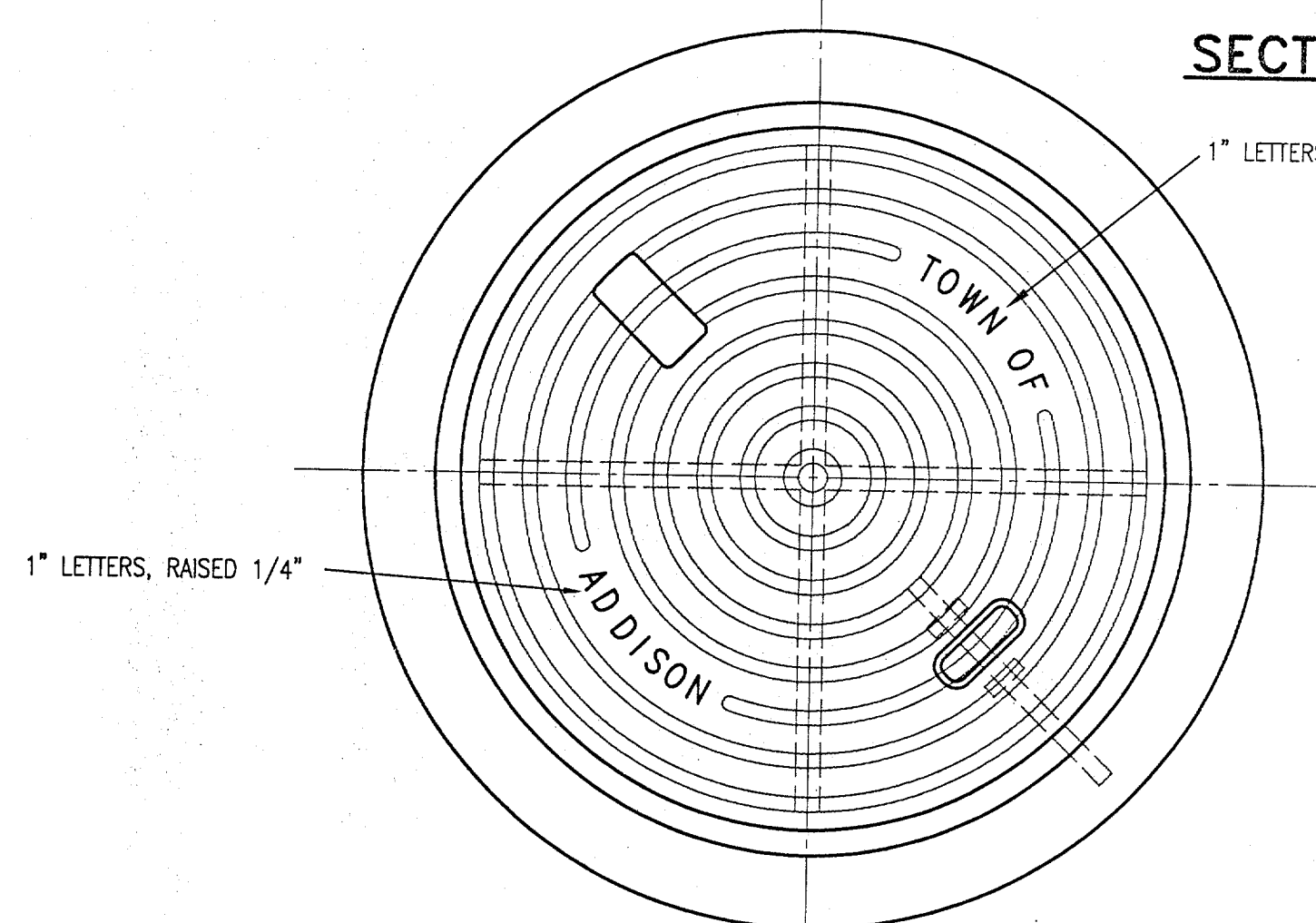


SECTION C-C



PLAN

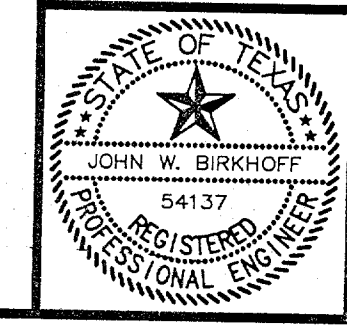
5 AND 10 FOOT STANDARD INLETS



INLET FRAME AND COVER

COVER 55#, FRAME 45#

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John W. Bullif
