

WESSELTE




File Folders




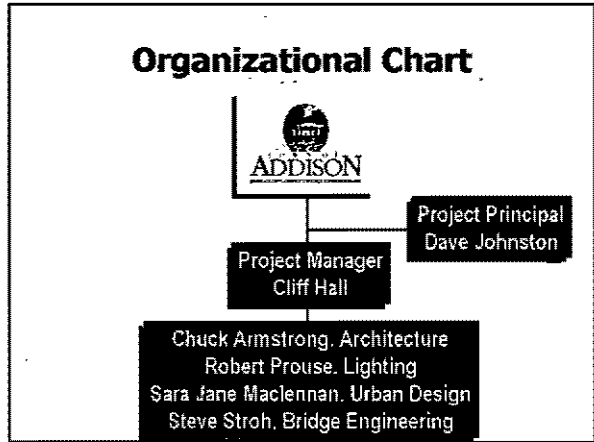
Paper made from 100% Recycled Fibers
Manufactured in U.S.A.

DESIGN
Cigarette
File

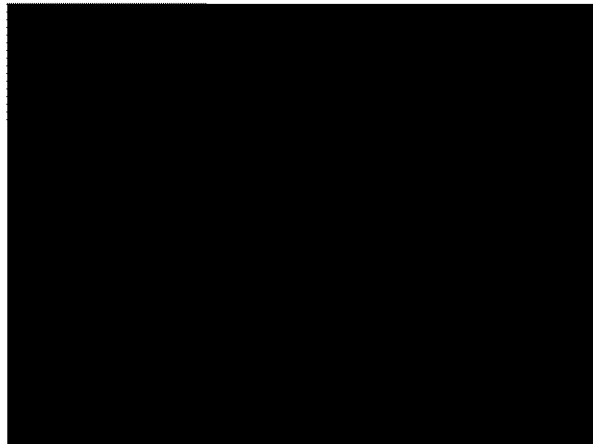


**ARAPAHO ROAD BRIDGE
DESIGN CONCEPT**

Presented by:

- ### Presentation Outline
- Introduction
 - Architecture / Lighting
 - Urban Design / Landscaping
 - Engineering
 - Team Approach & Experience
 - Q & A
 - Summary

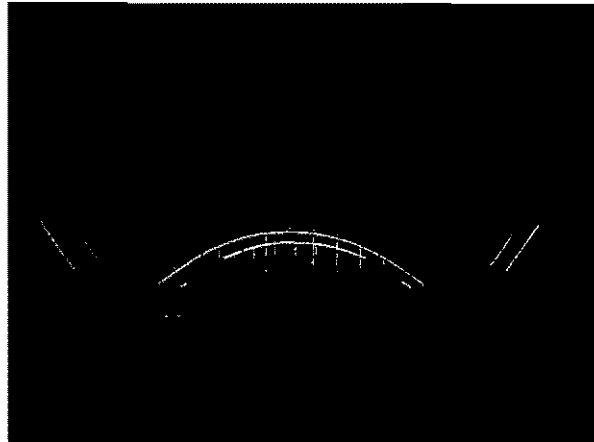
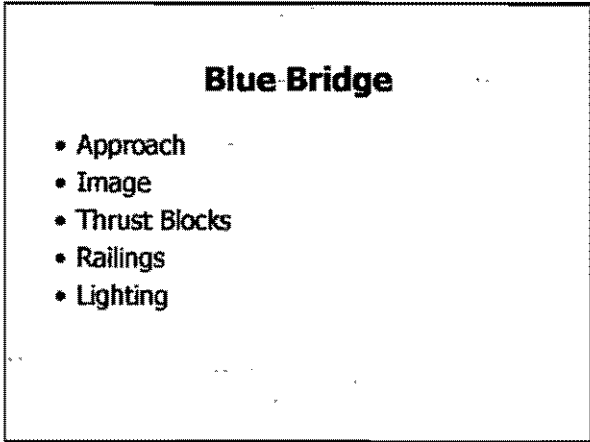
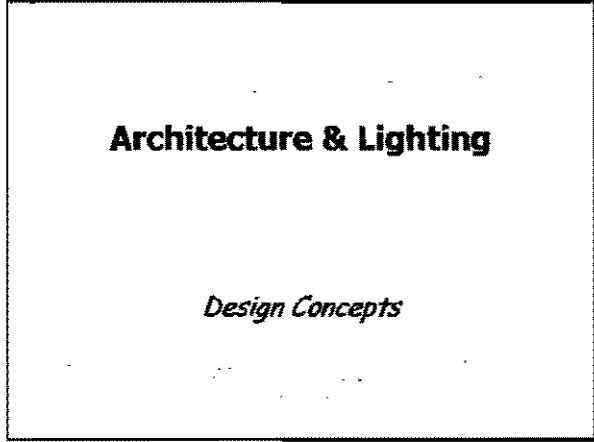
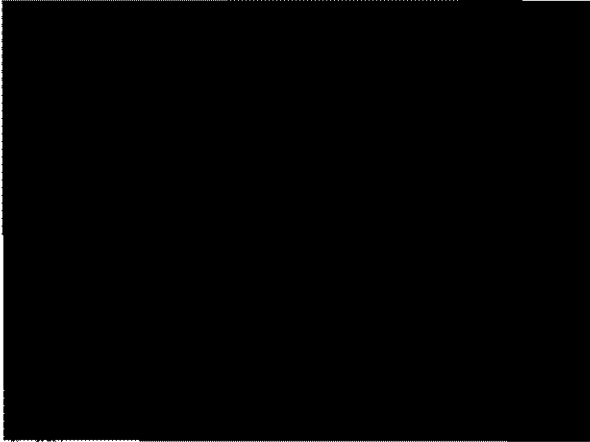


1. DESIGN CHARTER (MAY 27, 2002)

↓ JUN 30 DAYS

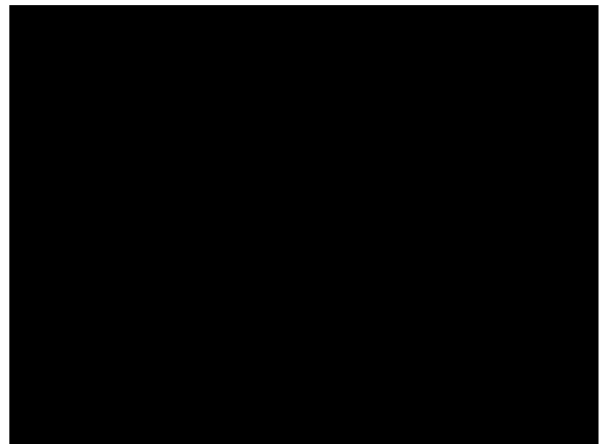
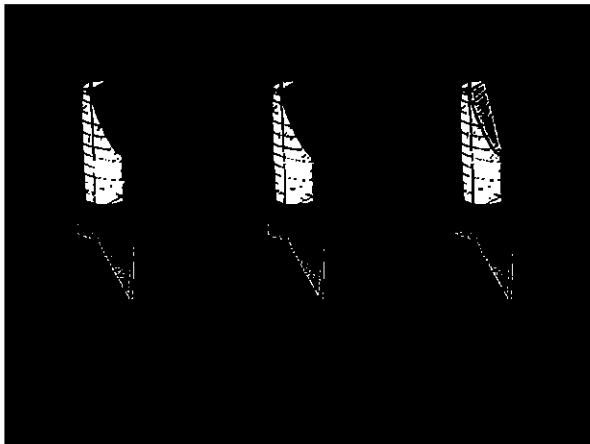
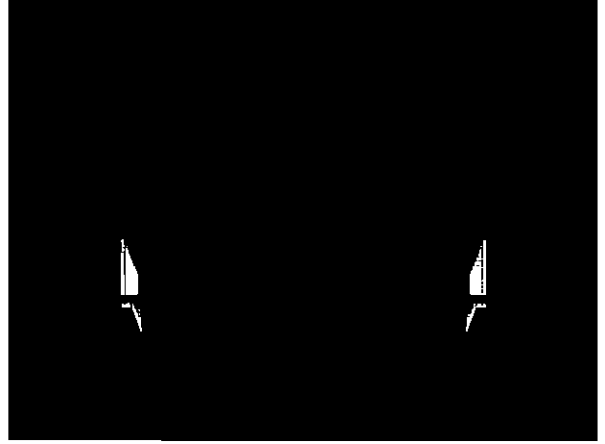
↓ JULY 60 DAYS

AUG 6 - 66 TOTAL DAYS



Beacon Bridge

- Approach
- Image
- Materials
- Beacons
- Public Art
- Lighting



Urban Design

Or how does it fit in the area

Urban Design

- Land Use
- Roadway System
- Context
- Analysis

Design Concepts

- Access
- Enhancements
- Landscape

Landscape Design Components

- Landscape and Features
- Pedestrian and Bicycle Access
- Parking

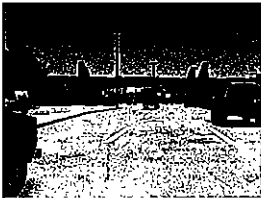
Engineering

Concept to Reality

Bridge Issues

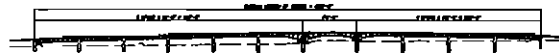
- Aesthetics
- Bridge Details, Typical Section and Layout
- Constructable/Maintainable
- Consideration of Drainage Structure
- Construction Cost
- Acoustics and Vibration
- Visibility of Adjacent Commercial Properties

Girder Alternative

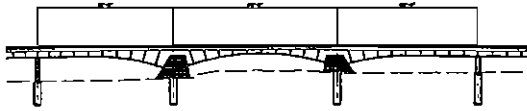


- Concrete Box Girder
- Cast-in-place on Falsework
- Post Tensioned
- Continuous Construction
- Drilled Shaft Foundations

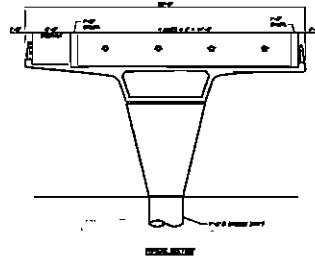
Span Layout – Girder Alternate



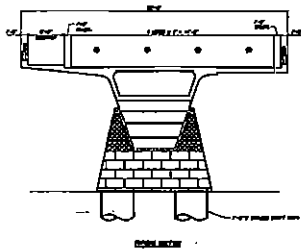
**Span Layout – Girder
Alternate**



**Girder Alternate
Approach Span Typical
Section**



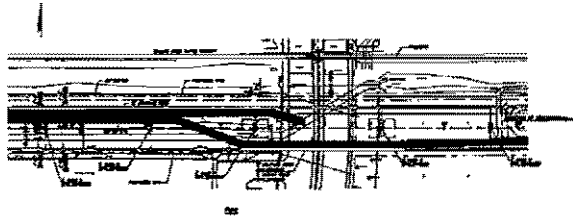
**Girder Alternate
Main Span Typical Section**



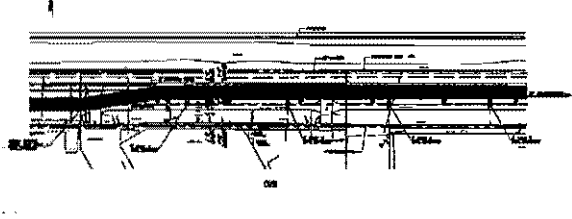
Plan – Girder Alternative



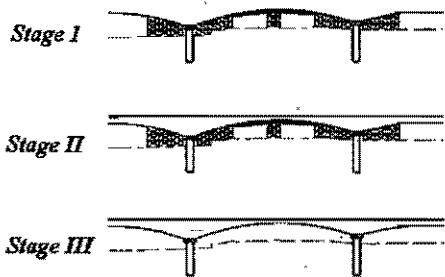
Plan – Girder Alternative



Plan – Girder Alternative



Construction – Girder Alternate



Arch Alternative

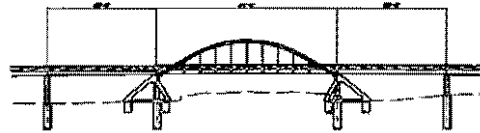


- Feature Main Span - Tubular Steel Arch
- Precast Texas U-Beam Superstructure
- Precast Superstructure
- Single Flared Piers
- Inverted T-Cap
- Drilled Shaft Foundations

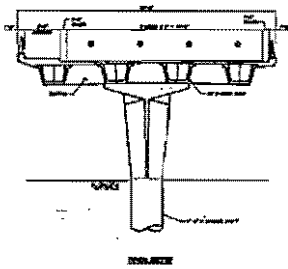
Span Layout – Arch Alternate



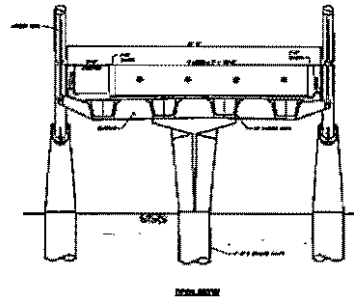
Span Layout – Arch Alternate



Arch Alternate Approach Span Typical Section



Arch Alternate Main Span Typical Section



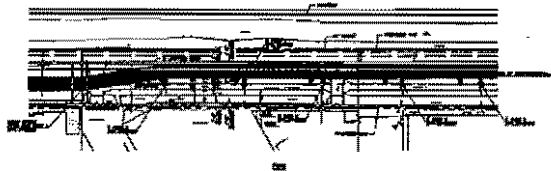
Plan – Arch Alternative



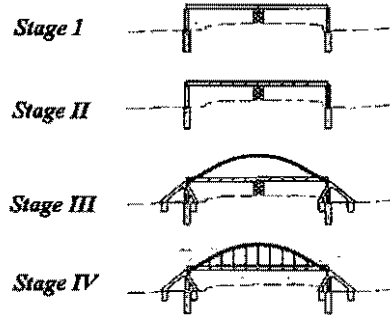
Plan – Arch Alternative



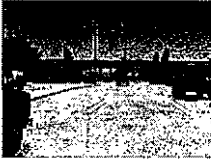
Plan – Arch Alternative



Construction – Arch Alternate



Construction Costs



Girder Alternate

- Bridge Cost \$5.3M



Arch Alternate

- Bridge Cost \$3.9M

Acoustics and Vibration

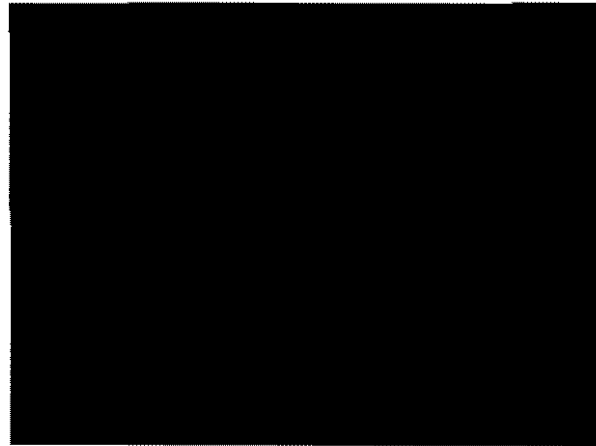
Construction Sources

- Vibration
 - Drilled Shaft Foundations
- Noise
 - Restrict Time for Construction

In-Service Sources

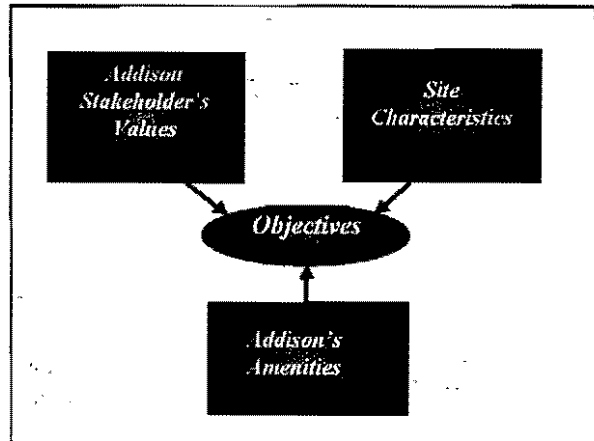
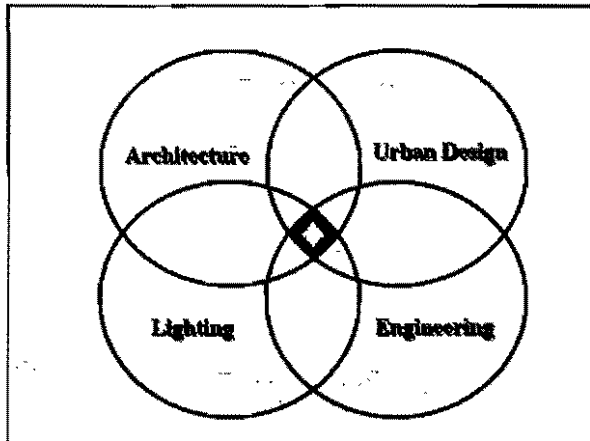
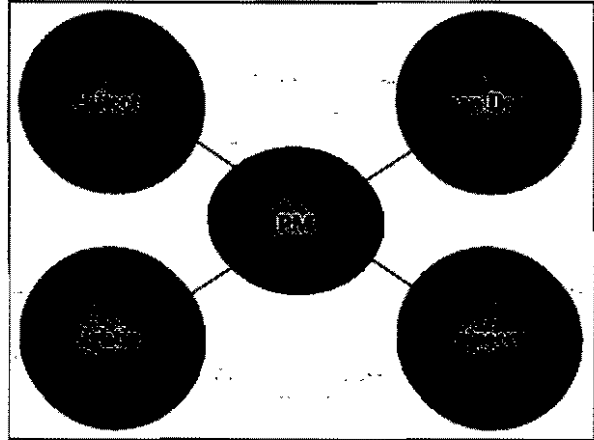
- Vibration
 - No extraordinary sources
- Noise
 - FHWA Traffic Noise Model
 - 66 dB Contour: 250 feet

Visibility of Adjacent Properties

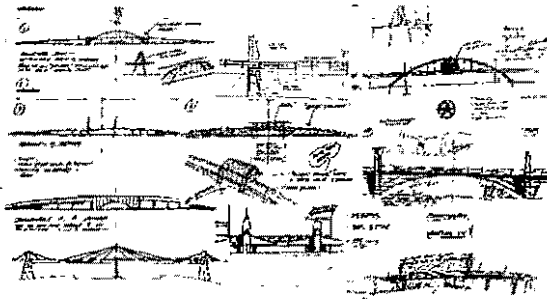


Team Approach

*Design Concept
Or how did we get here ?*



Free Flow of Creative Ideas



OBJECTIVES

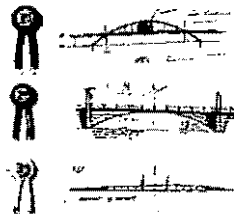
- 1) ESTABLISH STANDARDS FOR CIVIC PRIDE
- 2) GENERATE EXCITEMENT
- 3) GATEWAY ON MIDWAY
- 4) FIT WITHIN THE AREA
- 5) CLEAR SITE LINES TO BLDG - CLEAN STRUCTURE
- 6) LANDSCAPING
- 7) MAKE THE AREA FEEL SAFE (LIGHTING)
- 8) ACCOMMODATE THE PEDESTRIAN INTERFERENCE
- 9) MONUMENT? UNOBTRUSIVE?
- 10) REALITY CHECK \$4-\$5 M

Meet Objectives ?

