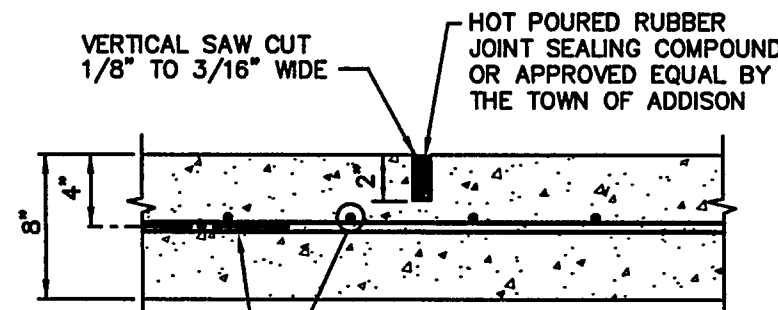
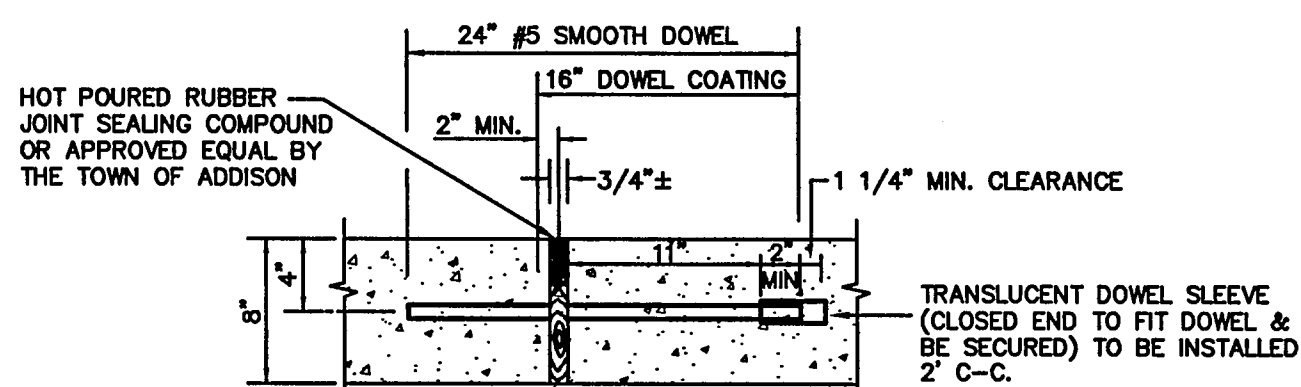


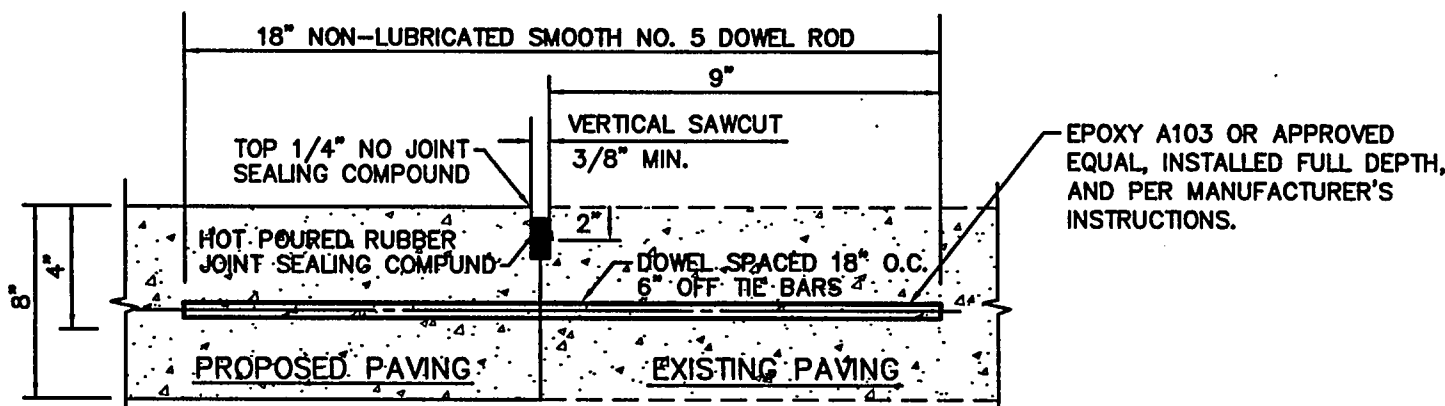
CONSTRUCTION JOINT
N.T.S.