DATE: 3/17/98

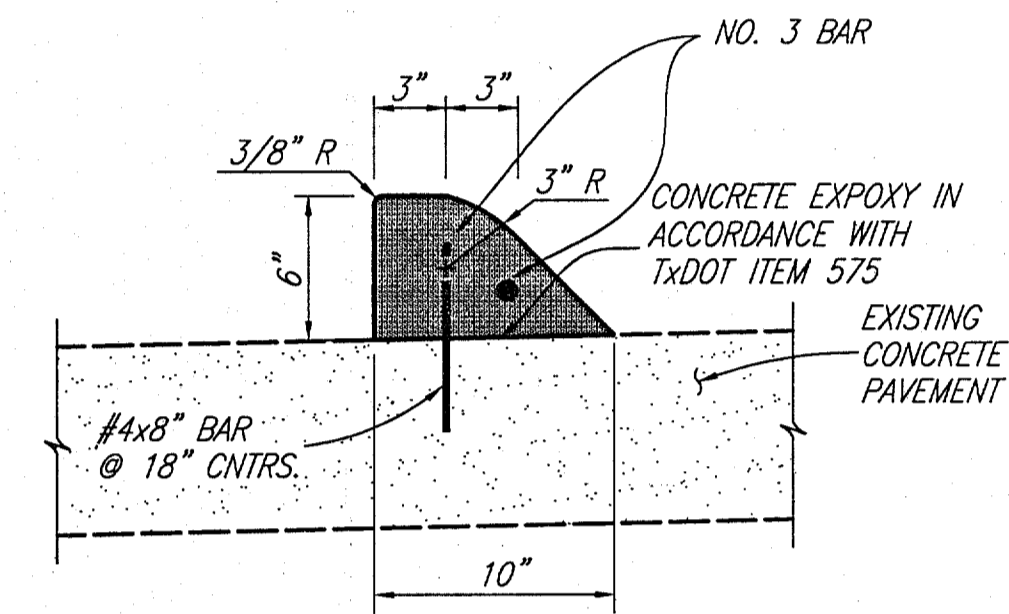


TOWN OF ADDISON, TEXAS
ADDISON AIRPORT PROPERTY RECLAMATION
DOOLEY ROAD DETAILS
SHIMEK, JACOBS & FINKLEA, L.L.P.
CONSULTING ENGINEERS
Dallas, Texas