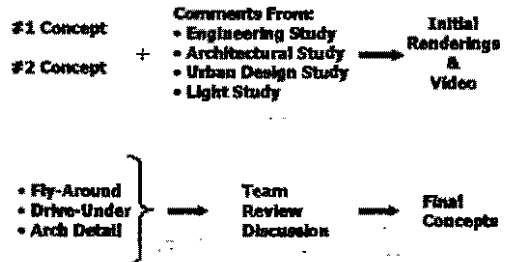
NO!



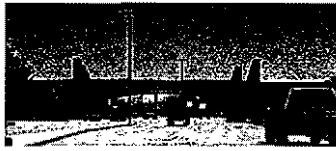
YES!



Concept Development



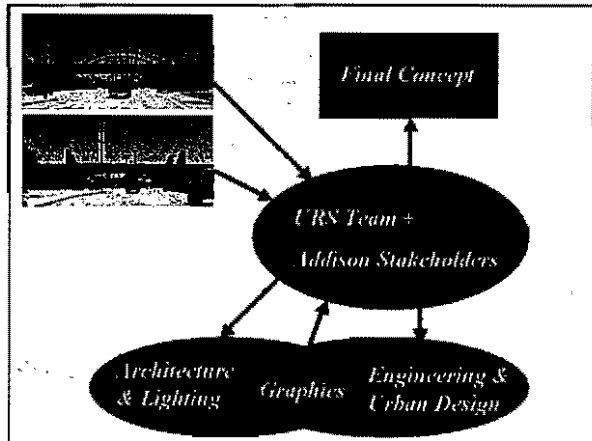
Final Concepts



Team Approach

After Selection
Or where do we go from here?

The Missing Piece



Final Design

- Design & Coordination
- Construction Plans, Specifications and Estimates
- Construction Services

Team Experience

Or who is going to get it done!

Cliff Hall, P.E. - Project Manager

Responsibilities

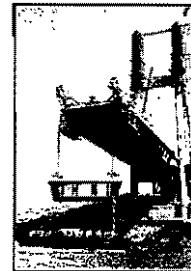
- Single Point of Contact
- Team Leader
- Coordination
- Maintain Schedule
- Lead Engineering Design



Cliff Hall, P.E. - Project Manager

Kap Shui Mun Bridge

- On-site Design PM
- Design Coordination
- Met Schedule
- Resolve Design & Construction Issues
- ACEC Grand Award



Chuck Armstrong, AIA - Architecture

Responsibilities

- Look & Feel
- Architectural Details
- Treatment Schemes
- Color Schemes



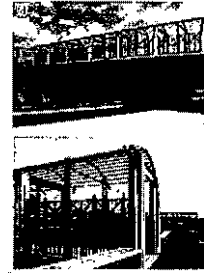
Chuck Armstrong, AIA - Architecture

Watkins Memorial

Drive Freeway

Bridge

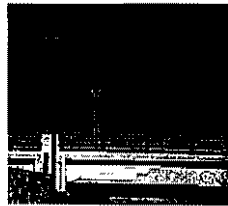
- Flagship Structure
- Compliment Historic District
- Urban Amenities
- ASLA Award



Sara Jane MacLennan, ASLA, AICP - Urban Design

Responsibilities

- Urban Design
- Landscaping
- Functional
 - Parking
 - Safety
 - Hike & Bike

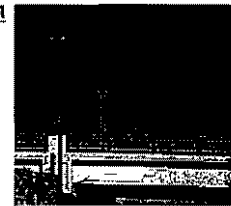


Sara Jane MacLennan, ASLA, AICP - Urban Design

Colorado Springs -

Downtown Action Plan

- Urban Design
- Redevelopment
- Bike Trail
- Landscaping & Streetscaping
- ASLA Award



Robert Prouse, IALD, MIES - Lighting

Responsibilities

- Lighting Design Scheme
- Feature Enhancement Lighting
- Functional Lighting

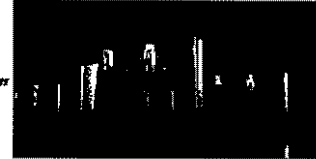


Robert Prouse, IALD, MIES - Lighting

Pearl Harbor

Memorial Bridge

- Lighting Design
- Create a "unique" look
- Dedicated as a Memorial



Steve Stroh, P.E. - Engineering

Responsibilities

- Bridge Details
- Constructability
- Civil Design
- Cost Control



Steve Stroh, P.E. - Engineering

17th Street Bridge

- Lead Engineer
- Concept Development
- Final Design
- Constructability
- Cost Control



**Dave Johnston, P.E.
Project Principal**

Responsibilities

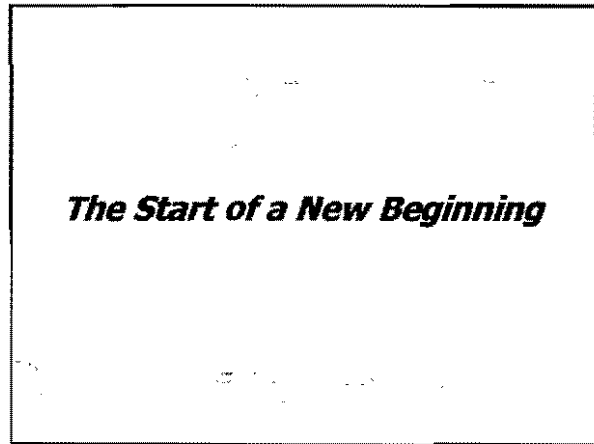
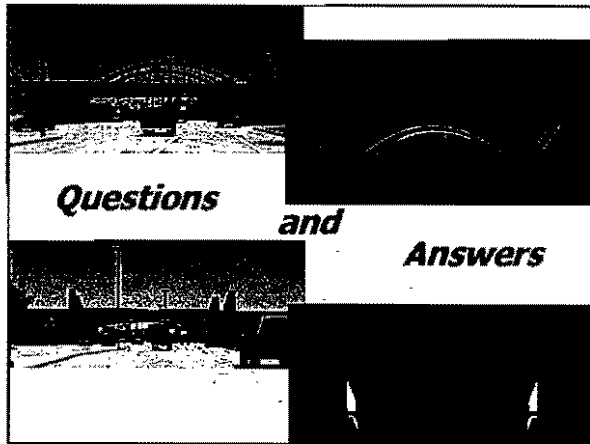
- Project Priority
- Commitment of URS Resources
- Peer Review

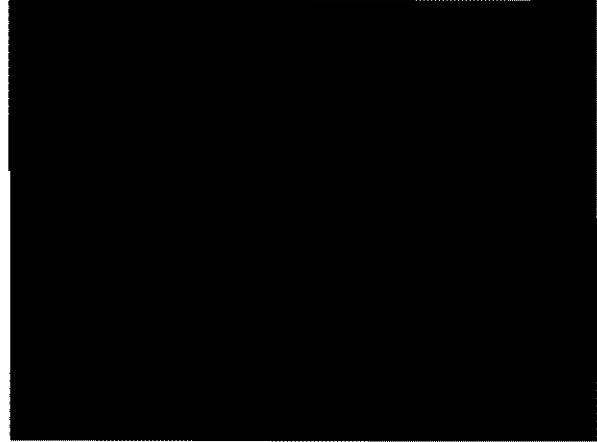
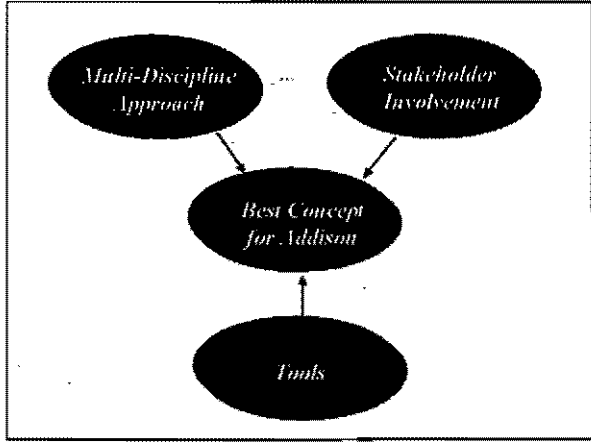


**Dave Johnston, P.E.
Project Principal**

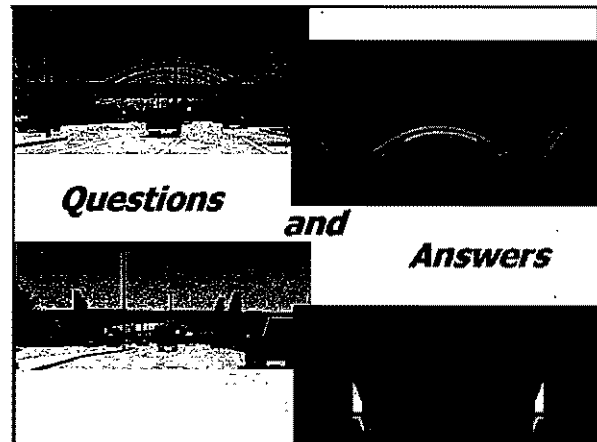
Key Projects

- SH 87, Bolivar Bridge over Houston Ship Channel
- Corpus Christi, Harbor Bridge Replacement
- Border Highway West Elevated Roadway





URS TEAM
THE RIGHT
CHOICE!



Bridge Design Charette

May 27, 2002

1. General Discussion

- What we currently have
 - a. Blue Arch Bridge
 - b. Lighted Bridge
- Blue Arch Bridge
 - a. What we like about it
 - b. What we don't like about it
- Lighted Bridge
 - a. What we like about it
 - b. What we don't like about it

2. New Ideas

- Lighting
- Landscaping
- Parking

3. Final Design Idea

1. BLUE IS GOOD IDEA
2. TENDS ITSELF TO LANDSCAPE
3. THINK ABOUT APPROACHES
4. LIKE THE PRACTICAL ARCH
5. GOOD VISIBILITY BENEATH
6. LIGHTING THAT CHANGES WITH SEASONS.
7. PEDESTRIAN PERSPECTIVE
8. CLEAN LOOK UNDER BRIDGE
9. CORN HZ. & WT. TO MAKE PEDS MORE COMFORTABLE

Bridge Design Charette

May 27, 2002

* BLUE IS GOOD IDEA

1. General Discussion

- > What we currently have
 - a. Blue Arch Bridge
 - b. lighted Bridge
- > Blue Arch Bridge
 - a. What we like about it
 - b. What we don't like about it
- > lighted Bridge
 - a. What we like about it
 - b. What we don't like about it

* PRACTICAL ARCHITECTURE!

* LOOK AT NIGHT

* LIGHTING

BLUE-SHARPIE

* BUILD CHANGE

2. New Ideas

- > lighting
- > landscaping
- > Parking

* NEUTRAL colored ~~tags~~

WASH in BLUE LIGHT.

3. Final Design Idea

VISIBILITY

BEING ABLE TO SEE UNDER THE BRIDGE

SOMETHING THAT CHANGES WITH THE SEASONS



FAA Re
Bridges

4651 Airport Parkway • Addison, Texas 75001 • 972-392-4854 • 972-788-9334 Fax

FAX COVER SHEET

DATE: 6-12

TO: Mark

FAX: 450-2825

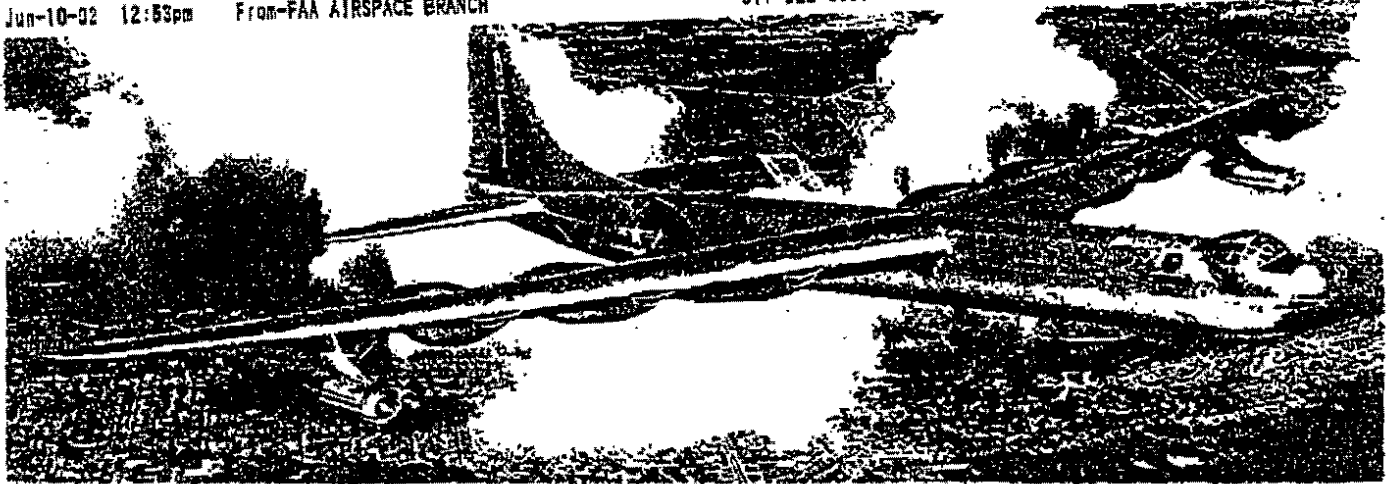
FROM: Darci

FAX: _____

Pages (including cover): _____

COMMENTS:

_____	Chris ✓	6/12/02	_____
_____	Mike -		_____
_____	Jim?		_____
_____	FyI		_____
_____	Mark		_____



B-36 THE PEACEMAKER

Bruce Beard - Senior Obstruction Evaluation Specialist
Federal Aviation Administration / Southwest Regional Office
Air Traffic Airspace Branch / Fort Worth TX 76193-0520
Office: 817-222-5536 / FAX: 817-222-5981 / bruce.beard @ faa.gov

TO: KEVIN JOHNSON

FAX #: 214-920-2565

REFERENCE: ADDISON BRIDGE

COVER SHEET + PAGES = 5

ADS APT # 009

ADS TWR # 018

www.faa.gov/ats/ata/ata400/oeaaa.cfm

A HARD COPY WILL BE SENT.
WILL NOT BE SENT.

Jun-10-02 12:53pm From-FAA AIRSPACE BRANCH

917-222-6681

T-725 P.02/05 F-566

Federal Aviation Administration
Southwest Region
Air Traffic Division, ASW-520
Fort Worth, TX 76193-0520

AERONAUTICAL STUDY
No: 02-ASW-1470-OE
PRIOR STUDY
No: 02-ASW-1466-OE

ISSUED DATE: 06/10/02

KEVIN JOHNSON
FREESE - NICHOLS
1700 MARKET ST STE #500 LB 51
DALLAS TX 75202-2901

**** PRESUMED HAZARD ****

The Federal Aviation Administration has conducted an aeronautical study under the provisions of 49 U.S.C., Section 44718 and, if applicable, Title 14 of the Code of Federal Regulations, part 77 concerning:

Description: ADDISON BRIDGE
POINT #2 (HIGHEST POINT OF BRIDGE)
Location: ADDISON TX
Latitude: 32-57-23.64 NAD 83
Longitude: 096-50-20.22
Heights: 200 feet above ground level (AGL)
826 feet above mean sea level (AMSL)

The initial findings of this study indicate that the structure as described above would exceed obstruction standards and/or would have an adverse physical or electromagnetic interference effect upon navigable airspace or air navigation facilities. Therefore, pending resolution of the issues described below, it is hereby determined that the structure is presumed to be a hazard to air navigation.

If the structure were reduced in height so as not to exceed 168 feet above ground level (794 feet above mean sea level), it would not exceed obstruction standards and a favorable determination could subsequently be issued.

Any height exceeding 168 feet above ground level (794 feet above mean sea level) will result in a substantial adverse effect and would warrant a Determination of Hazard to Air Navigation.

See attachment for further information.

A copy of this determination will be forwarded to the Federal Communications Commission if the structure is subject to their licensing authority.

