

Addison Airport - Westside Plans
1998

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12/11
200P52
DR-14



CONSTRUCT WEST TAXIWAY AND CONNECTORS WITH DRAINAGE IMPROVEMENTS (SOUTH SEGMENT) ADDISON AIRPORT

BID NUMBER: 98-04
TXDOT PROJECT NO. 9842 ADDSN
DECEMBER, 1997



BID SET
12-10-97

URS Greiner <small>1400 Jones Street, Suite 100 Fort Worth, TX 76104 (817) 342-8888</small>	<small>AN ASSOCIATION WITH</small> PSA ENGINEERING, INC. Huntington / SWL
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PROPOSED IMPROVEMENTS

TYPE OF CONSTRUCTION:

PAVING, DRAINAGE, SURFACE GRADING,
LIGHTING (M.I.T.L.), MARKING AND GUIDANCE SIGNS

REVISIONS		
DATE	DESCRIPTION	BY

RECOMMENDED FOR APPROVAL [Signature]
DESIGN ENGINEER

DATE _____
RECOMMENDED FOR APPROVAL [Signature]
PROJECT ENGINEER

DATE 12/1/97
APPROVED _____
DIRECTOR OF PUBLIC WORKS

DATE _____
SET _____

ITEM NUMBER	ITEM DESCRIPTION	UNITS	EST. QUANTITY		DYNAMIC ORDER	FINAL QUANTITY		REMARKS
			TOTAL	ADJ.		W/S	ADJ. RESERVE	
ALTERNATE ITEM NUMBER 2 (NORMAL)								
P-184-5.1	Anchored Pavement Gradation	CY	38,602	38,602				
P-184-5.2	Concrete Pavement Gradation	CY	1,740	1,740				
P-192-6.1	Unstabilized Gravel	CY	30,316	30,316				
P-194-5.1	Subbase Course	CV	3,192	3,720				
P-199-8.1	1 1/2" Thick Vitrified Subgrade	SF	18,888	18,888				
P-199-8.2	1 1/2"	SF	482	482				
P-208-5.1	Crushed Aggregate Base Course	CV	3,128	3,700				
P-601-8.1	Blastonite Surface Course	TON	4,402	4,402				
P-601-8.2	Blastonite Base Course	TON	6,041	6,041				
P-601-8.3	Blastonite Subbase Course	TON	489	489				
P-601-8.4	Wearing Surface	CY	300	300				
P-804-8.2	Portland Cement Concrete Pavement (CC)	CV	5,100	1,960				
P-802-8.1	Blastonite Surface Coat	TON	4,110	4,110				
P-802-8.2	Blastonite Seal Coat	TON	3,800	1,800				
P-820-5.1	Runway and Taxiway Paving	SF	17,870	12,700				
P-901-5.1	3" Reinforced Concrete Flyc (Open IC)	LF	708	708				
P-901-5.2	3" Reinforced Concrete Flyc (Close IC)	LF	716	716				
P-901-5.4	4" Reinforced Concrete Flyc (Close IC)	LF	1,622	1,622				
TA00F 400-1	Track Excavation Protection	LF	5,280	5,280				
P-701-6.1	30" Dia. 4' (Type "F" Mod.)	EA	8	8				
P-701-6.2	30" Dia. 4' (Type "F" Mod. w/ Metal Inserts)	EA	1	1				
P-701-6.3	Type "M" Manhole	EA	2	2				
P-801-8.1	Sealing and Muffling	SF	78,300	78,300				
P-911-1	Removal and Repair Existing Portland Cement Concrete Pavement	SF	335	335				
P-1108-6.1	L-3611 30" Dia. Open (1st. Mod.)	LF	16,138	18,500				
P-1108-6.2	L-3611 30" Dia. Open J. Cast-in-place (2nd. Mod.)	LF	4,305	4,300				
P-1110-5.1	F FHC One-way Electrical Drain - Type "F"	LF	3,700	3,700				
P-1110-5.2	F FHC One-way Drain - Type "F" (Concrete Channel)	LF	338	338				
P-1110-5.3	F FHC One-way Drain - Type "F" (Concrete Channel)	LF	88	88				
P-1110-5.4	3/4" x 1/2" 30" Dia. Steel - Type "F"	LF	75	75				
P-1110-5.5	3/4" x 1/2" 30" Dia. Steel - Type "F"	LF	80	80				
P-1110-5.6	3/4" x 1/2" 30" Dia. Steel - Type "F"	LF	80	80				
P-1110-5.7	F FHC One-way Drain - Type "F"	LF	80	80				
P-1110-5.8	30" Dia. Steel - Expanded Flat Drain - Type "F"	LF	30	30				
P-1110-5.9	30" Dia. Steel - Expanded Flat Drain - Type "F"	EA	2	2				
P-1110-5.10	30" Dia. Steel - Expanded Flat Drain - Type "F"	EA	2	2				
P-1120-8.1	Remove & Replace Existing SMCV Pilecap	EA	8	8				
P-1120-8.2	L-8011 30" Dia. Steel - Mounted (Flat) (Dist)	EA	92	92				
P-1120-8.3	L-8000 30" Dia. Steel - Mounted (Flat) (Dist)	EA	1	1				
P-1120-8.4	L-8088 Subbase Stop - Size #12 Mounted, Single Post	EA	4	4				
P-1120-8.5	L-8088 Subbase Stop - Size #12 Mounted, Single Post	EA	4	4				
P-1120-8.6	L-8088 Subbase Stop - Size #12 Mounted, Single Post	EA	2	2				
P-1120-8.7	L-8088 Subbase Stop - Size #12 Mounted, Single Post	EA	1	1				
P-1120-8.8	Remove & Replace Existing Subbase Stop	EA	1	1				
OP-200-11	24" Dia. Pile Inspection	LS	1	1				

CONTRACTOR'S SAFETY AND SECURITY REQUIREMENTS

SAFETY

- The Contractor will require his supervisory staff the OSHA safety and sanitation file on the project of the OSHA record sheet and will ensure that all employees are trained in safety and emergency or medical requirements and guidelines as safety standards in the Construction Safety Handbook as the contractor's safety manual.
- All Contractor vehicles that are authorized to operate on the project outside of the designated construction area are limited to local traffic as specified on the plans. Contractor vehicles in the motorist's right-of-way (MROW) shall display in full view above the vehicle a 7' x 7' orange or red flag with reflective material. Each Contractor vehicle shall also display in full view above the vehicle a 24" x 36" orange or red flag with reflective material. Each Contractor vehicle shall also display in full view above the vehicle a 24" x 36" orange or red flag with reflective material. Each Contractor vehicle shall also display in full view above the vehicle a 24" x 36" orange or red flag with reflective material. Each Contractor vehicle shall also display in full view above the vehicle a 24" x 36" orange or red flag with reflective material.
- All Contractor vehicles that are required to cross public streets shall be equipped with the proper equipment as specified in the OSHA record sheet. All Contractor vehicles shall be equipped with the proper equipment as specified in the OSHA record sheet. All Contractor vehicles shall be equipped with the proper equipment as specified in the OSHA record sheet.
- All Contractor vehicles that are required to cross public streets shall be equipped with the proper equipment as specified in the OSHA record sheet. All Contractor vehicles shall be equipped with the proper equipment as specified in the OSHA record sheet. All Contractor vehicles shall be equipped with the proper equipment as specified in the OSHA record sheet.
- Any construction activity within 100' of an active runway edge or of any other active runway edge shall be restricted to a minimum of 100' from the active runway edge. All Contractor vehicles shall be equipped with the proper equipment as specified in the OSHA record sheet.
- Discovered material shall be controlled in a manner to prevent movement resulting from aircraft jet blast or wind conditions in excess of 10 mph.
- Over, under, and around work shall be restricted to the OSHA record sheet. All Contractor vehicles shall be equipped with the proper equipment as specified in the OSHA record sheet.
- Drills, blasts and other activities capable of creating damage to adjacent buildings shall be restricted to the OSHA record sheet. All Contractor vehicles shall be equipped with the proper equipment as specified in the OSHA record sheet.
- The Contractor shall be responsible for the safety of all employees and for the safety of the general public. All Contractor vehicles shall be equipped with the proper equipment as specified in the OSHA record sheet.
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SECURITY

- Detailed instructions in the Contract and the Contractor shall comply with all requirements of the airport security plan and with the safety and security requirements of the OSHA record sheet. All Contractor vehicles shall be equipped with the proper equipment as specified in the OSHA record sheet.
- Contractor Personnel Security Operations: The Contractor shall be responsible for the security of all employees and for the safety of the general public. All Contractor vehicles shall be equipped with the proper equipment as specified in the OSHA record sheet.
- Access to the Site: Contractor's access to the site shall be as shown on the plans. All Contractor vehicles shall be equipped with the proper equipment as specified in the OSHA record sheet.
- Mileage to the Site: All Contractor's motorist orders for gasoline to the site shall be as shown on the plans. All Contractor vehicles shall be equipped with the proper equipment as specified in the OSHA record sheet.
- Contractor Area Limits: The location of construction, material storage areas, and equipment storage areas shall be as shown on the plans. All Contractor vehicles shall be equipped with the proper equipment as specified in the OSHA record sheet.
- Identification Personnel: All employees of Contractor or Subcontractor shall be identified by the Contractor. All Contractor vehicles shall be equipped with the proper equipment as specified in the OSHA record sheet.
- Security Officers: All security officers shall be trained in the OSHA record sheet. All Contractor vehicles shall be equipped with the proper equipment as specified in the OSHA record sheet.

Scale: 1" = 48'

Scale: 1" = 48'

Scale: 1" = 48'

Scale: 1" = 48'



ADDISON AIRPORT

SUMMARY OF QUANTITIES
AND CONTRACTOR'S SAFETY AND SECURITY REQUIREMENTS



Sheet

ITEM NUMBER	ITEM DESCRIPTION	UNITS	BE QUANTITY			CHANGE QUANTITY			FINAL QUANTITY			REMARKS
			QNA	ASP	QNA ASP				QNA	ASP	QNA ASP	
ALTERNATE NO NUMBER 3												
L-118-3.1	L-3142 20' Six Cube Enclosed in Steel	LF	5,750		5,750							
L-118-3.2	#2 AWG Bare Copper / Electrolytic Unannealed strand strand	LF	3,000		3,000							
L-118-3.3	2" PVC One-way Stand Stand - Type "A"	LF	4,440		4,440							
ALTERNATE NO NUMBER 4												
L-118-4.1	L-303 Dimpled Reflective Edge Marker	EA	30		30							

Scale: 1/4" = 1'-0"	Drawn by: STW/AMR		
Checked: WLR	Rev. no.: 00-00		
DATE: 0/0	Job no.: ATDNR/04	Scale:	PROJECT: 30

URS Groiner
4150 West Century Blvd., Suite 100
Fort Worth, TX 76102
(817) 342-0801

Engineers, Architects
and Planners

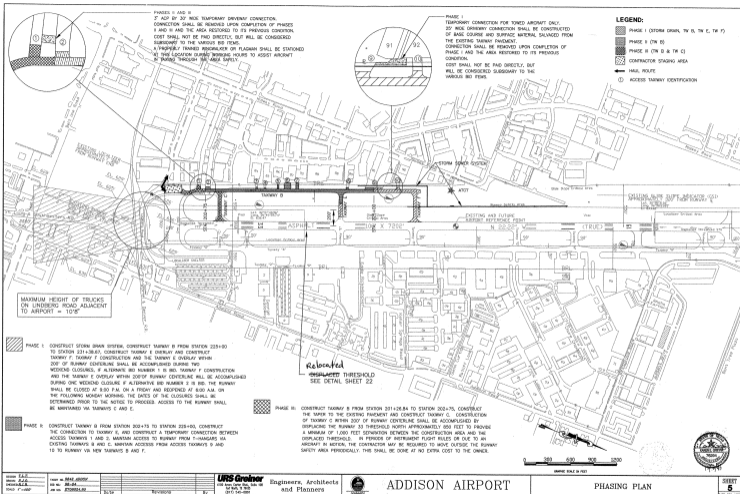


ADDISON AIRPORT

SUMMARY OF QUANTITIES
ADDITIVE ALTERNATE NO. 3



1997
4A
0000 000 000



PHASE II AND III
 2' ASP BY 30' WIDE TEMPORARY DRIVEWAY CONNECTION SHALL BE REMOVED UPON COMPLETION OF PHASES II AND III AND THE AREA RECOVERED TO ITS PREVIOUS CONDITION. COST SHALL NOT BE PAID DIRECTLY, BUT WILL BE CONSIDERED SUBORDINARY TO THE VARIOUS BID ITEMS.

A TRIPLEPLY TANKED ROADBLOCK ON FLAGMAN SHALL BE STATIONED AT THIS LOCATION DURING WORKING HOURS TO ASSIST AIRCRAFT IN PASSING THROUGH THE AREA SAFELY.

PHASE I
 TEMPORARY CONNECTION FOR TOWED AIRCRAFT ONLY. 20' WIDE DRIVEWAY CONNECTION SHALL BE CONSTRUCTED OF BASE COURSE AND SURFACE MATERIAL SALVAGED FROM THE EXISTING TAXIWAY PAVEMENT. CONNECTION SHALL BE REMOVED UPON COMPLETION OF PHASE I AND THE AREA RESTORED TO ITS PREVIOUS CONDITION. COST SHALL NOT BE PAID DIRECTLY, BUT WILL BE CONSIDERED SUBORDINARY TO THE VARIOUS BID ITEMS.

- LEGEND:**
- ▨ PHASE I (STORM DRAIN, TW 05, TW E, TW F)
 - ▨ PHASE II (TW 05)
 - ▨ PHASE III (TW D & TW C)
 - ▨ CONTRACTOR STAGING AREA
 - HAUL ROUTE
 - ⓪ ACCESS TAXIWAY IDENTIFICATION

MAXIMUM HEIGHT OF TRUCKS ON LINDBERG ROAD ADJACENT TO AIRPORT = 10'2"

PHASE I: CONSTRUCT STORM DRAIN SYSTEM, CONSTRUCT TAXIWAY B FROM STATION 225+00 TO STATION 231+26.84, CONSTRUCT TAXIWAY E OVERLAY AND CONSTRUCT TAXIWAY F, TAXIWAY F CONSTRUCTION AND THE TAXIWAY E OVERLAY WITHIN 200' OF RUNWAY CENTERLINE SHALL BE ACCOMPLISHED DURING TWO WEEKEND CLOSURES, IF ALTERNATIVE BID NUMBER 1 IS BID, TAXIWAY F CONSTRUCTION AND THE TAXIWAY E OVERLAY WITHIN 200' OF RUNWAY CENTERLINE WILL BE ACCOMPLISHED DURING ONE WEEKEND CLOSURE, IF ALTERNATIVE BID NUMBER 2 IS BID, THE RUNWAY SHALL BE CLOSED AT 8:30 P.M. ON A FRIDAY AND REOPENED AT 8:00 A.M. ON THE FOLLOWING MONDAY MORNING, THE DATES OF THE CLOSURES SHALL BE DETERMINED PRIOR TO THE NOTICE TO PROCEED. ACCESS TO THE RUNWAY SHALL BE MAINTAINED VIA TAXIWAYS C AND E.