SAWED DUMMY JOINT
N.T.S.



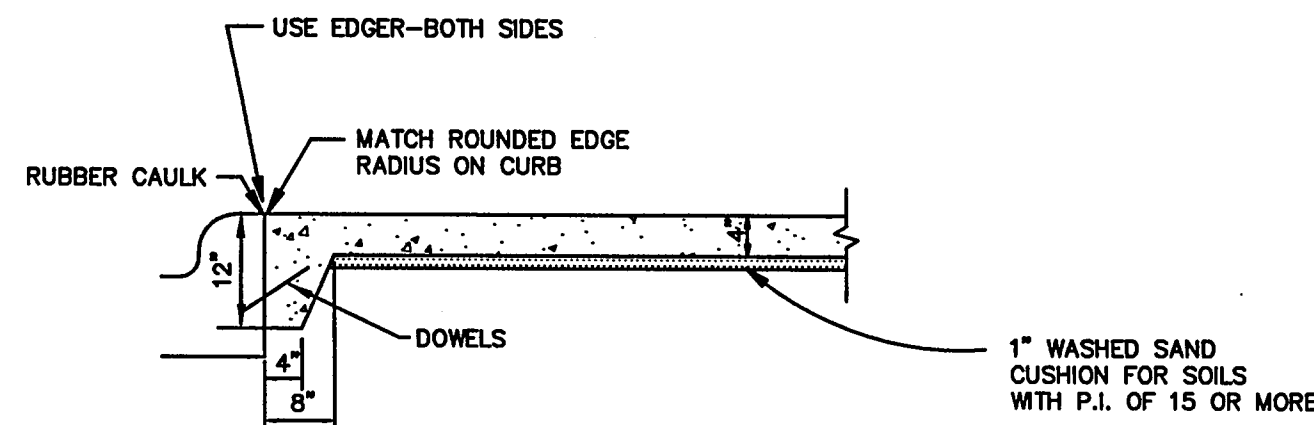
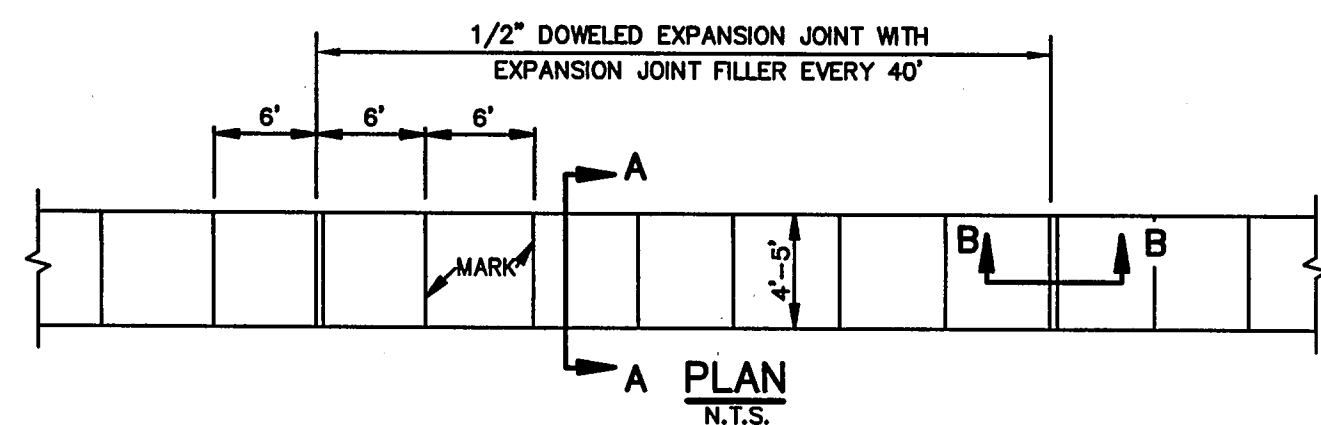
EXPANSION JOINT
N.T.S.