NOTE: PENDING RESOLUTION OF THE ISSUES DESCRIBED ABOVE, THE STRUCTURE IS PRESUMED TO BE A HAZARD TO AIR NAVIGATION. THIS DETERMINATION DOES NOT AUTHORIZE CONSTRUCTION OF THE STRUCTURE EVEN AT A REDUCED HEIGHT. ANY RESOLUTION OF THE ISSUES DESCRIBED ABOVE MUST BE COMMUNICATED TO THE FAA SO THAT A FAVORABLE DETERMINATION CAN SUBSEQUENTLY BE ISSUED.

IF MORE THAN 60 DAYS FROM THE DATE OF THIS LETTER HAS ELAPSED WITHOUT ATTEMPTED RESOLUTION, IT WILL BE NECESSARY FOR YOU TO REACTIVATE THE

STUDY BY FILING A NEW FAA FORM 7460-1, NOTICE OF PROPOSED CONSTRUCTION OR ALTERATION.

If we can be of further assistance, please contact our office at 817-222-5534. On any future correspondence concerning this matter, please refer to Aeronautical Study Number 02-ASW-1470-OE.



Bruce C. Beard
Specialist, Airspace Branch

(DPH)

Attachment

**ATTACHMENT PAGE
AERONAUTICAL STUDY NUMBER 02-ASW-1470-OE
ADDISON, TEXAS**

AGL = Above Ground Level / AMSL = Above Mean Sea Level

SIAP = Standard Instrument Approach Procedure

NM = Nautical Mile / RWY = Runway

MDA = Minimum Descent Altitude

The proposed construction would be located approximately 2,240 feet south of Rwy 33 at the Addison Airport, Addison, Texas. It would exceed the obstruction standards of Title 14 of the Code of Federal Regulations, part 77, as follows:

- Section 77.23 (a)(5) by 32 feet - a height exceeding the horizontal surface as applied to the Addison Airport.

In addition, the proposal would lie within the Addison Airport traffic pattern airspace required to be protected. The proposal would have a significant adverse impact on both arriving and departing aircraft.

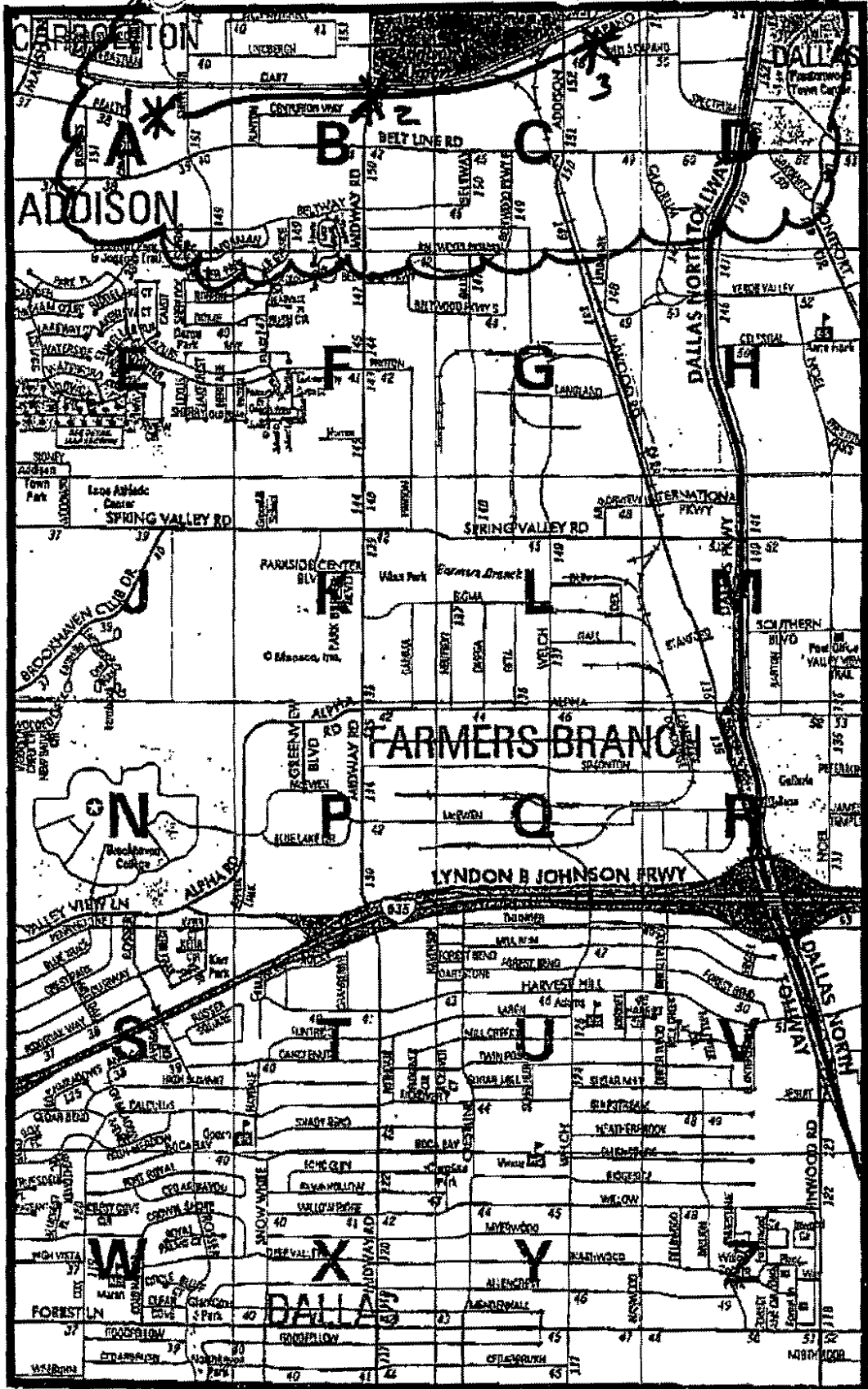
ONLY OPTION – 168 FEET AGL / 794 FEET AMSL

A favorable determination can be issued immediately at a structure height of 168 feet AGL / 794 feet AMSL, once we have received notification of acceptance of the lower height.

Any height that exceeds 168 feet AGL / 794 feet AMSL would warrant a Determination of Hazard to Air Navigation. This determination would be based on the significant adverse impact the structure would have on aircraft operating within the Addison Airport traffic pattern.

Due to the significant adverse impact on aeronautical operations at the Addison Airport, the maximum allowable height at your proposed location is 168 feet AGL / 794 feet AMSL.

We will hold this case in abeyance until we have been notified of your decision. Your response may be faxed to us at 817-222-5981 or you may e-mail it to bruce.beard@faa.gov. Please include the aeronautical study number on your correspondence.



CONTINUED ON MAP 13

CONTINUED ON MAP 15

SCALE IN MILES

CONTINUED ON MAP 24

SCALE IN FEET



June 6, 2002

Mr. James C. Pierce, Jr., PE
Assistant Public Works Director
16801 Westgrove Drive
P.O. Box 9010
Addison, TX 75001-9010

Re: **Arapaho Road Bridge at Midway Road**
Phase II – Design Development and Final Plans, Specifications and Estimate
Draft Scope of Services

cc Mike
Steve

for review &
comment
Jim

Dear Mr. Pierce:

Enclosed please find a draft copy of the Scope of Services for Phase II of the referenced project for your review. We would like to discuss this scope with you and perhaps your Consultant (HNTB) to ensure that there is no overlap of services within the draft scope before we finalize our fee proposal. We can be available any time next week that is convenient for you

We have also examined the construction costs in a very preliminary manner for the items detailed in our scope and based on the discussions in our May 28 meeting. As previously advised, we believe the construction cost of the bridge structure will be approximately \$4,000,000. The construction costs for the other items such as; landscaping and hardscaping along Arapaho and Midway Roads, retaining walls, grading and parking lot pavement under the bridge, parking lot drainage, parking lot lighting, Midway Road lighting, etc., we believe could be an additional \$2,500,000 to \$3,000,000. Additionally, standard estimating practice would apply a contingency factor to an estimate of this preliminary nature.

This project differs from a typical roadway project given the importance and scope of aesthetics, lighting, urban design and landscaping as well as the unique characteristics of the bridge structure. In addition, we are tasked with developing multiple concepts for some areas of lighting, landscaping and architectural details. While a typical project may consider 10% to 12% of the construction cost as a benchmark for an appropriate design fee, the uniqueness of this project may preclude such typical "rules-of-thumb".

We look forward to discussing our Scope of Services with you and finalizing our fee proposal for your approval.

Sincerely,

URS Corporation

Cliff R. Hall, PE
Project Manager

Enclosure

URS Corporation
Prestonwood Tower
5151 Beltline Road, Suite 700
Dallas, TX 75254
Tel: 972.980.4961
Fax: 972.991.7665

**ARAPAHO ROAD BRIDGE AT MIDWAY ROAD
WORK ORDER NO. 002**

**ATTACHMENT A
SCOPE OF SERVICES**

**PHASE II – DESIGN DEVELOPMENT AND FINAL PLANS, SPECIFICATIONS
AND ESTIMATES**

URS will provide the engineering, architectural, urban design and lighting design services including plans, specifications and estimates as it relates to Arapaho Road from approximate Station 40+67 to approximate Station 70+28 and as provided in the itemized scope. The construction will consist of an elevated four-lane roadway with sidewalk located within the proposed Arapaho Road right-of-way (ROW). URS shall prepare plans, details and compute quantities for a steel arch bridge over Midway Road, with prestressed concrete beam approach spans and mechanically stabilized earth (MSE) retaining walls. URS will also prepare plans, details and compute quantities for any grading, paving, landscaping, hardscaping, sidewalks, parking areas, drainage, lighting & illumination, pavement markings, traffic control, and storm water pollution prevention plans for the areas under and immediately adjacent to the bridge and retained wall portion of Arapaho Road with the exception of those portions to be prepared by the Town of Addison's Consultant. Additionally, URS will prepare plans, details and compute quantities for landscaping, hardscaping, and lighting & illumination along Midway Road from approximately Belt Line Road to approximately Lindberg Road. URS will prepare and submit technical memorandums, preliminary plans and preliminary construction cost estimates at the end of the Design Development phase for the Town's review. After resolution of comments and selection of alternatives provided, URS will provide all final detail plans, specifications, and estimates as previously described, to be included into one final construction package prepared by the Town's Consultant.

URS will coordinate with the Town of Addison and/or the Town's Consultant for all interface design issues as well as coordinate the format and consolidation of construction plans, specification and estimate into one final construction package. URS will coordinate with the Town and/or the Town's Consultant for revising the horizontal alignment and vertical profile of Arapaho Road to accommodate the proposed bridge structure. URS will coordinate with the Town and/or the Town's Consultant for the revised alignment and construction sequencing of the proposed box-culvert under Arapaho Road as well as surface and bridge drainage tie-ins. URS will coordinate with the Town and/or the Town's Consultant for all geotechnical information required for the foundation design for the bridge and retaining walls.

The Town of Addison will provide to URS all available Arapaho Road geometrics, including but not limited to electronic files for horizontal alignment, vertical profile, topography survey, field survey, and utility information. The Town will also provide boring logs, stability analysis, settlement analysis, soil parameters and foundation design recommendations required for the bridge and retaining wall foundation designs. The Town of Addison will provide to URS a field location survey of the existing 60-in. diameter water main, locating the water main precisely, both vertically and horizontally, along the project limits and specifically in the vicinity of the arch-bridge's main foundations.

All ROW documentation and plans, Arapaho Road geometrics and roadway design, noise modeling, survey, and geotechnical engineering are outside the scope of this agreement and will be performed by others.

Itemized Scope of Services Provided by URS

TASK I - URBAN DESIGN

A. Design Development

1. Inventory and Analysis of Project Area and Influence Area
 - Review existing plans
 - Identify prevailing patterns
 - Analyze project needs based on above
 - Develop urban design principles for project
 - Prepare technical memo of understanding of existing plans
2. Develop two scenarios for the primary project area (limits of project) that incorporate the design principles in item number 1.
 - Landscape
 - Parking and parking access
 - Hardscape
 - Element relocation, adjustment, elimination, or addition (utility poles, signage, medians, sidewalk, etc.)
 - Prepare plan & elevation with typical sections of primary project area
3. Develop 2 scenarios for the influence area (up to ½ mile)
 - Integration into existing urban design
 - Enhancement of connections to existing nearby areas (Belt Line Road, Arapaho hike/bike trail)
 - Develop designs and plans that integrate with existing plans
 - Prepare plan & perspective views with typical sections of influence area
4. Develop final preliminary concept
 - Identify a final preliminary concept to go forward into final design.
 - Prepare support drawings for final concept
5. Prepare for and attend meetings in Dallas to collect data and present options (2 total)

B. Final Design

1. Project Area
 - Design development of hardscape (surfaces, railings, etc.)
 - Plant identification, planting plan, plant list, maintenance requirements
 - Urban design and landscape construction documents for the project area
2. Influence Area
 - Draft report and graphics to integrate the project area into its surroundings
 - Final report document with supporting plan graphics.
3. Prepare for and attend meetings in Dallas (1)

TASK II - Engineering

A. Civil Site Works

1. Design Development
 - Geometric Layout / Paving Plans
 - Quantities & Cost Estimate
2. Final Civil Design & PS&E
 - Title Sheet (By Town's Consultant)
 - Index of Sheets (By Town's Consultant)
 - General Notes (By Town's Consultant)
 - Typical Sections
 - Summary of Quantities (By Town's Consultant)
 - Removal Plan
 - Geometric Layout
 - Grading and Drainage Plan
 - Paving Details
 - Storm Sewer Computation Data
 - Drainage Area Map

input from URS?

?

0

- Drainage Profiles ?
- Drainage Details
- Storm Water Pollution Prevention Plan
- Traffic Control Plan
- Utility Adjustment Plan
- Cross Sections
- Roadway Lighting Plan
- Miscellaneous Roadway Details
- Striping and Marking Plan
- Retaining Wall Layouts (2 walls ~ 2120' & 710') } ?
- Retaining Wall Typical Sections
- Retaining Wall Miscellaneous & Architectural Details
- Compile, Verify & Modify TxDOT Standard Drawings
- QA/QC
- Cost Estimate
- Special Provisions & Specifications
- Coordination with Town's Consultants

B. Bridges

1. Preliminary Bridge Design
 - Preliminary Bridge Layout (Finalize Bridge Location)
 - Typical Section
 - Refine Arch Shape
 - Size Thrust Block & Refine Shape
 - Size Foundation
 - Size Diaphragms
 - Size Traffic Railing Members
 - Investigate Culvert Layout
 - Quantities and Cost Estimate
 - QA/QC
2. Final Bridge Design, Plans, Specifications and Estimate
 - Final Bridge Layout
 - Quantities and Bearing Seats
 - Foundation Layout
 - Drilled Shaft Details
 - Abutment Plan & Elevation
 - Abutment Details
 - Bent Plan & Elevation
 - Bent Details
 - Thrust Block Plan & Elevation
 - Thrust Block Details
 - Prestressed Concrete Beam Unit – Deck Plan
 - Prestressed Concrete Beam Unit – Deck Sections
 - Miscellaneous Superstructure Details (drains, lighting)
 - Diaphragm Details
 - Closure Pour Details
 - Suspension Hanger Details
 - Steel Arch Design and Details
 - Bearing Details
 - Drainage Details
 - Railing Details
 - Stinger Design & Details
 - Approach Slab Details
 - Erection Sequencing
 - Prestressed Beam Tables
 - Compile, Verify & Modify TxDOT Standard Drawings
 - QA/QC

- Coordination with Town's Consultants
- Bridge Total Quantities & Cost Estimate
- Bridge Special Provisions & Specifications

C. Electrical Engineering

1. Design Development

- Develop a load study for Arapaho Road, bridge, parking and portion of Midway Road
- Prepare a preliminary cost estimate

2. Final PS&E

- Finalize a load study for each electrical service source.
- Prepare Lighting Calculations
- Illumination Layout (4500', 1200'/sht x 2 Elevations + 2 Midway*) —?
- Electric Service / Pole Summary
- Conduit Runs / Contents
- Insert Lighting Consultant Special Details
- Insert Latest Town or TxDOT Standards —?
- Quantity Summary
- Develop Final Cost Estimate (Using Estimator)
- Prepare Construction Scope of Work
- QA ON 90% PLANS
- Update Drawings per City Review

TASK III - ARCHITECTURAL

A. Design Development

1. Architectural Studies & Details

- Develop three rail options addressing the issues of hiker/biker separation from the vehicular traffic and the architectural options to realize the proposed triangular pattern. ?
- Coordinate with the engineering team to refine the curvature and size of the steel. Produce drawings representing viable options
- Explore options for the final material and form of the thrust block. Provide sketch options initially - followed by CADD drawings of preferred scheme.
- Develop detailed options and refine panel schemes for precast concrete retaining walls at approaches.
- Study and Present Options for center pier support shape.
- Explore and refine alternate "stinger" designs incorporating lighting as the primary feature.
- Attend Team Meetings and Conference Calls to coordinate the architectural aspects of the design with structural and lighting Consultants.

2. Computer Animation

- Minor updates to existing video animation for landscaping, parking, etc.
- Produce video animation in DVD and VHS tape format

B. Final Design

1. Coordination
2. Review
3. Specifications

TASK IV - Lighting Design

A. Design Development

1. Develop two alternatives for lighting of stingers.
2. Research and mock-up options for color changing on existing Addison sculpture (to determine range of possibilities for color changing on bridge structure).
3. Develop lighting concepts for Midway Road in the blocks immediately north and south of Arapaho.
4. Develop two alternatives for lighting of bicycle/pedestrian path.
5. Develop two alternatives for lighting of roadway.
6. Develop mounting concepts for bridge structure lighting.
7. Develop two alternatives for lighting of outboard railings.

8. Develop two alternatives for lighting of underside of bridge, roadway under bridge and any adjacent parking areas under bridge.