Relocated
 150' WET THRESHOLD
 SEE DETAIL SHEET 22

PHASE III: CONSTRUCT TAXIWAY B FROM STATION 231+26.84 TO STATION 202+75, CONSTRUCT THE SUPER TO THE EXISTING PAVEMENT AND CONSTRUCT TAXIWAY C. CONSTRUCTION OF TAXIWAY C WITHIN 200' OF RUNWAY CENTERLINE SHALL BE ACCOMPLISHED BY DISPLACING THE RUNWAY 33 THRESHOLD NORTH APPROXIMATELY 650 FEET TO PROVIDE A MINIMUM OF 1,000 FEET SEPARATION BETWEEN THE CONSTRUCTION AREA AND THE DISPLACED THRESHOLD. IN PERIODS OF INSTRUMENT FLIGHT RULES OR DUE TO AN AIRCRAFT IN MOTION, THE CONTRACTOR MAY BE REQUIRED TO MOVE OUTSIDE THE RUNWAY SAFETY AREA PERIODICALLY. THIS SHALL BE DONE AT NO EXTRA COST TO THE OWNER.

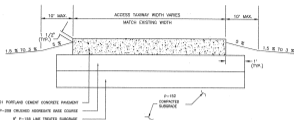
PHASE II: CONSTRUCT TAXIWAY B FROM STATION 202+75 TO STATION 225+00, CONSTRUCT THE CONNECTION TO TAXIWAY E, AND CONSTRUCT A TEMPORARY CONNECTION BETWEEN ACCESS TAXIWAYS I AND 2, MAINWAY ACCESS TO RUNWAY FROM 7-MANAGERS VIA EXISTING TAXIWAYS B AND C, MAINWAY ACCESS FROM ACCESS TAXIWAYS 0 AND 10 TO RUNWAY VIA NEW TAXIWAYS B AND F.

DATE: 11-2-11	SCALE: AS SHOWN	URS Greiner Engineers, Architects and Planners 250 West 10th Street, Suite 100 Minneapolis, MN 55401 TEL: 612-344-0000
DESIGNER: URS	NO. OF SHEETS: 21 OF 21	
PROJECT: ADDISON AIRPORT	SHEET NO.: 5	

ADDISON AIRPORT

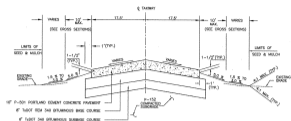
PHASING PLAN





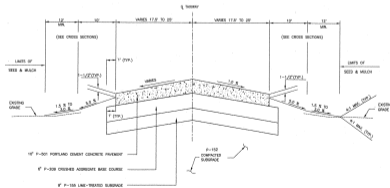
TYPICAL ACCESS TAXIWAY &
GUIDE SLOPE ACCESS ROAD SECTION
WITH CONCRETE SURFACE

(ALTERNATE BID NO. 9)



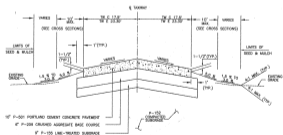
TAXIWAY F TYPICAL SECTION
STA. 2+50 TO 4+00

(ALTERNATE BID NO. 11)



TAXIWAY B TYPICAL SECTION

(ALTERNATE BID NO. 10)



TAXIWAY C TYPICAL SECTION
TAXIWAY E STA 0+00 TO 1+63.6
TAXIWAY F STA 0+00 TO 2+50

(ALTERNATE BID NO. 8)

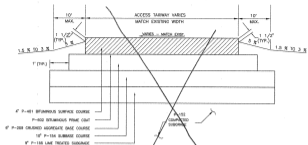
DATE: 2-1-77	DESIGN BY: BRAD ARDREY
REVISION: 2-1-77	DATE: 2-1-77
BY: J. R. B. C.	APP. BY: J. R. B. C.
NO. OF SHEETS: 1	TOTAL SHEETS: 1

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and Planners

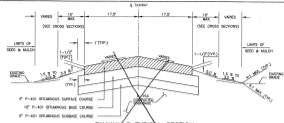


ADDISON AIRPORT

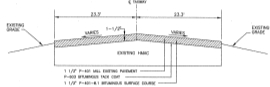
CONCRETE PAVEMENT TYPICAL SECTIONS



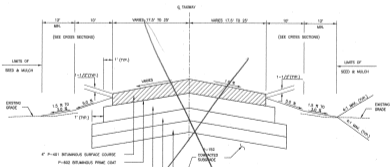
TYPICAL ACCESS TAXIWAY SECTION WITH ASPHALT SURFACE
 N13
 (ALTERNATE BID NO. 2)



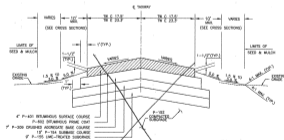
TAXIWAY F TYPICAL SECTION
 STA. 2+50 TO 4+00
 N13
 (ALTERNATE BID NO. 2)



TAXIWAY E OVERLAY TYPICAL SECTION
 N13
 STA. 1+63.5 TO STA. 3+15



TAXIWAY B TYPICAL SECTION
 N13
 (ALTERNATE BID NO. 2)



TAXIWAY C TYPICAL SECTION
 TAXIWAY E STA. 0+00 TO 1+63.6
 TAXIWAY F STA. 0+00 TO 2+50
 N13
 (ALTERNATE BID NO. 2)

DATE: 2-1-82
 DRAWN: J.P.S.
 CHECKED: J.P.S.
 TITLE: ASPHALT

PROJECT: 2884-ADP-82
 SHEET NO.: 28-04
 OF 28 SHEETS

NO.	DESCRIPTION	DATE

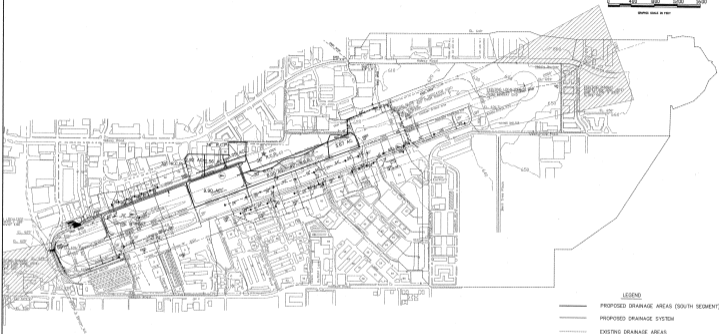
URS Greiner Engineers, Architects and Planners
 100 West Lake Ave., Suite 100
 West Lake, Ohio 44145
 (216) 545-0801



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ASPHALT PAVEMENT TYPICAL SECTIONS





- LEGEND**
- PROPOSED DRAINAGE AREAS (SOUTH SEGMENT)
 - PROPOSED DRAINAGE SYSTEM
 - EXISTING DRAINAGE AREAS
 - RC --- EXISTING DRAINAGE SYSTEM



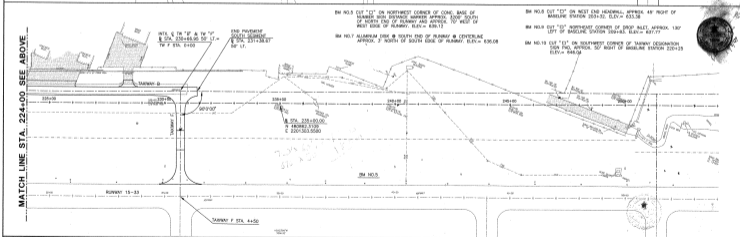
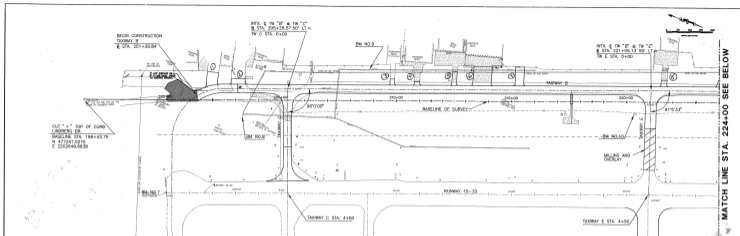
DATE: 2.17.11	ISSUE NO: 002	PROJECT NO: 100000000	SCALE: AS SHOWN
BY: J. G. GIBSON	CHKD BY: J. G. GIBSON	DATE: 2.17.11	PROJECT NO: 100000000
APP: J. G. GIBSON	APP: J. G. GIBSON	DATE: 2.17.11	PROJECT NO: 100000000

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 and Planners



ADDISON AIRPORT

DRAINAGE AREA MAP





MATCH LINE STA. 224+00 SEE BELOW

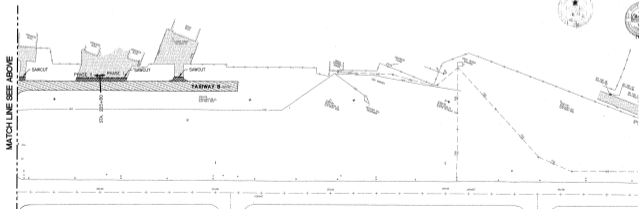
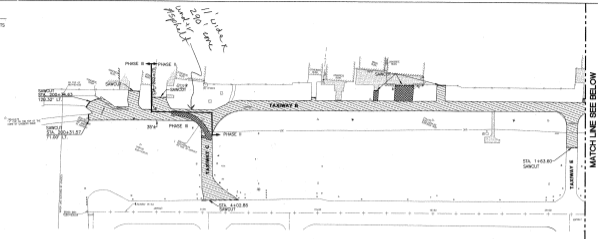
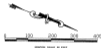
MATCH LINE STA. 224+00 SEE ABOVE

NOTES

SEE DRAWINGS 11-13 FOR DRIVEWAY DEMOLITION UNITS

LEGEND

-  REMOVE EXIST. ASPHALT PAVEMENT
-  REMOVE EXIST. PCC PAVEMENT



DATE: 2-27-10
 DRAWN BY: J.C.B.
 CHECKED BY: J.C.B.
 DESIGNED BY: J.C.B.

DATE OF REVISION:
 NO. OF REVISIONS:
 BY: J.C.B.

PROJECT NO.: 08000000
 SHEET NO.: 10
 TOTAL SHEETS: 13

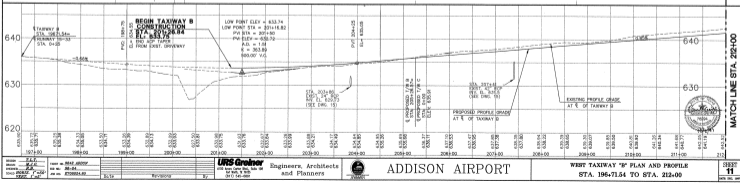
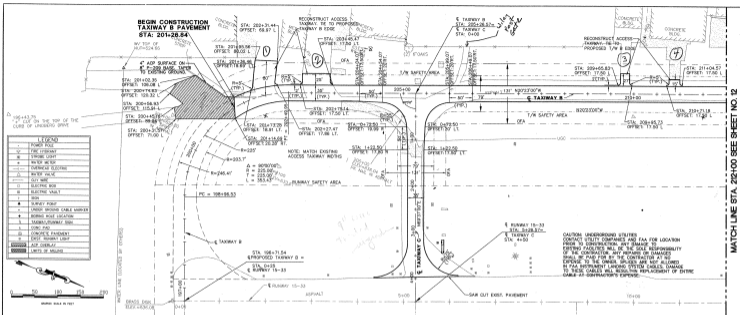
URS Greiner Engineers, Architects and Planners
 200 West 10th Street, Suite 100
 Fort Worth, TX 76102
 (817) 542-8800



ADDISON AIRPORT

**WEST TAXIWAY "B" PLAN AND PROFILE
 PAVEMENT DEMOLITION PLAN**

31955
10
 DATE: 02/27/10



DATE: 2-25	SCALE: AS SHOWN
DRAWN: J.P.	CHECKED: M.C.
TITLE: WEST TAXIWAY "B" PAVEMENT	PROJECT: ADDISON AIRPORT

STATION	ELEVATION	REMARKS
197+00	627.3	EXIST. GRADE
198+00	628.1	EXIST. GRADE
199+00	628.9	EXIST. GRADE
200+00	629.7	EXIST. GRADE
201+00	630.5	EXIST. GRADE
202+00	631.3	EXIST. GRADE
203+00	632.1	EXIST. GRADE
204+00	632.9	EXIST. GRADE
205+00	633.7	EXIST. GRADE
206+00	634.5	EXIST. GRADE
207+00	635.3	EXIST. GRADE
208+00	636.1	EXIST. GRADE
209+00	636.9	EXIST. GRADE
210+00	637.7	EXIST. GRADE
211+00	638.5	EXIST. GRADE
212+00	639.3	EXIST. GRADE

LRS Groder
 Engineers, Architects
 and Planners



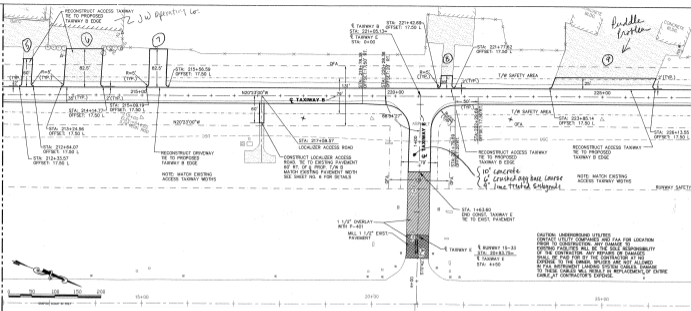
ADDISON AIRPORT

WEST TAXIWAY "B" PLAN AND PROFILE
 STA. 196+71.54 TO STA. 212+00

MATCH LINE STA. 212+00 SEE SHEET NO. 12

MATCH LINE STA. 212+00

MATCH LINE STA. 212+00 SEE SHEET NO. 11



MATCH LINE STA. 212+00



DATE: 04/12/00
 DRAWN BY: JMM/ARW
 CHECKED BY: JMM/ARW
 SCALE: AS SHOWN

DATE: 04/12/00
 DRAWN BY: JMM/ARW
 CHECKED BY: JMM/ARW
 SCALE: AS SHOWN

DATE: 04/12/00
 DRAWN BY: JMM/ARW
 CHECKED BY: JMM/ARW
 SCALE: AS SHOWN

DATE: 04/12/00
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 SCALE: AS SHOWN

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 SCALE: AS SHOWN

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DATE: 04/12/00
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 SCALE: AS SHOWN

DATE: 04/12/00
 DRAWN BY: JMM/ARW
 CHECKED BY: JMM/ARW
 SCALE: AS SHOWN

URS Greiner
 4500 Wilshire Blvd., Suite 100
 Los Angeles, CA 90048
 Tel: 310-207-0881

Engineers, Architects
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ADDISON AIRPORT