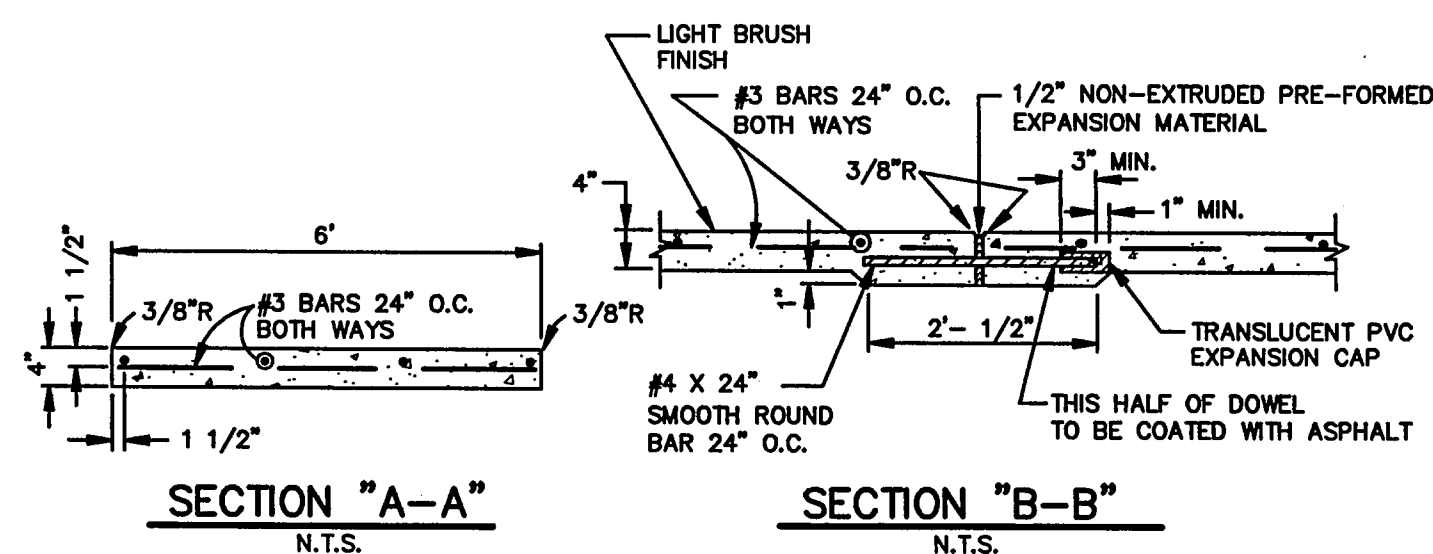
NOTE:
DOWEL BARS SHALL BE DRILLED INTO PAVEMENT HORIZONTALLY BY USE OF A MECHANICAL RIG. DRILLING BY HAND IS NOT ACCEPTABLE. PUSHING DOWEL BARS INTO GREEN CONCRETE IS NOT ACCEPTABLE.

CONNECTION TO EXISTING PAVEMENT
N.T.S.

PAVEMENT/JOINT DETAILS

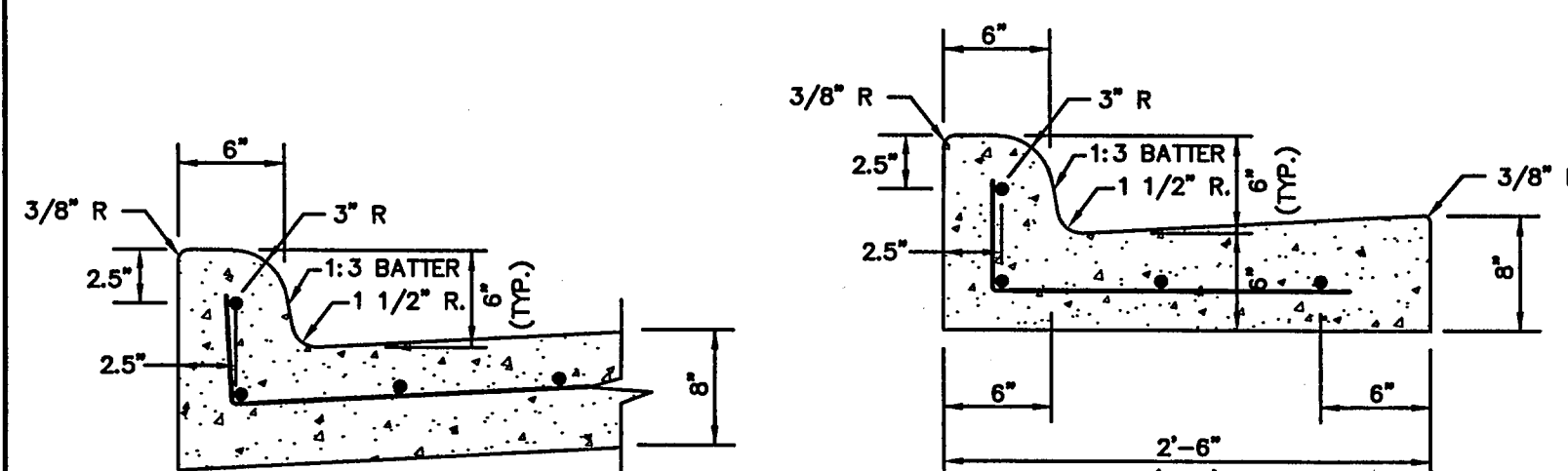


JOINT LUG DETAIL FOR MEDIAN PAVEMENT OR SIDEWALK ADJACENT TO CURB
N.T.S.