DESIGNED BY: J.W.B. PROJECT: 97 196 SHEET NO. 6
DRAWN BY: R.J.L. DATE: FEBRUARY 1998 OF 7 SHEETS

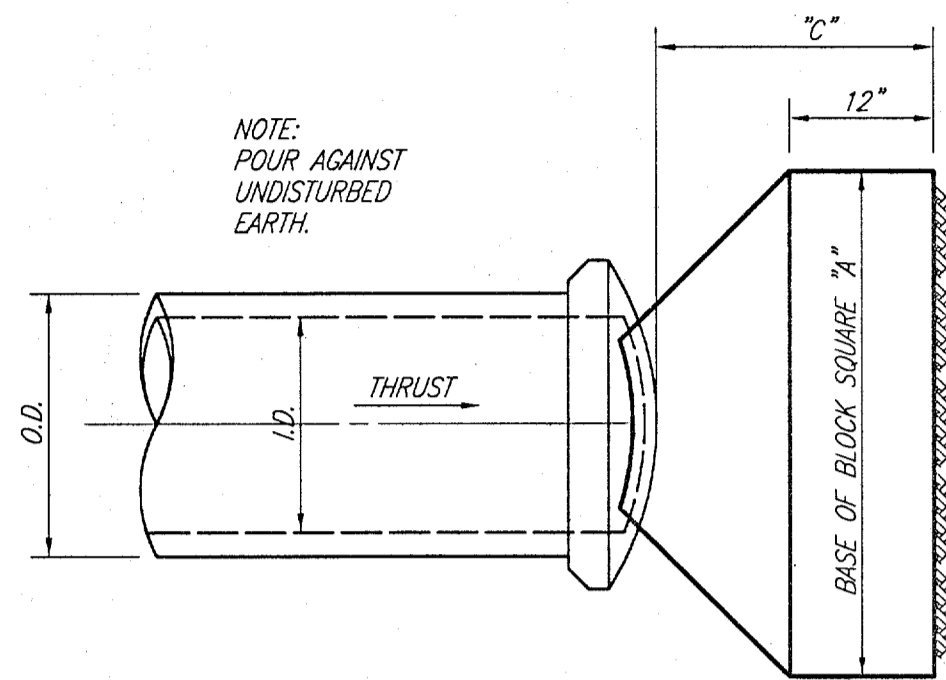
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REVISED 02/19/98 RLL 96169-03.DWG SCALE: 1"=1' BLOCKS: MH-COV2, STDINT, BARLIST



6" DOWELED MOUNTABLE CURB

SCALE: 1-1/2" = 1'-0"