TAXIWAY "B" PLAN AND PROFILE
 STA. 212+00 TO STA. 227+00

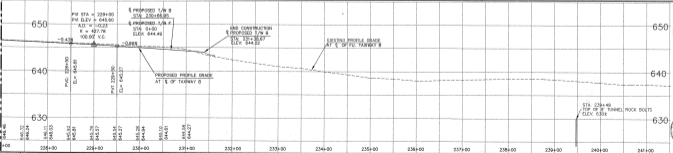
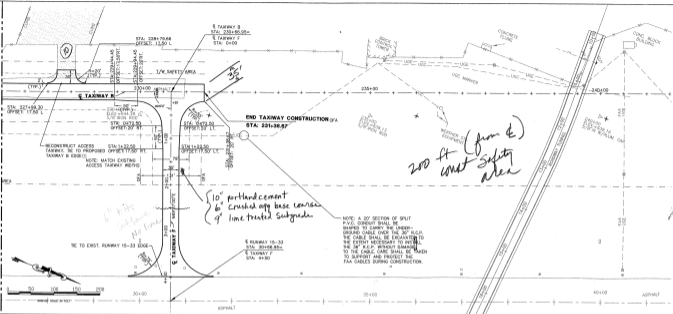
SHEET
12
 OF 12

MATCH LINE STA. 227+00 SEE SHEET NO. 13

MATCH LINE STA. 227+00

MATCH LINE STA. 227+00 SEE SHEET NO. 12

MATCH LINE STA. 227+00



DATE	BY	CHKD.
11/11/11	MM	MM
11/11/11	MM	MM
11/11/11	MM	MM

DESIGNED BY	MM
CHECKED BY	MM
DATE	11/11/11

NO.	DATE	BY	REVISION

URS
UNIVERSITY OF TEXAS SYSTEM
 4300 WEST LOOP SOUTH, SUITE 1000
 HOUSTON, TEXAS 77056
 TEL: 281.556.1000
 FAX: 281.556.1001

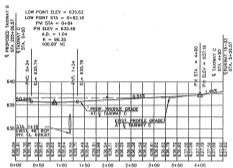
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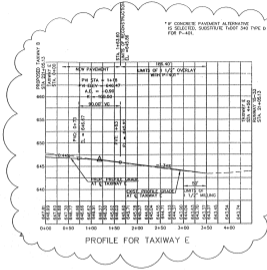
ADDISON AIRPORT

**WEST TAXIWAY "B" PLAN AND PROFILE
 STA. 227+00 TO STA. 241+00**

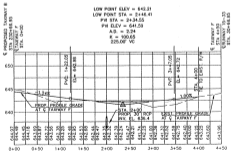




PROFILE FOR TAXIWAY C



PROFILE FOR TAXIWAY E

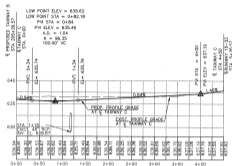


PROFILE FOR TAXIWAY F

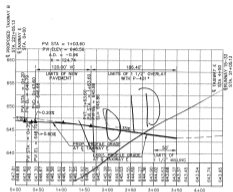
* IF CONCRETE PAVEMENT ALTERNATIVE IS SELECTED, SUBSTITUTE TA007 340 PNC D FOR P-46L

5-21-98

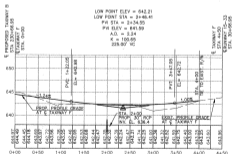




PROFILE FOR TAXIWAY C



PROFILE FOR TAXIWAY E



PROFILE FOR TAXIWAY F



Drawn: JAC	Checked: JAC	Scale: AS SHOWN
Design: JAC	Reviewed: JAC	Date: 08-04
Project: 03-0000	Client: AIRPORT	Sheet: 14

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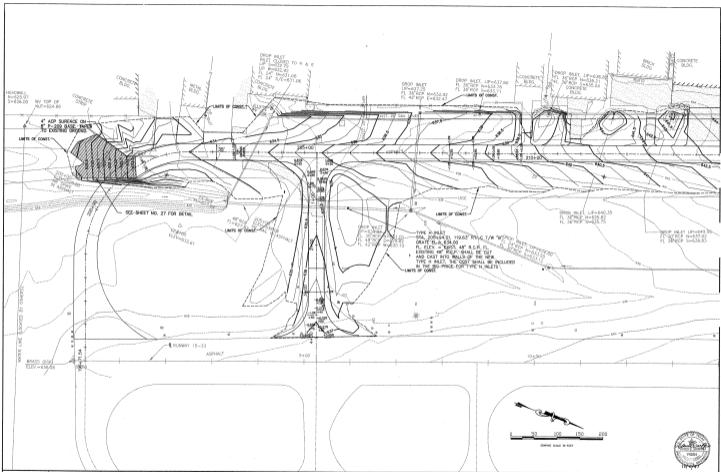


ADDISON AIRPORT

WEST TAXIWAY "F" & DRAINAGE IMPROVEMENTS
TAXIWAY C, E AND F PROFILES

SHEET
14
OF 14

MATCH LINE STA. 214+00 SEE SHEET NO.16



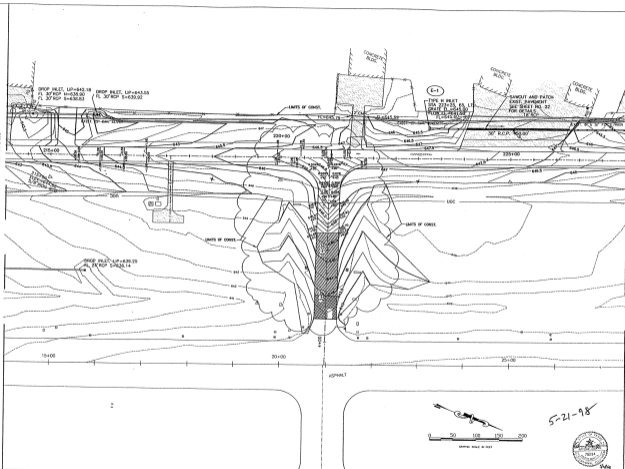
DATE: 2-12-09	ISSUED TO: WEST AIRPORT	BY: JLS
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DATE: 2-12-09	ISSUED TO: WEST AIRPORT	BY: JLS

URS Greiner Engineers, Architects and Planners
 200 West Loop West, Suite 500
 Fort Worth, TX 76102
 817.342.0800

ADDISON AIRPORT

WEST TAXIWAY 'B' & DRAINAGE IMPROVEMENTS GRADING PLAN

MATCH LINE STA. 244+00 SEE SHEET NO.15



MATCH LINE STA. 227+50 SEE SHEET NO.17

DATE: 7-27	BY: J.E. BROWN	APP: J.E. BROWN
DATE: 7-27	BY: J.E. BROWN	APP: J.E. BROWN
DATE: 7-27	BY: J.E. BROWN	APP: J.E. BROWN

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ADDISON AIRPORT

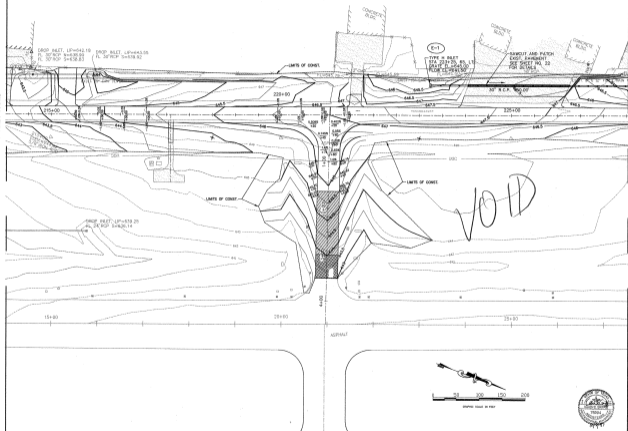
WEST TAXIWAY "H" & DRAINAGE IMPROVEMENTS
GRADING PLAN



SHEET
16

MATCH LINE STA. 244+00 SEE SHEET NO.15

MATCH LINE STA. 227+50 SEE SHEET NO.17



DESIGN: E.A.E.	DATE: 08/14/2013	BY: J.M.A.	NO. 1
DRAWN: J.C.C.	DATE: 08/14/2013	BY: J.M.A.	NO. 1
CHECKED: E.A.E.	DATE: 08/14/2013	BY: J.M.A.	NO. 1
SCALE: 1"=80'	DATE: 08/14/2013	BY: J.M.A.	NO. 1

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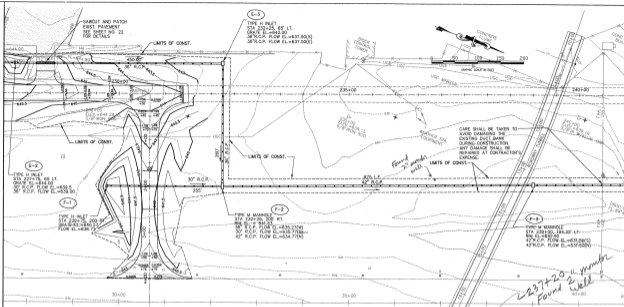


ADDISON AIRPORT

**WEST TAXIWAY "H" & DRAINAGE IMPROVEMENTS
 GRADING PLAN**

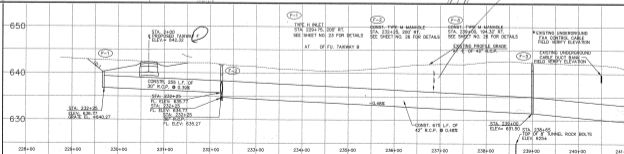
SHEET
16
 OF 16

MATCH LINE STA. 227+50 SEE SHEET NO.16



MATCH LINE STA. 241+00 SEE SHEET NO.18

MATCH LINE



MATCH LINE

DATE	BY	CHECKED	DATE
NOV. 21, 2011	J.A.	J.A.	NOV. 21, 2011
NOV. 21, 2011	J.A.	J.A.	NOV. 21, 2011
NOV. 21, 2011	J.A.	J.A.	NOV. 21, 2011

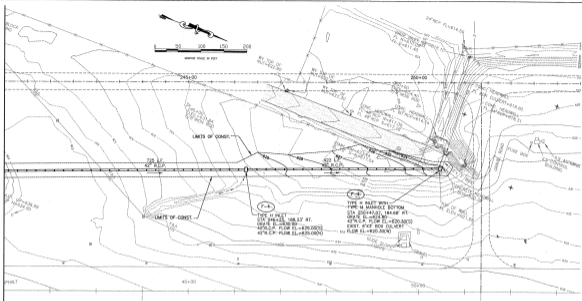
URS Greiner
 ENGINEERS, ARCHITECTS
 AND PLANNERS
 1800 W. 15th St., Suite 100
 Fort Collins, CO 80501
 (970) 225-0800



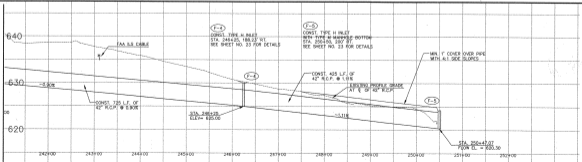
ADDISON AIRPORT

WEST TAXIWAY "B" & DRAINAGE IMPROVEMENTS
GRADING PLANSHEET
17
OF 22

MATCH LINE STA. 244+00 SEE SHEET NO.17



MATCH LINE



DATE: 2-17	PROJECT: 2014-0007
SCALE: 1/2" = 10'-0"	DATE: 02-26
DESIGNER: J.L.C.	APPROVED: J.S.BELLAR
CHECKER: T.M.P.	

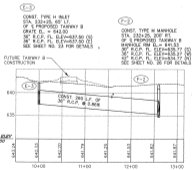
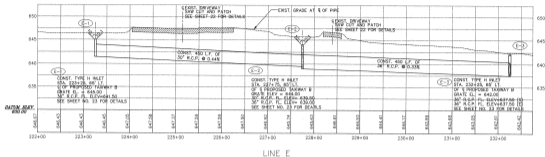
NO.	DESCRIPTION	DATE
1	REVISED	02/26/14

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ADDISON AIRPORT

**WEST TAXIWAY "B" & DRAINAGE IMPROVEMENTS
 GRADING PLAN**



CAUTION UNDERGROUND UTILITIES
 CONTACT UTILITY COMPANIES FOR LOCATION
 PRIOR TO CONSTRUCTION. ANY DAMAGE TO
 EXISTING FACILITIES WILL BE THE SOLE RESPONSIBILITY
 OF THE CONTRACTOR. ANY REDUCTION OR DAMAGE
 SHALL BE PAID FOR BY THE CONTRACTOR AT NO
 EXPENSE TO THE OWNER. SPACES ARE NOT ALLOWED
 IN PAVE SETTLEMENT LANSING SYSTEM CABLES. DAMAGE
 TO THESE CABLES WILL RESULT IN REPLACEMENT OF ENTIRE
 CABLE AT CONTRACTOR'S EXPENSE.

*EXISTING UNDERGROUND
 PAVE CONTROL CABLE
 FIELD POINT ELEVATION
 (P.A. CABLES TO BE PLACED
 IN SLOTTED CONC. OVER
 PROPOSED STREAM CHANNEL

TAXIWAY B STA. 232+25
 36" R.C.P. PROFILE



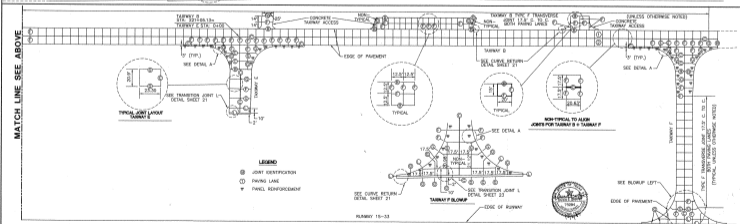
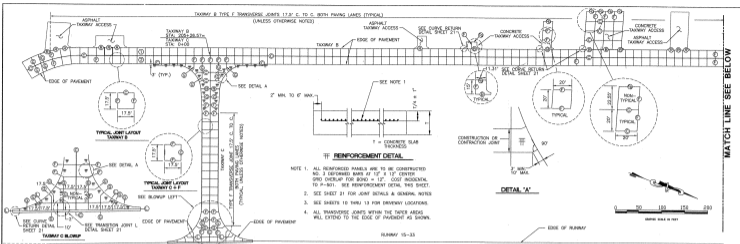
DESIGN	J.A.C.	CHECKED	W.B. ROY
DRAWN	J.A.C.	DATE	08-08
PROJECT	ADDISON AIRPORT	BY	W.B. ROY
SCALE	AS SHOWN	DATE	08-08

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ADDISON AIRPORT

WEST TAXIWAY 'B' & DRAINAGE IMPROVEMENTS
 STORM DRAIN PROFILES



MATCH LINE SEE ABOVE

DATE: 12-1-88
 DRAWN BY: J.S.S.
 CHECKED BY: J.S.S.
 TITLE: JOINT LAYOUT

PROJECT NO.: 2001-AB000
 TO NO.: 20-24
 SHEET NO.: 20-24

SHEET

OF 10

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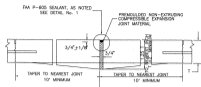
100 West 10th Street, Suite 100
 Minneapolis, MN 55401
 (612) 344-0000



ADDISON AIRPORT

JOINT LAYOUT PLAN

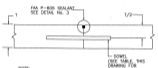
SHEET
20



NOTE:

PRECASTED JOINT MATERIAL SHALL BE NON-EXPANDING TYPE, CONFORMING TO ASTM D 1751 - PREFORMED POLYOLFOURIMINE ELASTOMERIC JOINT SEALS FOR CONCRETE. FOR CONCRETE PAVEMENT, NO COOK ALLOWANCE.

