

ERIC L. DAVIS ENGINEERING, INC.
 425 Pinson Road Suite "G"
 Forney, Texas 75126
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 E-Mail ericdavis@eidengineering.com

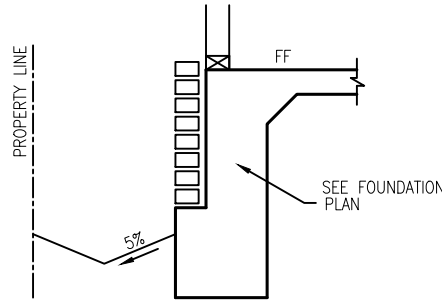
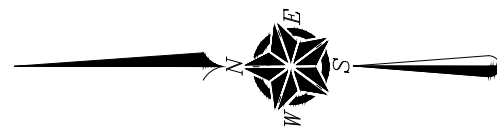
GRADING / SITE PLAN
 ENGINEERED FOR:

BENTLEY CUSTOM HOMES

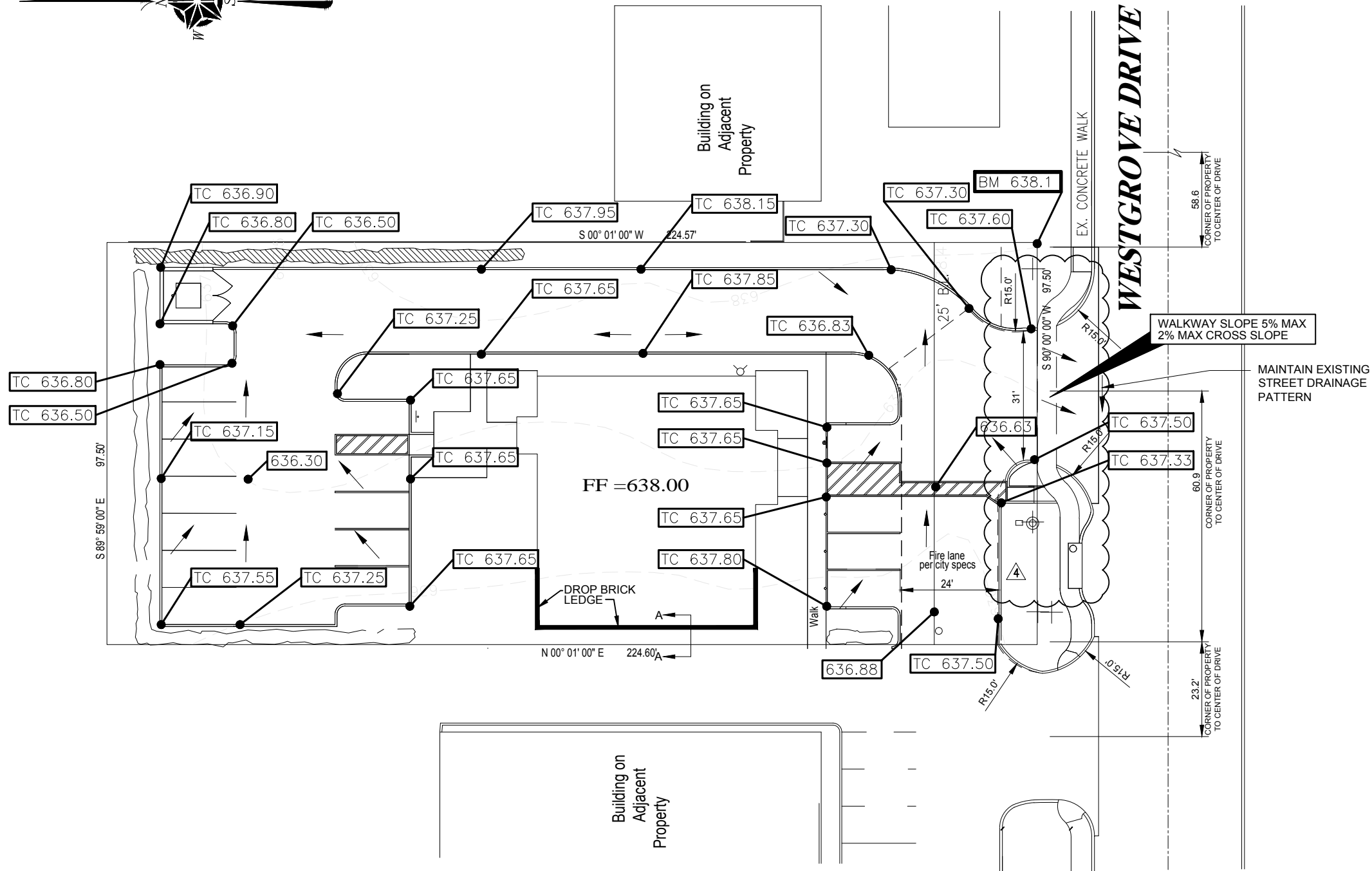
PLAN: GRADING
 BUILDER: BENTLEY CUSTOM HOMES
 ADDITION:
 ADDRESS: 4397 WESTGROVE
 LOT: BLOCK: A
 CITY: ADDISON, TEXAS
 ELD JOB NO.:
 DRAWN BY: BW
 FIRM REGISTRATION #: 3987

SCALE: 1" = 30'

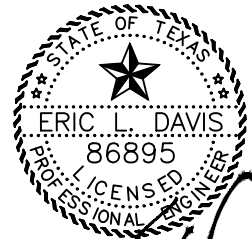
SHEET G01



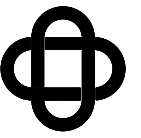
DROP BRICK LEDGE
 SECTION A-A
 NTS



REVISIONS	CODE
1/28/16 REVISED CURB AND SIDEWALK	T

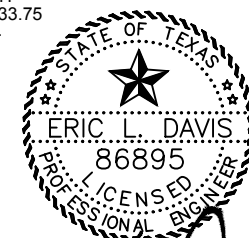


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 01/28/2016

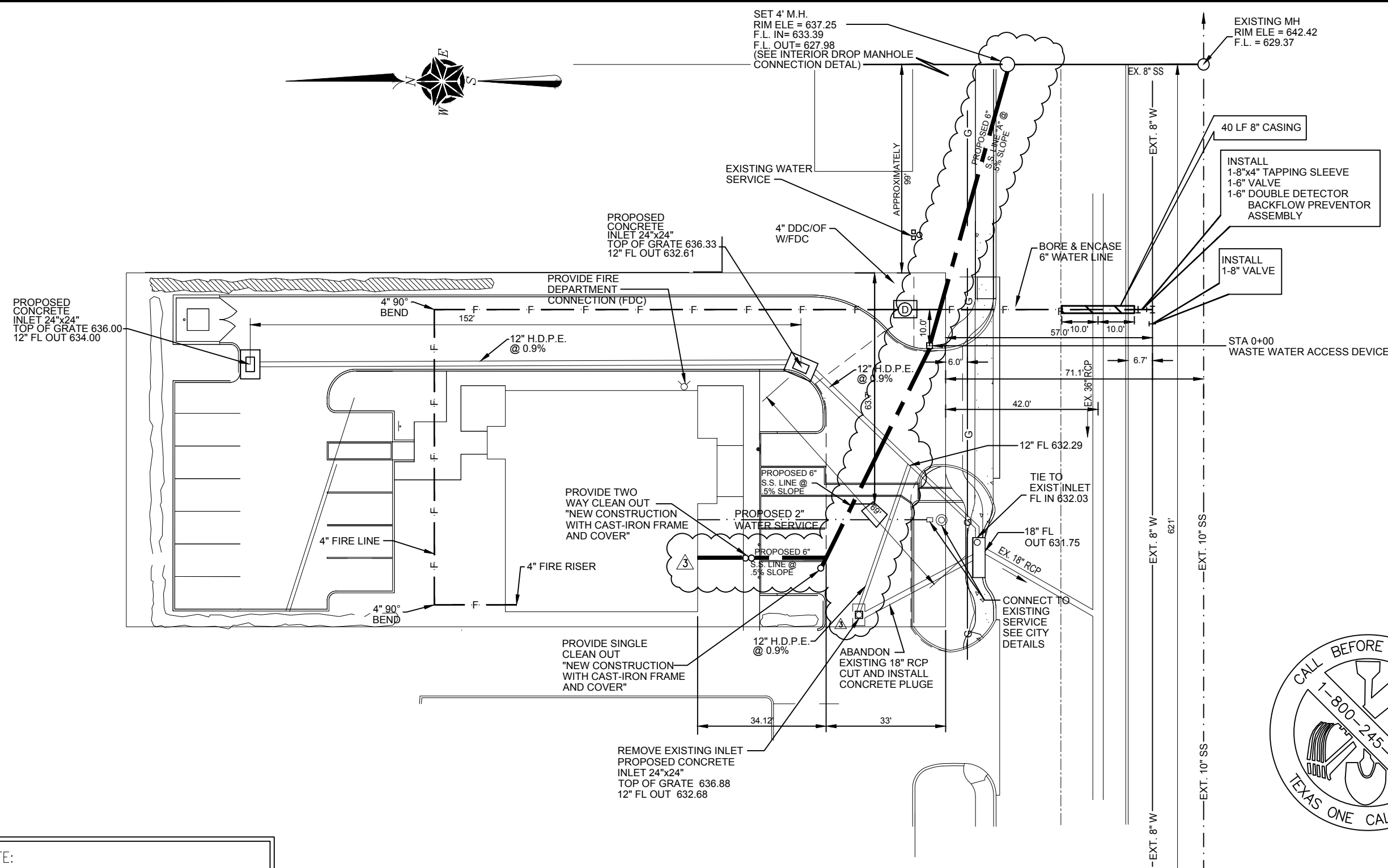


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UTILITY PLAN
 ENGINEERED FOR
BENTLEY CUSTOM HOMES



Eric L. Davis
 01/28/2016



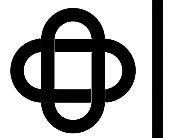
NOTE:
 ELEVATIONS AND LOCATIONS ARE FROM BEST SOURCES AVAILABLE. CONTRACTOR TO VERIFY ALL UTILITY LOCATIONS AND ELEVATIONS PRIOR TO WORK. IF DISCREPANCIES ARE NOTED, CONTACT ENGINEER IMMEDIATELY.

REVISIONS	CODE
1 5/12/14 RELOCATE SAN. SEWER LINE	T
2 1/29/15 CITY COMMENTS	T
3 1/21/16 RELOCATE SAN. SEWER LINE	T
4 1/28/16 CITY COMMENTS	

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SHEET G02



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UTILITY PLAN
 ENGINEERED FOR:
BENTLEY CUSTOM HOMES

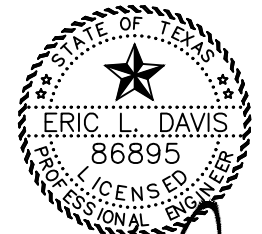
BUILDER: BENTLEY CUSTOM HOMES
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 LOT: BLOCK: A
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 PLAN: GRADING
 ELD JOB NO.:
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 FIRM REGISTRATION #: 3987

SCALE: NTS

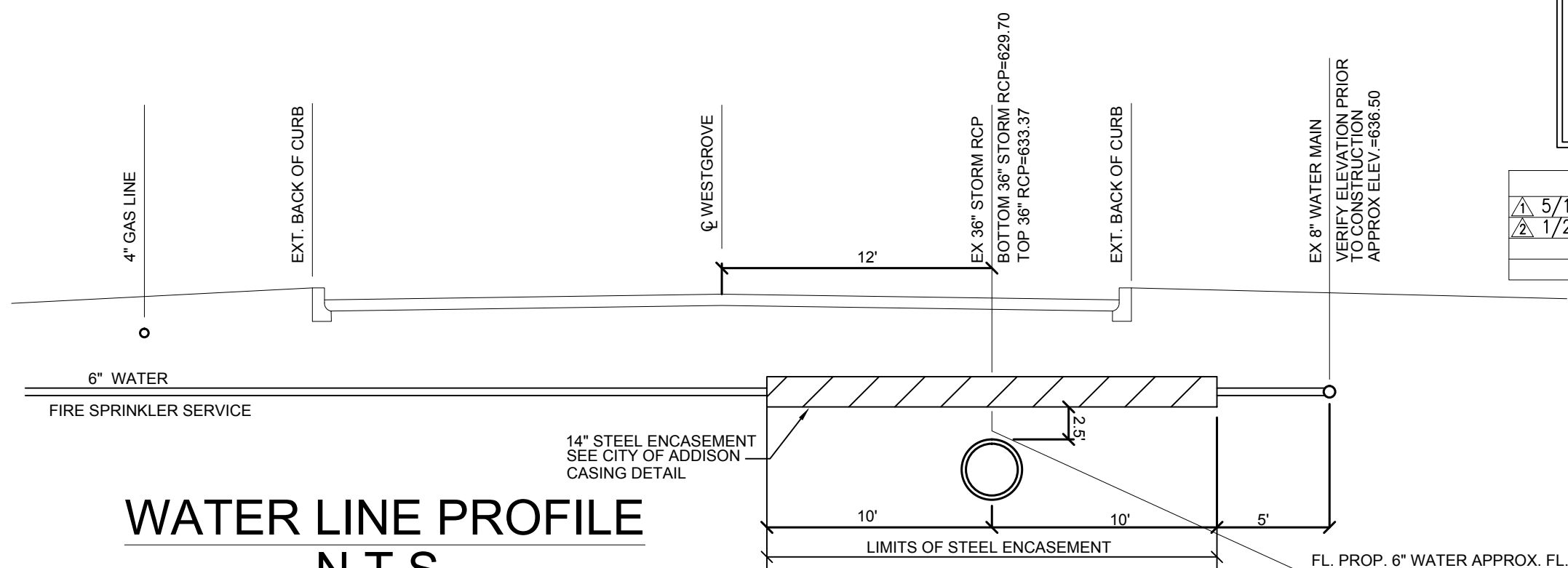
SHEET G02B

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 ELEVATIONS PRIOR TO WORK. IF
 DISCREPANCIES ARE NOTED, CONTACT
 ENGINEER IMMEDIATELY.