PLUG THRUST BLOCK PLAN

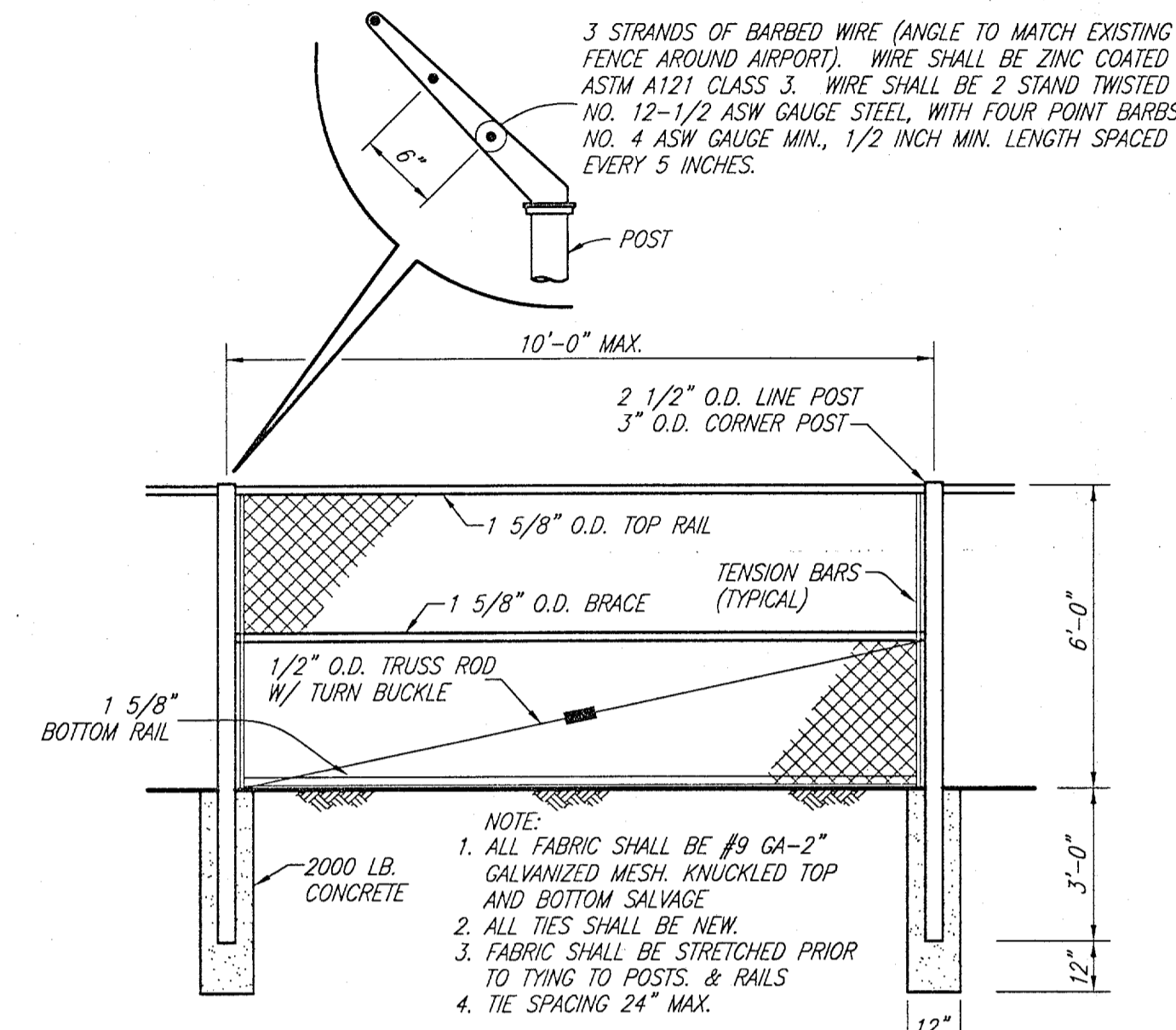
THRUST DATA - PLUGS

I.D. (INCHES)	THRUST (TONS)	C (FT.)	EARTH			ROCK	
			A (FT.)	VOL. (C.Y.)	A (FT.)	VOL. (C.Y.)	
4,6,8	5.1	1.5	2.5	0.3	2.0	0.2	
10,12	11.3	1.5	3.5	0.6	2.5	0.3	
16,18	25.5	2.0	5.5	1.6	4.0	0.9	
20	31.5	2.0	6.0	1.9	4.0	0.9	
24	45.2	2.5	7.0	3.1	5.0	1.7	
30	53.0	3.0	7.5	4.1	5.5	2.4	