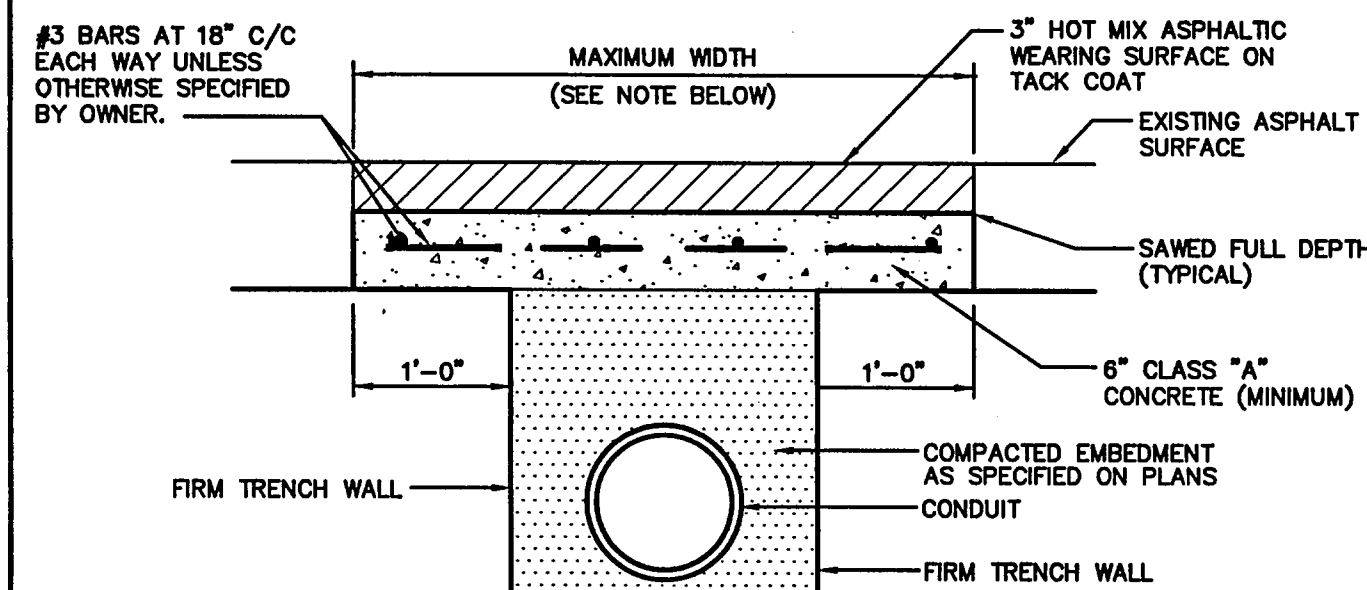
NOTE:
1. CROSS SLOPE OF SIDEWALK SHALL BE ± 1/4" PER FT. MIN. TO ± 3/8" PER FT. MAX.
2. OTHER THAN 6'-0" SIDEWALK WIDTH MAY BE SPECIFIED BY OWNER.
3. SIDEWALK SHALL BE CLASS "A" CONCRETE UNLESS OTHERWISE SPECIFIED BY OWNER.

SIDEWALK DETAILS
N.T.S.



NOTE:
1. REINFORCEMENT SHALL BE NO. 4 BARS.
2. CONCRETE SHALL BE CLASS "C".
3. "C" IS 6" UNLESS OTHERWISE SPECIFIED.
4. ALL CURBS ARE CONSTRUCTED OF PORTLAND CEMENT CONCRETE UNLESS OTHERWISE SHOWN.
5. GRADE SHALL BE MEASURED AT BACK OF CURB.