REVISIONS	CODE
1 5/12/14 RELOCATE SAN. SEWER LINE	T
2 1/28/16 RELOCATE SAN. SEWER LINE	T



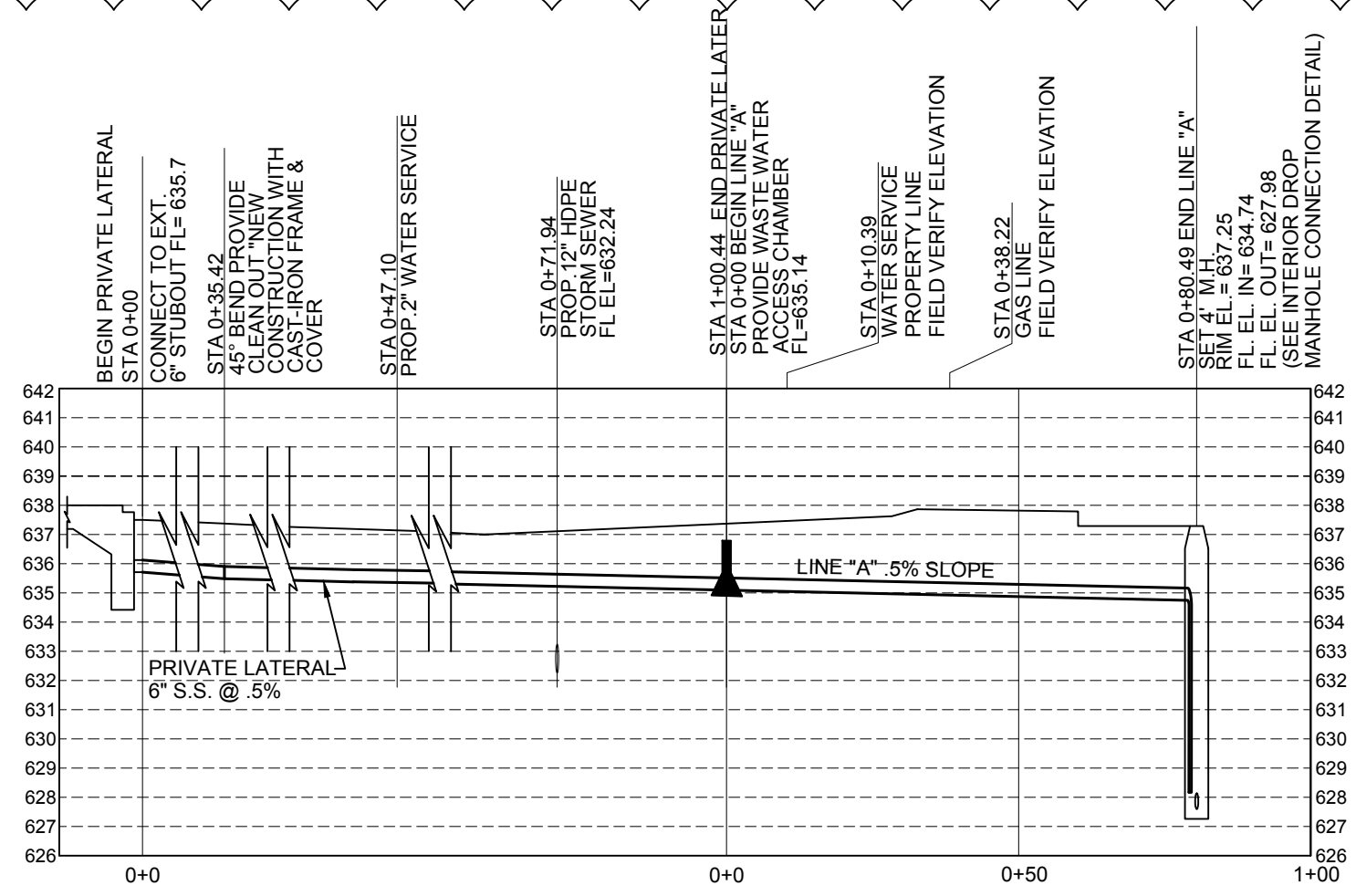
Eric L. Davis
 01/28/2016



WATER LINE PROFILE N.T.S.

FL. PROP. 6" WATER APPROX. FL. EL.=636.1
 BOTTOM OF STEEL ENCASUREMENT = 635.9
 TOP OF STEEL ENCASUREMENT = 637.1

2

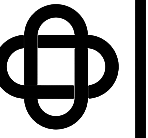


SANITARY SEWER PROFILE

HORIZONTAL 1:30
 VERTICAL 1:5

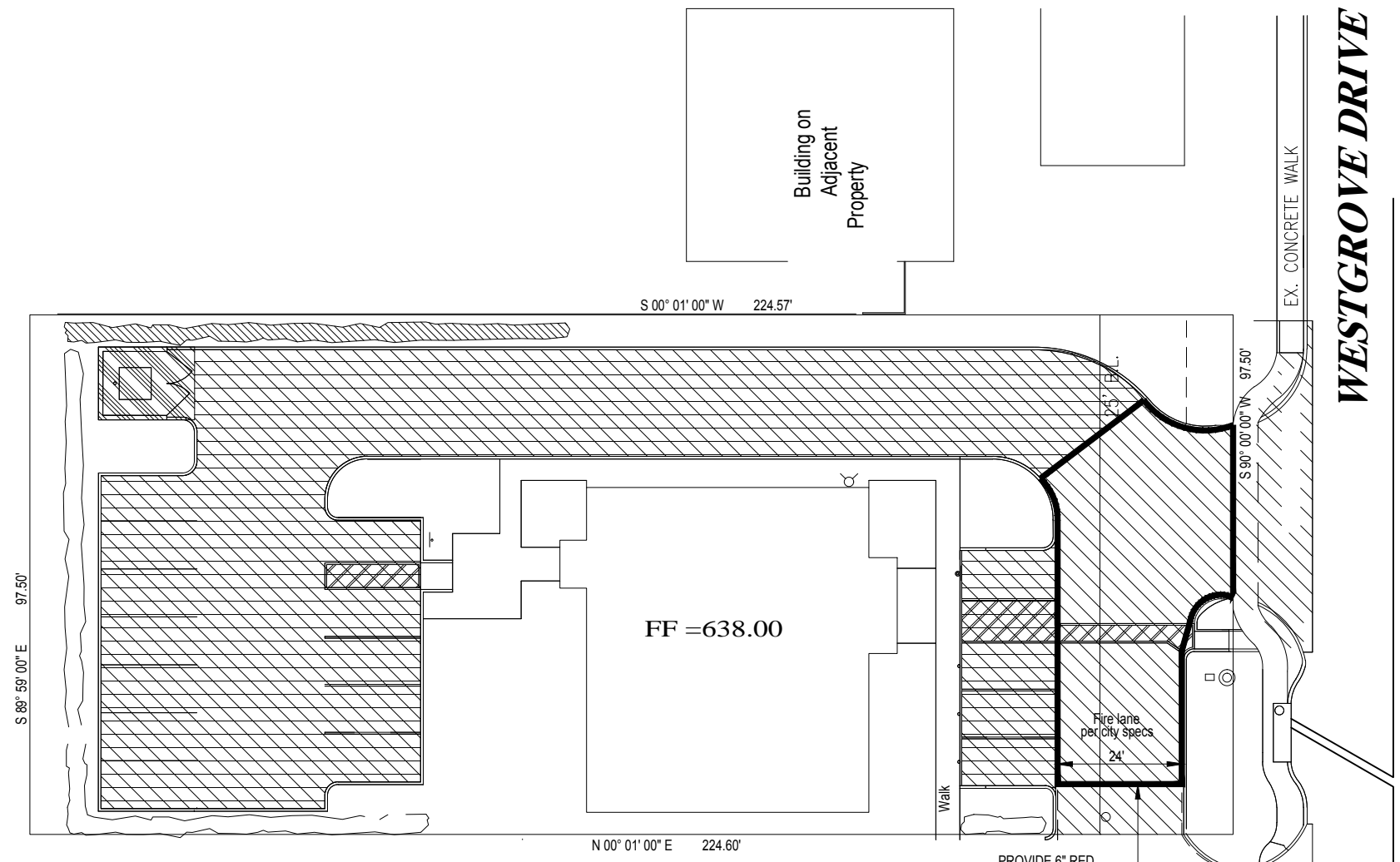
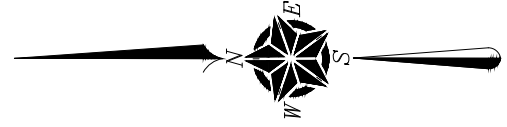


NOTE:
 EXISTING UTILITIES ARE FROM BEST
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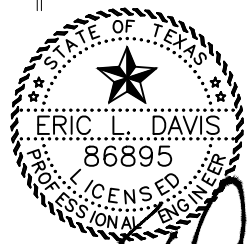
PAVING PLAN
 ENGINEERED FOR
BENTLEY CUSTOM HOMES



PAVING REQUIREMENTS				
SECTION	FIRE LANE	PARKING	SIDEWALK	DUMPSTER PAD
PAVING	8" - 3600 PSI 6 SACK CONC #3 BARS @ 10" OCEW	5" - 3000 PSI 6 SACK CONC #3 BARS @ 12" OCEW	4" - CONC #3 BARS @ 14" OCEW	8" - 3600 PSI 6 SACK CONC. #3 BARS @ 10" OCEW
SUBGRADE	SOIL COMPACTED TO 95% STANDARD PROCTOR	SOIL COMPACTED TO 95% STANDARD PROCTOR	2" SAND CUSHION; SOIL COMPACTED TO 95% STANDARD PROCTOR	SOIL COMPACTED TO 95% STANDARD PROCTOR

- FIRE LANE
- DRIVEWAY/PARKING
- DUMPSTER PAD

NOTE:
 FIRE LANES SHALL BE MARKED BY 6" WIDE, DURABLE RED STRIPE RUNNING THE ENTIRE LENGTH OF THE FIRE LANE ON BOTH SIDES. THE WORDS "FIRE LANE NO PARKING" SHALL BE APPLIED IN 4" WHITE LETTERING AT 30' INTERVALS. WHENEVER POSSIBLE ALL MARKINGS WILL BE PLACED ON CURBS.

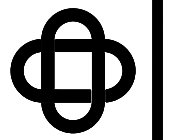


Eric L. Davis
 01/28/2016

PLAN: GRADING
 BUILDER: BENTLEY CUSTOM HOMES
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SHEET G03



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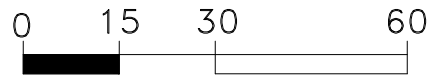
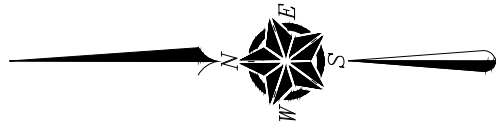
DRAINAGE AREA
BENTLEY CUSTOM HOMES

PLAN: GRADING
 ELD JOB NO.:
 DRAWN BY: BW
 FIRM REGISTRATION #: 3987

BUILDER: BENTLEY CUSTOM HOMES
 ADDITION:
 ADDRESS: 4397 WESTGROVE
 LOT: BLOCK: A
 CITY: ADDISON, TEXAS

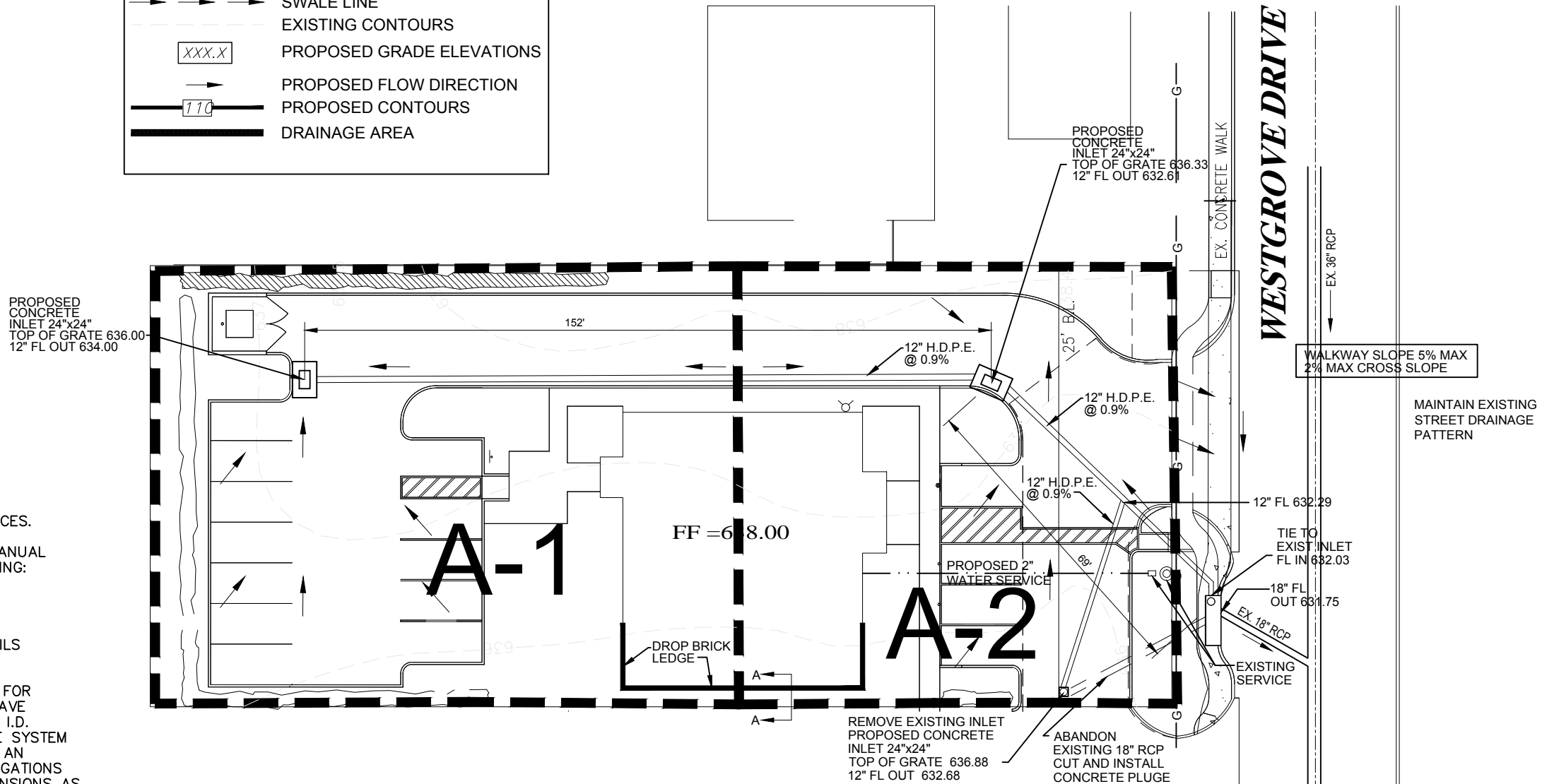
SCALE: 1" = 30'

SHEET G04



LEGEND

- SWALE LINE
- EXISTING CONTOURS
- PROPOSED GRADE ELEVATIONS
- PROPOSED FLOW DIRECTION
- PROPOSED CONTOURS
- DRAINAGE AREA



- UTILITY NOTES;**
- 1) MAINTAIN 10'-0" HORIZONTAL CLEARANCE BETWEEN WATER & SANITARY SEWER STRUCTURES AND SERVICES.
 - 2) REFER TO THE TOWN OF ADDISON PUBLIC WORKS MANUAL FOR SPECIFICATIONS AND DETAILS FOR THE FOLLOWING:
 - A) WATER, FIRE AND IRRIGATION TAP AND METERS
 - B) SANITARY SEWER TAP AND SERVICE.
 - C) PAVEMENT CUT, REMOVAL AND REPLACEMENT
 - D) DRIVE WAY CONNECTION AND CURB RETURN DETAILS
 - E) STORM SEWER CONNECTION DETAILS
 - 3) ENGINEER OF RECORD ASSUMES NO RESPONSIBILITY FOR UNDERSIZED FIRE SERVICES. INTERNAL DIMENSIONS HAVE BEEN TAKEN FROM PLANS BY OTHERS. FIRE SERVICE I.D. & METER I.D. ULTIMATELY DETERMINED BY FINAL FIRE SYSTEM DESIGN. ENGINEER OF RECORD HAS NOT PERFORMED AN ANALYSIS FOR THE SIZING OF DOMESTIC, FIRE & IRRIGATIONS TAPS, METERS & SERVICE LINES. ALL INTERNAL DIMENSIONS AS SPECIFIED BY OWNER AND OTHER CONTRACTORS.