THRUST BLOCKS

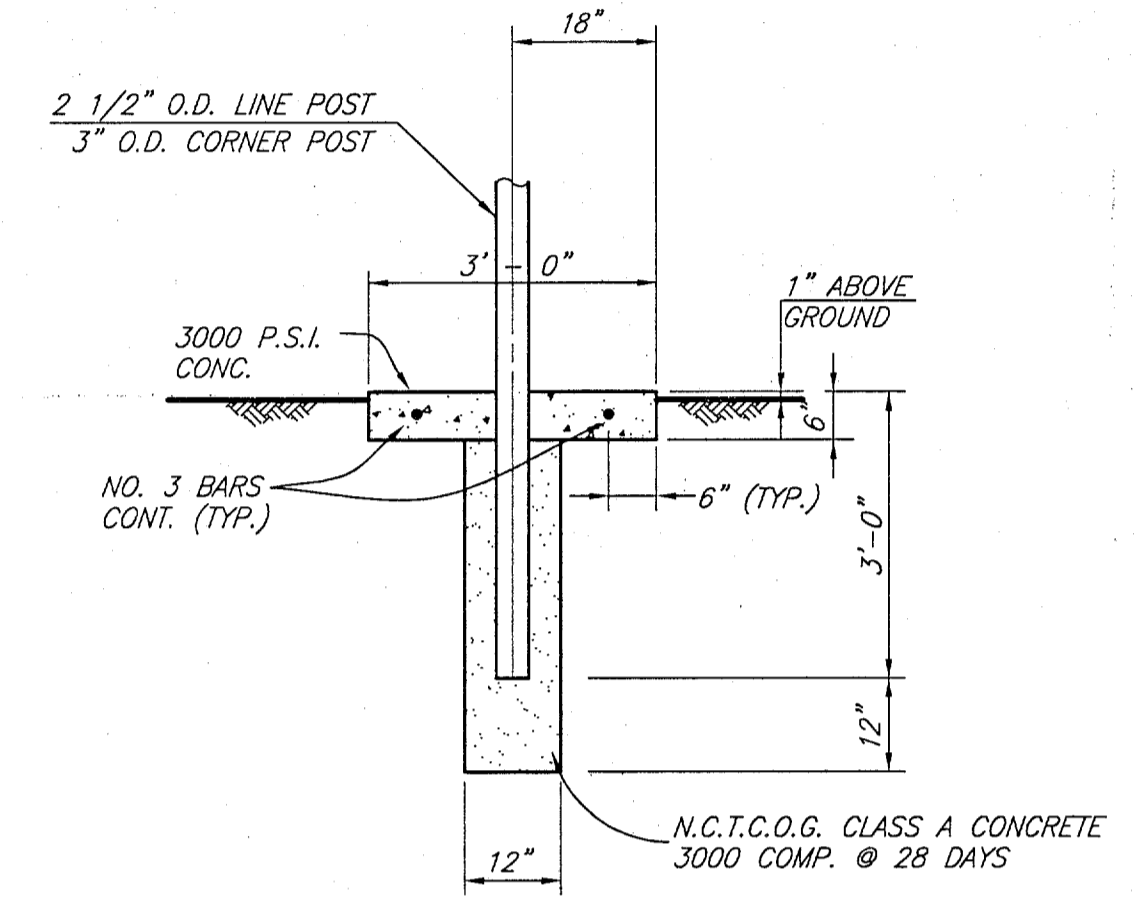
NO SCALE

NOTE: CONTRACTOR SHALL POUR ADEQUATE BLOCKING TO OVERCOME WORKING PRESSURES IN WATER LINE.