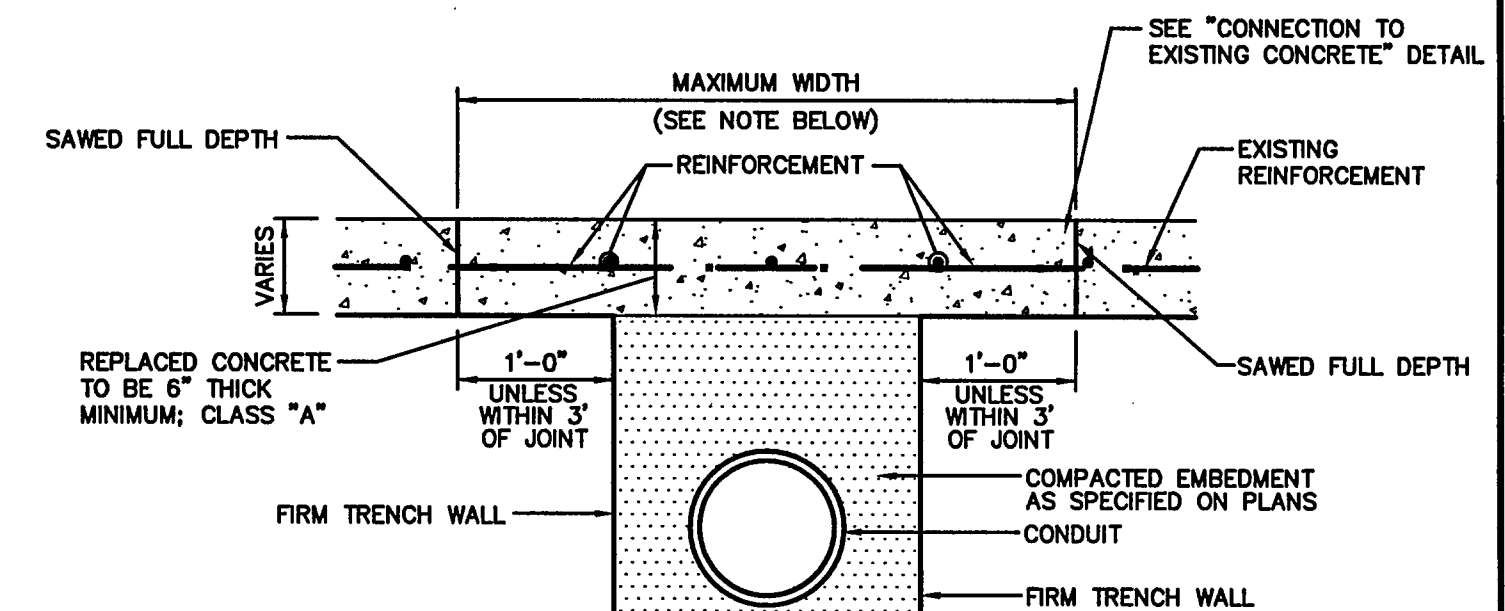
CURB DETAILS
N.T.S.



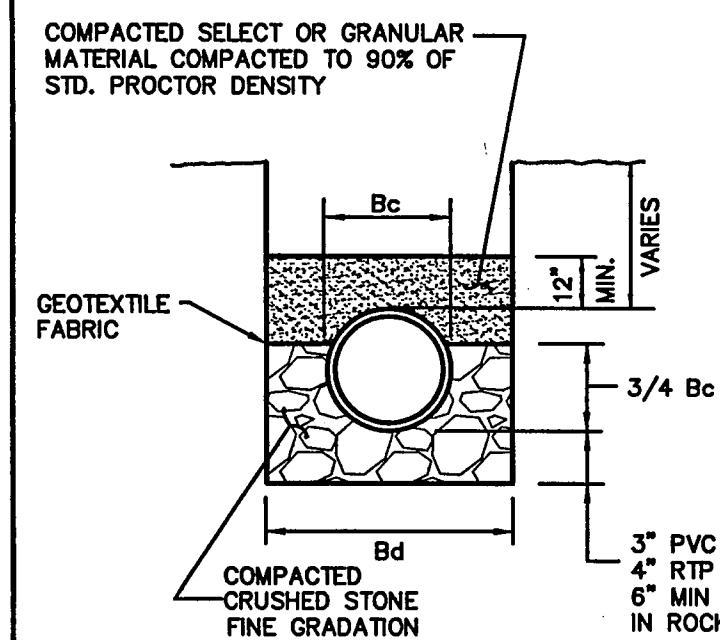
ASPHALT PAVEMENT
N.T.S.