- SITE NOTES:**
- 1) PRIVATE STORM DRAIN PIPE TO BE CONSTRUCTED OF H.D.P.E. ADS-N12 OR EQUAL
 - 2) ALL GRATE INLETS TO 12"x12" SQUARE LOW PROFILE WITH $\frac{5}{16}$ " OR GRATER GRATE OPENINGS WITH A MINIMUM SURFACE AREA OPENING OF 0.30 FT OR 43 IN
 - 3) 24"x24" GRATE INLET TO BE SQUARE LOW PROFILE WITH A MINIMUM OF $\frac{3}{8}$ " GRATE OPENING WITH A MINIMUM SURFACE AREA OPENING OF 1.2FT OR 172 IN.
 - 4) ALL ROOF DOWN SPOUTS SHALL CONNECT DIRECTLY TO PRIVATE STORM SEWER SYSTEM

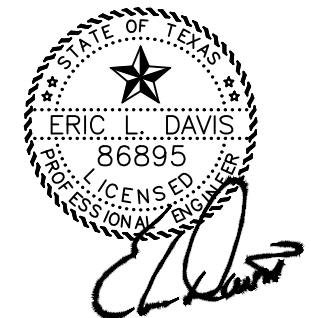
24" GRATE INLET
 $Q = (CO)A (2gd)^{.5}$
 Where:
 Q= INLET CAPACITY
 Co= ORFICE CAPACITY=0.69
 A= CLEAR OPENING AREA OF GRATE (Ft)
 g= 32.2 Ft/S
 d= DEPTH OF WATER ABOVE TOP OF GRATE (Ft)
 $Q_{in} = .67 (1.2)(2*32.2*25)^{.5} = 3.21cfs$
 $Q_{en} = .67 (1.2)(2*32.2*5)^{.5} = 4.6cfs$

EXISTING DRAINAGE CALCULATIONS

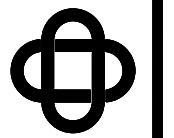
Drainage Area No.	Impervious Area	Pervious Area	Total Area (Acres)	"C" Impervious	"C" Pervious	"CW"	"TC" (min)	I100	Q ₁₀₀ (cfs)
A-1	0.24	0.04	0.28	1.0	0.25	0.89	10	9.27	2.31
A-2	0.24	0.02	0.26	1.0	0.25	0.94	10	9.27	2.27

PROPOSED DRAINAGE CALCULATIONS

Drainage Area No.	Impervious Area	Pervious Area	Total Area (Acres)	"C" Impervious	"C" Pervious	"CW"	"TC" (min)	I100	Q ₁₀₀ (cfs)
A-1	0.22	0.06	0.28	1.0	0.25	0.84	10	9.27	2.18
A-2	0.23	0.03	0.26	1.0	0.25	0.91	10	9.27	2.19



01/28/2016



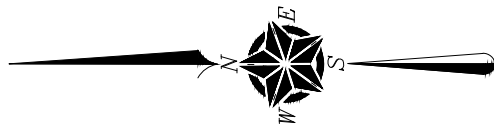
ERIC L. DAVIS ENGINEERING, INC.
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STORM WATER POLLUTION PREVENTION PLAN
BENTLEY CUSTOM HOMES

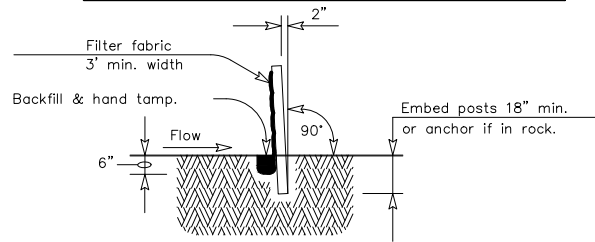
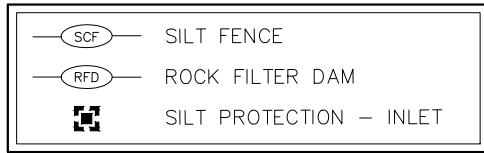
PLAN: GRADING
BUILDER: BENTLEY CUSTOM HOMES
ELD JOB NO.:
ADDRESS: 4397 WESTGROVE
LOT: BLOCK: A
CITY: ADDISON, TEXAS
DRAWN BY: BW
FIRM REGISTRATION #: 3987

SCALE: 1" = 30'

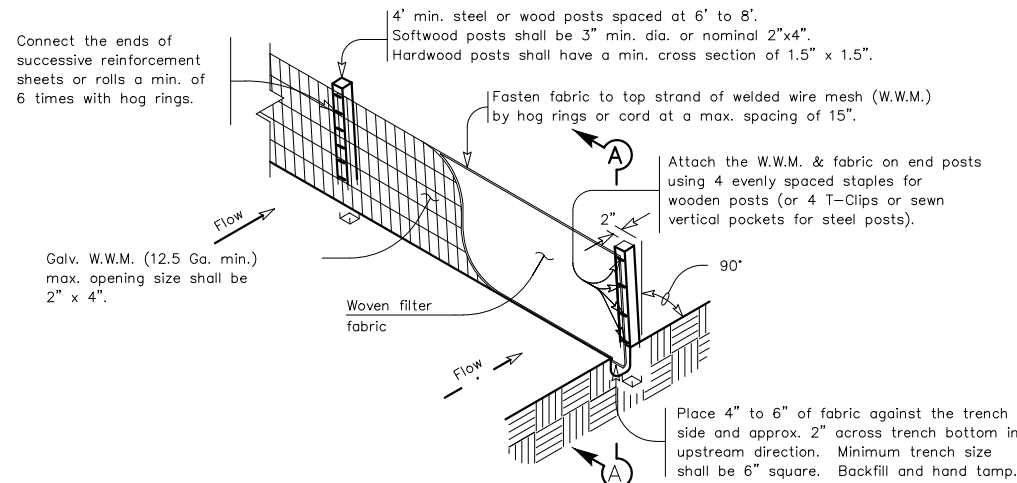
SHEET G05



PLAN SHEET LEGEND



SECTION A-A



TEMPORARY SEDIMENT CONTROL FENCE



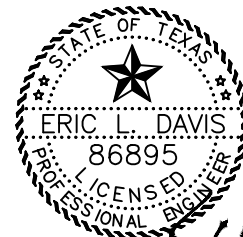
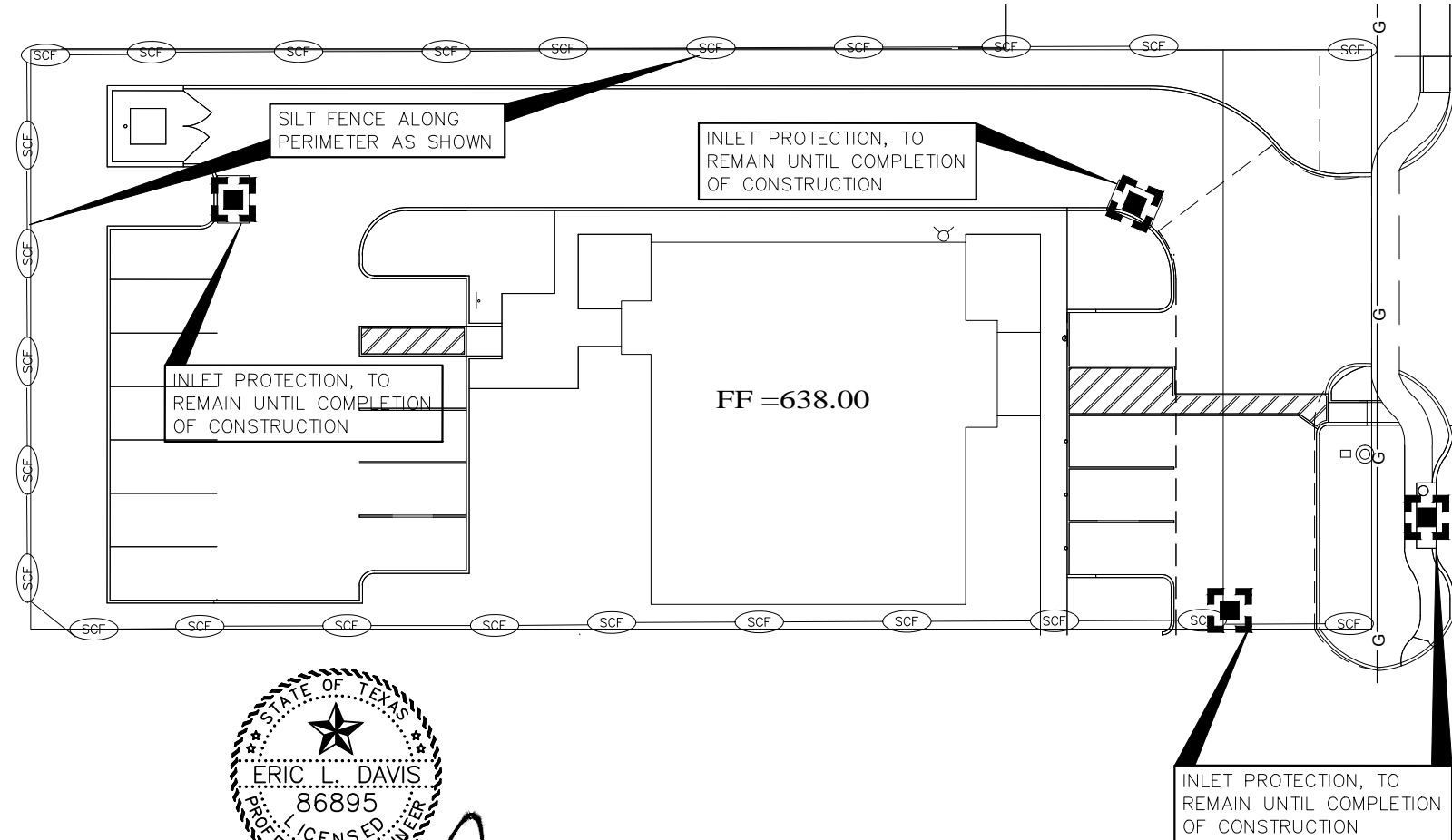
GENERAL NOTES

- The guidelines shown hereon are suggestions only and may be modified by the Engineer.

SEDIMENT CONTROL FENCE USAGE GUIDELINES

A sediment control fence may be constructed near the downstream perimeter of a disturbed area along a contour to intercept sediment from overland runoff. A 2 year storm frequency may be used to calculate the flow rate to be filtered.

Sediment control fence should be sized to filter a max. flow through rate of 100 GPM/FT. Sediment control fence is not recommended to control erosion from a drainage area larger than 2 acres.



Eric L. Davis
 01/28/2016

I GENERAL:

Each Contractor shall have a designated area in which to stage his construction for this project. This site will be designated by the Contractor and shall be approved by the Owner. It will be the Contractor's responsibility to construct berms around its site so as to minimize the possibility of run-off water being exposed to pollutants and to detritus in any water that might potentially be exposed to pollutants. It shall also be the responsibility of each Contractor to install dikes around any diesel, gasoline or oil tanks, as well as any other hazardous chemicals stored on site, in order to contain any possible contamination to the smallest area should a leak or spill occur. If a leak or spill should occur, the Contractor responsible for such an accident is required to report it to the Owner immediately.

This shall also be the designated area for equipment maintenance and repair. All maintenance will take place inside this area and any repairs to equipment should be performed here, if possible. Should the equipment not be able to be moved to this location, special care will be taken to prevent pollutants (oil, diesel, hydraulic fluid, etc.) from entering, either directly or indirectly, into the storm sewer, runoff of waterways.

Solid waste receptacles for trash and other debris such as soft drink cans or bottles, wrapping paper, metal or plastic banding strips and all other waste products shall be provided by the Contractor and shall be maintained during its tenure and removed at the end of his portion of the work. Contractor shall police the Project and maintain the cleanest site possible. An adequate number and properly maintained sanitary facilities (such as Chem-Cans) shall be supplied by Contractor for, and is to be used by, its employees.

A. Storage of Material on Site

Materials shall be stored in the designated staging area whenever feasible to do so. When materials are to be stored outside this area, it should be done in such a way as to prevent as much exposure as possible. Sand and rock should be stored in a location that will prevent the flow of run-off storm water from coming into contact with it. Any time diesel, gasoline, oil or other hazardous materials are stored outside the staging area (even if overnight), a retention dike is to be immediately constructed.

B. Entering and Exiting Project

It is the responsibility of Contractor to make sure that all vehicles either owned or employed by him are free of soils which could be tracked onto any adjacent roadways. In the event that soils are tracked onto a roadway, clean up of this soils to be performed immediately, and in such a manner as not to contaminate offsite storm drainage facilities.

1. Equipment Wash Down Areas:

Equipment Wash Down areas are not a mandatory part of the Plan unless a Contractor wishes to wash his equipment or other vehicles on site. If a Contractor wishes to do so, he may elect to use the stabilized construction entrance as a Wash Down area, or he may build a similar structure inside the Project which will retain all water produced by the washing of equipment. It will not be acceptable to wash any equipment on adjacent roadways or property.

C. Paving:

Paving should be done in a manner as to ensure that the sediment controls are in place above and below where the construction is taking place and not to leave a waterway open so that the sediment laden water is allowed to leave the site without being filtered.

D. Dust Control:

During construction, water trucks will be used by each contractor to reduce dust as needed. After construction, the site will be re-vegetated to reduce dust.

II MAINTENANCE AND REPORTING PROCEDURES

A. Maintenance and Repair:

Inspections are to be performed by a qualified person provided by discharger, and are to be performed a minimum of once every fourteen (14) calendar days and within twenty-four hours of any storm event of greater than 0.5 inches. Please note that an inspection required after a "storm event" does not replace the required bi-monthly inspection and is to be in addition to monthly scheduled inspections. Written reports shall be given to owner and appropriate inspector.