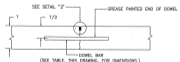
TYPE B THICKENED EDGE EXPANSION JOINT



NOTE:

- FOR TYPE B JOINTS, DRILLING EXISTING P.C.C. PAVEMENT. CONTRACTOR SHALL DRILL AND EPOXY DOWEL BAR INTO EXISTING P.C.C. PAVEMENT.
- DO NOT PLACE CLOSER THAN 1\"/>

TYPE C CONSTRUCTION JOINTS

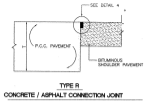


- DO NOT PLACE CLOSER THAN 1\"/>

TYPE D DOWELED JOINT

REVISION	DATE	BY	REASON

CONTRACTION JOINTS

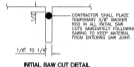


CONCRETE / ASPHALT CONNECTION JOINT

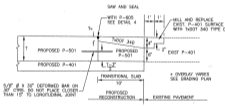
N.T.S.



BEVELED EDGE DETAIL

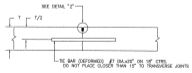


INITIAL SAW CUT DETAIL



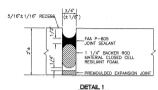
TYPE L TRANSITION JOINT

N.T.S.

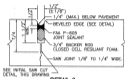


TYPE P HEAVED JOINT WITH TIE BAR

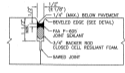
TIE BAR (DEFORMED #7) SHALL BE ON 1\"/>



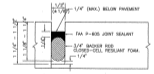
DETAIL 1



DETAIL 2



DETAIL 3



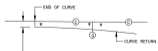
DETAIL 4

DIMENSIONS & SPACING OF DOWEL BARS			
THICKNESS OF SLAB (T)	DL. (D) MIN.	LENGTH	SPACING
4" - 5"	3/4"	10"	12"
6" - 7"	1"	12"	12"
8" - 10"	1 1/4"	20"	15"
11" - 20"	1 1/2"	30"	18"
21" - 34"	2"	30"	18"

DOWEL BAR INSTALLATION DEVICE (BIBY, CASE OR BASKET) AND METHOD OF ANCHORING INTO POSITION SHALL BE APPROVED BY THE CHAIRMAN.

GENERAL NOTES

- ALL JOINT CONSTRUCTION AND MATERIALS SHALL BE IN ACCORDANCE WITH F.A.A. SPEC. P-501 AND P-505 OR AS NOTED.
- ALL MATERIALS AND WORK REQUIRED FOR JOINTS ARE INCIDENTAL TO PAVEMENT.
- DOWELS SHALL BE SHOP FINISHED WITH ONE COAT OF LEAD OR ZINC PAINT AND THE FREE HALF GRADED IN THE FIELD WITH A HIGH MELTING POINT GRADE.
- THE MATERIALS AND METHODS SHOWN ON THIS DRAWING ARE THE MINIMUM REQUIREMENTS. OTHER MATERIALS AND METHODS MAY BE USED IF APPROVED BY THE DRAWER.
- ALL JOINTS SHALL BE SEALED IN ACCORDANCE WITH F.A.A. SPEC. P-505 OR AS NOTED.
- EDGES OF SLAB SHALL BE SPRAWED WITH AN APPROVED CURING MATERIAL BEFORE MAKING SMOOTH FLOOR. A TRANSVERSE CONTRACTION JOINT SHALL BE INSTALLED WHEN PAVING OPERATIONS ARE INTERRUPTED FOR MORE THAN 30 MINUTES OTHER THAN AT EXPANSION JOINTS OR END OF SLAB. SEE TYPE "C" JOINT DETAIL. IF THE INTERRUPTION IS LESS THAN 6 FEET FROM A PLANNED JOINT THE FRESH CONCRETE SHALL BE FINISHED OTHER BACK TO THE PREVIOUSLY INSTALLED JOINT OR TO A POINT 6 FEET MINIMUM FROM THE NEXT PLANNED JOINT.
- ALL CONTRACTION JOINTS THAT ARE NOT OPEN AND FREE OF FOREIGN MATERIAL TO THE FULL WIDTH AND DEPTH SHALL BE CLEANED BY SCRAPING FULL WIDTH AND DEPTH.
- THIS STANDARD PLAN CONFORMS TO AND AMENDS F.A.A. AC 150/5370-10, SPECIFICATIONS FOR SURFACE CONSTRUCTION OF AIRPORTS, AND AC 150/5320-6L, AIRPORT PAVEMENT DESIGN AND EVALUATION.
- THE CONTRACTOR SHALL MAKE SPECIAL PROVISIONS TO KEEP THE FINISH CONCRETE SURFACE FREE OF FOREIGN MATERIAL, (I.E. ACCUMULATED DIRT, ETC.) THROUGHOUT THE PROJECT. SPREADERS, MAGNUM TRUCKS, ETC. SHALL BE UTILIZED AS NECESSARY BY THE CONTRACTOR AND / OR AS DIRECTED BY THE RESIDENT ENGINEER TO MAINTAIN THIS PROVISION.
- TWO PART DOWELS MAY BE ALLOWED FOR TYPE C JOINT UNDER THE FOLLOWING CONDITIONS:
 - THE DOWEL MUST BE APPROVED BASED ON SHOP DRAWINGS SUBMITTED TO THE ENGINEER.
 - THE CONTRACTOR MUST DEMONSTRATE ON THE FIRST DAY OF PAVING THAT THE METHOD OF INSERTION INTO THE FRESH CONCRETE WILL RESULT IN THE CORRECT SPACING AND ALIGNMENT OF THE DOWEL. THE INSERTION METHOD SHALL NOT RESULT IN EXCESSIVE EDGE SLUMP. IF THE CONTRACTOR CANNOT MANUFACTURELY INSERT THE DOWELS TO THE CORRECT LINE AND GRADE ON THE FIRST DAY OF PAVING, THEN THE CONTRACTOR SHALL DRILL AND EPOXY THE DOWELS FOR THE REMAINDER OF THE PAVING OPERATION.



DETAIL TP CURVE RETURN TIE-IN DETAIL

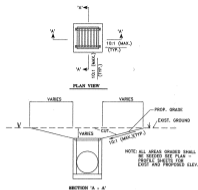
N.T.S.



URS Greiner Engineers, Architects and Planners
 410 West 12th Street, Suite 100
 Omaha, Nebraska 68102-4202
 TEL: 402-552-1000 FAX: 402-552-1001

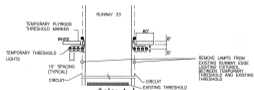
ADDISON AIRPORT

JOINT LAYOUT DETAILS



TYPICAL INLET GRADING DETAILS

A.S.C.

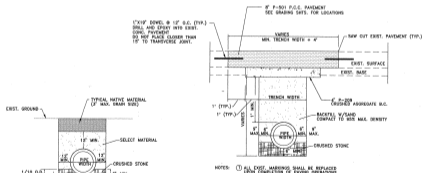


TEMPORARY DISPLACED THRESHOLD

A.S.C.

NOTES:

- TEMPORARY DISPLACED THRESHOLD SHALL BE PLACED AT THE LOCATION SHOWN ON THE PLANS AND SHALL INCLUDE TEMPORARY THRESHOLD LIGHTS IN L-SHED, SPALT LENS, RED, GREEN, [3] BARS, TEMPORARY PLYWOOD THRESHOLD MARKERS (P 1 RES) AND TEMPORARY LIGHT CIRCUITS. EXISTING RUNWAY THRESHOLD LIGHTS MAY BE USED. THE COST OF INSTALLING, MAINTAINING AND REMOVING OF THE TEMPORARY DISPLACED THRESHOLD IS RESIDENTIAL TO THE PROJECT.
- MARKING FOR TEMPORARY DISPLACED THRESHOLD R/W 33 INCLUDES OUTLINED THRESHOLD MARKERS.



EMBEDMENT DETAIL FOR STORM SEWER

A.S.C.

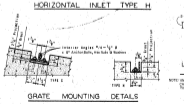
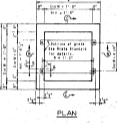
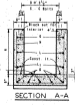
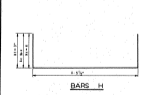
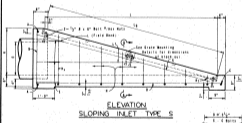
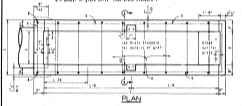
TEMPORARY REPLACEMENT DETAIL

A.S.C.



SLOPE	N	H	TABLE OF DIMENSIONS			BILL OF REINFORCING STEEL FOR SLOPING INLET																TOTAL WEIGHT	TOTAL COST																			
			X	H	L	BARS A		BARS B		BARS C		BARS D		BARS E		BARS F		BARS G																								
						NO.	SIZE	LNTH.	WT.	NO.	SIZE	LNTH.	WT.	NO.	SIZE	LNTH.	WT.	NO.	SIZE	LNTH.	WT.	NO.	SIZE	LNTH.	WT.	NO.	SIZE	LNTH.	WT.	NO.	SIZE	LNTH.	WT.	NO.	SIZE	LNTH.	WT.					
1:1	2'	18"	2'-0"	2'-0"	12'-0"	2	#4	3'-0"	3	#4	2'-0"	3	#4	2'-0"	2	#4	12'-0"	27	#4	12'-0"	27	#4	12'-0"	18	12	#4	12'-0"	33	2	#4	12'-0"	3	2	#4	12'-0"	3	2	#4	12'-0"	3	110	1.1
1:1	3'	24"	3'-0"	3'-0"	18'-0"	2	#4	3'-0"	5	#4	2'-0"	5	#4	2'-0"	2	#4	18'-0"	45	#4	18'-0"	45	#4	18'-0"	24	18	#4	18'-0"	51	3	#4	18'-0"	5	3	#4	18'-0"	5	3	#4	18'-0"	5	130	1.3
1:1	4'	30"	4'-0"	4'-0"	24'-0"	2	#4	3'-0"	7	#4	2'-0"	7	#4	2'-0"	2	#4	24'-0"	63	#4	24'-0"	63	#4	24'-0"	30	24	#4	24'-0"	69	4	#4	24'-0"	7	4	#4	24'-0"	7	4	#4	24'-0"	7	140	1.4
1:1	5'	36"	5'-0"	5'-0"	30'-0"	2	#4	3'-0"	9	#4	2'-0"	9	#4	2'-0"	2	#4	30'-0"	81	#4	30'-0"	81	#4	30'-0"	36	30	#4	30'-0"	87	5	#4	30'-0"	9	5	#4	30'-0"	9	5	#4	30'-0"	9	150	1.5
1:1	6'	42"	6'-0"	6'-0"	36'-0"	2	#4	3'-0"	11	#4	2'-0"	11	#4	2'-0"	2	#4	36'-0"	99	#4	36'-0"	99	#4	36'-0"	42	36	#4	36'-0"	105	6	#4	36'-0"	11	6	#4	36'-0"	11	6	#4	36'-0"	11	160	1.6

Note: For pipe sizes of 24", 30", and 36" use table for pipe sizes 18", 24", and 30" respectively.
 N = Number of grate units. (See Grate Standard.)



SECTION B-B



BARS C

REINFORCING STEEL
 Reinforcing steel should be of the Contractor's information only. Specify quantity and size of the steel provided with the bill for each item of the type specified. Reinforcing steel shall be furnished in the form of a reinforced professional engineer will be responsible for correct construction of bars.
 Steel drawings will not be required.
 The contractor may, with the approval of the Engineer furnish lists of equivalent structural design.
 In areas of conflict between reinforcing steel, structural, pipe, sheet metal or other contracting trades, the contractor shall be held responsible to clear as directed by the Engineer.
 If possible, horizontal grate units should be mounted such that each lattice and drain water approx parallel to bars on grate. If this is not possible, orientation shall meet practical trial.



Reinforcing steel should extend within 10" of edge of concrete slab. If necessary, provide steel at least 2" above or below center of slab on top or bottom of slab.

The top diameter of "I" or "L" bars should be 1/2" above bottom to bottom of bottom of concrete of "Type H" bars shall be the largest size of bar unless otherwise noted. The bars shall extend full length with dimension. For vertical dimension use "Type H" or "Type L" of steel. Higher pipe or in the slab with a 2" minimum diameter.



STATE DEPARTMENT OF HIGHWAYS AND PUBLIC TRANSPORTATION

SLOPING INLET TYPE S AND HORIZONTAL INLET TYPE H

IL-S IL-H

NO. 200	REVISED	DATE	BY
1	1	10-15-53	J.M.
2	1	1-15-54	J.M.
3	1	1-15-54	J.M.
4	1	1-15-54	J.M.
5	1	1-15-54	J.M.
6	1	1-15-54	J.M.
7	1	1-15-54	J.M.
8	1	1-15-54	J.M.
9	1	1-15-54	J.M.
10	1	1-15-54	J.M.

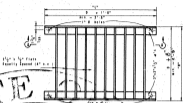
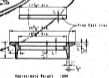
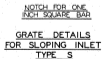
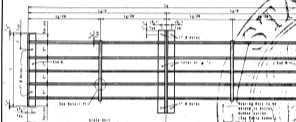


V2 ANCHOR BOLT DETAIL

NOTE: ANCHOR BOLTS SHALL BE SET IN PLACE AT THE TOP OF THE WALL.

GRADE	NO. OF GRATE UNITS	GRATE QUANTITIES FOR SLOPING INLET														TOTAL WEIGHT			
		BEARING BARS @ 3" C.C.				INTERIOR ANGLES				END ANGLES				F# BARS					
		NO.	SIZE	LGTH.	WT.	NO.	SIZE	LGTH.	WT.	NO.	SIZE	LGTH.	WT.	NO.	WT.				
1.1	25'	3	1.8	25.0	47.1	110	3	2.0	25.0	50	3	2.0	25.0	50	3	2.0	25.0	50	150
1.1	30'	3	1.8	30.0	47.1	140	3	2.0	30.0	60	3	2.0	30.0	60	3	2.0	30.0	60	180
1.1	35'	3	1.8	35.0	47.1	170	3	2.0	35.0	70	3	2.0	35.0	70	3	2.0	35.0	70	210
1.1	40'	3	1.8	40.0	47.1	200	3	2.0	40.0	80	3	2.0	40.0	80	3	2.0	40.0	80	240
1.1	45'	3	1.8	45.0	47.1	230	3	2.0	45.0	90	3	2.0	45.0	90	3	2.0	45.0	90	270
1.1	50'	3	1.8	50.0	47.1	260	3	2.0	50.0	100	3	2.0	50.0	100	3	2.0	50.0	100	300
1.1	55'	3	1.8	55.0	47.1	290	3	2.0	55.0	110	3	2.0	55.0	110	3	2.0	55.0	110	330
1.1	60'	3	1.8	60.0	47.1	320	3	2.0	60.0	120	3	2.0	60.0	120	3	2.0	60.0	120	360

N = Number of grate units.
For pipe diameters of 24", 27", and 30" use grades for pipe diameters of 24", 27" and 30" respectively.