NOTES:
1. PAYMENT TO THE CONTRACTOR FOR REPLACEMENT OF PAVEMENT AND/OR DRIVEWAYS WILL BE BASED ON ACTUAL MEASUREMENTS UP TO A MAXIMUM WIDTH EQUAL TO THE SPECIFIED MAXIMUM TRENCH WIDTH (PER STD. SPEC. ITEM 6.2.) PLUS 2 FEET. ANY EXISTING PAVEMENT DAMAGED OR REMOVED IN EXCESS OF THE MAXIMUM LIMITS SHALL BE AT THE EXPENSE OF THE CONTRACTOR.
2. ALL SAW CUTS SHALL BE FULL DEPTH. CONTRACTOR SHALL DRILL AND EPOXY GROUT 18" X #4 SMOOTH DOWEL 9" INTO EXISTING CONCRETE. REINFORCEMENT SHALL BE #3 BARS AT 18" C/C EACH WAY UNLESS OTHERWISE SPECIFIED BY THE OWNER.

PAVEMENT/DRIVEWAY REPAIR DETAILS
N.T.S.



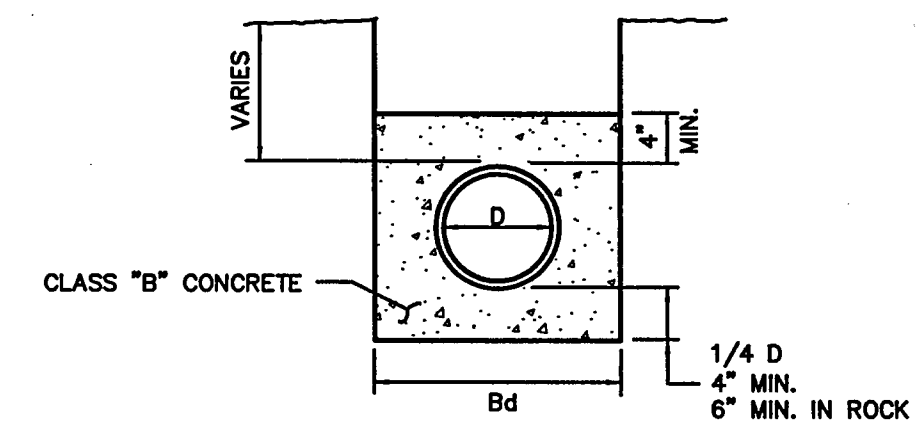
CONCRETE PAVEMENT
N.T.S.



CLASS "B-2"
N.T.S.

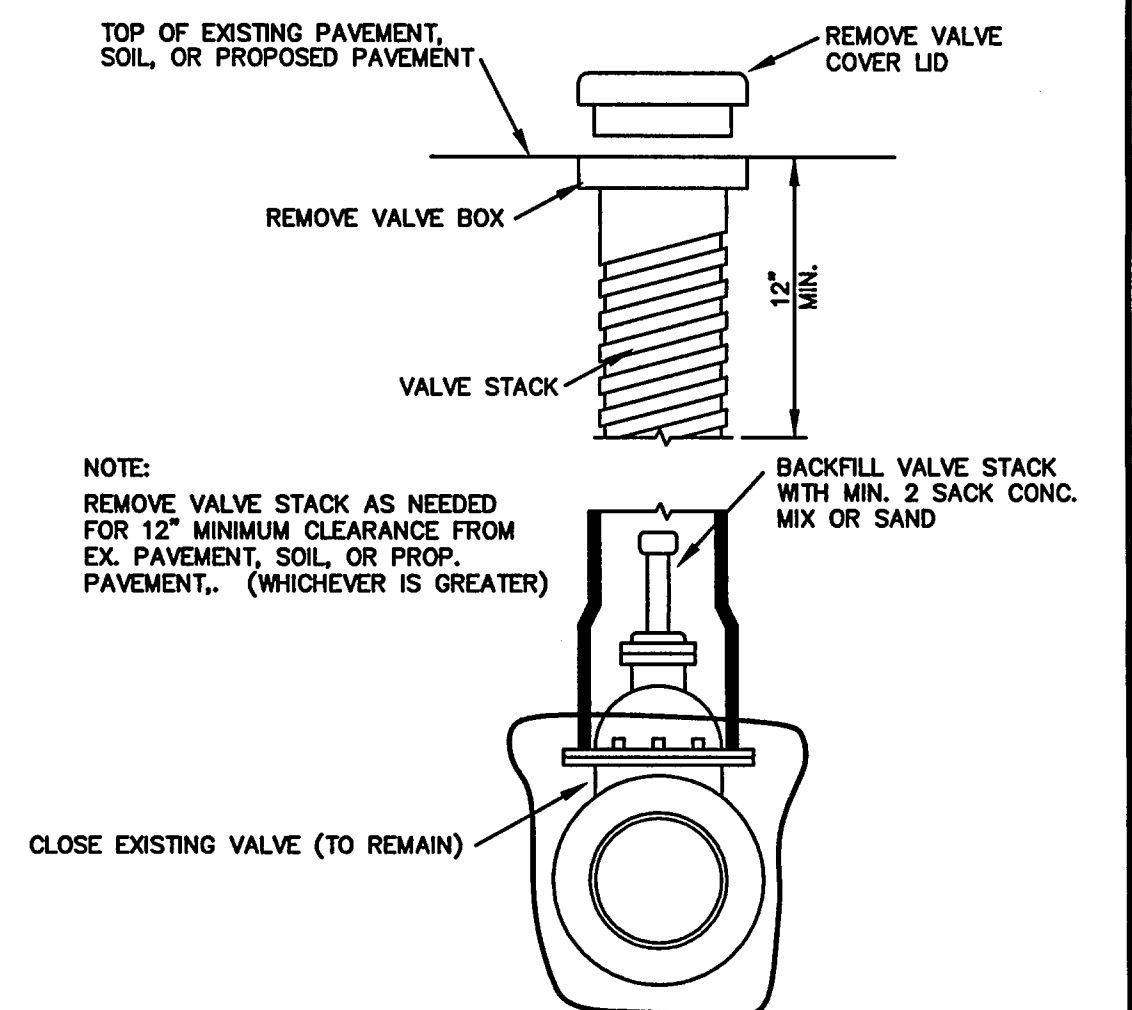
NOTE:
1. Bc = OUTSIDE DIAMETER OF PIPE
2. Bd = TRENCH WIDTH