When damage repair to the erosion or sediment controls are needed, the work shall be performed within five (5) days of written notice from the Owner. If requested repairs are not performed within such five day period, the Owner shall employ a contractor to make the required repairs and will deduct such amount from the Contractor that damaged the controls.

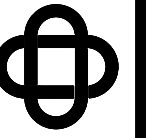
Maintenance of controls shall be performed within five (5) days of written notice from the Owner to the Contractor responsible for the maintenance of such control.

III RE-VEGETATION

A. At the completion of the paving and final grading the entire disturbed area shall be re-vegetated with seeds and rates approved by the Owner.

B. All silt fence shall remain in place until re-vegetation has been completed.

C. Prior to final paving, each Contractor shall be responsible for removing any mud or silt that collects on the existing pavement as a result of their operations. After the installation of the new paving and prior to acceptance by the appropriate authority, the paving Contractor shall be responsible for removing any mud or silt that collects on the existing and new pavement. After acceptance by the appropriate authority, the Owner shall be responsible for removal of the mud or silt that collects on the pavement.



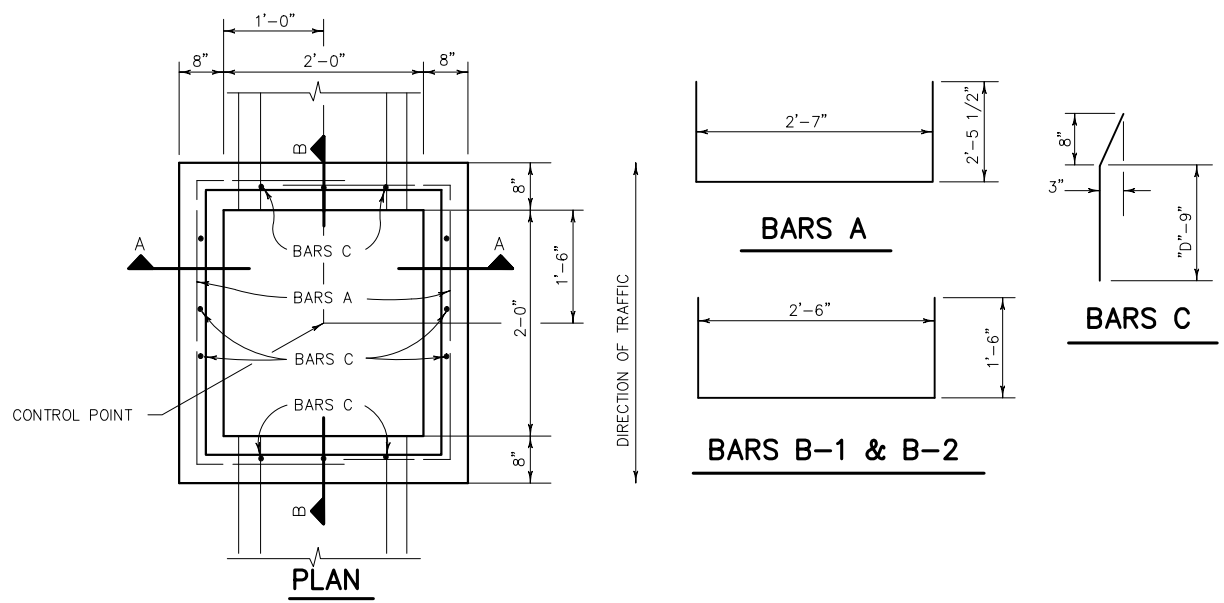
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BENTLEY CUSTOM HOMES
 DETAILS
 CATCH BASIN

BUILDER: BENTLEY CUSTOM HOMES
ADDITION:
ADDRESS: 4397 WESTGROVE
LOT: BLOCK: A
CITY: ADDISON, TEXAS
PLAN: GRADING
ELD JOB NO.:
DRAWN BY: BW
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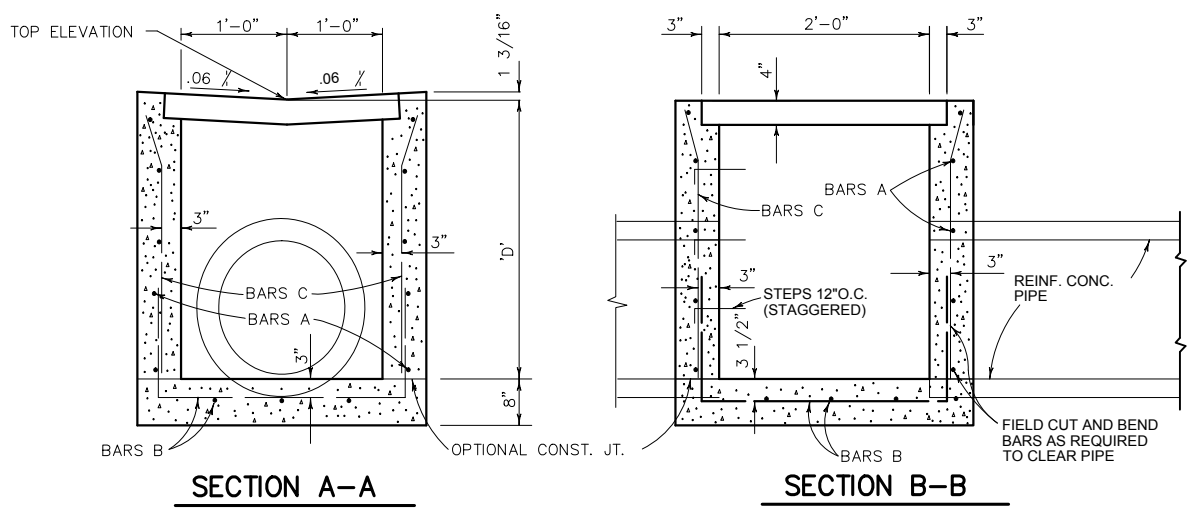
SCALE: N.T.S.

SHEET G06

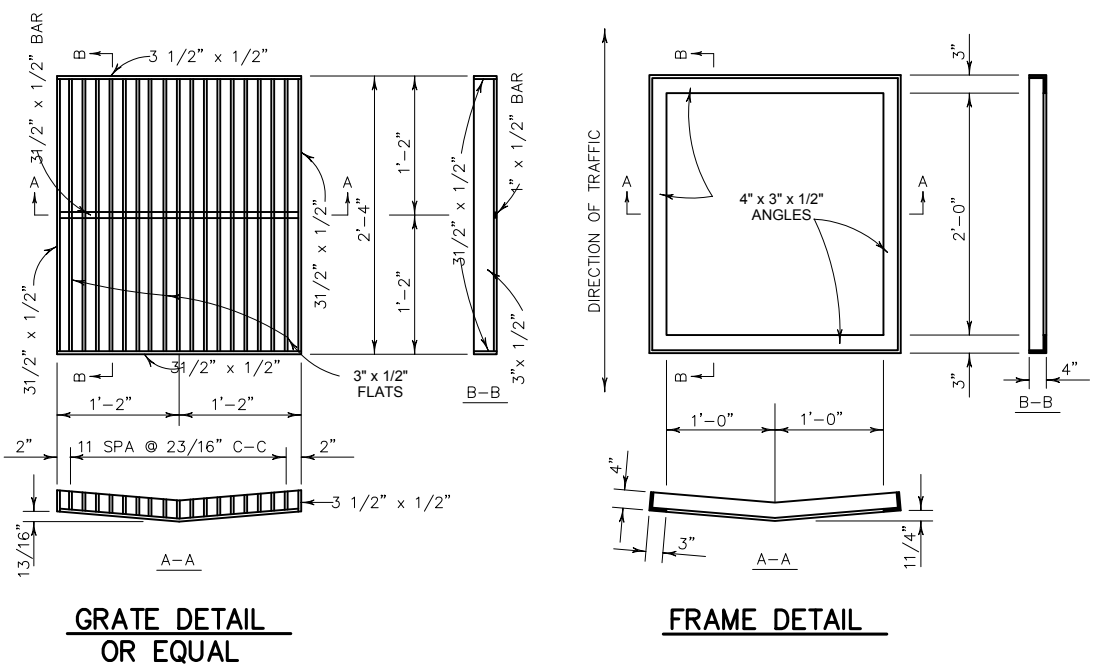


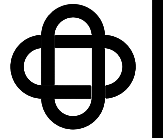
- GENERAL NOTES:**
1. ALL CONCRETE SHALL BE 3000 PSI MN.
 2. ALL REINFORCING STEEL SHALL BE NO. 4 BARS. DIMENSIONS RELATING TO STEEL ARE TO CENTERS OF BARS. FIELD CUT AND BEND BARS AS REQUIRED TO ACCOMMODATE TIES TO LOWER STRUCTURE AND TO CLEAR PIPES.
 3. GRATE AND FRAME SHALL CONFORM TO ITEM 471.
 4. INLET TOP MAT BE CAST MONOLITHICALLY. IF NOT PLACED MONOLITHICALLY, A SEALED 3/8" EXPANSION JOINT SHALL BE PLACED AROUND THE TOP OF THE INLET STRUCTURE, JOINT SEALANT SHALL BE USED TO SEAL JOINT.

NOTE: ALL BARS @ 12" O.C.E.W. (MAX)



TYPE 3 STRUCTURE



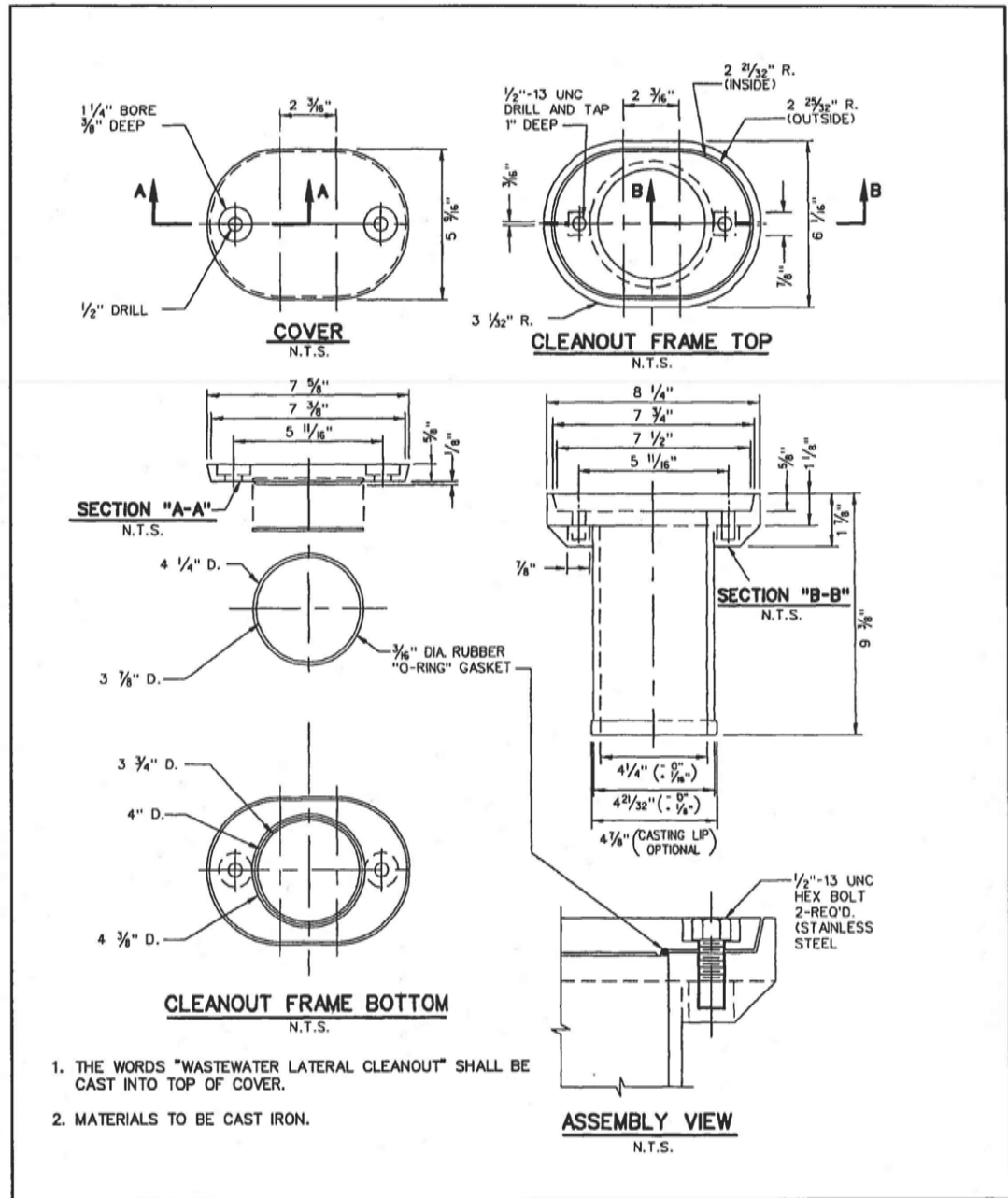


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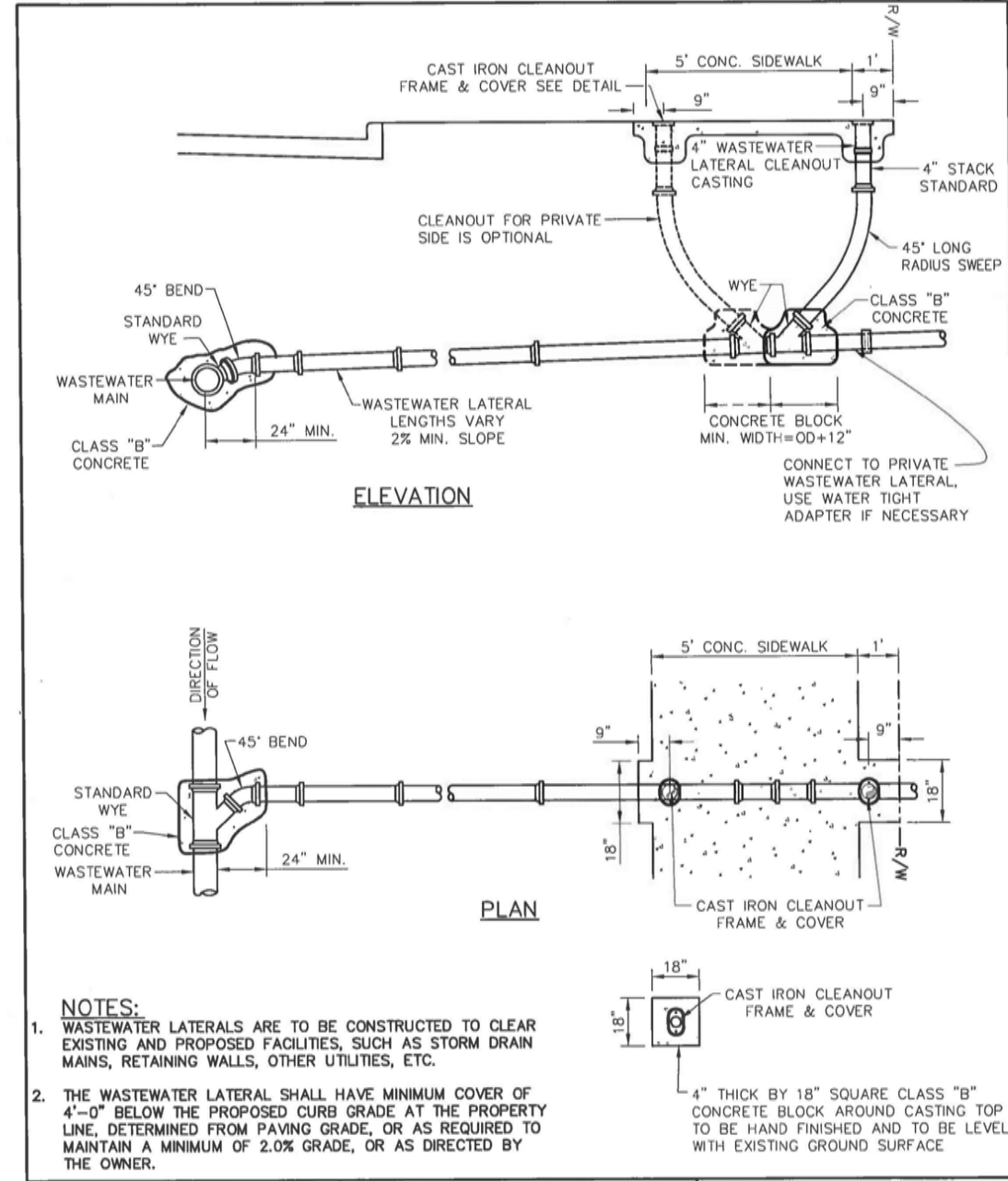
DETAILS
 CATCH BASIN
BENTLEY CUSTOM HOMES