B. Final Design

1. Final details for lighting of stingers.
2. Final details of fixtures and mounting for bridge structure illumination.
3. Final details of fixtures and mounting for roadway lighting.
4. Final details of fixtures and mounting for bicycle/pedestrian pathway lighting.
5. Final details of fixtures and mounting for outboard railing illumination.
6. Final details of fixtures and mounting for lighting of underside of bridge, roadway under bridge and any adjacent parking areas under bridge.
7. Provide control concept diagrams and other information suitable for use by electrical engineer describing control intent.

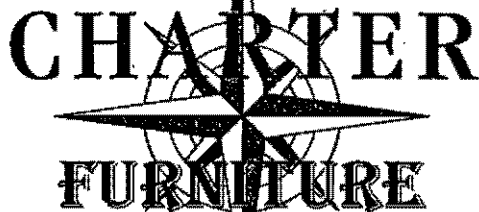
TASK V - PROJECT MANAGEMENT

A. Reports and Invoices

1. Prepare Project Management Plan
2. Prepare Progress Reports
3. Prepare Invoices and Billings

B. Coordination

1. Coordinate/Administer the Project
2. Prepare and Update Schedule
3. Manage Subconsultants
4. Implement Quality Assurance/Quality Control Program
5. Prepare for and Attend Town Council or other Town Meetings (3 total)
6. Prepare for and run internal project coordination meetings (8 total)
7. Prepare for and attend project meetings with Addison Public Works (8 total)



CHARTER
FURNITURE

April 29, 2002

City of Addison
Addison, Texas 75001

RE: Arapaho Road Project

Gentlemen:

Establishing a fair and accurate value for the condemnation of land, coupled with the negative impact the bridge construction may have on Charter Furniture is very difficult to calculate.

Using your appraisal as a guide the following are my conclusions and requests.

First, let me start with some questions and statements I have concerning the Appraisal, which I think are relevant to the conclusions drawn in the Appraisal and subsequently the offer.

Statement: Page two of the Appraisal states there are no known contracts or offers for the sale of the subject property as of January 23, 2002.

Point: The original contract for the sale of the property was dated December 2001. The final agreed upon price of \$3,400,000.00 was signed and an earnest money check was deposited with Chicago Title on January 14, 2002.

Statement: The Appraisal on page 29 states the building is tilt wall.

Point: The building construction is brick veneer over concrete block, a superior and more expensive type of construction.

Land Valuation

The value of 3.5 acres of retail land fronting Midway in Addison is not \$6.50 per foot. The comparisons used were properties on secondary streets and not retail property.

SALES

15101 Midway Road
Addison, Texas 75001
(972) 385-3411
(972) 385-0621 Fax

RENTAL

14430 Midway Road
Dallas, Texas 75244
(972) 385-3204
(972) 385-7650 Fax

SALES

8100 Bedford-Eules Road
North Richland Hills, Texas 76180
(817) 577-2300 metro
Fax (817) 577-5333 metro

Value Offer Acquisition

Page 72 of the Appraisal specifically states using the "sales comparison and the income approach to establish value."

Sales Comparison

Value before Acquisition	Value after Acquisition
\$3,400,000.00 Purchase Price	Per Appraisal
\$3,500,000.00 After Re-Model	\$2,850,000.00

Difference \$650,000.00

Income Approach

24,000 Ft.	X	\$ 8.00 per Ft.	= \$ 192,000.00
28,800 Ft.	X	\$12.00 per Ft.	= <u>\$ 345,000.00</u>
		Times 10 Cap	= \$5,370,000.00

Page 64 of the Appraisal states 6,851 square feet of the building will be non-conforming. This equates to a 13% loss of income. $6,851 / 52,800 = 13\%$ times $\$5,370,000.00 = \$698,100.00$.

Facts: Retail space at Midway and LBJ leases for \$30.00 per foot. The only other 30,000 square foot retail space on Midway is Englishmen's Antiques at \$12.00 per square foot.

Mapsco made a \$200,000.00 improvement to their space to obtain a \$7.00 triple net lease.

Appraisal Offer Verses Land Value

The current offer from the City of Addison is \$450,000.00 based on a land value of \$6.50 per foot. If the land value is \$10.00 per foot and the same percentage adjustment is used, then the value is \$691,041.00. If the land value is \$12.00 per foot and the same percentage adjustment is used, then the value is \$829,249.00

Summary

As I stated in the outset, it is difficult to calculate the loss; only time will tell. We think a fair price today is \$700,000.00 with the following concessions from the City of Addison.

1. City to build and maintain lighted parking under the bridge. The building owner to be granted permanent and formal rights to the parking.
2. Exemption from set back requirements, ability to rebuild to present size.
3. City responsible for damage to the building caused by soil and/or drainage changes due to construction.
4. Retain present curb cuts.
5. Sign variance for better visibility. Protect visibility for southbound traffic.
6. City will not close Midway Road during business hours including weekends during construction.
7. City endeavor to keep road and bridge as far from building lines as possible, the absolute minimum is 10 feet.

The above represents our efforts for a fair response based on current conditions. Please feel free to contact me at (972) 385-32411 with any questions or concerns you may have.

Sincerely,

Bill Crepeau
CEO Charter Furniture

WMC:dc

Cc: file

Addison!

**PUBLIC WORKS
MEMORANDUM**

5/09/02

Presentation Script for Bridge Design Engineers

1. Brief Summary of Process

- How did we get where we are today ✓
- Intro of Bridge Design selection committee names
- Final selection process
- Why we chose URS / Emphasis on Team concept and Approach

2. Introduce the Computerized video

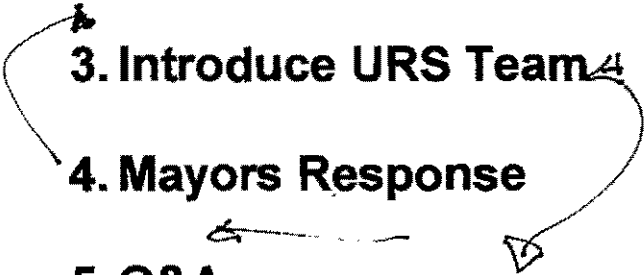
- Play video (approx. 10 minutes)

2 conceptual designs
one on an arch
one on a pier & beam

3. Introduce URS Team

4. Mayors Response

5. Q&A



214
210
8760
8706 set

Addison!

**PUBLIC WORKS
MEMORANDUM**

Presentation Script for Bridge Design Engineers

1. Brief Summary of Process

- *How did we get where we are today*
 - *Seeking a Meaningful Bridge Design*
 - *Wanted to get ideas from several design firms*
 - *Decided on competition to accomplish goal*

- *Intro of Bridge Design selection committee*
 - *Scott Wheeler – Mayor*
 - *Diane Mallory – Council Person*
 - *Bill Creapeau – Owner of Charter Furniture*
 - *Ron Whitehead – City Manager*
 - *Chris Terry – Assistant City Manager*
 - *Mike Murphy – Director of Public Works*

- *Final selection process*

Narrowed all Submittals of Qualifications down to three firms

Paid each of the three finalists a stipend of \$10,000

Each firm made a formal presentation to the Design Selection Committee

Selection Committee evaluated each firm based on the following criteria:

NAME OF FIRM _____

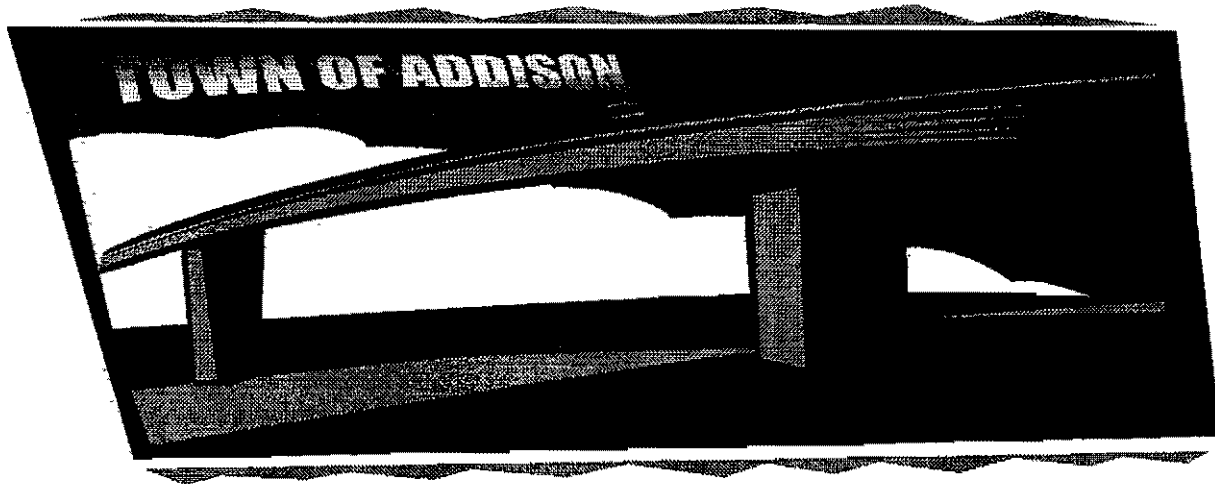
SCORE SHEET

- 1. Aesthetics – appearance day and night, should include lighting design plan (1-10) _____ POINTS**
- 2. Landscaping (1-10) _____ POINTS**
- 3. Acoustics (how will noise affect adjacent buildings) (1-10) _____ POINTS**
- 4. Vibrations (how will motion impact adjacent buildings) (1-10) _____ POINTS**
- 5. Estimated cost of construction of the bridge design (1-10) _____ POINTS**
- 6. Functionality / build ability (parking, pedestrian and bike users, safety, ADA etc.) (1-10) _____ POINTS**
- 7. How does the bridge minimize obstruction to adjacent buildings from roadway (1-10) _____ POINTS**
- 8. Overall quality and creativity of presentation (vision, team plan / effort) (1-10) _____ POINTS**

TOTAL SCORE _____ POINTS

Presentation Schedule:

<u><i>Time (April 11th)</i></u>	<u><i>Firm</i></u>	<u><i>Conference Centre Location</i></u>
7:30am – 8:00am	Committee Members	Board Room
8:00am – 10:00am	URS Griener	Stone Cottage
10:30am – 12:30pm	Freese & Nichols	Board Room
12:30pm – 1:30pm	Lunch	Board Room
1:30pm – 3:30pm	HNTB	Stone Cottage
3:30pm – 5:00pm	Panel Discussion/Selection	Board Room



Arapaho Road Bridge Competition Agenda April 11, 2002

- I. Welcome and introductions: *Ron Whitehead, City Manager***
- II. Directions to Committee Members: *Mike Murphy, Director of Public Works***
 - A. Announce presentation schedule:**
 - 1. First Presentation URS 8:00am-10am**
 - 2. Second Presentation Freese & Nichols
10:30am-12: 30pm**
 - 3. Third Presentation HNTB 1:30pm-3: 30pm**
- III. Grading will be based on the following criteria**
 - A. Aesthetics – appearance day and night, should include lighting design plan**
 - B. Landscaping**
 - C. Acoustics (how will noise affect adjacent buildings)**
 - D. Vibrations (how will motion affect adjacent buildings)**
 - E. Estimated cost of construction of the bridge design**
 - F. Functionality / build ability (parking, pedestrian and bike users, safety, ADA etc.)**
 - G. How does the bridge minimize obstruction to adjacent buildings from roadway**
 - H. Overall quality and creativity of presentation (vision, team plan / effort)**

Evaluation Guide Lines:

- *Each firm will be given two hours to make their presentations and answer questions from the panel, with at least 30 minutes dedicated to question and answer.*
- *Group will have open discussions about individual concerns and opinions.*
- *Panel will use evaluation criteria stated in information below, with each item having equal value to assist in ranking each firm.*
- *Each member will then individually grade each firm, in a similar manner, giving each item a grade from 1-10, with 10 being best.*
- *Each member based on the total score given will rank the firms in order of grade 1st, 2nd, or 3rd.*
- *All panel members will submit their rankings to establish initial order.*
- *Each firm will be given an overall grade based on rank submitted by each member. 1st place will be awarded one point, 2nd place will be awarded 2 points and 3rd place will be awarded 3 points. The firm with the fewest total points will be the competition winner.*

BRIDGE PRESENTATION

- BRIEF SUMMARY OF PROCESS
- HOW DID WE GET TO WHERE WE ARE TODAY
- * INTRODUCE THE SELECTION COMMITTEE
- * FINAL SELECTION & WHY WE CHOSE URS / EMPHASIS ON TEAM APPROACH
- * INTRODUCE VIDEO
- * RUN VIDEO -
- * INTRO URS TEAM -
- * MAYORS RESPONSE
- * Q & A

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4009
22/24

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March 7, 2002

Dear Committee Member,

First of all, I would like to express my appreciation on behalf of the Town of Addison. This is an exciting infrastructure project for the Town and I am pleased you have agreed to participate in our evaluation and recommendation process for the Bridge Competition. As you are all aware, the competition is scheduled for April 11th from 7:30am – 5:00pm at the Addison Conference Centre (see schedule below). I would also request that all committee members meet in the Board Room at 7:30 am to go over the upcoming days events. *There will be breakfast refreshments available.*

Just a very brief update on what events took place to get us to the point at which we are today. In February 2000, the Town of Addison conducted a bond election in which the town of Addison voters approved \$20.5 million to go toward the design and construction of the extension of Arapaho Road from Addison Road to Marsh Lane. A key and crucial element to the overall project is the design and construction of a bridge to cross over Midway Road.

In November of 2001, we initiated the process by requesting Statements of Qualifications from engineering and architectural firms. The Town received 12 proposals from a wide variety of firms with connections around the nation. These 12 proposals were evaluated by a selection committee and reduced to the three finalists for the competition (HNTB, URS-Griener and Freese & Nichols). The Town of Addison City Council, prior to Request for Qualifications, approved a \$10,000 stipend to be paid to each of the three finalists to assist in the costs associated with preparation of their proposed bridge designs.

The evaluation process will be conducted as follows:

- *Each firm will be given two hours to make their presentations and answer questions from the panel, with at least 30 minutes dedicated to question and answer.*
- *Panel will use evaluation criteria stated in information below, with each item having equal value to assist in ranking each firm.*
- *Each member will then individually grade each firm, in a similar manner, giving each item a grade from 1-10, with 10 being best.*
- *Each member based on the total score given will rank the firms in order of grade 1st, 2nd, or 3rd.*
- *All panel members will submit their rankings to establish initial order.*

- *Group will have open discussions about individual concerns and opinions.*
- *All panel members will review their initial rankings and re-submit for final order.*
- *Each firm will be given an overall grade based on rank submitted by each member. 1st place will be awarded one point, 2nd place will be awarded 2 points and 3rd place will be awarded 3 points. The firm with the fewest total points will be the competition winner.*

Please note that I have included a copy of each firm's original *Statement of Qualifications* submittal.

Thank you, and I look forward to seeing all of you on April 11th. Should you have any questions prior to April 11th, feel free to contact my office at 972-450-2871 and speak with me or the Assistant Director of Public Works, Jim Pierce.

Sincerely,

Michael E. Murphy/Director of Public Works

Have discussion 1st
WE RANK BEFORE

**Arapaho Road Bridge Pre-Competition Meeting
Agenda
February 14, 2002**

- I. Welcome and introductions: *Ron Whitehead, City Manager***

- II. Directions to Consultant: *Mike Murphy, Director of Public Works***
 - A. Announce tentative day and time of presentations**
 - 1. First Presentation 8:00am-10am**
 - 2. Second Presentation 10:30am-12: 30pm**
 - 3. Third Presentation 1:30pm-3: 30pm**

 - B. No more than two designs**

- III. Grading will be based on the following criteria**
 - A. Aesthetics – appearance day and night, should include lighting design plan**
 - B. Landscaping**
 - C. Acoustics (how will noise affect adjacent buildings)**
 - D. Vibrations (how will motion affect adjacent buildings)**
 - E. Estimated cost of construction of the bridge design**
 - F. Functionality / build ability (parking, pedestrian and bike users, safety, ADA etc.)**
 - G. How does the bridge minimize obstruction to adjacent buildings from roadway**
 - H. Overall quality and creativity of presentation (vision, team plan / effort)**

Presentation Schedule:

<u><i>Time (April 11th)</i></u>	<u><i>Firm</i></u>	<u><i>Conference Centre Location</i></u>
7:30am – 8:00am	Committee Members	Board Room
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10:30am – 12:30pm	Freese & Nichols	Board Room
12:30pm – 1:30pm	Lunch	Board Room
1:30pm – 3:30pm	HNTB	Stone Cottage
3:30pm – 5:00pm	Panel Discussion/Selection	Board Room

EXAMPLE SCORE SHEET

1. **Aesthetics – appearance day and night, should include lighting design plan (1-10) 10 POINTS**
2. **Landscaping (1-10) 7 POINTS**
3. **Acoustics (how will noise affect adjacent buildings) (1-10) 5 POINTS**
4. **Vibrations (how will motion impact adjacent buildings) (1-10) 8 POINTS**
5. **Estimated cost of construction of the bridge design (1-10) 6 POINTS**
6. **Functionality / build ability (parking, pedestrian and bike users, safety, ADA etc.) (1-10) 4 POINTS**
7. **How does the bridge minimize obstruction to adjacent buildings from roadway (1-10) 10 POINTS**
8. **Overall quality and creativity of presentation (vision, team plan / effort) (1-10) 9 POINTS**


TOTAL SCORE 59 POINTS

COMMITTEE	1	2	3	4	5	6	7	TOTAL SCORE	
FIRM #1	3	1	3	2	1	3	3	16	#3
FIRM #2	2	2	2	1	2	1	2	12	#1
FIRM #3	1	3	1	3	3	2	1	14	#2

Cliff Hall, P.E. - Project Manager

Responsibilities

- Single Point of Contact
- Team Leader
- Coordination
- Maintain Schedule
- Lead Engineering Design