GRATE DETAILS FOR HORIZONTAL INLET TYPE H (MOD)

GENERAL NOTES:
Structural steel for grates shall conform to the requirements of AISC Specification 4.2a or AISC Specification 4.2b.

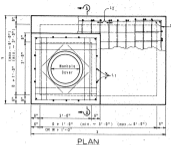
Grates and covers of slightly different dimensions and specifications may be substituted if approved by the Engineer.



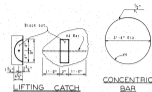
STATE DEPARTMENT OF HIGHWAYS AND PUBLIC TRANSPORTATION
GRATE (TYPE S),
GRATE (TYPE H)(MOD)
RING & COVER (TYPE C)
ILG-S ILG-H(MOD)RC-C

DATE	24
BY	
CHECKED	
APPROVED	

Note: Bars, either cast in place or concrete pipe, may be located as any shown.

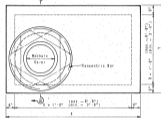


PLAN

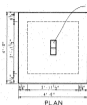


LIFTING CATCH

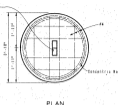
CONCENTRIC BAR



PLAN



PLAN



PLAN



ELEVATION



ELEVATION

CAST-IN-PLACE RISER COVER

CONCRETE PIPE RISER COVER

OPTIONAL PRECAST CONCRETE LIFT-OFF COVERS



GENERAL NOTES

Unless otherwise shown in the plans, covers shall be made for each Manhole of the T. & M.

Exposed edges shall conform to 209.

The Contractor may propose alternate structures for the location, size of Manholes, including greater depth. Plans for such proposed alternative shall be submitted to the Engineer for review and approval before construction.

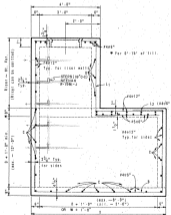
In areas of conflict between reinforcing steel, steel pipes, drains, manhole boxes or other cast-in-place, the reinforcement shall be bent or adjusted to clear as directed by the Engineer.

The riser may be constructed of reinforced concrete or steel or of Manufactured Concrete Pipe, 3600 PSI, in accordance with the Department Code. If pipe is used, joints shall conform to the Code (Manufactured Concrete Pipe Co., Inc.), Product Concrete Lift Off Cover may be substituted for "Lift and Cover".

Connecting pipes shall occur within 12" of ground in either side of Manhole. Use "Cast in Place" rebar, 1/2" diameter, shall be used to stay within this limit.

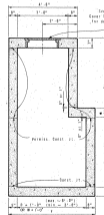


There may be water and/or air wells. The maximum size of pipe shall not be more than 12". Note that one pipe may enter a side, connect to the structure and discharge down. The clear distance between adjacent pipes shall be 6" minimum.

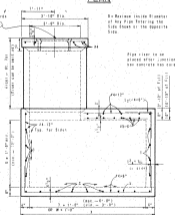


ELEVATION

MANHOLE WITH CAST-IN-PLACE RISER

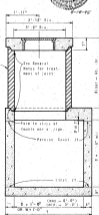


SECTION A-A



ELEVATION

OPTIONAL MANHOLE WITH PIPE RISER



SECTION B-B



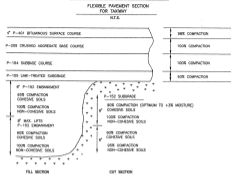
STATE DEPARTMENT OF HIGHWAYS AND PUBLIC TRANSPORTATION

MANHOLE TYPE M
(JUNCTION BOX WITH ACCESS)

MH-M

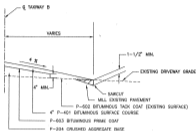
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COMPACTION SCHEDULE



GENERAL ASPHALT PAVING NOTES

- P-132 MATERIAL EXCAVATED FROM AREAS REQUIRING EXCAVATION TO CONFORM TO THE LIME AND GROUND SHOWS ON THE PLANS MAY BE REINCORPORATED INTO IMPROVEMENT AREAS IF APPROVED IN WRITING BY THE ENGINEER. ANY ADDITIONAL GEOTECH MATERIALS REQUIRED FOR FURNISHMENT SHALL BE OBTAINED FROM OFFSITE SOURCES. ALL MATERIALS REQUIRED FOR ENHANCEMENT SHALL MEET ALL THE REQUIREMENTS SPECIFIED IN P-100 EXCAVATION AND ENHANCEMENT. THE MAXIMUM DIAMETER OF ANY ROCK INCORPORATED INTO THE ENHANCEMENT SHALL BE 4 INCHES. ALL COSTS ASSOCIATED WITH FURNISHMENT SHALL BE INCLUDED IN ITEM P-100-4.2 ENHANCEMENT IN PLACE.
- ALL EXISTING AREAS SHALL BE SECTED AND MATCHED IN ACCORDANCE WITH SPECIFICATION 1-201 SECTING AND 1-108 MATCHING.
- P-154 THE CONTRACTOR'S ATTENTION IS DIRECTED TO THE WORKING LOSS ON SHEETS 28-29. IN THE EVENT THAT A SOLID LAYER OF LIMESTONE IS ENCOUNTERED ABOVE THE BOTTOM GRADE OF THE P-100 LIME-TREATED SUBGRADE, THE THICKNESS OF THE SUBGRADE COURSE MAY BE REDUCED SO THAT THE SUBGRADE RESTS DIRECTLY ON THE LIMESTONE. PRIOR TO PLACING SUBGRADE ON TOP OF LIMESTONE, THE TOP OF THE LIMESTONE SHALL BE CLEANED TO REMOVE CLAY AND OTHER UNSOUNDABLE MATERIALS. THE THICKNESS OF THE BITUMINOUS SURFACE COURSE AND THE CRUSHED AGGREGATE BASE SHALL NOT BE REDUCED FROM THE THICKNESSES SHOWN ON THE PLANS.
- P-155 THE CONTRACTOR'S ATTENTION IS DIRECTED TO THE WORKING LOSS ON SHEETS 28-29. IN THE EVENT THAT A SOLID LAYER OF LIMESTONE IS ENCOUNTERED ABOVE THE BOTTOM GRADE OF THE P-100 LIME-TREATED SUBGRADE, THE THICKNESS OF THE LIME-TREATED SUBGRADE MAY BE REDUCED SO THAT THE LIME-TREATED RESTS DIRECTLY ON THE LIMESTONE. PRIOR TO PLACING THE PORTLAND CONCRETE PAVEMENT OR THE BITUMINOUS SURFACE COURSE AND THE CRUSHED AGGREGATE BASE SHALL NOT BE REDUCED FROM THE THICKNESSES SHOWN ON THE PLANS.
- P-401 BITUMINOUS SURFACE, LEVING AND BASE COURSES - THE JOB MIX FORMULA SHALL BE PREPARED BY THE CONTRACTOR IN ACCORDANCE WITH THE SPECIFICATIONS. COURSE LIFTS SHALL BE LIMITED TO 1-1/2" OF LIME-TREATED SUBGRADE SURFACE AND LEVING COURSES AND 7" MAXIMUM THICKNESS FOR BASE COURSES. THICKNESS LIFTS SHALL BE LIMITED IF THE CONTRACTOR CAN PROVE TO THE SATISFACTION OF THE ENGINEER THAT ALL DENSITY REQUIREMENTS ARE MET.
- P-402 ESTIMATED RATE OF APPLICATION IS 0.25 GAL/SY.
- P-403 BITUMINOUS TACK COAT - MATERIAL SHALL BE 58-1# DILUTED AT APPROXIMATELY 50 PERCENT ASPHALT AND 40 PERCENT WATER 90-10. AT THE SAME DILUTION RATE, IS AN ALLOWABLE SUBSTITUTE, IF APPROVED IN ADVANCE BY THE ENGINEER. THE ESTIMATED QUANTITY IS BASED ON AN AVERAGE APPLICATION RATE OF 8.10 GAL/SY OF DILUTE MIXTURE. ASSUMES WITH 1-1/2" P-401 OVERLAY LIFTS. THE ACTUAL RATE ON THE FIELD MAY BE VARYING OR INFLUENCED BY THE ENGINEER, DEPENDING ON THE CONDITION OF SURFACES BEING TACKED.

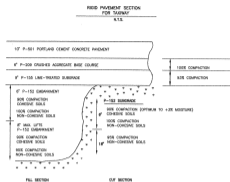


PAVEMENT TIE-IN DETAIL AT ACP TAPER FROM TAXIWAY TO DRIVEWAY

GENERAL PAVING NOTES

- P-100 RATE OF LIME APPLICATION IS ESTIMATED AT 44.55 LBS./SY DRY WEIGHT. RATE MAY BE ADJUSTED BASED ON ACTUAL DRY DENSITY OF SOILS TO ACHIEVE 8% LIME BASED ON DRY WEIGHTS.

COMPACTION SCHEDULE



PAVEMENT DESIGN CRITERIA

FLEXIBLE PAVEMENT TAXIWAY B & CONNECTORS

DUAL WHEELS AT 60,000 LBS.
SUBGRADE IRI = 3.0
EQUIVALENT ANNUAL DEFLECTIONS = 8700

RIGID PAVEMENT TAXIWAY B & CONNECTORS

DUAL WHEELS AT 60,000 LBS.
SUBGRADE FUNDATION MODULUS = 50
EQUIVALENT ANNUAL DEFLECTIONS = 8700

GENERAL CONCRETE PAVING NOTES

- P-501 PORTLAND CEMENT CONCRETE PAVEMENT - THE CONTRACTOR SHALL PREPARE THE JOB MIX FORMULA IN ACCORDANCE WITH THE SPECIFICATIONS. ALL JOINTS SHALL BE CAREFULLY FINISHED. OTHER METHODS CAN BE SUBMITTED TO PRODUCE EQUAL RESULTS AND APPROVED BY THE ENGINEER. THE CONTRACTOR SHALL FLUSH THE JOINT CURT WITH WATER IMMEDIATELY AFTER THE SECOND PAVING OF THE JOINT. THE CONTRACTOR SHALL EXERCISE CAREFUL CARE DURING AND AFTER PAVING OPERATIONS TO AS NOT TO FILL OPEN JOINTS WITH SLURRY, DEBRIS, SAND, ETC. PRIOR TO JOINT SEALING. SUBSEQUENT JOINT ALIGNMENT SHALL MATCH EXISTING JOINTS. ALL JOINT SEALING SHALL BE INDICATED TO ITEM P-501.
- P-603 JOINT SEALING FILLER - ALL JOINT SEALING FOR PAVEMENT SHALL BE IDENTICAL TO ITEM P-501. THE CONTRACTOR SHALL FURNISH THE ENGINEER WITH HIS PLAN FOR JOINT CONSTRUCTION INCLUDING MATERIAL SAMPLES AND TEST DATA TO DEMONSTRATE ITS ACCEPTANCE. UNLESS THE SPECIFICATIONS ALL JOINTS, NEW AND EXISTING, SHALL BE THOROUGHLY CLEANED OF ALL SLURRY, DEBRIS, SAND, AND OR ANY OTHER NON-COMPRESSIBLE MATERIAL. IMPACTED PRIOR TO SEALING. SEALING WITH COMPRESSED AIR OR HE-AIRING, CORE MATERIAL, SHALL NOT BE USED.

DATE	1-2-78
BY	W. B. B. / J. B. B.
CHKD	J. B. B.
APP'D	J. B. B.
NO.	40-200000
DATE	1-2-78

URS Greiner
INCORPORATED
100 W. GARDEN CITY BLVD.
GARDEN CITY, N.Y. 11530
(516) 461-0000

Engineers, Architects
and Planners

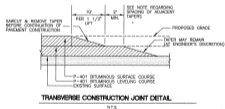
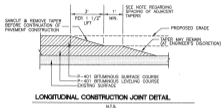
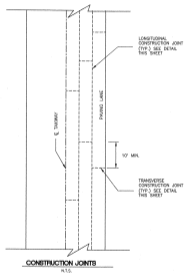


ADDISON AIRPORT

PAVING DETAILS



Sheet
26
of 30



CONSTRUCTION JOINT DETAILS

- CONSTRUCTION JOINTS OCCURRING IN 1-1/2" SURFACE COURSE FROM PREVIOUS DAYS CONSTRUCTION SHALL BE SANDED AND REMOVED PRIOR TO CONTINUATION OF NEW ADJACENT PAVEMENT CONSTRUCTION. TAPERS MAY BE ELIMINATED AT THE ENGINEER'S DISCRETION, IF PAVING LANES ARE NOT OPEN TO TRAFFIC.
- CONSTRUCTION JOINTS OCCURRING IN LEVELING COURSE MAY REMAIN IN PLACE AT THE ENGINEER'S DISCRETION.
- THE CONTRACTOR SHALL MAKE EVERY EFFORT TO COMPLETE A DAYS OPERATION ACROSS THE FULL WIDTH OF THE PAVEMENT IN ORDER TO MINIMIZE THE AMOUNT OF LONGITUDINAL CONSTRUCTION JOINTS.
- THE LONGITUDINAL JOINT IN ONE LAYER SHALL OFFSET THAT IN THE LAYER IMMEDIATELY BELOW BY AT LEAST ONE FOOT (30 CM). HOWEVER, THE JOINT IN THE TOP LAYER SHALL BE AT THE CENTERLINE OF THE PAVEMENT. TRANSVERSE JOINTS IN ONE LAYER SHALL BE OFFSET BY AT LEAST TWO FEET (60 CM) FROM TRANSVERSE JOINTS IN PREVIOUS LAYERS. TRANSVERSE JOINTS IN ADJACENT LINES SHALL BE OFFSET A MINIMUM OF 10 FEET (3 M).
- NO SEPARATE PAYMENT SHALL BE MADE FOR CONSTRUCTION JOINTS (TAPERS). COST FOR CONSTRUCTION AND REMOVAL OF CONSTRUCTION JOINTS (TAPERS) SHALL BE INCIDENTAL TO ITEM P-401. BITUMINOUS MATERIALS USED IN CONSTRUCTION OF JOINTS (TAPERS) SHALL BE PAID AT THE CONTRACT UNIT PRICE OF ITEM P-401.



DATE: 11-17
 DRAWN: J.S.B.
 CHECKED: J.S.B.

PROJECT NO. 2004-0001
 SHEET NO. 27.0
 OF 27.0

DATE: 11-17

REVISIONS

URS Greiner
 FOR ADDISON AIRPORT, 2004-0001
 217.540.0401

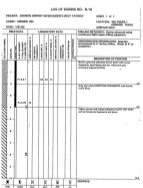
Engineers, Architects
 and Planners

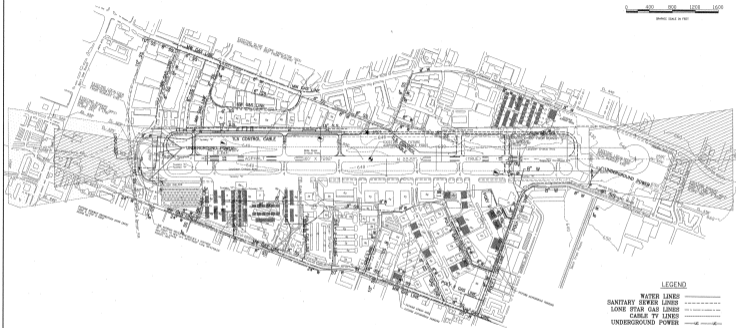
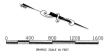


ADDISON AIRPORT

ASPHALT JOINT DETAILS

SHEET
 27
 OF 27





- LEGEND**
- WATER LINES ————
 - SANITARY SEWER LINES - - - - -
 - LONE STAR GAS LINES - · - · -
 - CABLE TV LINES ————
 - UNDERGROUND POWER ————



THE SEAL APPLICABLE ON THIS DRAWING WAS REGISTERED BY STATE & FEDERAL SEAL.