PLANNING: GRADING
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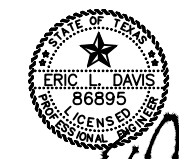
SCALE: N.T.S.
 SHEET G07



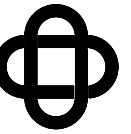
PUBLIC WORKS DEPARTMENT	CAST IRON CLEANOUT FRAME & COVER	STANDARD CONSTRUCTION DETAILS WASTEWATER		
		DATE: AUGUST, 2010	REV DATE: -	SHEET : SD-WW03



PUBLIC WORKS DEPARTMENT	TWO-WAY CLEANOUT NEW CONSTRUCTION	STANDARD CONSTRUCTION DETAILS WASTEWATER		
		DATE: AUGUST, 2010	REV DATE: -	SHEET : SD-WW02



01/28/2016

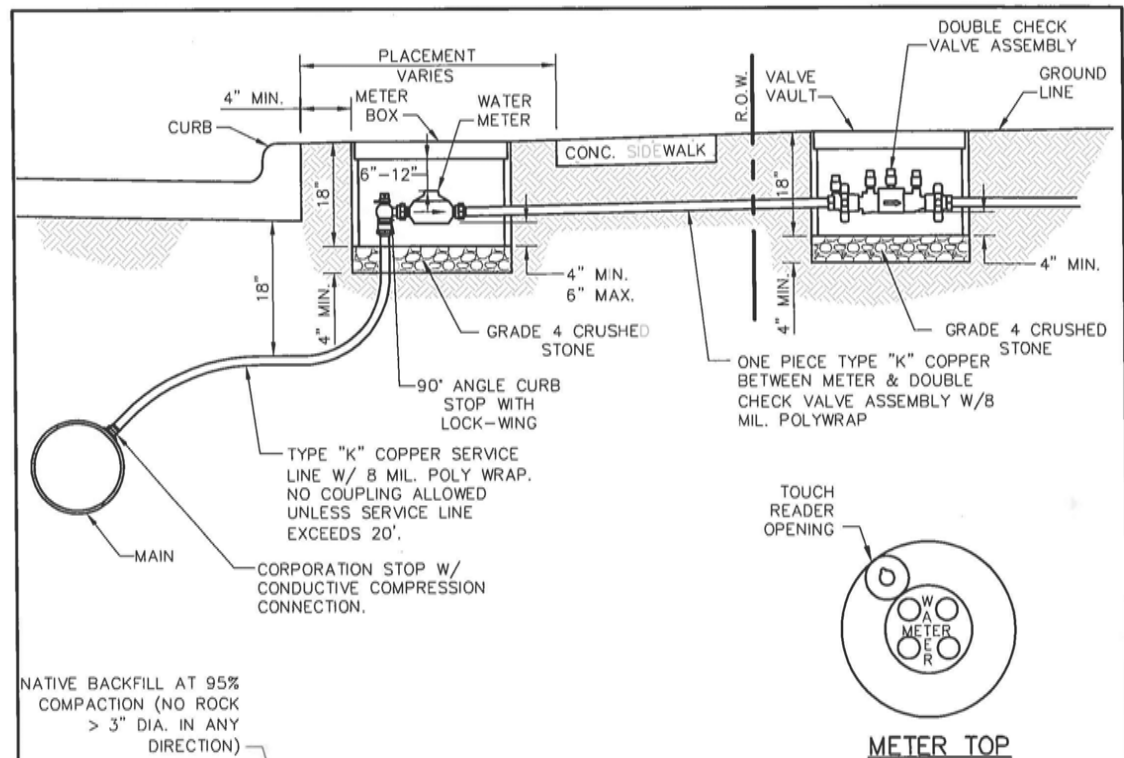


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 972/564-0592 Fax 972/564-6523
 E-Mail ericdavis@eidengineering.com

DETAILS
 CATCH BASIN
BENTLEY CUSTOM HOMES

PLAN: GRADING
 ELD JOB NO.:
 DRAWN BY: BW
 FIRM REGISTRATION #: 3987
 BUILDER: BENTLEY CUSTOM HOMES
 ADDRESS: 4397 WESTGROVE
 LOT: BLOCK: A
 CITY: ADDISON, TEXAS

SCALE: N.T.S.
 SHEET G08



NATIVE BACKFILL AT 95% COMPACTION (NO ROCK > 3" DIA. IN ANY DIRECTION)

- GENERAL INSTALLATION NOTES:**
1. WATER METER SHALL BE PLACED IN CENTER OF LOT WITH THE SANITARY SEWER SERVICE TO BE LOCATED 10' DOWNSTREAM.
 2. METER AND BOX SHALL BE SET BY THE CONTRACTOR TO ADDISON STANDARDS IN ALL CASES.
 3. THE METER BOX SHALL BE SET WITHIN THE R.O.W. OR A DEDICATED UTILITY EASEMENT. IN ALL CASES, THE METER BOX SHALL BE PROTECTED FROM VEHICULAR TRAFFIC.
 4. WATER SERVICES SHALL NOT BE CONNECTED TO DEAD END LINES OR FIRE HYDRANT LEADS.
 5. ALL MATERIALS SHALL CONFORM TO THE TOWN OF ADDISON WATER SYSTEM REQUIREMENTS.

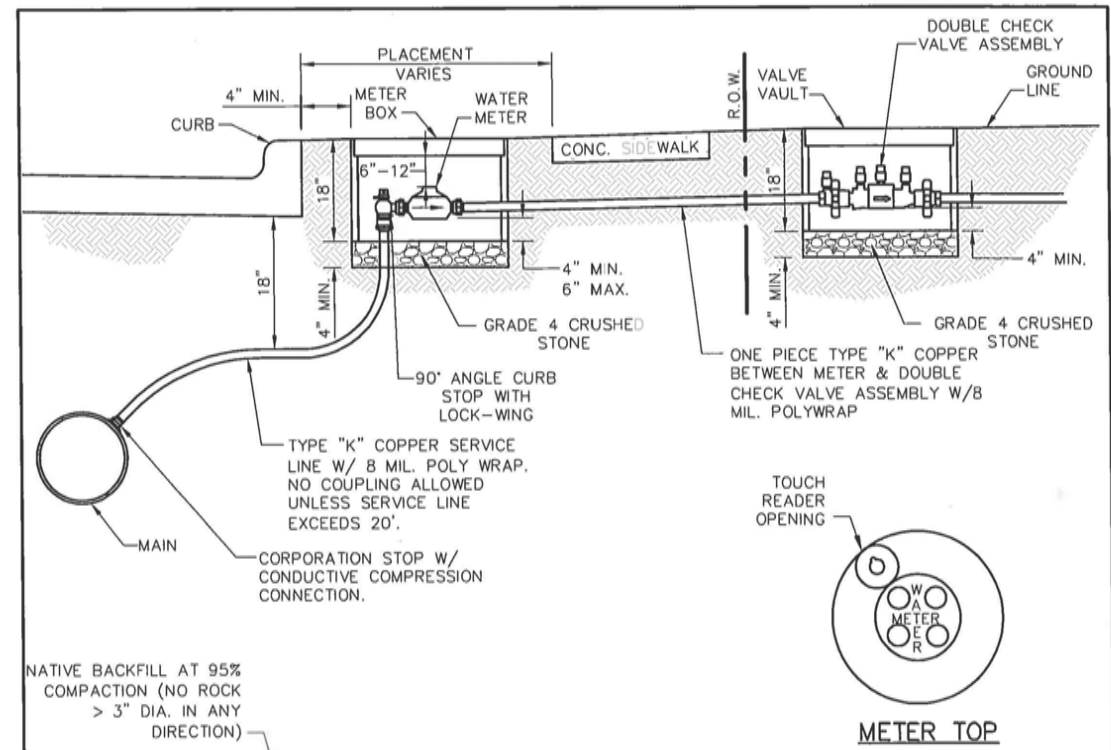
Type "K" Copper Service Pipe.	Double Strap Bronze Service Saddle with C.C. Threads	East Jordan Iron Works Galvanized Meter Box With Iron Ring & Cover				
		Min. Dia.	Covers & Rigns	Min. Ht.	Min. Can Weight	Min. Total Weight
3/4 Inch	3/4 Inch	18 Inches	12 1/2 Inches	18 Inches	13 lbs.	42 lbs.
1 Inch	1 Inch	24 Inches	20 1/2 Inches	18 Inches	17 lbs.	98 lbs.
1 1/2 Inch	1 1/2 Inch	28 Inches	20 1/2 Inches	18 Inches	20 lbs.	127 lbs.
2 Inch	2 Inch	28 Inches	20 1/2 Inches	18 Inches	20 lbs.	127 lbs.

Addison! PUBLIC WORKS DEPARTMENT

SERVICE CONNECTION WITH METER BOX

STANDARD CONSTRUCTION DETAILS WATER

DATE: AUGUST, 2010 REV DATE: - SHEET: SD-W20



NATIVE BACKFILL AT 95% COMPACTION (NO ROCK > 3" DIA. IN ANY DIRECTION)

- GENERAL INSTALLATION NOTES:**
1. WATER METER SHALL BE PLACED IN CENTER OF LOT WITH THE SANITARY SEWER SERVICE TO BE LOCATED 10' DOWNSTREAM.
 2. METER AND BOX SHALL BE SET BY THE CONTRACTOR TO ADDISON STANDARDS IN ALL CASES.
 3. THE METER BOX SHALL BE SET WITHIN THE R.O.W. OR A DEDICATED UTILITY EASEMENT. IN ALL CASES, THE METER BOX SHALL BE PROTECTED FROM VEHICULAR TRAFFIC.
 4. WATER SERVICES SHALL NOT BE CONNECTED TO DEAD END LINES OR FIRE HYDRANT LEADS.
 5. ALL MATERIALS SHALL CONFORM TO THE TOWN OF ADDISON WATER SYSTEM REQUIREMENTS.

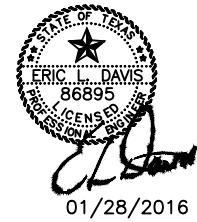
Type "K" Copper Service Pipe.	Double Strap Bronze Service Saddle with C.C. Threads	East Jordan Iron Works Galvanized Meter Box With Iron Ring & Cover				
		Min. Dia.	Covers & Rigns	Min. Ht.	Min. Can Weight	Min. Total Weight
3/4 Inch	3/4 Inch	18 Inches	12 1/2 Inches	18 Inches	13 lbs.	42 lbs.
1 Inch	1 Inch	24 Inches	20 1/2 Inches	18 Inches	17 lbs.	98 lbs.
1 1/2 Inch	1 1/2 Inch	28 Inches	20 1/2 Inches	18 Inches	20 lbs.	127 lbs.
2 Inch	2 Inch	28 Inches	20 1/2 Inches	18 Inches	20 lbs.	127 lbs.