Cliff Hall, P.E. - Project Manager

Kap Shui Mun Bridge


- On-site Design PM
- Design Coordination
- Met Schedule
- Resolve Design & Construction Issues
- ACEG Grand Award



Chuck Armstrong, ATA Architecture


Responsibilities

- Look & Feel
- Architectural Details
- Treatment Schemes
- Color Schemes



**Chuck Armstrong, ATA
Architecture**


- Watkins Memorial Drive Freeway Bridge
- Flagship Structure
- Compliment Historic District
- Urban Amenities
- ASLA Award



**Sara Jane MacLennan,
ASLA, AICP - Urban Design**

Responsibilities:


- Urban Design
- Landscaping
- Functional
 - Parking
 - Safety
 - Hike & Bike



**Sara Jane MacLennan,
ASLA, AICP - Urban Design**

Colorado Springs
Downtown Action Plan

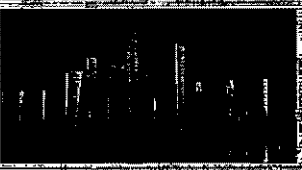
- Urban Design
- Redevelopment
- Bike Trail
- Landscaping & Streetscaping
- ASLA Award



**Robert Prouse, IALD, MIES
Lighting**

Responsibilities


- Lighting Design Scheme
- Feature Enhancement Lighting
- Functional Lighting



**Robert Prouse, IALD, MIES
Lighting**

Pearl Harbor Memorial Bridge


- Lighting Design
- Create a "unique" look
- Dedicated as a Memorial



Steve Stroh, P.E. - Engineering

Responsibilities

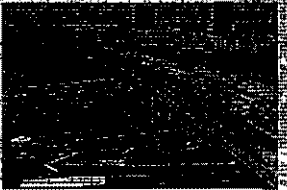
- Bridge Details
- Constructability
- Civil Design
- Cost Control



Steve Stroh, P.E. - Engineering

17th Street Bridge


- Lead Engineer
- Concept Development
- Final Design
- Constructability
- Cost Control



**Dave Johnston, P.E.
Project Principal**

Responsibilities


- Project Priority
- Commitment of URS Resources
- Peer Review

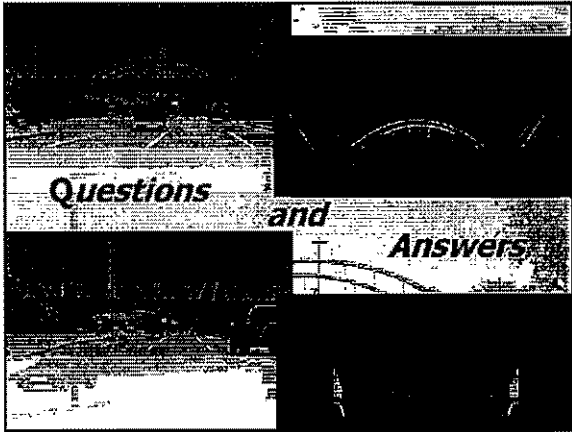


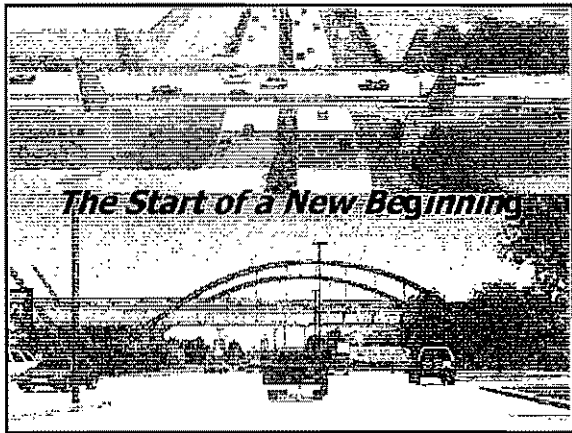
**Dave Johnston, P.E.
Project Principal**

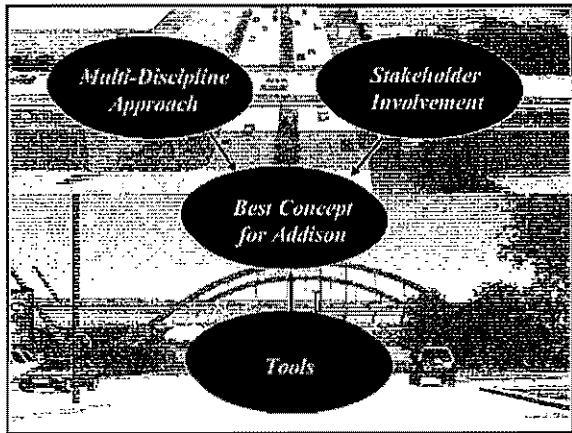
Key Projects:

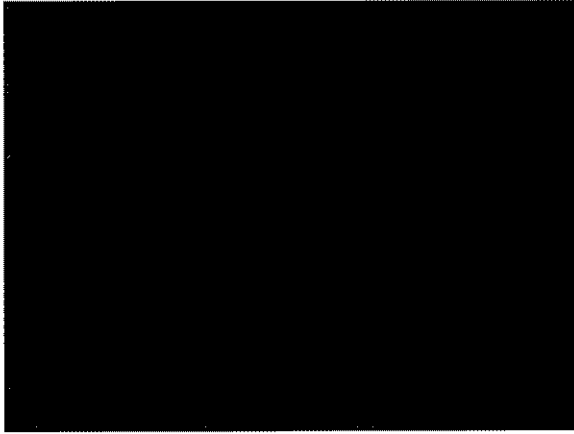
- SH 87, Bolivar Bridge over Houston Ship Channel
- Corpus Christi Harbor Bridge Replacement
- Border Highway West Elevated Roadway



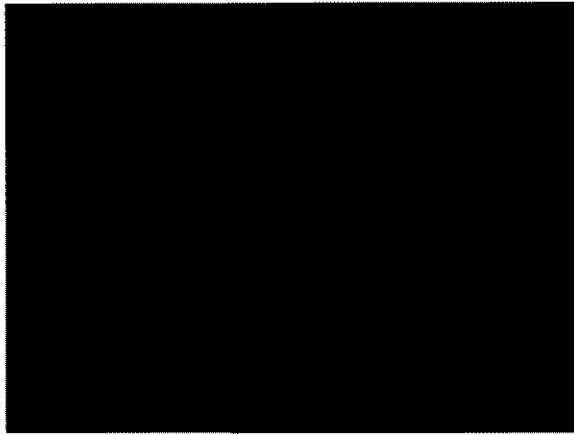




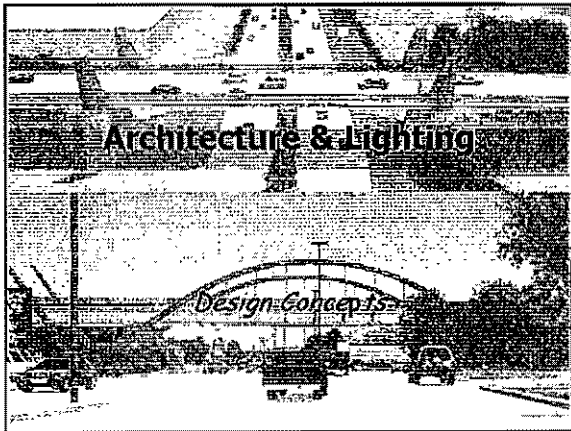


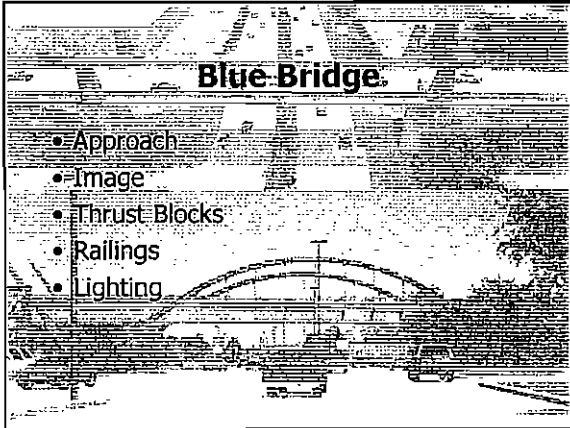


FLASHING PICTURE
INTRODUCTION

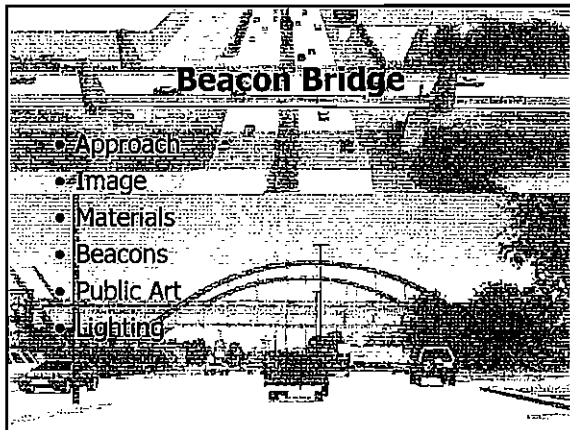


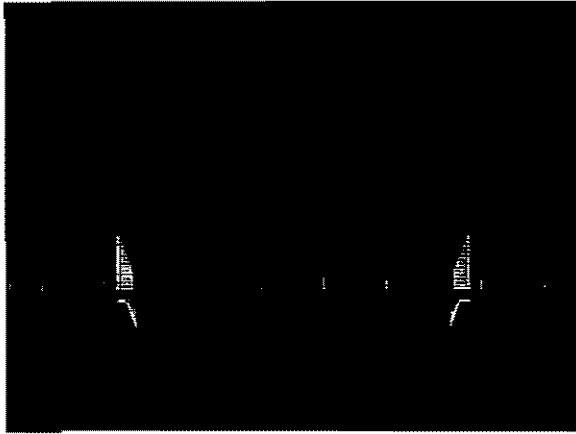
ELEVATION VIEW
"PAN" VIDEO

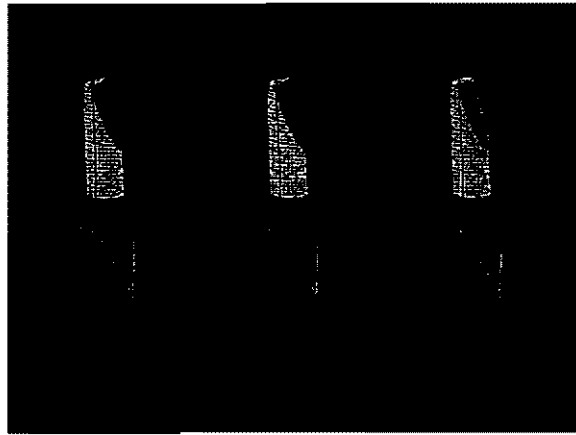


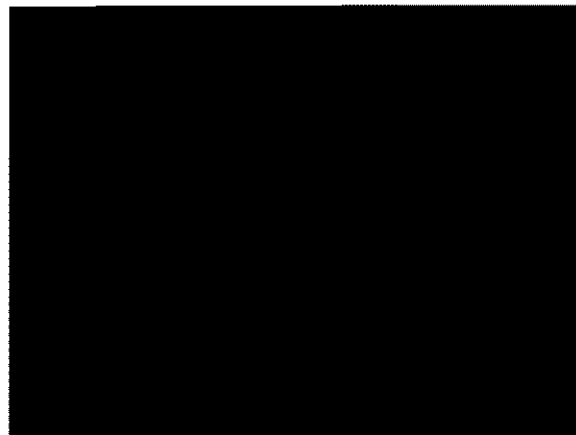




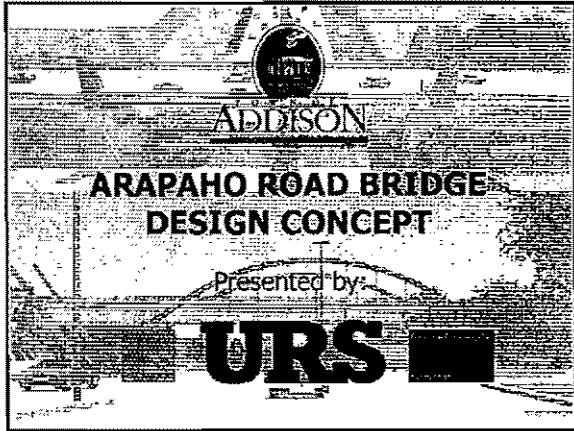




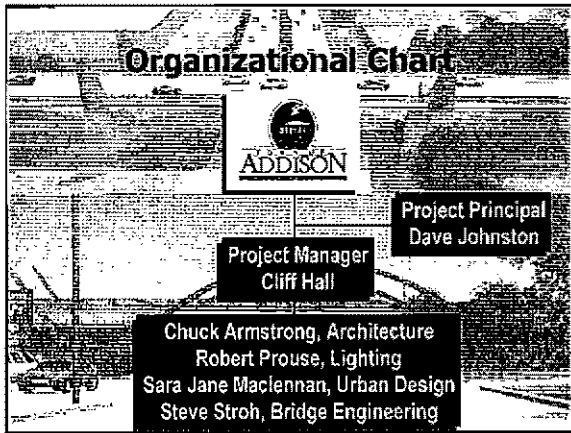


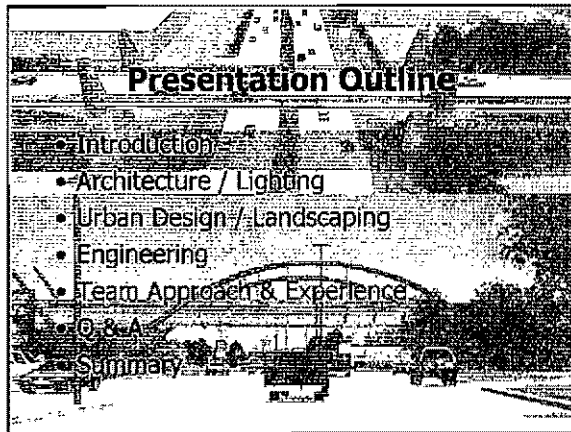


360° HELICOPTER
VIEW VIDEO

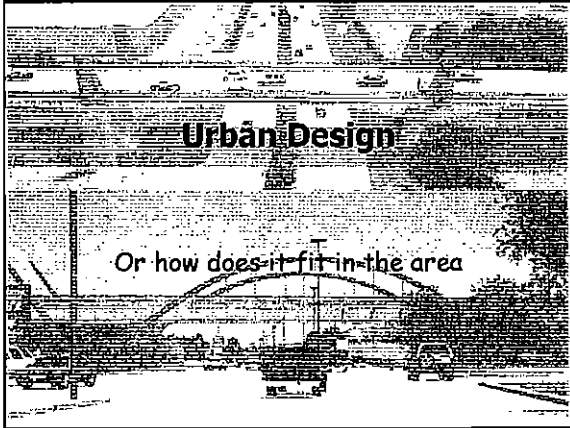


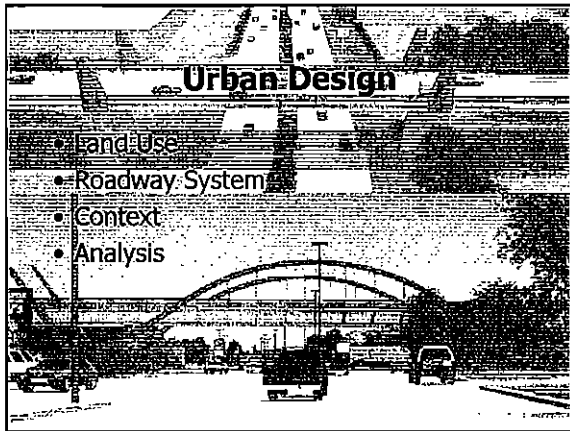
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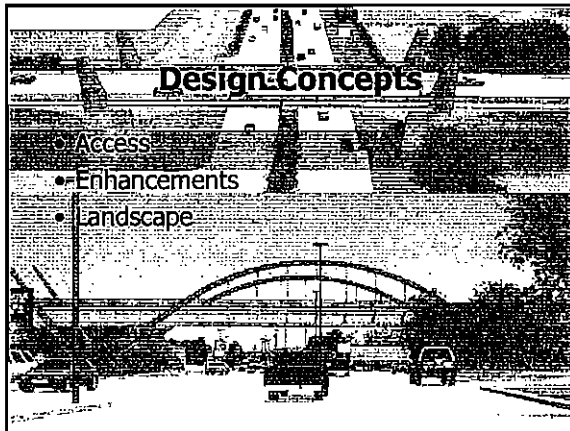


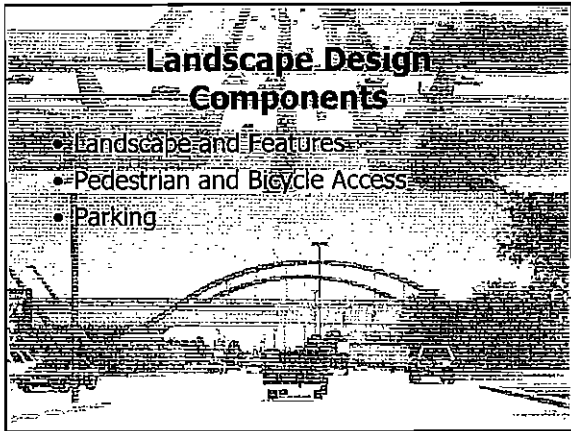


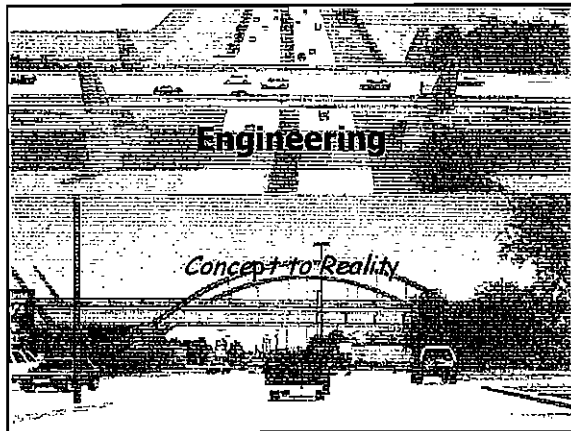
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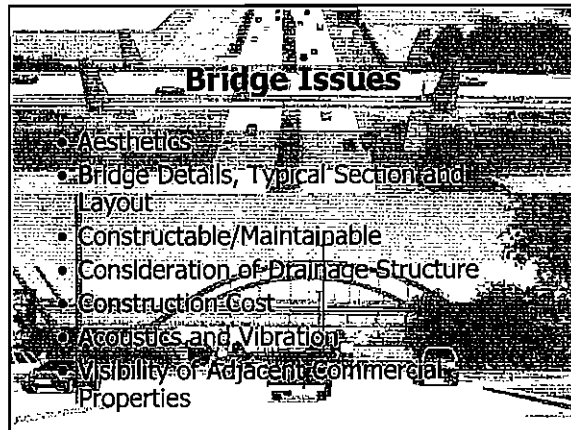