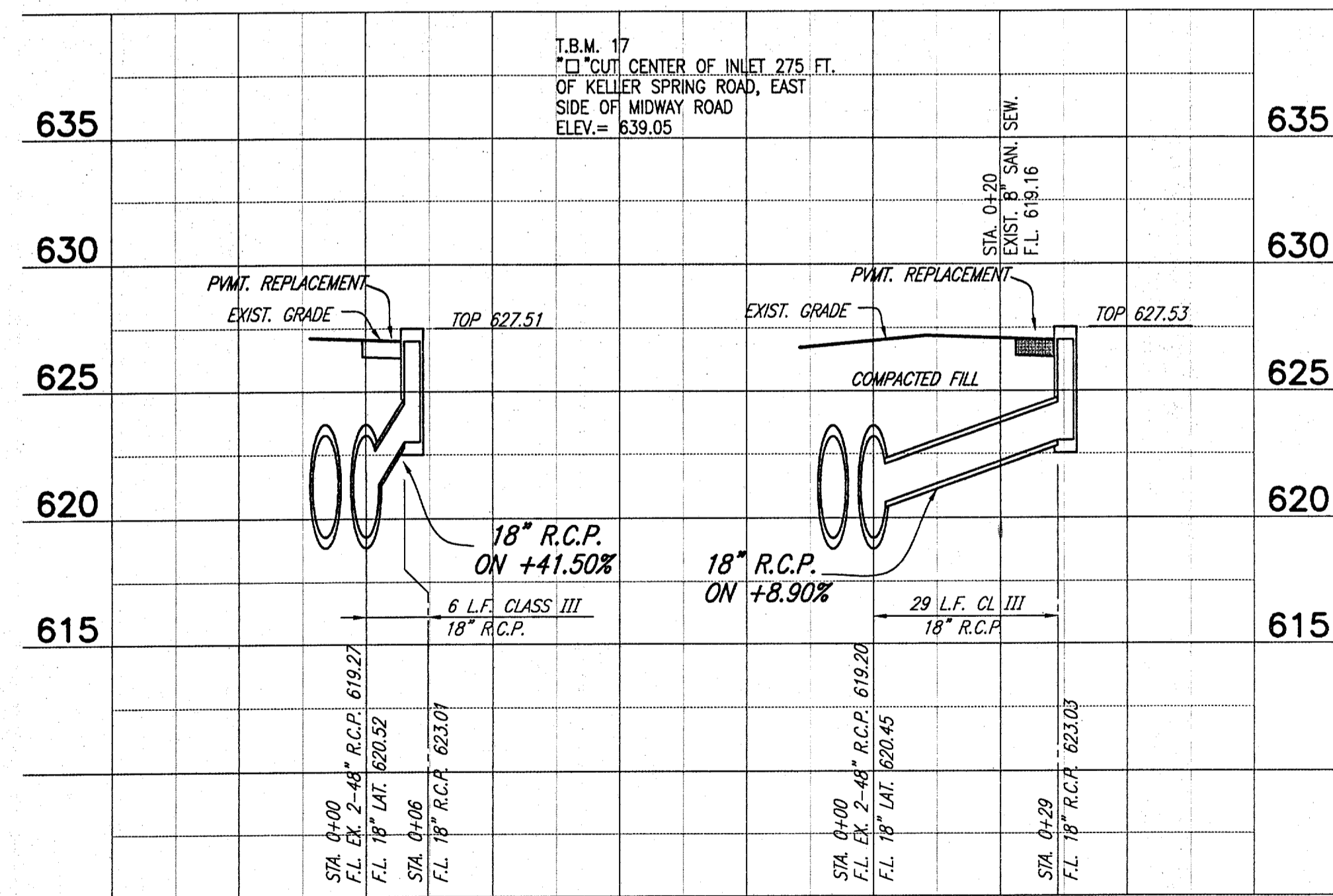
SALVAGED CHAIN LINK FENCE RECONSTRUCTION

NO SCALE