Addison! PUBLIC WORKS DEPARTMENT

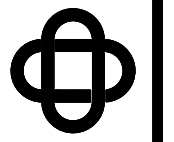
SERVICE CONNECTION WITH METER BOX

STANDARD CONSTRUCTION DETAILS WATER

DATE: AUGUST, 2010 REV DATE: - SHEET: SD-W20

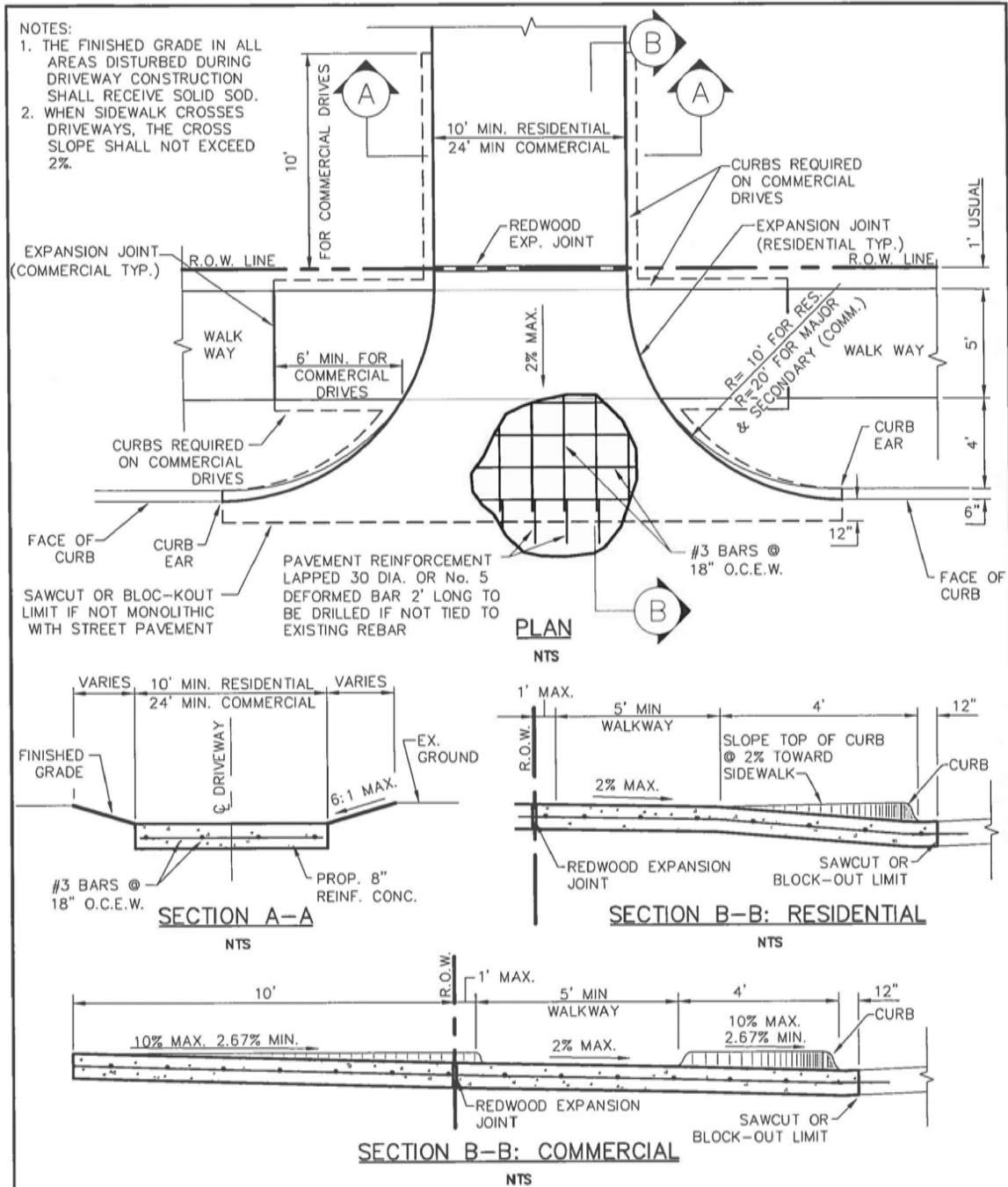


01/28/2016

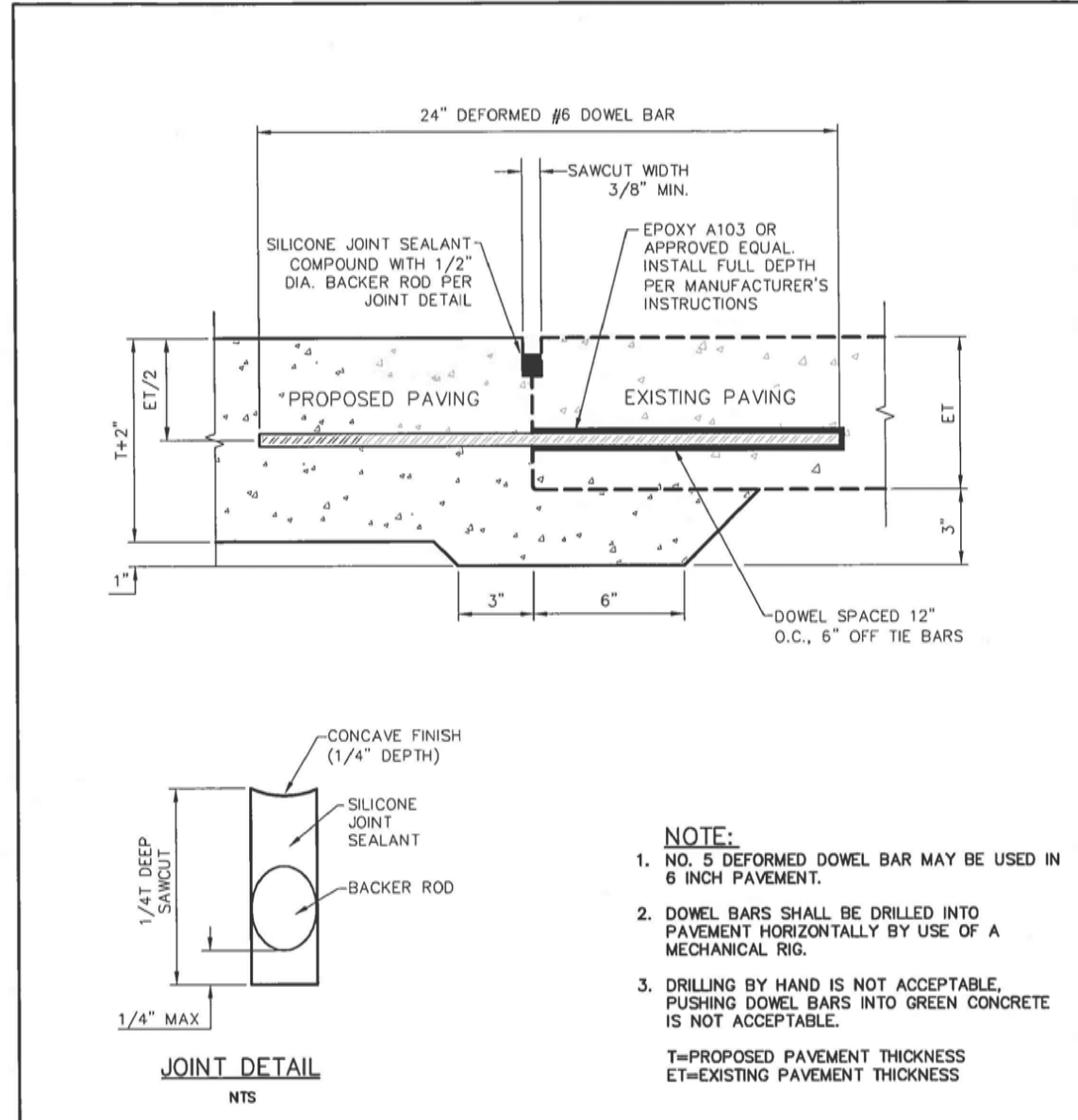


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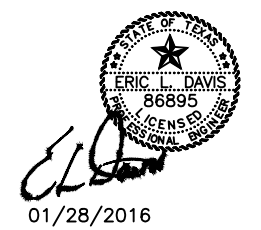
DETAILS
 CATCH BASIN
BENTLEY CUSTOM HOMES



 PUBLIC WORKS DEPARTMENT	DRIVEWAY RETURN DETAILS	STANDARD CONSTRUCTION DETAILS PAVING		
		DATE: AUGUST, 2010	REV DATE: -	SHEET: SD-P35



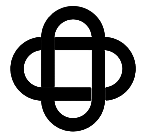
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		DATE: AUGUST, 2010	REV DATE: -	SHEET: SD-P16



01/28/2016

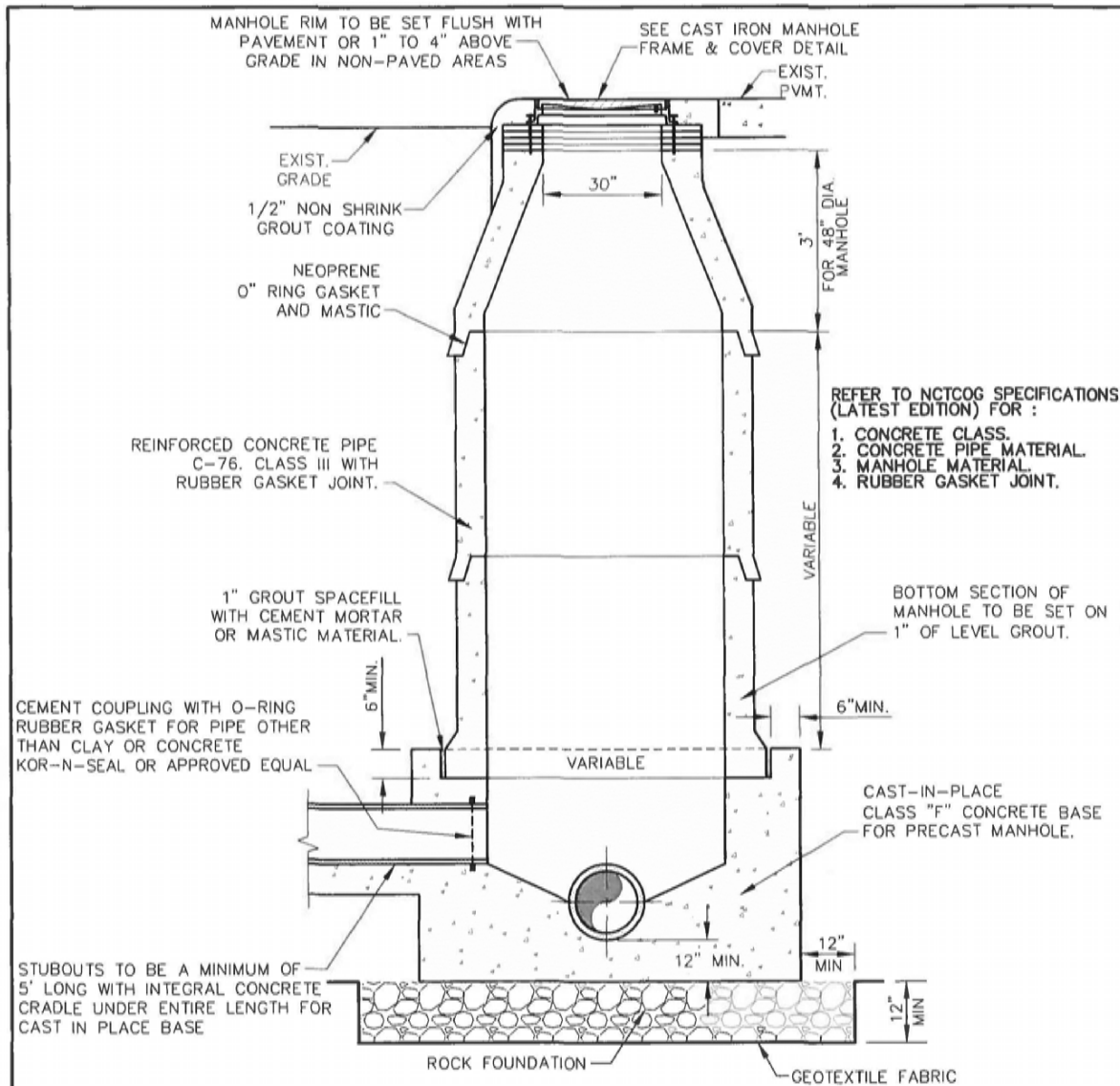
SCALE: N.T.S.

SHEET G09



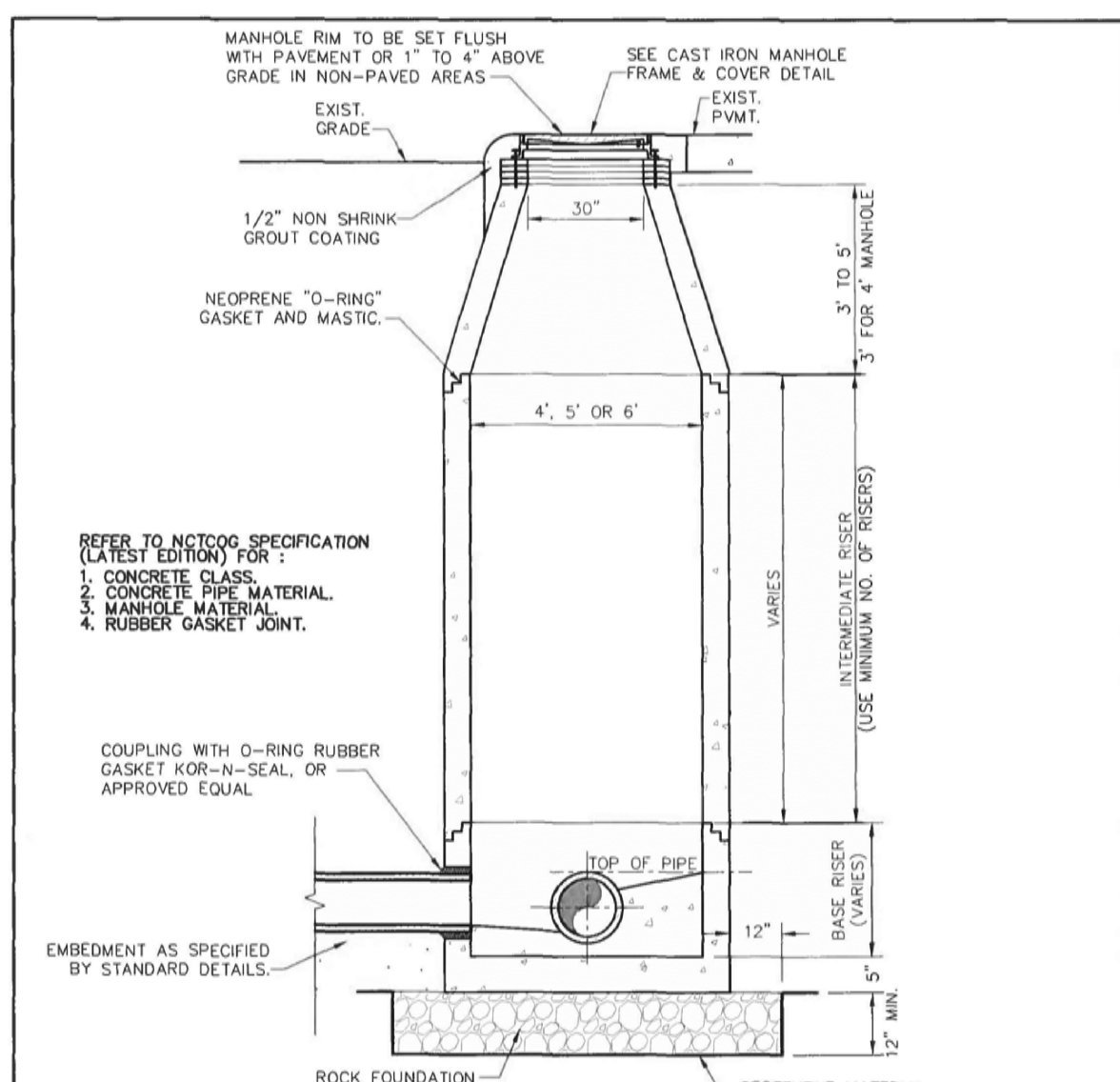
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DETAILS
 CATCH BASIN
BENTLEY CUSTOM HOMES



- NOTES:**
1. IF FALSE MANHOLE BOTTOMS ARE REQUIRED, THEY SHALL BE CONSTRUCTED, INSTALLED, AND REMOVED PER WASTEWATER MANHOLE FALSE BOTTOM STD. DETAIL.
 2. WHERE MANHOLE'S ARE OUTSIDE OF PAVEMENT, FRAME & COVER SHALL BE CENTERED IN 5'x5' CONCRETE PAD CLASS 'A' CONCRETE, 4" THICK
 3. ALL MANHOLES SHALL PASS VACUUM TEST AS PER NCTCOG SPECIFICATIONS

<i>Addison!</i> PUBLIC WORKS DEPARTMENT	PRECAST CONCRETE PIPE MANHOLE ALTERNATE "A"		STANDARD CONSTRUCTION DETAILS WASTEWATER		
	DATE: AUGUST, 2010	REV DATE: -	SHEET: SD-WW06		



- NOTES:**
1. IF FALSE MANHOLE BOTTOMS ARE REQUIRED, THEY SHALL BE CONSTRUCTED, INSTALLED, AND REMOVED PER WASTEWATER MANHOLE FALSE BOTTOM STD. DETAIL.
 2. WHERE MANHOLE'S ARE OUTSIDE OF PAVEMENT, FRAME & COVER SHALL BE CENTERED IN 5'x5' CONCRETE PAD CLASS 'A' CONCRETE, 4" THICK
 3. ALL MANHOLES SHALL PASS VACUUM TEST AS PER NCTCOG SPECIFICATIONS

<i>Addison!</i> PUBLIC WORKS DEPARTMENT	PRECAST CONCRETE PIPE MANHOLE ALTERNATE "B"		STANDARD CONSTRUCTION DETAILS WASTEWATER		
	DATE: AUGUST, 2010	REV DATE: -	SHEET: SD-WW07		

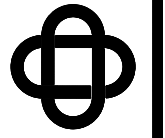


Eric L. Davis
 01/28/2016

BUILDER: BENTLEY CUSTOM HOMES	PLAN: GRADING
ADDRESS: 4397 WESTGROVE	ELD JOB NO.:
LOT: BLOCK: A	DRAWN BY: BW
CITY: ADDISON, TEXAS	FIRM REGISTRATION #: 3987

SCALE: N.T.S.