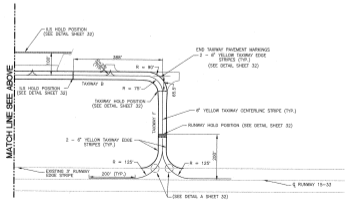
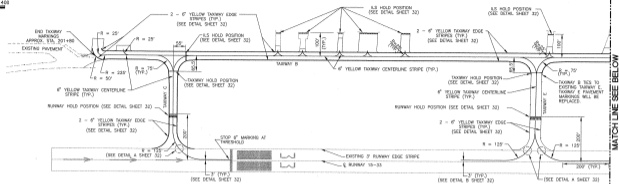
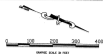
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DESIGNED BY: [Signature]	CHECKED BY: [Signature]	PROJECT NO. 2000000000
DATE: 2-17	BY: [Signature]	SCALE: AS SHOWN

Geber, Inc.
 Engineers, Architects
 and Planners
 127 West, Texas



ADDISON AIRPORT

UTILITY PLAN



MATCH LINE SEE BELOW

MATCH LINE SEE ABOVE



DATE: 04/14/10	DESIGNED BY: J. L. ALLEN	CHECKED BY: J. L. ALLEN	DATE: 04/14/10
DRAWN BY: J. L. ALLEN	APPROVED BY: J. L. ALLEN	DATE: 04/14/10	
SCALE: 1"=100'	PROJECT: ATTERHOLM		

URS Greiner
 Engineers, Architects
 and Planners

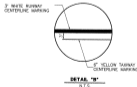
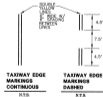
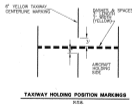
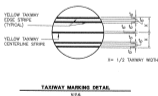
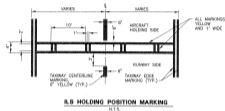
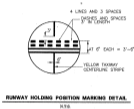
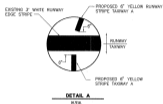


ADDISON AIRPORT

PAVEMENT MARKING PLAN

GENERAL MARKING NOTES

1. ALL TAXIWAY MARKING SHALL BE REFLECTIVE PURVON YELLOW.
2. EXISTING TAXIWAY MARKINGS SHALL BE REMOVED TO THE APPROVAL OF THE ENGINEER, PRIOR TO REPAIRING FOR PROPOSED STRIPING LAYOUT.
3. ALL MARKINGS SHALL BE IN ACCORDANCE WITH SPECIFICATION 3-620.
4. HAND SPRAYING IS NOT ACCEPTABLE.
5. ANY MARKING TO BE OBLITERATED SHALL BE INCIDENTAL TO THE COST OF P-SIDE MARKING.
6. ALL MARKINGS ON CONCRETE PAVEMENT SHALL BE OUTLINED WITH 1/2" WIDE BLACK PAINT BORDER (SEE SPEC. IT-3-111) EXCEPT WHERE ADJACENT TO SHOULDER.



DATE: 12/12/10	DATE: 12/12/10	DATE: 12/12/10	DATE: 12/12/10
BY: JTB	BY: JTB	BY: JTB	BY: JTB
CHECKED: JTB	CHECKED: JTB	CHECKED: JTB	CHECKED: JTB
DATE: 12/12/10	DATE: 12/12/10	DATE: 12/12/10	DATE: 12/12/10
BY: JTB	BY: JTB	BY: JTB	BY: JTB

URS Greiner
440 West Loop West, Suite 400
Houston, TX 77027
Tel: 713.249.2000
Fax: 713.249.2001

Engineers, Architects
and Planners



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PAVEMENT MARKING DETAILS

SHEET 32
OF 32

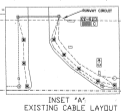
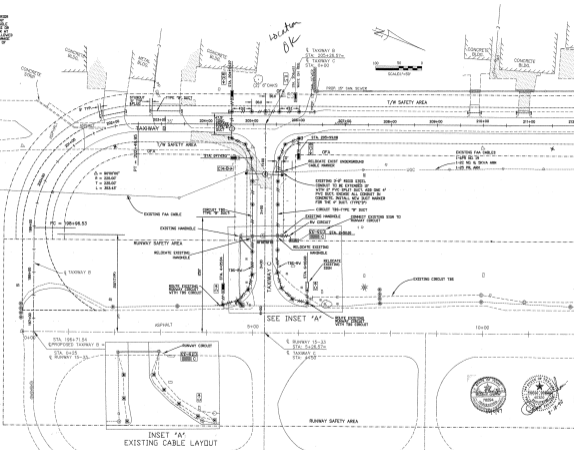
CAUTION UNDERGROUND UTILITIES

CONTACT UTILITY COMPANIES AND FAA AT LEAST 24 HOURS PRIOR TO CONSTRUCTION TO LOCATE AND IDENTIFY ANY EXISTING UTILITIES IN THE PROPOSED PROJECT AREA. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE LOCATION AND DEPTH OF ALL UTILITIES. THE CONTRACTOR SHALL NOT ALLOW ANY WORK TO PROCEED UNTIL ALL UTILITIES ARE IDENTIFIED AND THE DATA IS RECORDED AS SHOWN IN REVISIONS TO THESE DRAWINGS. NO WORK SHALL BE PERFORMED IN THE PROJECT AREA UNLESS THE CONTRACTOR HAS OBTAINED THE NECESSARY PERMITS FROM ALL AFFECTED AGENCIES.

location OK

LEGEND	
○	PINNY POLE
◊	FIRE EXTINGUISHER
■	EXPOSED LIGHT
◡	OVERHEAD ELECTRIC
▽	WATER VALVE
◇	PIPE JOINT
▲	EDGE OF ASPHALT
■	ELECTRIC BOX
□	BOX
◐	UNDER GROUND CABLE MARK
+	ARROW SHALL LOCATION
○	DISTANCE TO GO SIGN
◆	CONC. PAV.
■	CONCRETE PAVEMENT
■	NEW 4" CONC. CURB
■	CONCRETE CURB
○	EXIST. DISTANCE SIGN
○	NEW TYP. STREET
○	EXISTING TYP. STREET
○	NEW TYP. MARKERS
○	EXISTING TYP. MARKERS
○	EXISTING HANGAR
○	NEW HANGAR
○	CONCRETE HANGAR
○	24" PVC DUCT W
○	24" PVC DUCT W
○	3" RISER
○	2" RISER & CAP

* NUMBER IN CIRCLE INDICATES SIZE OF CONCRETE LINED INSIDE NUMBER OF DUCT.



MATCH LINE-SEE SHEET 34



DATE: 3-22-2011	SCALE: 1/8" = 1'-0"
BY: JLS	PROJECT: 1048-24860-10
CHECKED: JLS	NO. OF SHEETS: 24
DATE: 3-22-2011	DATE: 3-22-2011
DATE: 3-22-2011	DATE: 3-22-2011
DATE: 3-22-2011	DATE: 3-22-2011

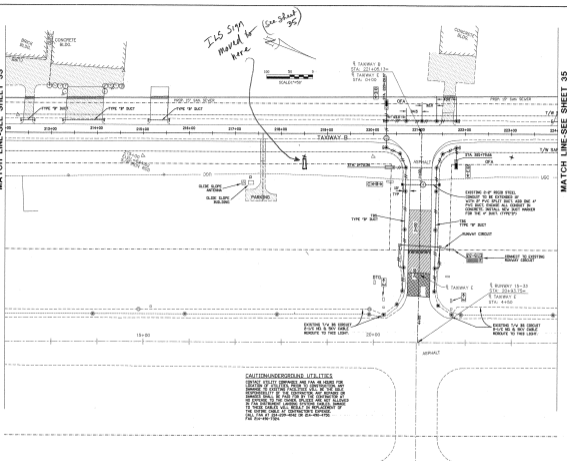
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ADDISON AIRPORT

TAXIWAY "B" LIGHTING LAYOUT-STA. 201+50 TO 212+00

MATCH LINE-SEE SHEET 33



MATCH LINE-SEE SHEET 35

1	1" LIP LED
2	FLASH LIGHT
3	FLASH LIGHT
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97	FLASH LIGHT
98	FLASH LIGHT
99	FLASH LIGHT
100	FLASH LIGHT

NUMBERS IN CIRCLES INDICATE SIZE OF CONCRETE LIGHT INDICATE NUMBER OF LIGHTS.



DATE: 12/31/23
DRAWN BY: J.S.
CHECKED BY: J.S.
SCALE: 1" = 50'

PROJECT NO.: 23000001
SHEET NO.: 34 OF 34
JOB NO.: 23000001

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Greener, Inc.
100 W. 10th St.

ASD
ENGINEERING
ARCHITECTURE



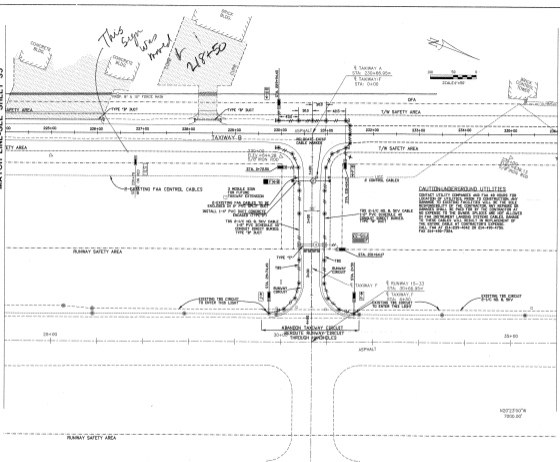
ADDISON AIRPORT

TAXIWAY "B" LIGHTING
LIGHTING LAYOUT-STA. 212+00 TO 224+00

SHEET
34
OF 34

MATCH LINE-SEE SHEET 35

STA. 236+00



CAUTION-UNDERGROUND UTILITIES
CONTACT UTILITY COMPANIES AND THE AIR FORCE FOR
LOCATIONS OF UTILITIES PRIOR TO CONSTRUCTION. ANY
DAMAGE TO EXISTING UTILITIES WILL BE THE SOLE
RESPONSIBILITY OF THE CONTRACTOR. NO WORKMAN IS
TO BE EXPOSED TO THE UNDERGROUNDS AND NOT ALLOWED
TO PROCEED UNTIL ALL NECESSARY CHECKS ARE MADE
TO THESE CABLES WILL BE MADE IN A RECORD BOOK OF
THE WORK. CALLS BY CONTRACTORS' OFFICE
AND CALL AT 812-493-1142 IN 84-99-4776.
140 24-1100-024

□	EDGE LIGHT
□	FIXED WALE
□	FIRE EXTINGUISHER
□	FLARE LIGHT
□	FLYING WHEEL LIGHT
△	WATER VALVE
○	BOX VENT
×	EDGE OF ASPHALT
⊗	ELECTRIC BOX
□	SIGN
□	UNDER BRIDGE DRAIN
+	BORING HOLE LOCATION
+	REFLECTION TO BE DONE
□	CONE PILE
□	CONCRETE PAVEMENT
□	NEW 4" DIA. CONC. & GRN
□	CONCRETE FORM
□	EXIST. BRASS SIGN
□	NEW T.V. CIRCUIT
□	EXISTING T.V. CIRCUIT
□	EXISTING R.V. CIRCUIT
□	EXISTING T.V. CIRCUIT
□	EXISTING R.V. LIGHT
□	EXISTING 1/2" HOLE
□	NEW 1/2" HOLE
□	EXISTING 1/2" HOLE
□	NEW 1/2" HOLE
□	EXISTING UNDERLAY
□	NEW UNDERLAY
□	EXISTING UNDERLAY
□	NEW UNDERLAY
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□	NEW UNDERLAY
□	EXISTING UNDERLAY
□	NEW UNDERLAY

* NUMBER IN CIRCLE INDICATES SIZE OF
CIRCUIT LINES INDICATE SIZES OF
SIGNALS



DESIGN: J.R.S.	PROJECT NO.: 0414 2336				
DATE: 2/83	STATIONING: STA. 224+00 TO 236+00				
DRAWN: J.R.S.	SCALE: AS SHOWN				
DATE: 11/83					

Greiner, Inc.
P.O. Box 200
Ft. Worth, Texas

ASD
ASSOCIATED SIGN DESIGN



ADDISON AIRPORT

**TAXIWAY 'B' LIGHTING
LIGHTING LAYOUT-STA. 224+00 TO 236+00**

SHEET 35

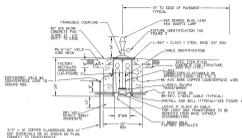


FIGURE 1
L-8611 ELEVATOR TV LIGHTING INSTALLATION
103

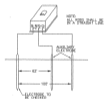


FIGURE 2
CABLE SPLICES
102

GROUNDING TEST - NOTING ANY WORK IS COMPLETED BY THE OWNER, THIS DATA SHALL BE TESTED IN THE PRESENCE OF THE DESIGNER AS WRITTEN BELOW IN THE RESULTS OF EACH INSPECTION. THIS DATA IS PREPARED AND SIGNED BY THE CONTRACTOR AND INDICATES A PROJECT ACCEPTANCE AND SUFFICIENTLY PROTECTS THE CONTRACTOR AGAINST ANY LITIGATION. THIS DATA IS NOT TO BE USED FOR ANY OTHER PURPOSES. THIS DATA IS THE PROPERTY OF THE CONTRACTOR AND SHALL BE KEPT IN CONFIDENCE. THIS DATA IS NOT TO BE USED FOR ANY OTHER PURPOSES. THIS DATA IS THE PROPERTY OF THE CONTRACTOR AND SHALL BE KEPT IN CONFIDENCE. THIS DATA IS NOT TO BE USED FOR ANY OTHER PURPOSES. THIS DATA IS THE PROPERTY OF THE CONTRACTOR AND SHALL BE KEPT IN CONFIDENCE.