EMBEDMENT/ENCASEMENT DETAILS
N.T.S.

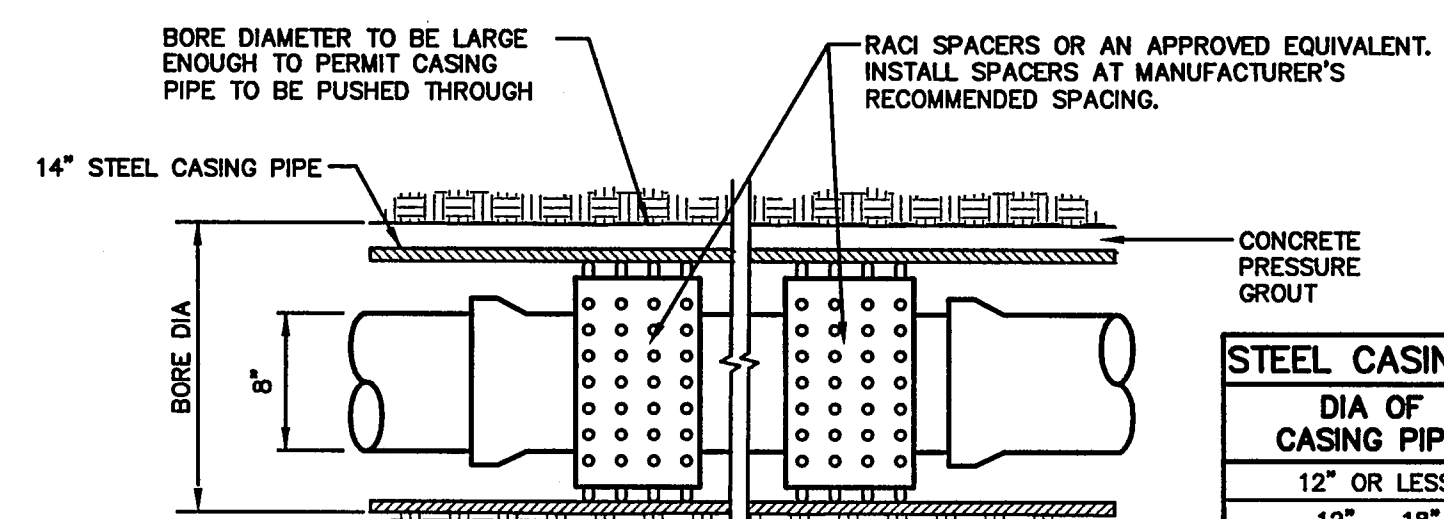


CONCRETE ENCASEMENT
N.T.S.