SHEET G10



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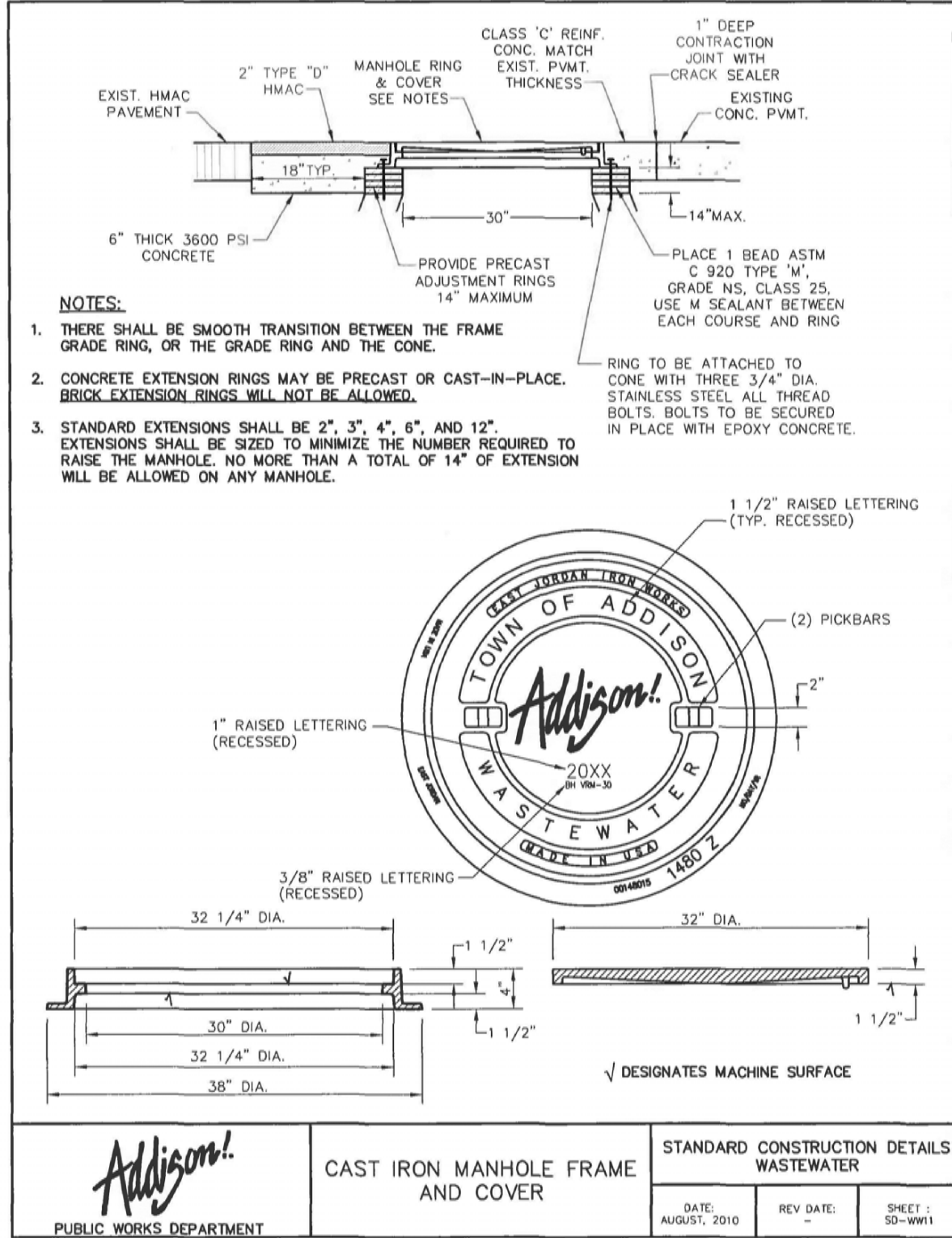
DETAILS
 CATCH BASIN
BENTLEY CUSTOM HOMES

PLAN: GRADING
 ELD JOB NO.:
 DRAWN BY: BW
 FIRM REGISTRATION #: 3987

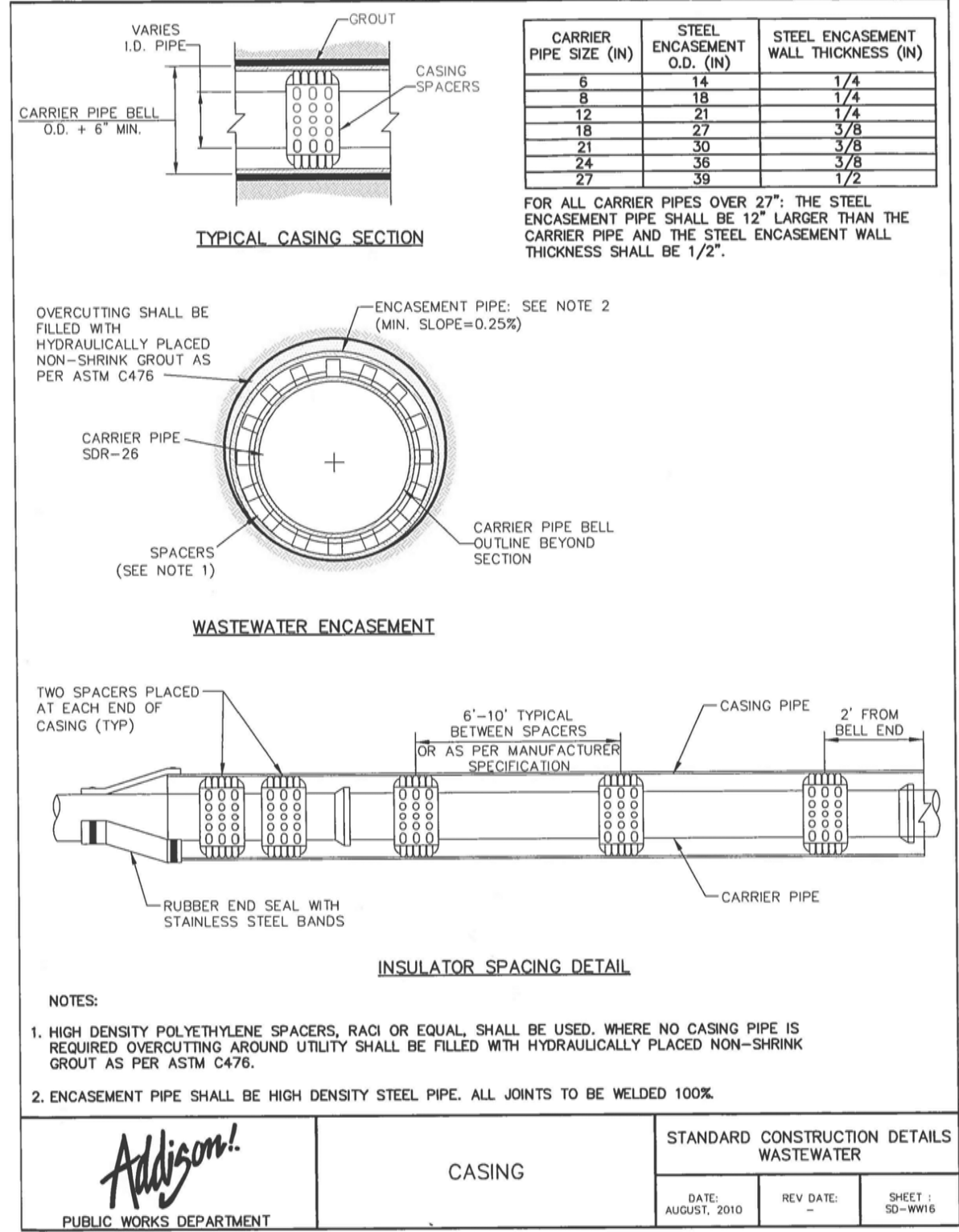
BUILDER: BENTLEY CUSTOM HOMES
 ADDITION:
 ADDRESS: 4397 WESTGROVE
 LOT: BLOCK: A
 CITY: ADDISON, TEXAS

SCALE: N.T.S.

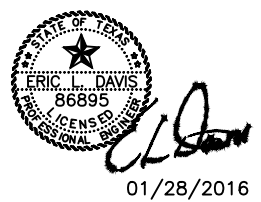
SHEET G11



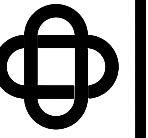
- NOTES:**
1. THERE SHALL BE SMOOTH TRANSITION BETWEEN THE FRAME GRADE RING, OR THE GRADE RING AND THE CONE.
 2. CONCRETE EXTENSION RINGS MAY BE PRECAST OR CAST-IN-PLACE. BRICK EXTENSION RINGS WILL NOT BE ALLOWED.
 3. STANDARD EXTENSIONS SHALL BE 2", 3", 4", 6", AND 12". EXTENSIONS SHALL BE SIZED TO MINIMIZE THE NUMBER REQUIRED TO RAISE THE MANHOLE. NO MORE THAN A TOTAL OF 14" OF EXTENSION WILL BE ALLOWED ON ANY MANHOLE.



PUBLIC WORKS DEPARTMENT	CASING	STANDARD CONSTRUCTION DETAILS WASTEWATER		
		DATE: AUGUST, 2010	REV DATE: -	SHEET: SD-WW16