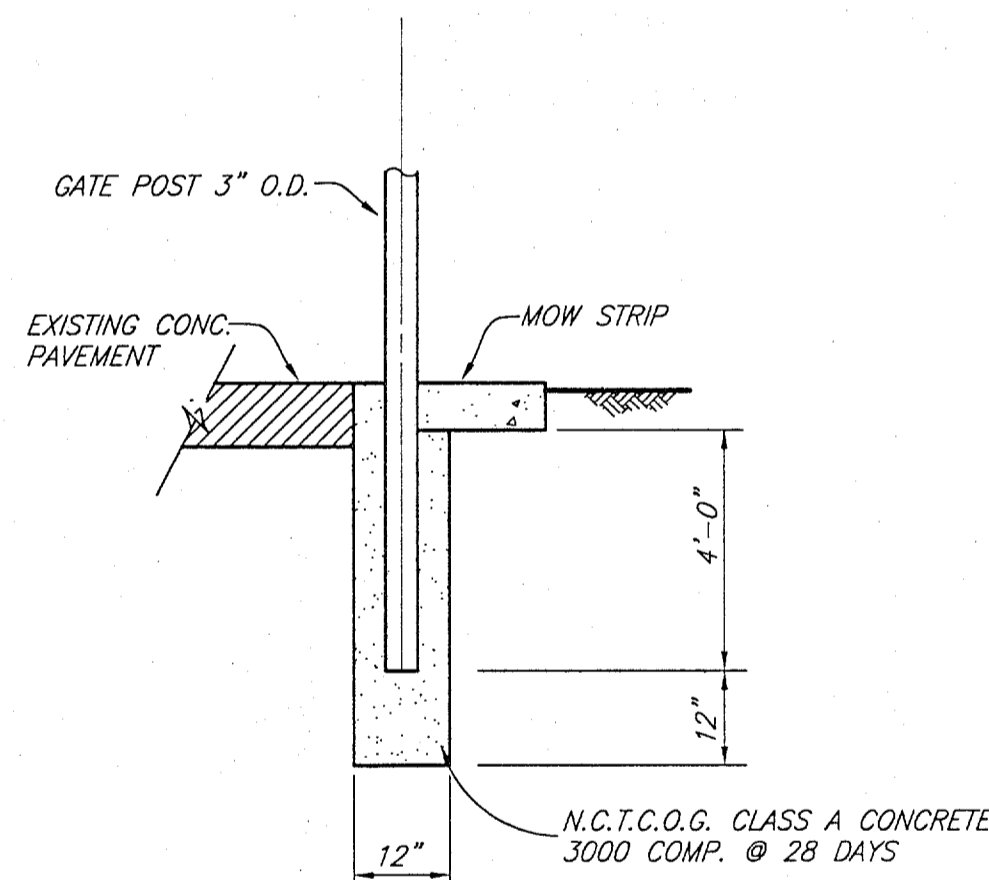
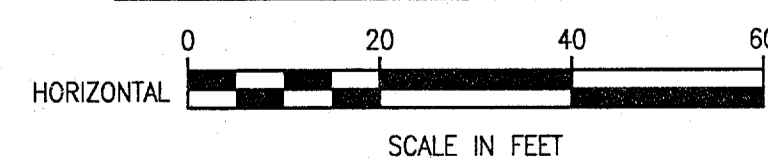
MOW STRIP AT LINE POST