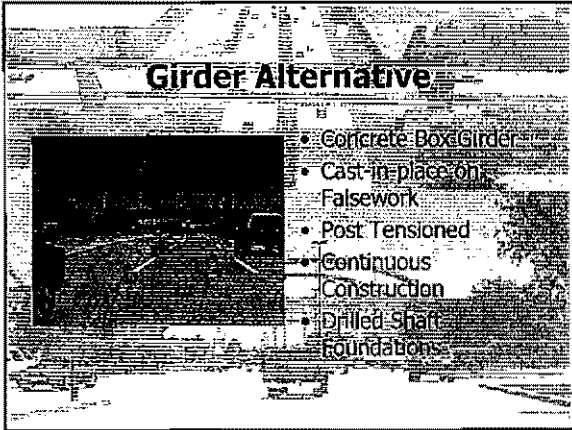


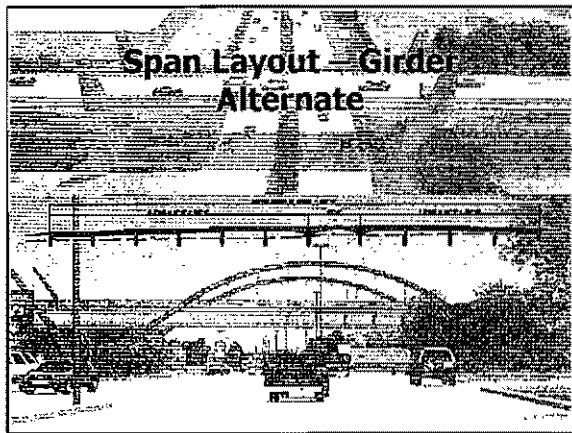


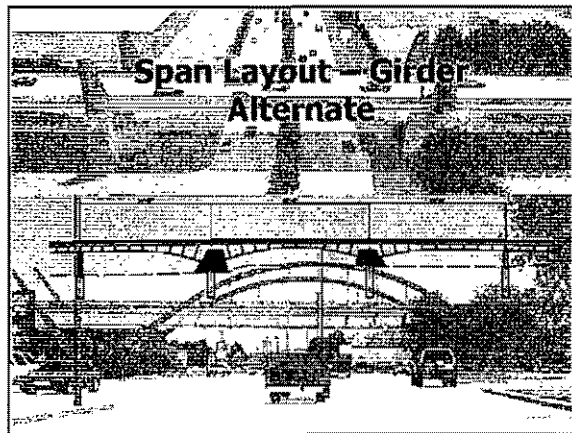


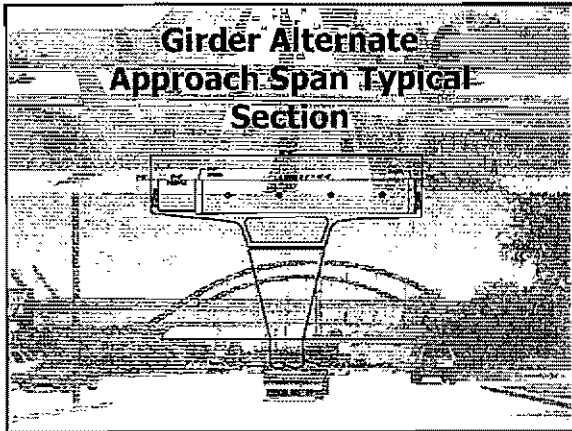


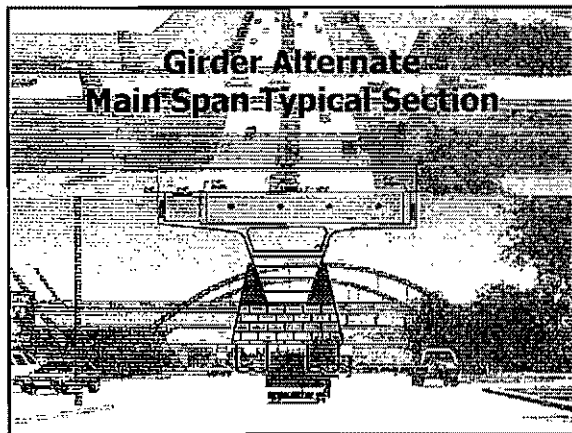
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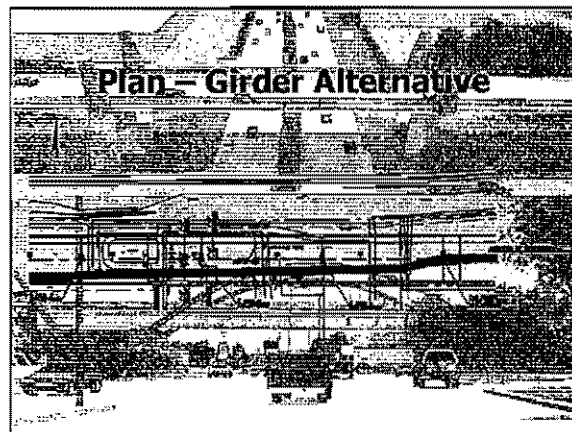