FIGURE 3
GROUND TEST - FALL OF POTENTIAL METHOD
101

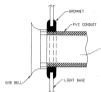
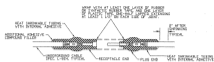


FIGURE 4
CONDUIT ENTRANCE TO BASE
100



FIGURE 5
FIXTURE IDENTIFICATION TAG
99

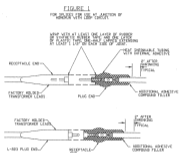


FIGURE 6
FOR SPLICES AT REMOTE LIGHTS
98

FIGURE 7
CABLE SPLICES
97

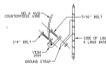


FIGURE 7
FACTORY GROUND LUG DETAIL
96



DATE: 10/1/88	BY: J. J. JONES	NO. 1000
REV. 10/1/88	BY: J. J. JONES	NO. 1000
REV. 10/1/88	BY: J. J. JONES	NO. 1000

Greiner, Inc.
For Work, Test

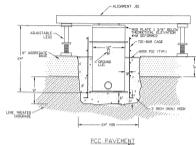
ASO
ADDISON AIRPORT



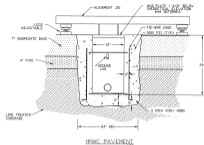
ADDISON AIRPORT

TAXIWAY "B" LIGHTING
TAXIWAY EDGE LIGHTS & CABLE DETAIL

SHEET
36

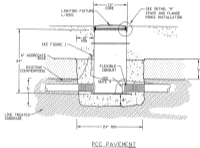


PCC PAVEMENT

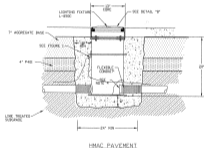


HMAc PAVEMENT

ALIGNMENT JIG DETAIL
NO REFERENCE EDGE AVAILABLE



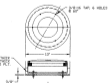
PCC PAVEMENT



HMAc PAVEMENT

NOTES:

1. THE USE OF HOT PLASTIC CONSTRUCTION PAPER, EXTENDING ONE DECADEMENT BY EACH TRANSVERSE.
2. OPERATIONAL TOLERANCE FOR THE HOLE IS 1/4" SOURCE FROM THE ADJUSTED DESIGN ON PLANS.
3. 1-1/2" HOLE IN A 12" X 12" L-100 BASE.
4. CONCRETE SHALL STRUCTURE INTO LIGHT BASE JOIST FOR P.V.C. INCLUDING TOP BELL.
5. ADD APPROPRIATE DIMENSIONS OR WEIGHT TO TOP TO PREVENT FLOATING.
6. HMAc CONCRETE UNDER BASE TO ASSURE THERE IS NO HOSE IN CONCRETE UNDER BASE.
7. HOLE SHALL BE TWO FEET OF BLACK DRAIN BUTTER OF HOLE TO HOLE ANCHORS JACKET AND DRILL TERMINATION.



DETAIL 'B'

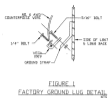
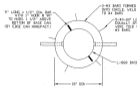


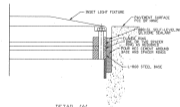
FIGURE 1
FACTORY GROUND LUG DETAIL



PLAN

ELEVATION

TIE BAR CAGE DETAIL



DETAIL 'A'

NOTES:

1. FIXTURE EDGE SHALL BE FLUSH WITH FINISHED GRADE.
2. ATTACH THE JOIST FEATURE TO THE LIGHT BASE WITH PROTECTED STEEL BOLTS AND WEDGED LOCKING ANCHORS AT 4" SPACES (COMMENCED BY THE MANUFACTURER OF THE LIGHT BASE) TO THE JOIST. (SEE PLAN) AND A 3/8" DIA. BOLT AT EACH END. (SEE ELEVATION) SHALL BE PLACED IN PLANS. HOLE FOR THE LIGHT-BASE FLANGE.



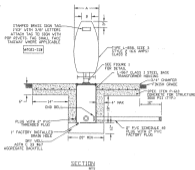
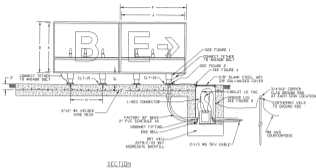
Drawn: J.C.C.	Revised: 04/11/2018	Project: 18-00000000	Sheet: 37
Checked: J.C.C.	Revised: 04/11/2018	Project: 18-00000000	Sheet: 37
Scale: 1"=4'-0"	Revised: 04/11/2018	Project: 18-00000000	Sheet: 37

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P.O. Box 1000
Fort Worth, Texas



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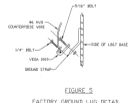
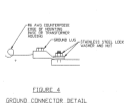
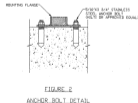
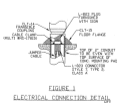
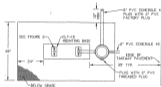
TAXIWAY "B" LIGHTING
RUNWAY SEMI-FLUSH LIGHT DETAIL

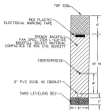


ITEM	DESCRIPTION
1	STAMPED BRASS SIGN TAG
2	STAMPED BRASS SIGN TAG ATTACH TAG TO SIGN WITH PER 304(1) TAG SMALL TAG TAGGING NUMBER APPLICABLE
3	STAMPED BRASS SIGN TAG
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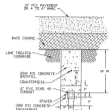
SIZE 3 SIGN DIMENSIONS

- NOTES
1. SIGN CASE IS CONSTRUCTED OF ALUMINUM (NO CORROSION PROTECTION) OR ALUMINUM TOP IS PROVIDED FOR LIGHTED PANEL.
 2. LIGHT SHALL BE PROVIDED BY STANDARD FLUORESCENT LAMP.
 3. MOUNTING SHALL BE WITH FRAMING OR OTHERWISE.
 4. SIGN SHALL HAVE LETTERING AND BACKGROUND COLOR AS PER SPECIFICATIONS.
 5. SIGNAGE SHALL BE CONFORM TO ALL APPLICABLE FEDERAL, STATE AND LOCAL REGULATIONS AND STANDARDS.
 6. SIGNAGE SHALL BE DURABLE.
 7. SIGN FACE SHALL BE LEVEL INFORMATION AS SHOWN ON THE PLAN. SIGN FACE SHALL HAVE SIGNAGE SHALL HAVE BLACK PANELS.

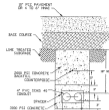




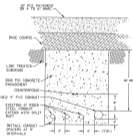
2" PVC ONE-WAY ELECTRICAL DUCT-TYPE "A"
1/12



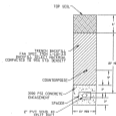
2" PVC ONE-WAY ELECTRICAL DUCT-TYPE "B"
UNDER PAVEMENT
1/12



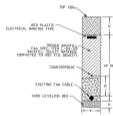
2" PVC TWO-WAY ELECTRICAL DUCT-TYPE "C"
UNDER PAVEMENT
1/12



THREE & FOUR-WAY FAA DUCTS-TYPE "D", "E" & "F"
UNDER PAVEMENT
1/12



2" PVC ONE-WAY SPLIT FAA DUCT-TYPE "G"
1/12
NOTE: OVER STORM DRAIN - ENCASED FIBER CABLE IF MORE THAN ONE CABLE
MAINTAIN 4" CLEARANCE BETWEEN CONDUITS.



SAND ENCASE FAA CABLE-TYPE "H"
1/12

NOTES PERTAIN TO FAA CABLE DUCTS

- THAWAY "A" EXTEND 3 EXISTING 3" REINFORCED CONCRETE CONTAINING 3-#4 BARS (CONCRETE CABLE EACH END) WITH SAND VIBRO AMM. CABLE BEING PVC SPLIT DUCT (EXTENDS TO SECOND EDGE OF THAWAY).
INSTALL ONE 4" PVC CONDUIT WITH NYLON FILL, 1/2" PLUGS OR CAP BOTH ENDS.
- THAWAY "B" EXTEND 3 EXISTING 3" REINFORCED CONCRETE CONTAINING 3-#4 BARS (CONCRETE CABLE EACH END) WITH SAND VIBRO AMM. CABLE BEING PVC SPLIT DUCT (EXTENDS TO SECOND EDGE OF THAWAY).
INSTALL ONE 4" PVC CONDUIT WITH NYLON FILL, 1/2" PLUGS OR CAP BOTH ENDS.
- THAWAY "C" INCLUDE 2 EXISTING FIBER CABLE WITH 4" PVC SPLIT DUCT (3" BEHIND THE SUBWAY EDGE) INSTALL ONE 4" PVC CONDUIT WITH NYLON FILL, 1/2" PLUGS OR CAP EACH END.

GENERAL NOTES:

1. THIS NOTE IS APPLIED TO ALL UNDER PAVEMENT DUCTS.
2. THAWAY "A" & "B" DUCTS SHALL BE INSTALLED AFTER THE PLACEMENT OF THE 4" LINE TRINCO BRACKET AND PRIOR TO THE PLACEMENT OF 4" GRADED AGGREGATE BASE.
3. THAWAY "C" & "D" DUCTS SHALL BE INSTALLED AFTER THE PLACEMENT OF THE SUBWAY AND PRIOR TO THE PLACEMENT OF THE 4" FIBER FIBER THE RETAINABLE BASE COURSE.
4. REINFORCED CONCRETE PAVEMENT TYPICAL SECTION SHEET NO. 5 AND AIRPORT PAVEMENT TYPICAL SECTION SHEET NO. 6 FOR ACTUAL BASE COURSE DEPTH FOR VARIOUS THAWAYS AND TYPE OF FINISH COURSE.



DESIGN: J.E.A.	DATE: 01/15/2012
CHECK: J.E.A.	APP. NO. 20-000000-00
DATE: 12-01-12	REV. NO. 00-00
SCALE: 1"=20'	AN NO. 200000

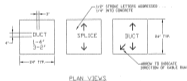
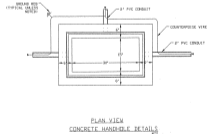
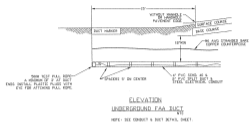
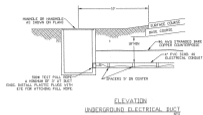
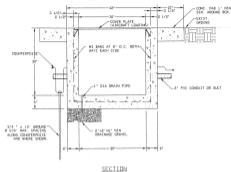
DATE	REVISION	BY

Groher, Inc.
10000 W. 10th Street
Overland Park, KS 66204



ADDISON AIRPORT

TAXIWAY "B" LIGHTING
ELECTRICAL CONDUIT & DUCT DETAIL



DATE	BY	CHKD BY	APP'D BY
10/19/00	J. B. BROWN		
10/19/00	J. B. BROWN		
10/19/00	J. B. BROWN		
10/19/00	J. B. BROWN		

Greiner, Inc.
CONCRETE, T.C.
100 W. 10th St.

ASD
CONCRETE, T.C.
100 W. 10th St.



ADDISON AIRPORT

TAXIWAY "B" LIGHTING
HANDHOLE DETAIL

SHEET
40

CAUTION: UNDERGROUND UTILITIES

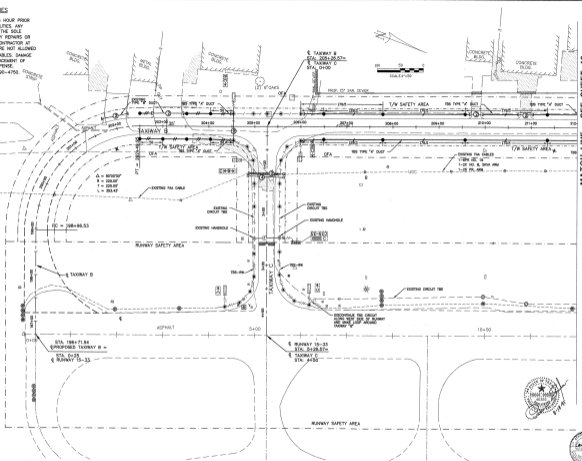
CONTACT UTILITY COMPANIES AND 72 HRS PRIOR TO CONSTRUCTION FOR LOCATION OF UTILITIES. ANY CHANGE TO EXISTING FACILITIES WILL BE THE SOLE RESPONSIBILITY OF THE CONTRACTOR. ANY REPAIRS OR CHANGES SHALL BE PAID FOR BY THE CONTRACTOR AT NO EXPENSE TO THE OWNER. SPACES ARE NOT ALLOWED IN PAV INTERMEDIATE LANDING SYSTEM CABLES. CHANGE TO THESE CABLES WILL RESULT IN REPLACEMENT OF THE EXISTING CABLE AT CONTRACTOR'S EXPENSE.
CALL FAA AT 214-239-4042 OR 214-460-4750.
FAA 214-460-7204.

LEGEND	
●	POWER POLE
○	LINE SPUR
⊖	TRUCK LIGHT
⊕	OVERHEAD ELECTRIC
△	TRUCK HOLE
○	DOT MARK
○	ELECTRIC SIGN
○	MARK
○	UNDER GROUND CABLE MARK
○	DOWN HOLE LOCATION
○	SPURANCE TO 20 BUSH
○	CONC. PILE
○	CONCRETE PAVEMENT
○	CONCRETE CURB
○	CONCRETE BLOCK
○	EXIST. GRAVITY SIGN
○	EXIST. 1/2" DUCT
○	EXISTING 2" W/ LIGHT
○	EXISTING 1/2" DUCT
○	EXISTING RUNWAY LIGHT
○	EXISTING MARKING
○	EXISTING SIGNAGE
○	EXIST. HANDHOLE
○	EXIST. UNDERGROUND
○	EXIST. UNDERGROUND
○	EXIST. UNDERGROUND
○	EXIST. UNDERGROUND
○	EXISTING CLUT MARK
○	2" - 1" PFC DUCT *
○	1" - 1" PFC DUCT *
○	DOT MARKER
○	2" SUBSOIL & GP

* NUMBER IN CIRCLE INDICATES SIZE OF DUCT. LINES INDICATE NUMBER OF DUCTS.

ALTERNATE NO. 4
 1. EXIST. UNDERGROUND
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 98. EXIST. UNDERGROUND
 99. EXIST. UNDERGROUND
 100. EXIST. UNDERGROUND

DATE: 4-2-02	PROJECT NO.: 2011-2020
ISSUE NO.: 1	DATE: 04-02-02
SCALE: 1" = 20'	BY: J. B. BARNES



MATCH LINE - SEE SHEET 42



Greiner, Inc.
Fort Worth, Texas

ASD ENGINEERS

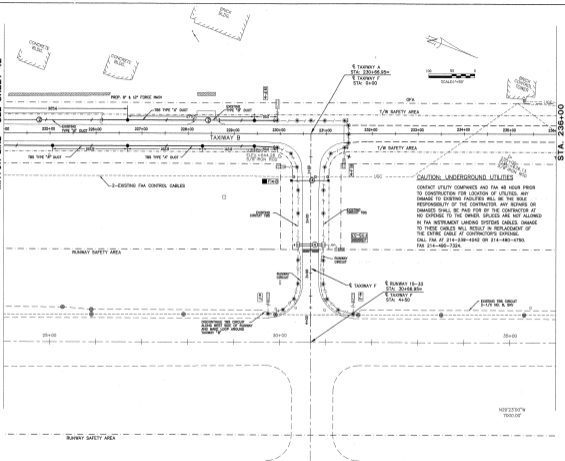


ADDISON AIRPORT

ADDITIVE ALT. NO. 34 - TAXIWAY "B"
LIGHTING LAYOUT - STA. 201+50 TO 212+00

3/8/02
41
REV. 02/02

MATCH LINE - SEE SHEET 42



STA. 236+00

CAUTION: UNDERGROUND UTILITIES

CONTACT UTILITY COMPANIES AND FAA 48 HOURS PRIOR TO CONSTRUCTION FOR LOCATION OF UTILITIES. ANY DAMAGE TO EXISTING FACILITIES WILL BE THE SOLE RESPONSIBILITY OF THE CONTRACTOR. ANY REPAIRS OR DAMAGES SHALL BE PAID FOR BY THE CONTRACTOR AT NO EXPENSE TO THE OWNER. SPIKES ARE NOT ALLOWED IN FAA INSTRUMENT LANDING SYSTEMS CABLES. DAMAGE TO THESE CABLES WILL RESULT IN REPLACEMENT OF THE ENTIRE CABLE AT CONTRACTOR'S EXPENSE. CALL FAA AT 214-338-4242 OR 214-482-5793. FAX 214-482-7324.