SCALE: 1"=2"



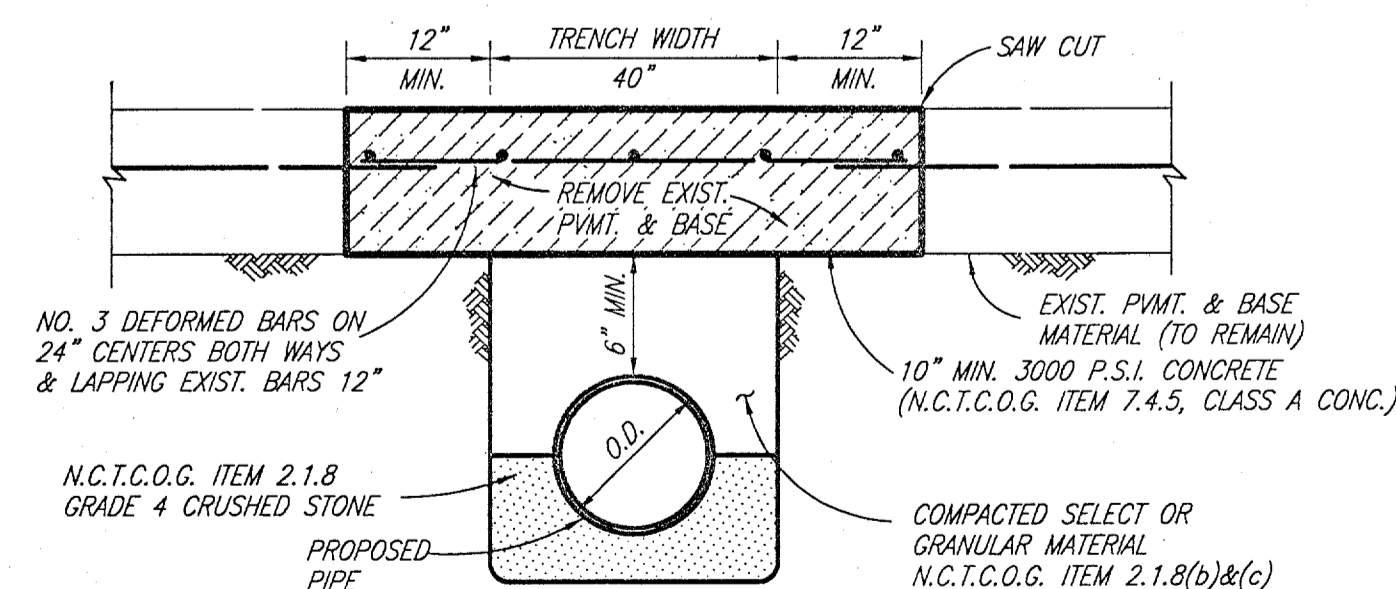
LATERAL A-1 STORM SEWER PROFILES

LATERAL A-2 STORM SEWER PROFILES



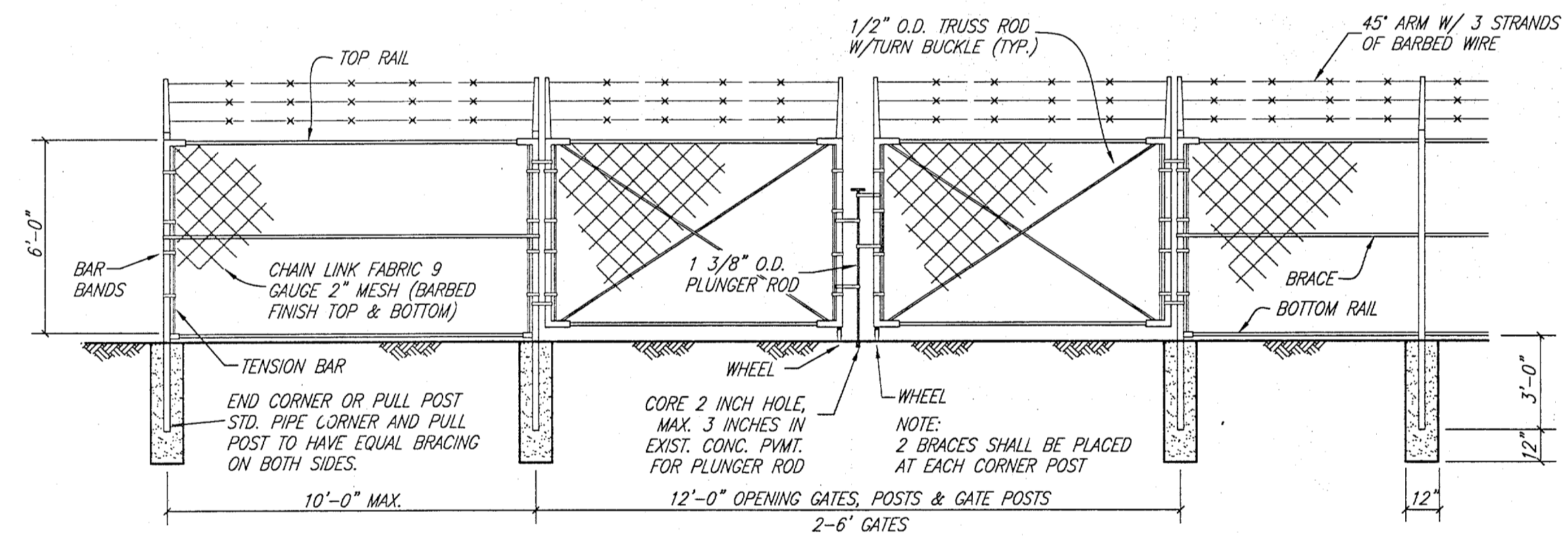
GATE POST

NOT TO SCALE



EMBEDMENT & PVMT. REPAIR

NO SCALE



TYPICAL CHAIN LINK GATES

NO SCALE

NOTE: GATE AND APPURTENANCES HOT DIPPED GALVANIZED.

NCTCOG GRADE 4 CRUSHED STONE:

SIEVE SIZE	PERCENT RETAINED
1 1/2"	0%
1"	0-5%
1/2"	40-75%
NO. 4	90-100%
NO. 8	95-100%

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DRAWN BY: R.J.L.	DATE: FEBRUARY 1998	OF 7 SHEETS

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DATE: 3/17/98