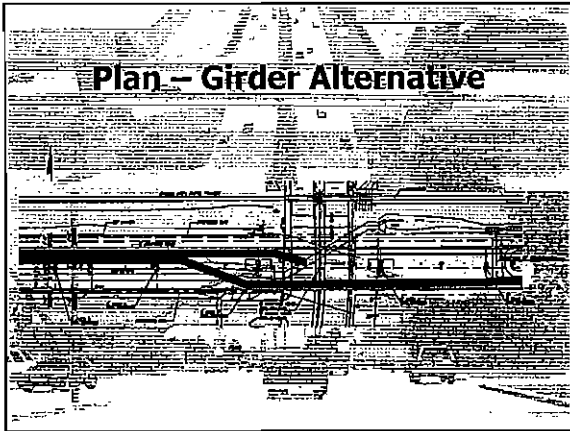


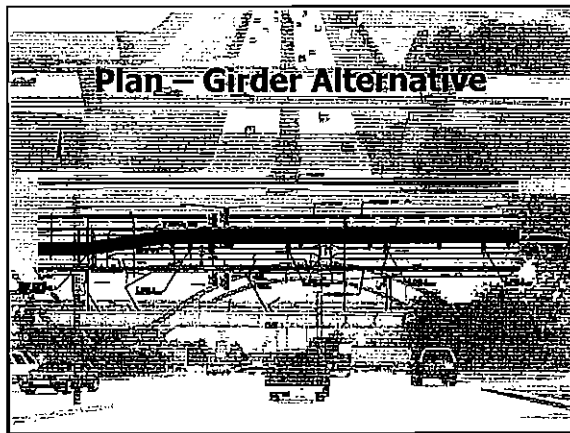


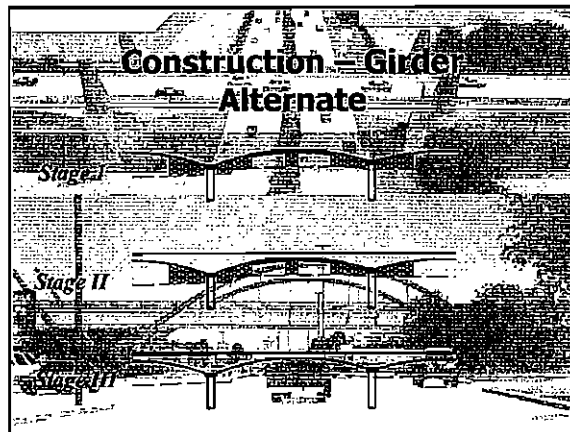


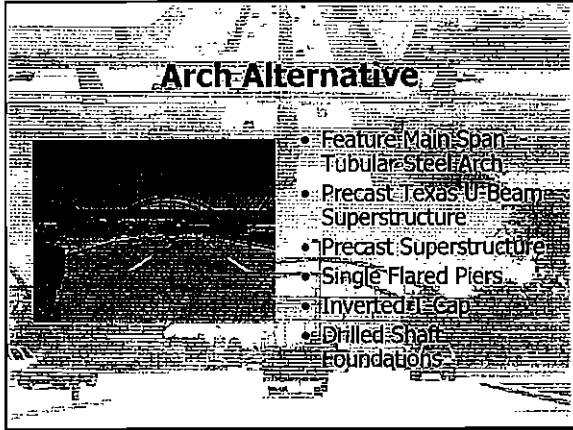


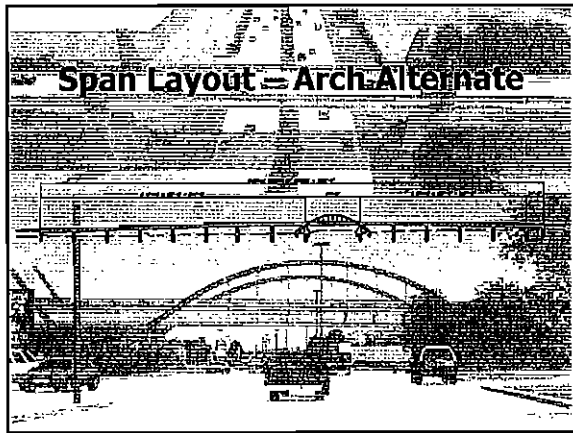


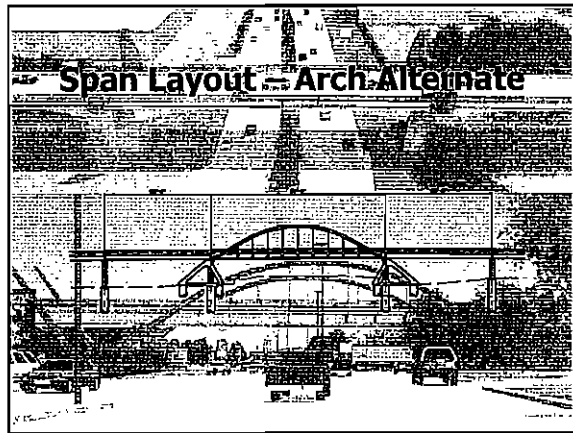


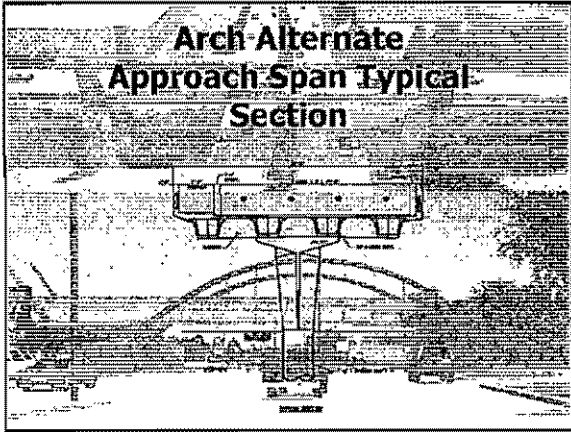


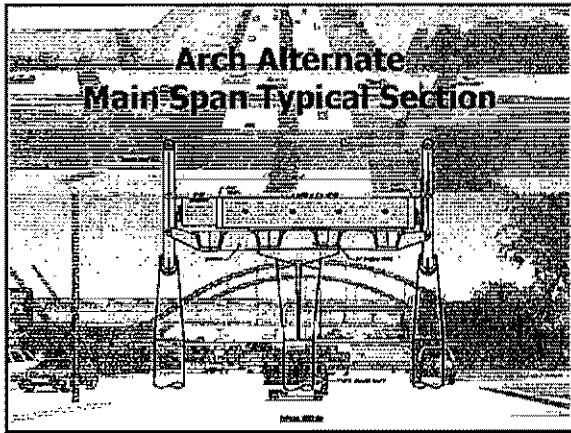


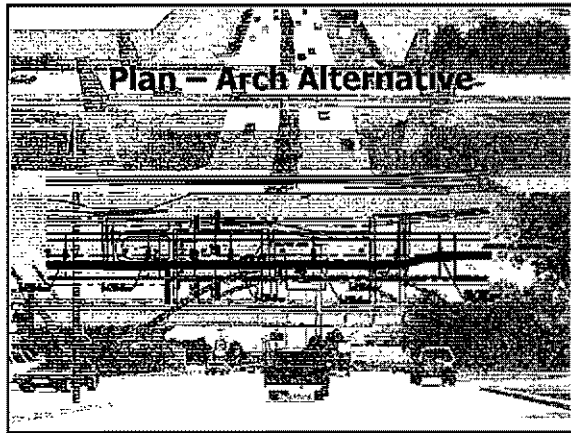


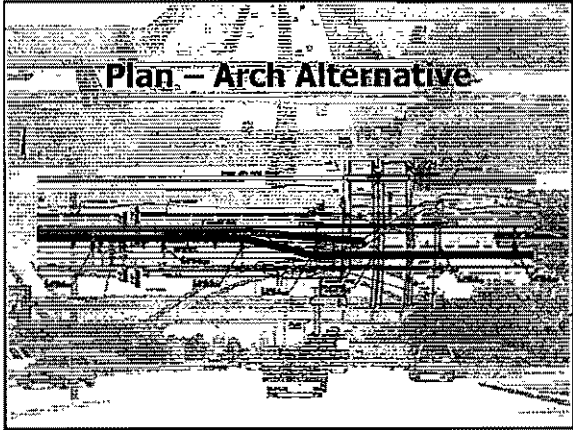


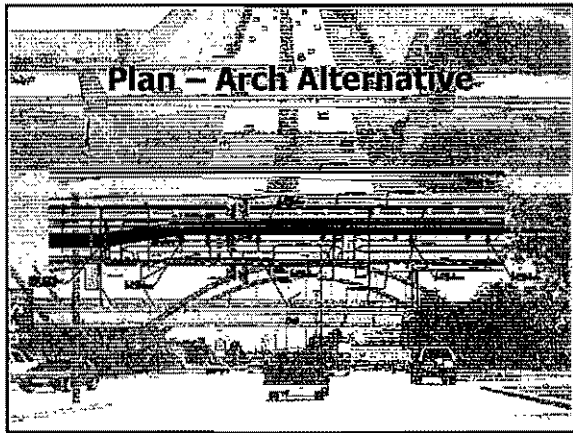


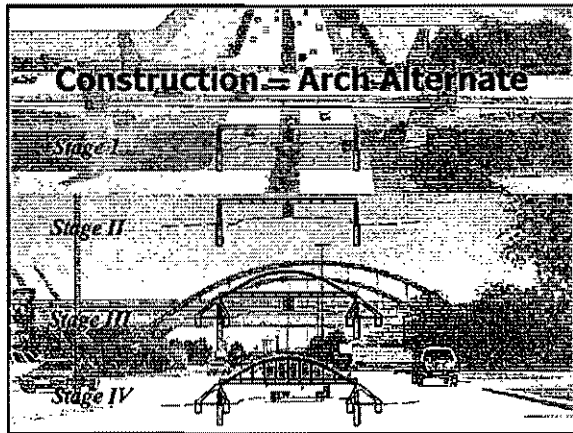


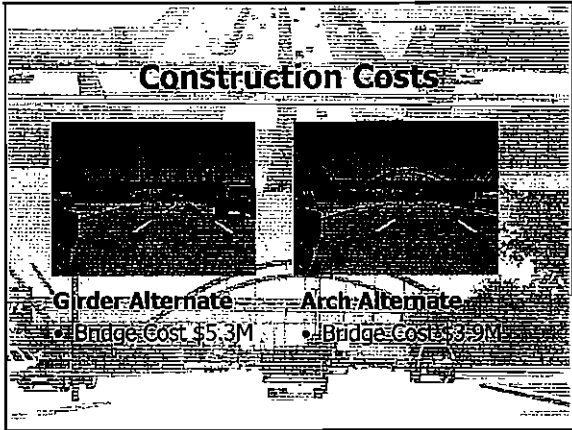


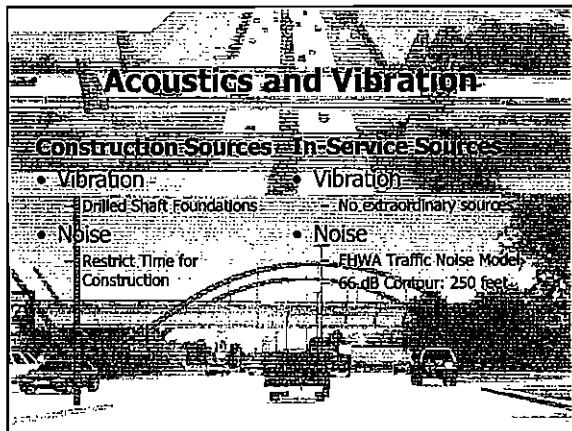


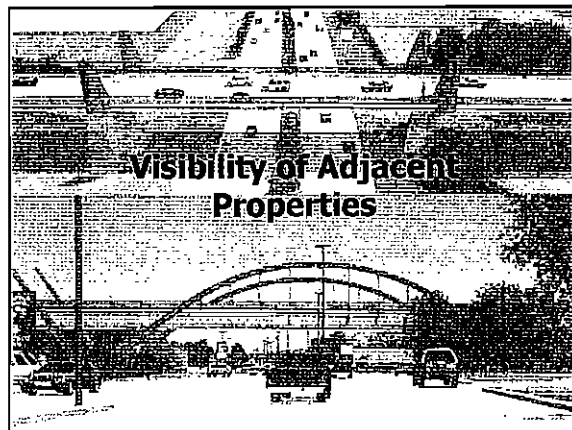


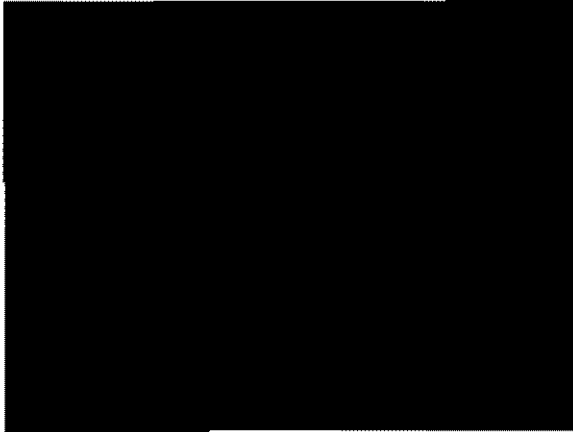




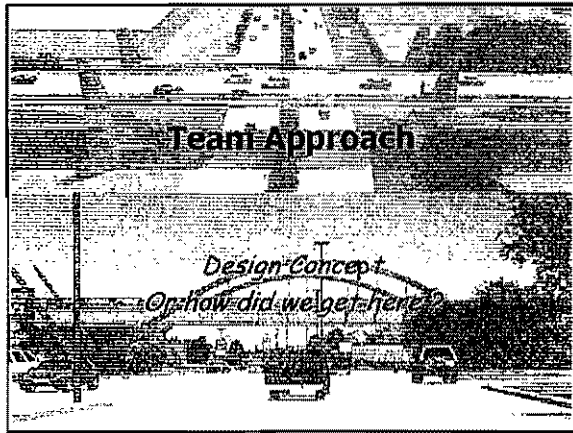




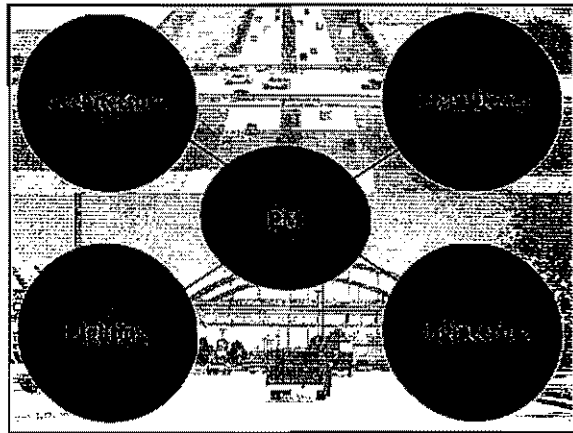




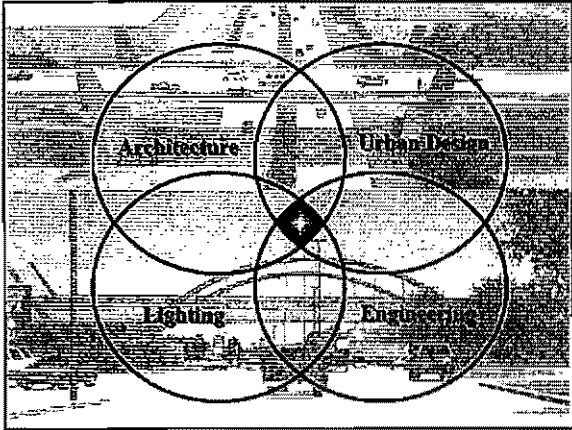
DRIVE UNDER
VIEW



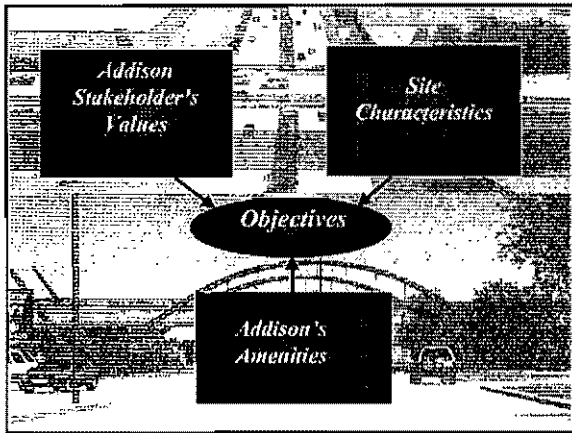
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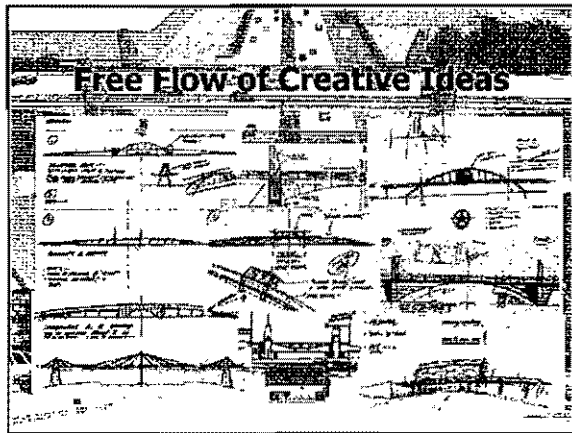
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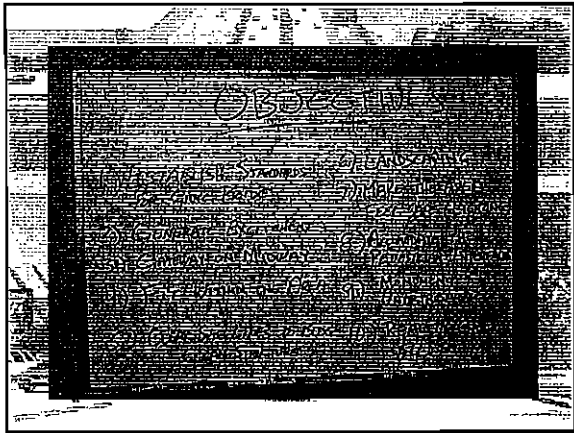


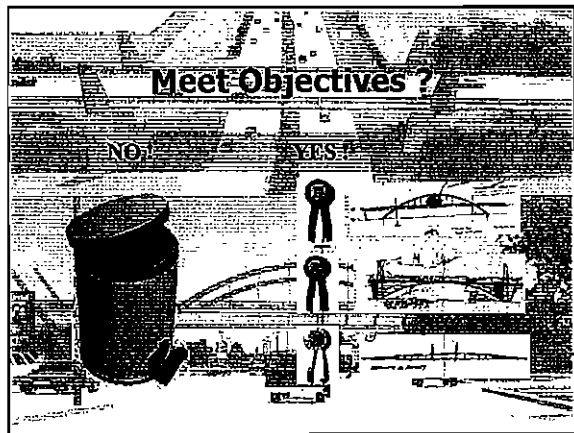
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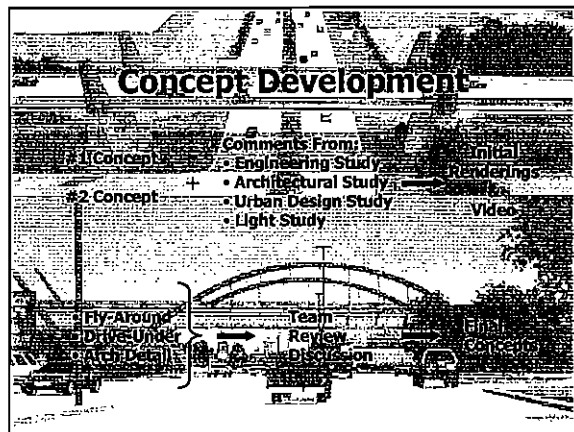


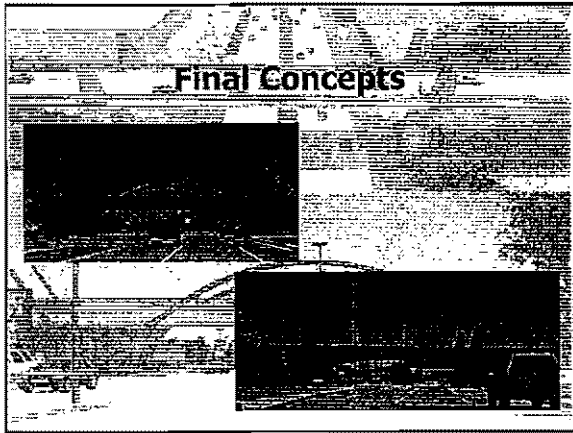
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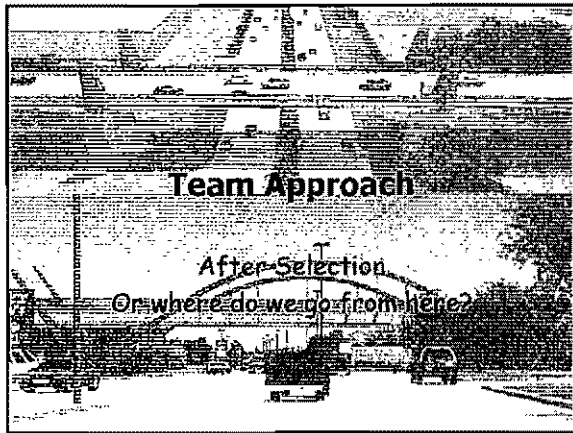




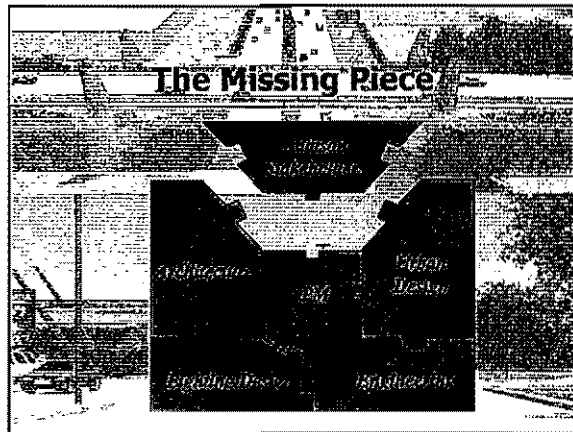




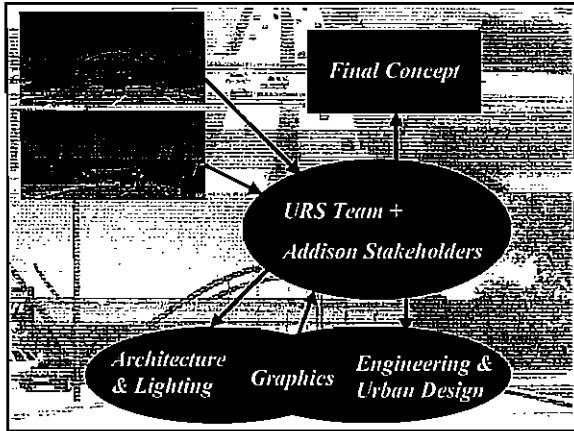




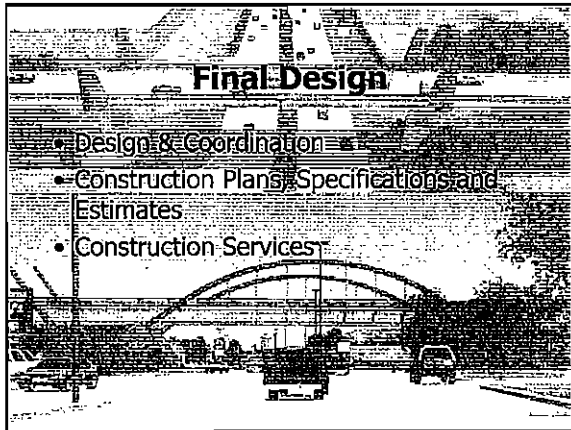
YES



YES



Yes



Yes





May 9, 2002

Mr. James C. Pierce, Jr., PE
Assistant Public Works Director
Town of Addison
16801 Westgrove Drive
P.O. Box 9010
Addison, TX 75001-9010

Re: Arapaho Road Bridge at Midway Road - Presentation Files

Dear Mr. Pierce:

As requested, we have enclosed a CD with the files required for our PowerPoint presentation. These files include two PowerPoint files and three mpeg video files. To view the entire presentation all files will have to be loaded on the computer. The files include:

Powerpoint Files:

Arapaho Presentation.ppt (main PowerPoint presentation)
Draft Presentation.ppt (continuous loop of pictures of bridges & concepts fading in and out)

Video Files

arapahoe_driver_view.mpg (drive under video)
arapahoe_pans.mpg (pan across elevation video)
arapahoe_spins.mpg (360 degree helicopter view video)

It should be noted that within Arapaho Presentation.ppt, slide number four is linked to the Draft Presentation.ppt file, slide numbers five and 70 are linked to the arapahoe_pans.mpg file, slide number 12 is linked to the arapahoe_spins.mpg file and slide number 40 is linked to the arapahoe_driver_view.mpg file. Deletion of any of these files will delete the link and remove the video or continuous loop from the presentation. Also, when viewing the slide show in PowerPoint, the continuous loop can only be stopped by pressing the "escape" key on the computer. The slide show can then be resumed by pressing the down arrow.

As there may be some "quirks" in running the slide show we will be happy to assist you in any way we can. Please call if you have any problems viewing the presentation or would like our assistance.

Sincerely,

URS Corporation

Cliff R. Hall, PE
Project Manager

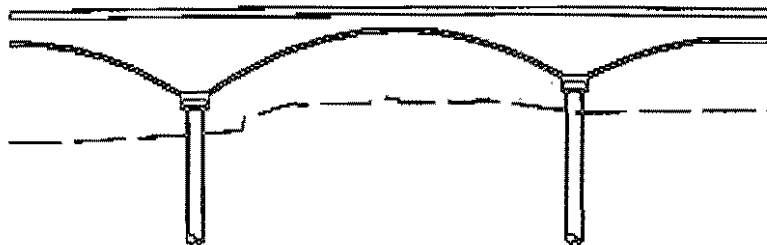
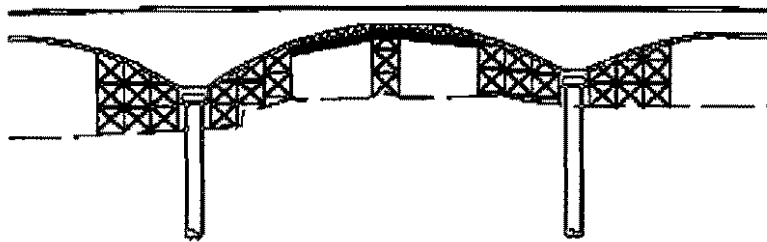
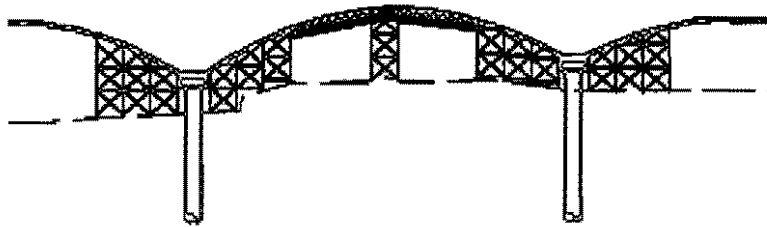
Enclosure - CD

URS Corporation
Prestonwood Tower
5151 Beltline Road, Suite 700
Dallas, TX 75254
Tel: 972.980.4961
Fax: 972.991.7665

CAPITAL PROJECT

SUMMARY BOOK

JUNE 2002



MIKE MURPHY

THESE ARE MAYOR'S PERSONAL
NOTES FROM THE 4-18-02
TOWN MEETING PRESENTATION.