NOTE:
1. D = INSIDE DIAMETER OF PIPE
2. Bd = TRENCH WIDTH



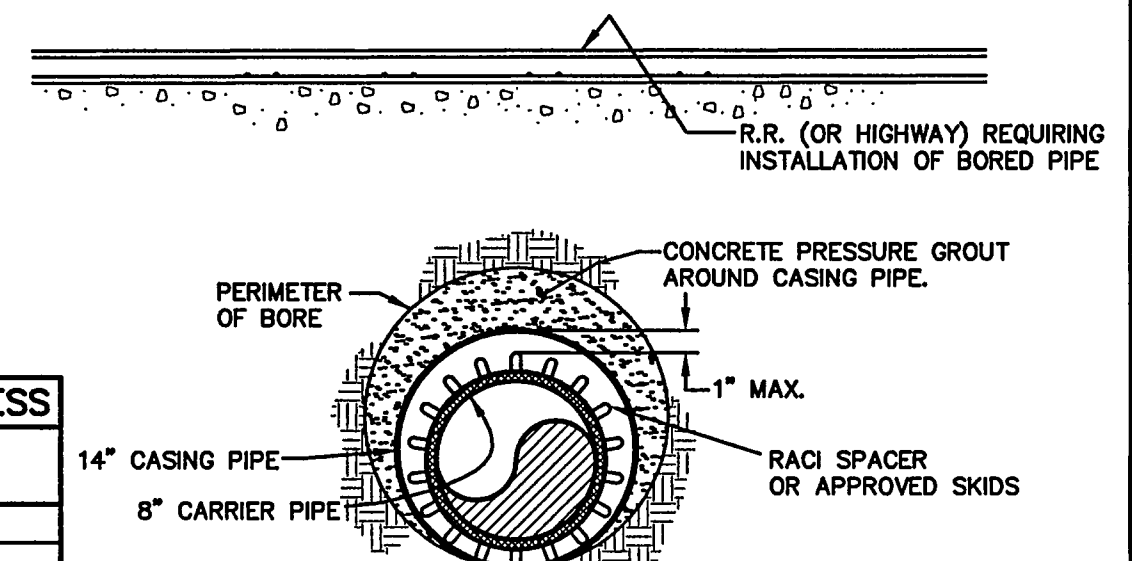
WATER VALVE ABANDONMENT DETAIL
N.T.S.



TYPICAL LONGITUDINAL VIEW

STEEL CASING WALL THICKNESS	
DIA OF CASING PIPE	MIN THICKNESS
12" OR LESS	0.25"
12" - 18"	0.3125"
18" - 22"	0.4375"
22" - 28"	0.4375"

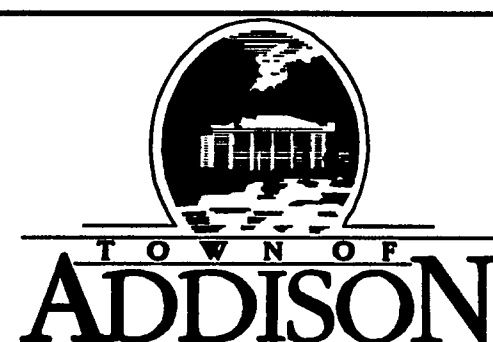
CASING PIPES SHALL BE SMOOTH STEEL PIPES WITH A MIN YIELD STRENGTH OF 35,000psi



TYPICAL END VIEW

BORE AND CASING ROADWAY CROSSING DETAIL
N.T.S.

NO.	REVISION	BY	DATE

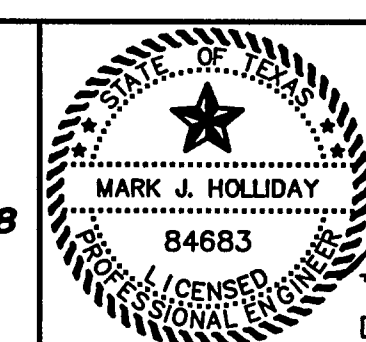


SCALE	HORIZ	N/A
	VERT	N/A
	DATE	FEB 2004



TEAGUE NALL AND PERKINS
ENGINEERS • SURVEYORS • CONSULTANTS

1100 Macon Street Fort Worth, Texas 76102 (817) 338-5773
235 W. Hickory Street #100 Denton, Texas 76201 (940) 383-4177
12180 N. Abrams Road #508 Dallas, Texas 75243 (214) 461-9867



Date: 2-9-07

TOWN OF ADDISON, TEXAS
TALISKER APARTMENTS
WATER LINE IMPROVEMENTS

MISCELLANEOUS DETAILS 2