01/28/2016

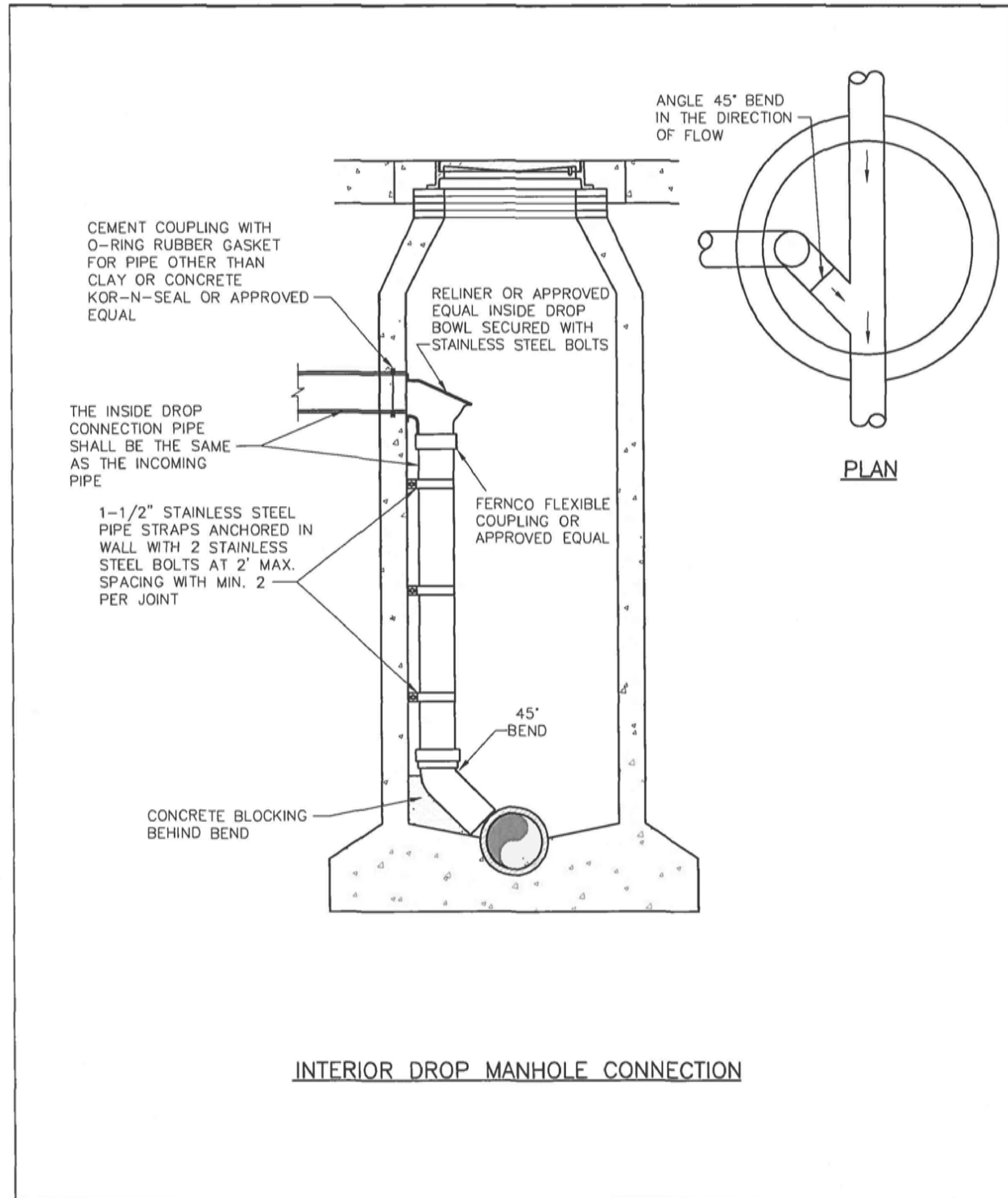


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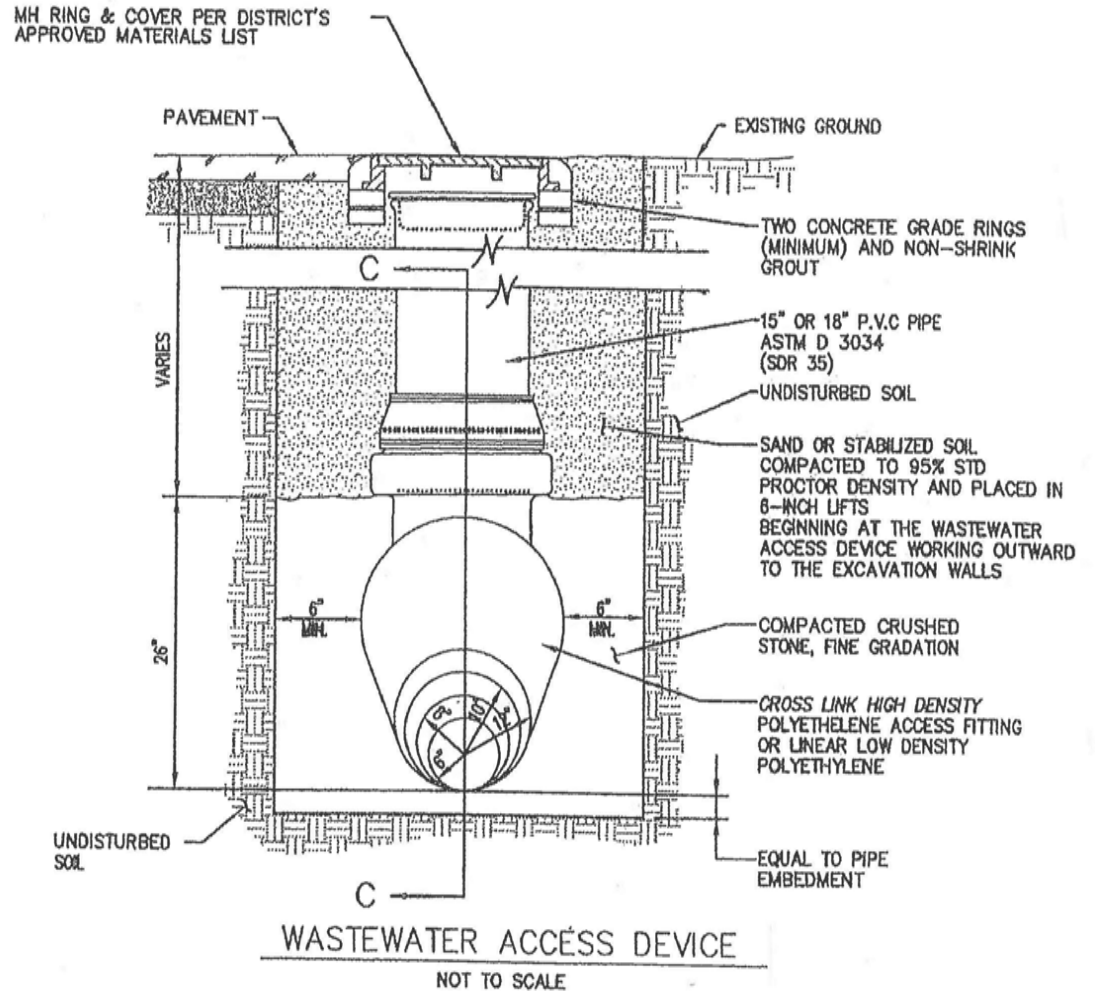
DETAILS
 CATCH BASIN
BENTLEY CUSTOM HOMES

PLAN: GRADING
 ELD JOB NO.:
 DRAWN BY: BW
 FIRM REGISTRATION #: 3987
 BUILDER: BENTLEY CUSTOM HOMES
 ADDITION:
 ADDRESS: 4397 WESTGROVE
 LOT: BLOCK: A
 CITY: ADDISON, TEXAS

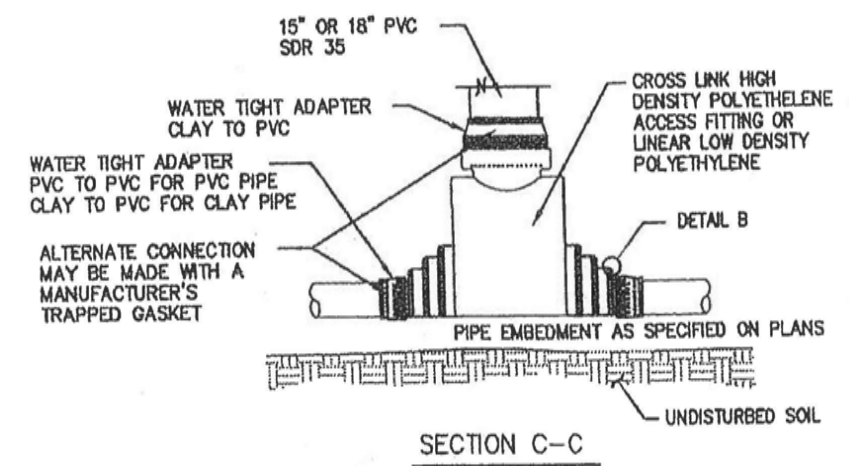
SCALE: N.T.S.
 SHEET G11B



INTERIOR DROP MANHOLE CONNECTION

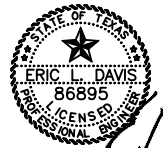


WASTEWATER ACCESS DEVICE
 NOT TO SCALE

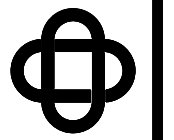


SECTION C-C

 PUBLIC WORKS DEPARTMENT	INTERIOR DROP MANHOLE CONNECTION	STANDARD CONSTRUCTION DETAILS WASTEWATER		
		DATE: AUGUST, 2010	REV DATE: -	SHEET: SD-WW09



01/28/2016



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DETAILS
 CATCH BASIN
 BENTLEY CUSTOM HOMES

BUILDER: BENTLEY CUSTOM HOMES	PLAN: GRADING
ADDRESS: 4397 WESTGROVE	ELD JOB NO.:
LOT: BLOCK: A	DRAWN BY: BW
CITY: ADDISON, TEXAS	FIRM REGISTRATION #: 3987

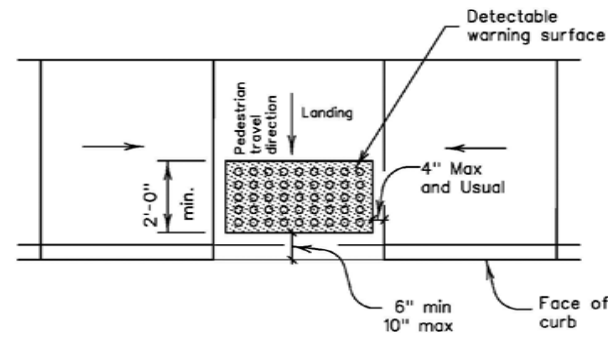
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SHEET G12

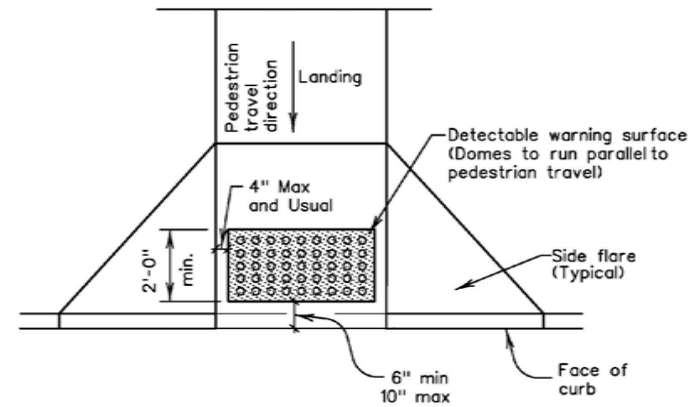
**Pedestrian Facilities
 General Notes**

- All slopes are maximum allowable. The least possible slope that will still drain properly should be used. Adjust curb ramp length or grade of approach sidewalks as directed.
- The minimum sidewalk width is 5'. Where the sidewalk is adjacent to the back of curb, a 6' sidewalk width is encouraged. Where a 5' sidewalk can not be provided due to site constraints, a minimum 3' sidewalk with 5' x 5' passing areas at intervals not to exceed 200' is required.
- Landings shall be 5' x 5' minimum with a maximum 2% slope in any direction.
- Maneuvering space at the bottom of curb ramps shall be a minimum of 4' x 4' wholly contained within the crosswalk and wholly outside the parallel vehicular travel path.
- Maximum allowable cross slope on sidewalk and curb ramp surfaces is 2%.
- Curb ramps with returned curbs may be used only where pedestrians would not normally walk across the ramp, either because the adjacent surface is planting or other non-walking surface or because the side approach is substantially obstructed. Otherwise, provide flared sides.
- Additional information on curb ramp location, design, light reflective value and texture may be found in the current edition of the Texas Accessibility Standards (TAS) and 16 TAC 68.02.
- To serve as a pedestrian refuge area, the median should be a minimum of 5' wide. Medians should be designed to provide accessible passage over or through them.
- Small channelization islands, which do not provide a minimum 5' x 5' landing at the top of curb ramps, shall be cut through level with the surface of the street.
- Crosswalk dimensions, crosswalk markings and stop bar locations shall be as shown elsewhere in the plans. At intersections where crosswalk markings are not required, curb ramps shall be aligned with theoretical crosswalks, or as directed by the Engineer.
- Existing features that comply with TAS may remain in place unless otherwise shown on the plans.
- Handrails are not required on curb ramps. Provide curb ramps wherever an accessible route crosses (penetrates) a curb.
- Curb ramps and landings shall be constructed and paid for in accordance with Item 531 "Sidewalks".
- Separate curb ramp and landings from adjacent sidewalk and any other elements with precast or board joint of 1/4" unless otherwise directed by the Engineer.
- Provide a smooth transition where the curb ramps connect to the street.
- Curbs shown on sheet 1 within the limits of payment are considered part of the curb ramp for payment, whether it is concrete curb, gutter, or combined curb and gutter.
- Flare slope shall not exceed 10% measured along curb line.

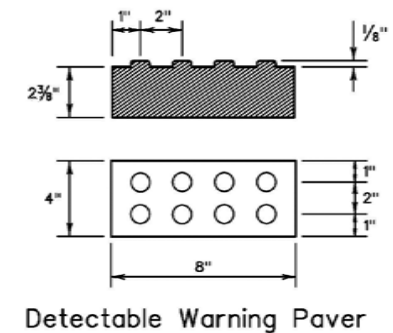
DETECTABLE WARNINGS



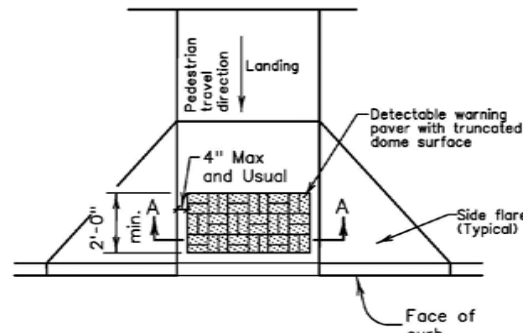
Typical placement of detectable warning surface on landing at street edge.



Typical placement of detectable warning surface on sloping ramp run.

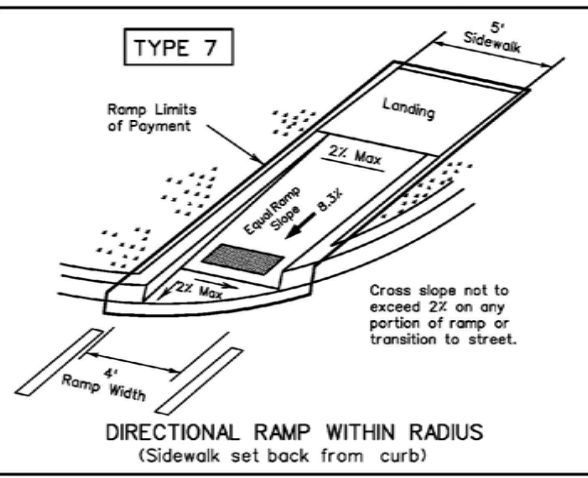


Detectable Warning Paver



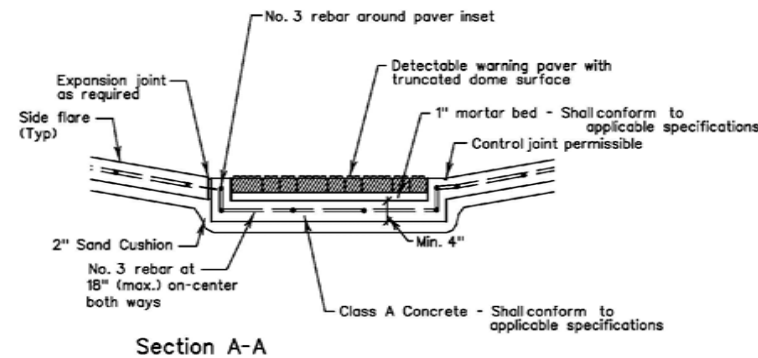
Truncated Dome Pattern Curb Ramp

DETECTABLE WARNING PAVER (OPTION)



**General Notes for
 Detectable Warnings**

- Curb ramps must contain a detectable warning surface that consists of raised truncated domes complying with Section 4.29 of the Texas Accessibility Standards (TAS). The surface must contrast visually with adjoining surfaces, including side flares. Furnish dark brown or dark red detectable warning surface adjacent to uncolored concrete, unless specified elsewhere in the plans.
- Detectable warning surfaces must be slip resistant and not allow water to accumulate.
- Align truncated domes in the direction of pedestrian travel when entering the street.
- Shaded areas on Sheet 1 of 4 indicate the approximate location for the detectable warning surface for each curb ramp type.
- Detectable warning surfaces shall be a minimum of 24" in depth in the direction of pedestrian travel, and extend the full width of the curb ramp or landing where the pedestrian access route enters the street.
- Detectable warning surfaces shall be located so that the edge nearest the curb line is a minimum of 6" and a maximum of 10" from the extension of the face of curb. Detectable warning surfaces may be curved along the corner radius.
- TxDOT maintains a list of Qualified Detectable Warning Materials. Details are provided herein for the placement of landscape pavers. For other materials, refer to the manufacturer's product manual for proper installation.



Section A-A

General Notes (Pavers)

Furnish detectable warning paver units meeting all requirements of ASTM C-936, C-33. Lay in a two by two unit basket weave pattern or as directed.

Lay full-size units first followed by closure units consisting of at least 25 percent of a full unit. Cut detectable warning paver units using a power saw.