LEGEND	
○	POWER POLE
□	FIRE HYDRANT
⊗	WELDER LIGHT
⊕	EXISTING EXTERIOR
△	WATER VALVE
○	5/4" WIRE
□	ELECTRIC BOX
○	SPW
○	UNDER AND OVER WIRE
○	BEARING HOLE LOCATION
○	DEFINITE TO GO AWAY
○	CONC. PAD
○	CONCRETE FOOTING
○	EXIST. SIGNALING BOX
○	NEW 1/4" CIRCUIT
○	EXISTING 1/4" CIRCUIT
○	EXISTING 3/4" CIRCUIT
○	EXISTING 1/4" WIRE
○	EXISTING RUNWAY LIGHT
○	EXISTING TAXIWAY LIGHT
○	EXISTING MIDDLE
○	EXISTING 1/4" WIRE
○	EXISTING 3/4" WIRE
○	NEW 1/4" WIRE
○	NEW 3/4" WIRE
○	EXISTING SLUG SIGN
○	2-1/2" DIA. SLUG
○	1-1/2" DIA. SLUG
○	SLUG MARKER
○	EXISTING 1/4" WIRE

* NUMBER IN CIRCLE INDICATES SIDE OF CONDUIT LINES INDICATE NUMBER OF SLOTS.

ALTERNATE NO. 4
 • CONDUIT LINES
 ○ CONDUIT LINES
 ○ CONDUIT LINES
 ○ CONDUIT LINES



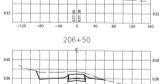
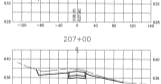
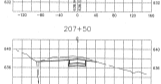
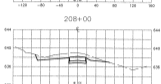
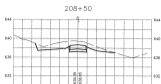
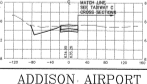
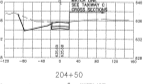
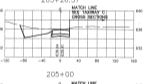
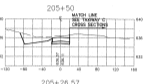
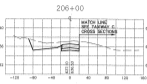
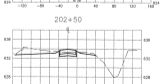
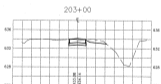
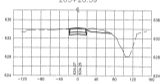
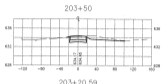
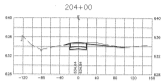
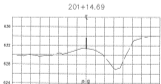
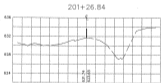
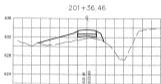
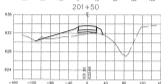
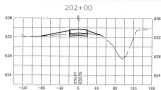
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CHECKED BY: J.S.	DATE: 10/27/2010	DATE: 10/27/2010	DATE: 10/27/2010
DATE: 10/27/2010	DATE: 10/27/2010	DATE: 10/27/2010	DATE: 10/27/2010

Graber, Inc.
 201 W. 15th, Suite 200
 Fort Worth, Texas 76102



ADDISON AIRPORT

ADDITIVE BID NO. 34- TAXIWAY "B"
 LIGHTING LAYOUT - STA. 224+00 TO 236+00



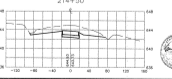
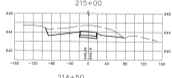
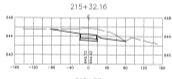
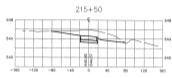
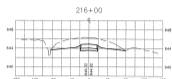
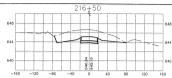
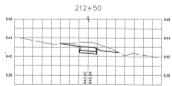
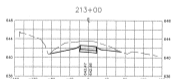
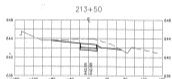
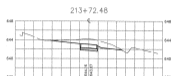
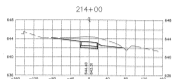
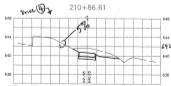
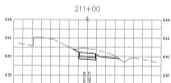
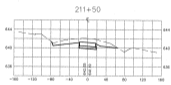
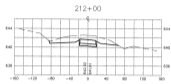
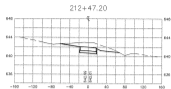
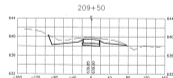
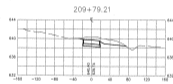
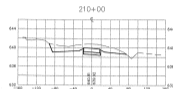
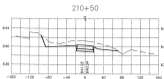
DATE	BY	CHKD	APP'D
01/11/11	URS	URS	URS
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01/11/11	URS	URS	URS

URS Greiner
Engineers, Architects
and Planners



ADDISON AIRPORT

**WEST TAXIWAY "B" & DRAINAGE IMPROVEMENTS
TAXIWAY B CROSS SECTIONS**



Drawn: J.L.P.	Check: J.L.P.	Scale: AS SHOWN
Design: J.L.P.	Reviewed: J.L.P.	Date: 02/08/10
Project: 21000000	Sheet: 45 of 45	

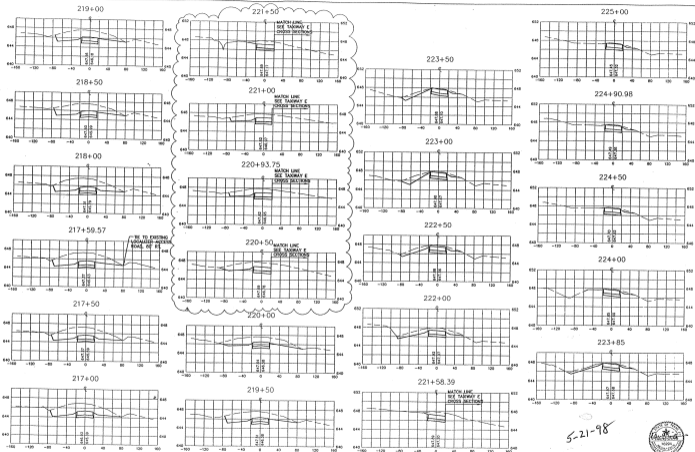
URS Greiner
 Engineers, Architects
 and Planners



ADDISON AIRPORT

**WEST TAXIWAY "B" & DRAINAGE IMPROVEMENTS
 TAXIWAY B CROSS SECTIONS**

SHEET 45



DATE	BY	CHKD BY	APP'D BY
10/11/98	J.E.	J.E.	J.E.
10/11/98	J.E.	J.E.	J.E.
10/11/98	J.E.	J.E.	J.E.

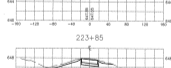
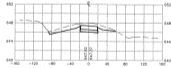
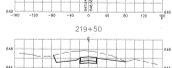
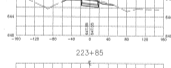
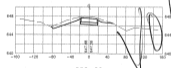
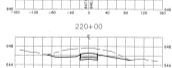
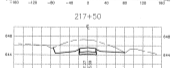
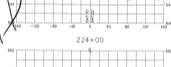
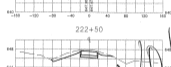
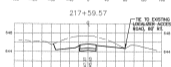
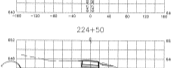
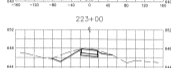
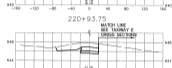
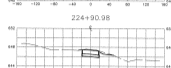
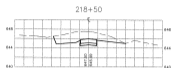
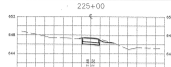
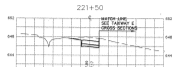
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ADDISON AIRPORT

WEST TAXIWAY "B" & DRAINAGE IMPROVEMENTS
 TAXIWAY B CROSS SECTIONS

SWEET
46

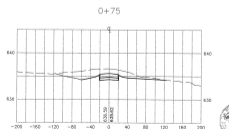
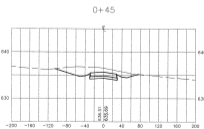
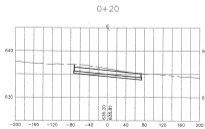
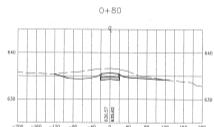
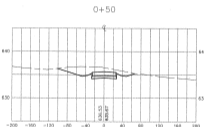
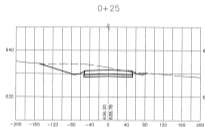
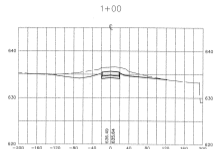
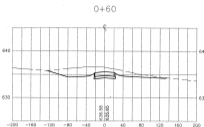
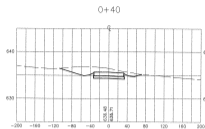


DESIGN: E.A.C.	DATE: 02.12.2007	PROJECT: WEST TAXIWAY 'B' & DRAINAGE IMPROVEMENTS
DRAWN: J.L.B.	NO. OF: 02.01	
CHECKED: J.L.B.	DATE: 02.08.07	
SCALE: 1" = 20'-0"		

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**WEST TAXIWAY 'B' & DRAINAGE IMPROVEMENTS
TAXIWAY B CROSS SECTIONS**



DATE: 1-1-04
 DRAWN BY: JAC
 CHECKED BY: JAC
 IN CHARGE: JAC

PROJECT NO: WEST TAXIWAY 'B' & DRAINAGE IMPROVEMENTS
 SHEET NO: 48 OF 54
 CONTRACT NO: 2770001.01

NO.	REVISION	BY

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 ENGINEERS, ARCHITECTS
 AND PLANNERS
 200 West Loop East, Suite 100
 Houston, Texas 77027
 Tel: 713-865-8900

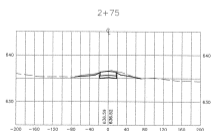
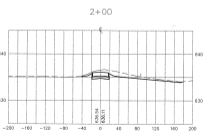
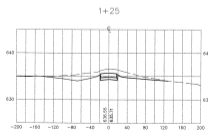
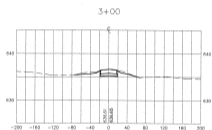
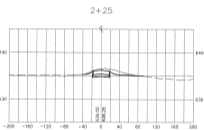
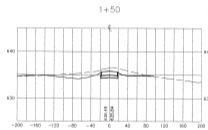
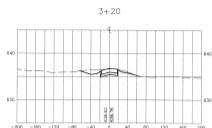
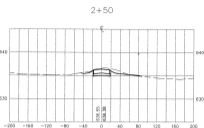
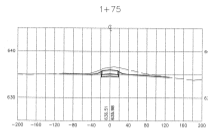
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**WEST TAXIWAY "B" & DRAINAGE IMPROVEMENTS
 TAXIWAY C CROSS SECTIONS**

3/007
48



DATE	12/15/00	REVISION	
BY	ALM	NO.	
CHKD	ALM	DATE	
APP'D	ALM	DATE	
DATE	12/15/00	BY	

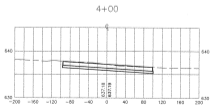
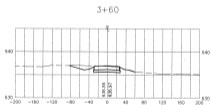
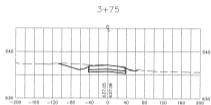
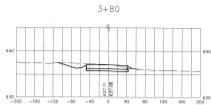
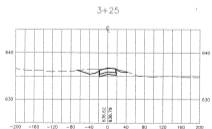
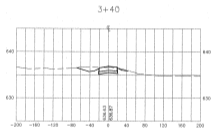
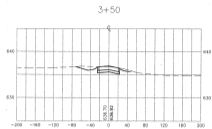
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**WEST TAXIWAY 'B' & DRAINAGE IMPROVEMENTS
 TAXIWAY C CROSS SECTIONS**

**SHEET
 49**



DESIGN	T.A.C.	DATE	10/21/00
DRAWN	P.L.C.	SCALE	AS SHOWN
CHECKED	P.L.C.	APP. NO.	200000000
PROJECT	ADDISON AIRPORT	BY	

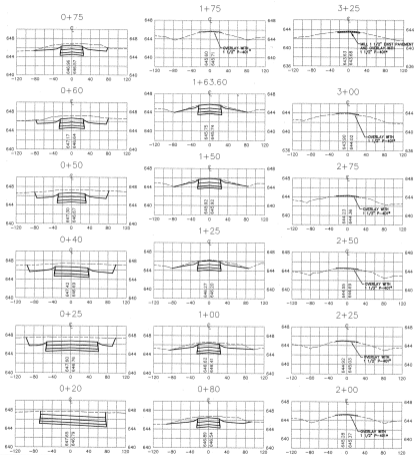
NO. 50	REVISIONS	BY
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**WEST TAXIWAY "F" & DRAINAGE IMPROVEMENTS
TAXIWAY C CROSS SECTIONS**



* IF EXISTING PAVEMENT IS TO REMAIN
 & EXISTING SURFACE POINTS ARE TO BE
 USED



DATE: 7-25-80
 DRAWN BY: J.S.
 CHECKED BY: J.S.
 SCALE: AS SHOWN

PROJECT: TAXIWAY E
 SHEET NO.: 27/30

NO.	DATE	DESCRIPTION

URS Greiner
 200 West Loop, Suite 500
 Fort Worth, Texas 76102
 Tel: 817-341-0201

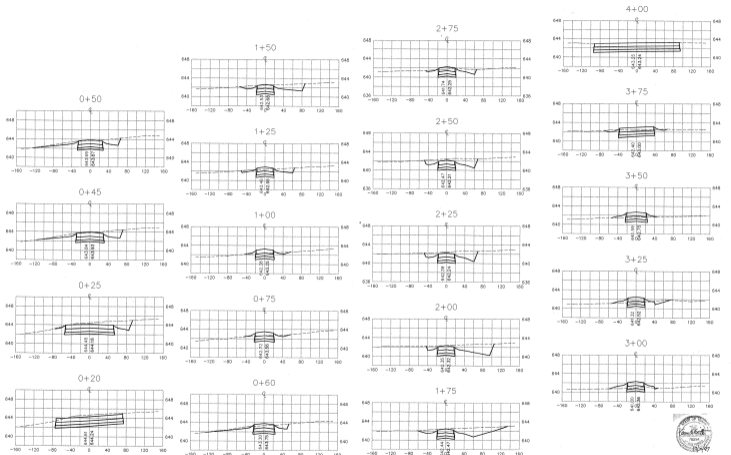
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TAXIWAY "E" & DRAINAGE IMPROVEMENTS

38837
51



DRAWN BY: J.L.V.
 CHECKED BY: J.L.V.
 PROJECT NO.: 220000000
 DATE: 2/20/00

100% (1)	100% (1)
90% (1)	90% (1)
80% (1)	80% (1)
70% (1)	70% (1)
60% (1)	60% (1)
50% (1)	50% (1)
40% (1)	40% (1)
30% (1)	30% (1)
20% (1)	20% (1)
10% (1)	10% (1)
0% (1)	0% (1)

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WEST TAXIWAY 'F' & DRAINAGE IMPROVEMENTS
TAXIWAY F CROSS SECTIONS