Bill

INTRODUCTION:

- Introduce City Council and City Management
- LP Cookingham award – ICMA – top 5 awards - Mentorship
- In 1995, Intern project -- collect photos
- Andy Eads – Town Historian
- New Career
- Photos, journals, certificates, newspaper clippings, report cards, and tax statements
- Book Idea Approved 01/97
- Arduous process The book quickly grew from 128 pages to 192 pages.
- December of 2001, 192 pages
- Some said it might never happen
- Andy Proclamation

TRIVIA ANSWERS:

1. Who was the Town of Addison Named after?
 - Addison Robertson
 - 1838-1927
 - Confederate Captain
 - Carpenter
 - Addison Postmaster
 - Under Cover Secret Agent in WW1
2. What Year was the Town of Addison incorporated?
 - Settled in early 1840's
 - Incorporated in 1953 to avoid annexation by Dallas
3. Who was Keller Springs named after?
 - Henry and Mary Jane Keller
 - Former slaves in Tennessee
 - Settled North of Keller Springs Road.
 - Natural springs on their property -- shared with other local farmers
 - Land was between Preston Road and the Tollway, north of Keller Springs Road
4. What was Town Hall before?
 - Stonegate Hall built in 1939 for Maud and J. B. McEntire
 - Mrs. McEntire's dream house
 - Named for the stone gate entrance that used to be on Belt Line Road,
 - Driveway Part of the old Preston Trail System
 - Friends – Langfords lived there

5. What was dubbed the new "State Bird of Texas" in the 1980's due to its prevalence in Addison and North Dallas?
 - o Building cranes

 6. Where was the first restaurant in Addison?
 - o At the Addison airport, which opened in 1957

 7. What is the oldest commercial building in Addison?
 - o The Addison State Bank
 - o 1913
 - o Broadway Street and Addison Road
 - o Grocery store in the late 1920's
 - o Refurbished in 1983 -- awarded a Historical Marker
 - o Network Funding Corporation (a residential mortgage bank)

 8. What is the oldest church in Addison?
 - o The White Rock Chapel - Winnwood
 - o Only traditional church in Addison.
 - o Built in the mid 1880's by freed slaves
 - o Still going strong today.
 - o S.S. Noell land story
-

THEN AND NOW:

- Transportation and accessibility
- Commerce
- Neighborhoods
- Schools
- Amenities
- Numbers
- Other Information

PIONEERING AND ENTREPRENEURIAL SPIRIT:**Yesterday:**

- Early farmers
- Visionaries S. S. Noell and W.W. Julian -- \$ & land for Cotton Belt
- Preston Witt - 1 of 1st known settlers, first mill and trading post
- Landscaping Vision
- Zoning Vision

Today:

- We have entrepreneurs building businesses from the ground up
- Fortune 500 companies
- Restaurant owners
- Residents who care about the community

THEN, NOW AND FUTURE

- Pioneering and entrepreneurial spirit
 - Vision
-
-

TRANSPORTATION/ACCESSIBILITY

BELT LINE, BELT LINE, BELT LINE

Yesterday:

- 70's - 4 lane asphalt road

Today and Future:

- Thriving, busy road
- Reinventing Beltline
- Bond Program
- ULI
- Opportunities for change:
 - Entertainment
 - Alleviating traffic
 - Attracting new tenants
 - Making Beltline pedestrian friendly
 - Beautification
 - Safety
 - Utility Relocation
- Arapaho extension
 - Phase II -- 12 mos. ahead -- \$300K under budget
 - Phase III - Design Underway
 - Bridge over Midway
 - Design Competition

TOLLWAY

Yesterday:

- Extension to Addison -- Early 1990's
- Dig down and landscape

Today:

- Tollway still provides that main access to Addison
- 450K cars per day

AIRPORT

Yesterday:

- Opened in 1957

Today:

- More than 750 aircraft based
- AA - busiest general aviation airport in Texas
- Top 3 to 4 busiest general aviation airports in USA
- National Air Transportation Association -- one of the 100 most needed airports in the nation

COTTON BELT

Yesterday:

- Came to Addison -- late 1890's
- Depot in 1904

Today:

- Bring Dart trains to Addison on the Cotton Belt line

COMMERCE

Yesterday:

- Started with farming
- Then, area's center of commerce
- General Merchandise -- Grocery Stores
- Due to Train Depot at Addison Road and Broadway
- "Wet" town in 1975 -- Addison a dining destination

Today:

- 100's of businesses
- Technology start-ups to established Fortune 500 HQ's
- 21 hotels
- 150 restaurants
- Retail -- Village on the Parkway is 90% full - Kmart - CompUSA
- Addison and surrounding area employ more people than downtown Dallas

Future:

- Long-term viability
- Look at everything -- ordinances, etc.(tax abatements and incentives)
- Attract & retain businesses
- Restaurants: Tin Star, Flying Fish, Dukes Original Road House, Enchiladas, etc.
- We want people to come to Addison to dine and stay for the fun!
- Office buildings make it happen

NEIGHBORHOODS:

Yesterday:

- Farms
- Visionaries -- Landscape -- Make Addison Livable

Today:

- Addison is proud to have neighborhoods of all types and socio-economic levels
- Integrated and Urban

Future:

- Health Club Update -- Contractor -- Site Work -- Swim Next Summer
- Live, Work and Play in Close Proximity
- Density
- Addison Circle
- Plans for high rise condo's and apartments

SCHOOLS

Yesterday:

- 1st schools – 1 Room
- Addison School built in 1914
- In the 1950's -- Addison made the decision to fold our schools in with the Dallas Independent School District

Today:

- Addison School now Magic Time Machine Restaurant
- Oldest Building in Addison

Future:

- New DISD school site – closer to Addison residents

RESIDENT & VISITOR AMENITIES

Yesterday:

- In 1975, Addison built Town Park
- Sidney Drive -- Brookhaven Club Neighborhood

Today:

- 10 parks , over 55 acres
- Preserve and Enhance the Town's exceptional quality of life
- Through Landscaping of the Community's Public & Private Properties.
- Planted 88,000 Tulip Bulbs this year
- Planted over 300 trees in last two years
- 90 new Crape Myrtles -- Tollway service road
- Created Arbor Foundation
- Enhance Town's spring and fall color presentation by planting flowers and trees in parks and greenways
- Numerous Awards:
 - American Society of Landscape Architects
 - Texas Forest Service
 - Professional Grounds Management Society
 - Texas Recreation and Parks Society

#1!!! - Yesterday, today and in the future

- Addison is safe and clean!
- Arts and events district update

THE NUMBERS

	<u>1976</u>	<u>2002</u>
General fund revenues	\$586,232	\$21,889,920
General fund expenditures	\$649,376	\$22,647,720
Property tax (GF only)	\$141,232	\$5,731,820
Sales tax	\$210,000	\$9,700,000

Today:

- **Financially Sound**
 - 25% Fund Balances
 - Still no budget cuts (X Hotel Fund)
 - Moody's - A vibrant local economy and sizable tax base highlighted by low debt levels and solid financial operations
 - S&P - The continued diversification of the tax base coupled with a financial position that continues to strengthen.
- **Sales Tax**
 - Numbers (Fiscal thru Feb)
 - Addison +1.87%, Dallas -7.67%, Richardson -18.25%, Dallas County -8.97%
 - Latest Month
 - Sales Tax Importance
 - Pays for 1/2 salary of police and fire
 - Ad campaign
- **Advertising Campaign for shopping and restaurants**
 - Radio
 - Dallas Morning News Guide
 - D Mag
 - TX Monthly
 - DBJ
 - Guide Live.com
 - Dallasdinesout.com

- ~~\$250,000~~
- Here forward - \$75,000
- Much more on PR
- **Hotels/Hotel Tax/Occupancy**

Year over year	% of Occupancy	% of Occupancy Tax
July 2001	Down 8%	Down 14%
October 2001	Down 28%	Down 43%
December 2001	Down 12%	Down 28%
February 2002	Down 9.7%	Down 12%

- \$480K 9/10/11, 2001
- Ad Campaign - with hotel association
- Travel click, StarCite, David Green Leads
- \$280K

OTHER INFORMATION:**Hoops & Hopes check presentations to charities**

- \$2965 each charity
- Lynn Pokorny of TCA - Lisa Landry Childress Fund.
- Nancy Gale, Executive Director of Bryan's House
- Bryan's House is a local non-profit agency that provides day care and medical assistance for small children who are HIV-positive and children with siblings or parents who have AIDS.
- The Lisa Landry Childress Foundation was founded in 1995 in memory of former Dallas Cowboys coach Tom Landry and Mrs. Landry's youngest daughter Lisa. The organization aims to further public awareness of organ and tissue donation and transplantation.

Elections

- Early Voting - Now through April 30th, Central Fire
- Saturday, May 4th, Election Day, Central Fire Station and Fire Station Two

Special Events

- Arbor Day, Les Lacs Pavilion - Saturday, April 27th
- Addison Garage Sale - Saturday, May 11th
- Taste Addison - May 18-19
- Shakespeare Festival - June 27-29
- Kaboom Town - Wednesday, July 3rd
- Spikefest - July 20-21
- Oktoberfest - September 19-22

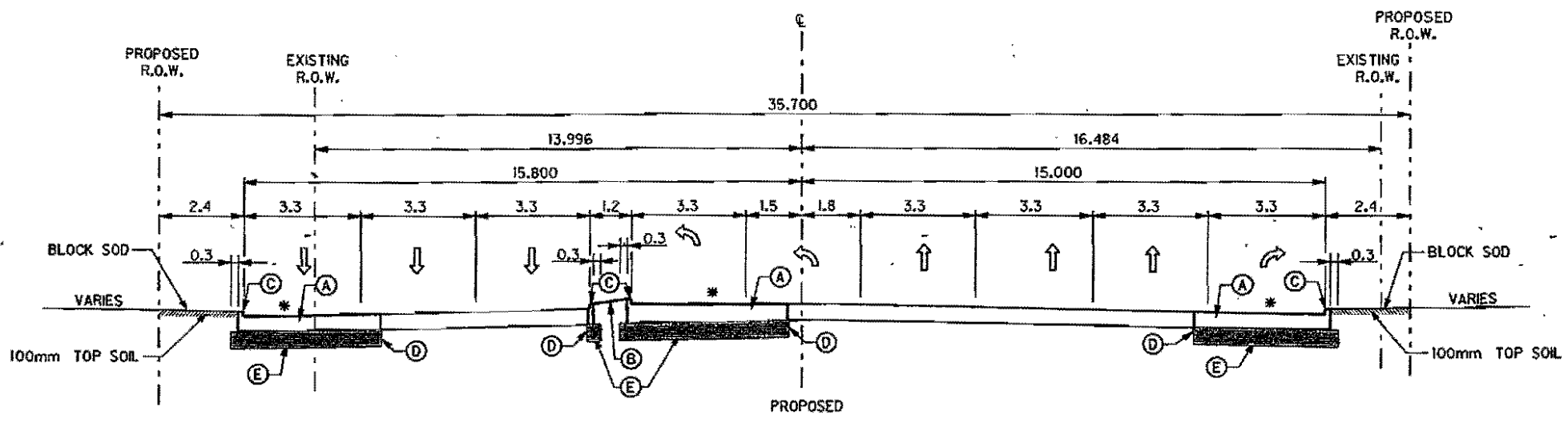
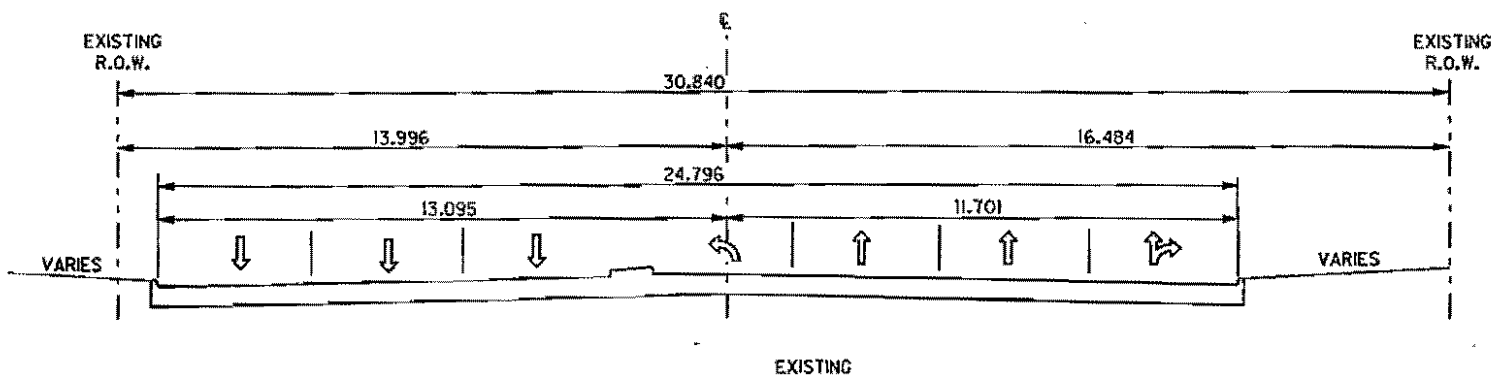
CLOSE

- Small Town to Entertainment Destination and Business Hub
- Addison -- Dedicated to Quality
- Making it Better to Live, Work and Play in Addison
- Finite Area - Keeps Improving
- Stable politically
- ~~Fiscally Sound~~
- Safe and Clean
- Embarking on New Directions and Endeavors
- Bright New Future

Q & A

① Note to those who voted for DARTS.

② Miranda - Power line
kind here, next
segment



SECTION C-C MIDWAY ROAD

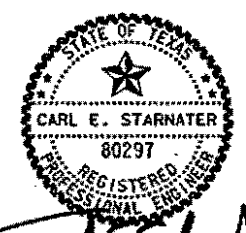
* MATCH EXISTING CROSS SLOPE

LEGEND

- (A) 200mm CONCRETE PAVEMENT- CPCD
- (B) RAISED MEDIAN
- (C) MONOLITHIC CURB (TYPE II)
- (D) FULL DEPTH SAW CUT
- (E) 100mm ACP TY B BASE
- (F) 100mm CONCRETE SIDEWALK
- (G) RETAINING WALL

GENERAL NOTES

1. SEE PAVING PLANS FOR SECTION LOCATIONS.
2. ALL DIMENSIONS SHOWN ARE IN METERS UNLESS OTHERWISE NOTED.
3. TYPICAL SECTIONS ARE NOT DRAWN TO SCALE.
4. MATCH EXISTING CROSS SLOPE.

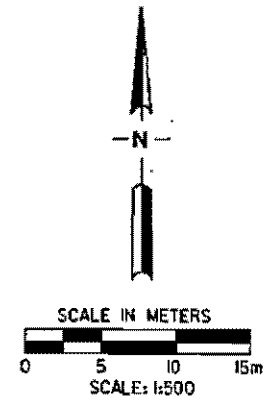


JES
3/14/02

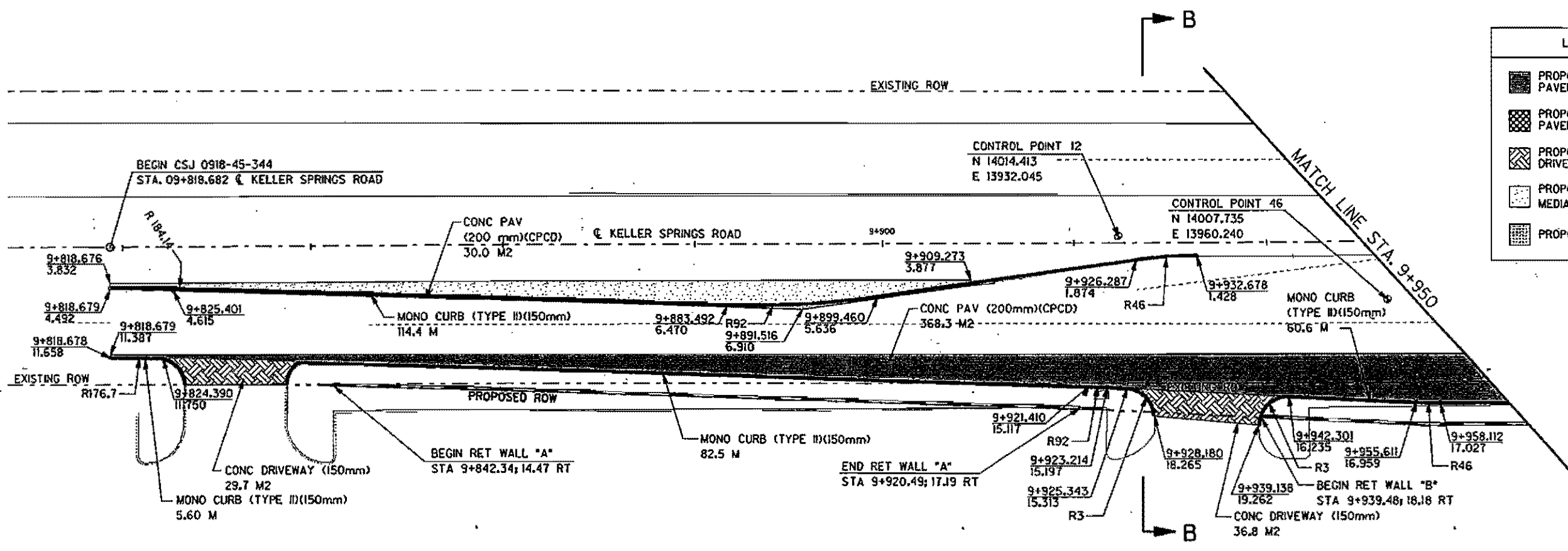
Carter-Burgess
Texas Department of Transportation

TYPICAL SECTIONS
KELLER SPRINGS ROAD
AT MIDWAY
SHEET 2 OF 2

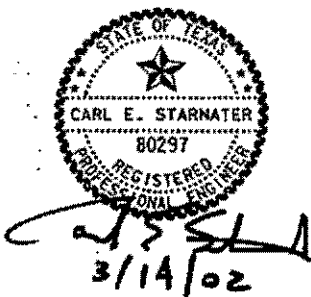
DESIGNED: DMH	FILE NO. 071-001	STATE TEXAS	FEDERAL AID PROJECT NO. CM 571 (87)	PRIMARY NO. 06
DRAWN: R-S	STATE DIST. NO. DALLAS	COUNTY DALLAS	SUBSECTION NO. 45	SHEET NO. 344
CHECKED: CES				72



LEGEND	
	PROPOSED CONCRETE PAVEMENT
	PROPOSED LANDSCAPE PAVER MEDIAN
	PROPOSED CONCRETE DRIVEWAY
	PROPOSED GRASS MEDIAN
	PROPOSED SIDEWALK



- NOTE:
1. SEE SIGNAL DRAWING FOR DETAILS.
 2. SEE INLET EXTENSION DETAILS FOR PIPE SIZES AND ELEVATIONS.
 3. ALL CALLOUTS ARE TO FACE OF CURB UNLESS OTHERWISE NOTED.
 4. PAVER AND SUB-BASED TYPES TO BE APPROVED BY TOWN OF ADDISON.
 5. SEE M.S. DETAILS SHEET FOR LANDSCAPE PAVER DETAILS.

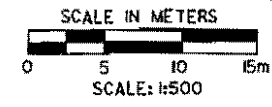


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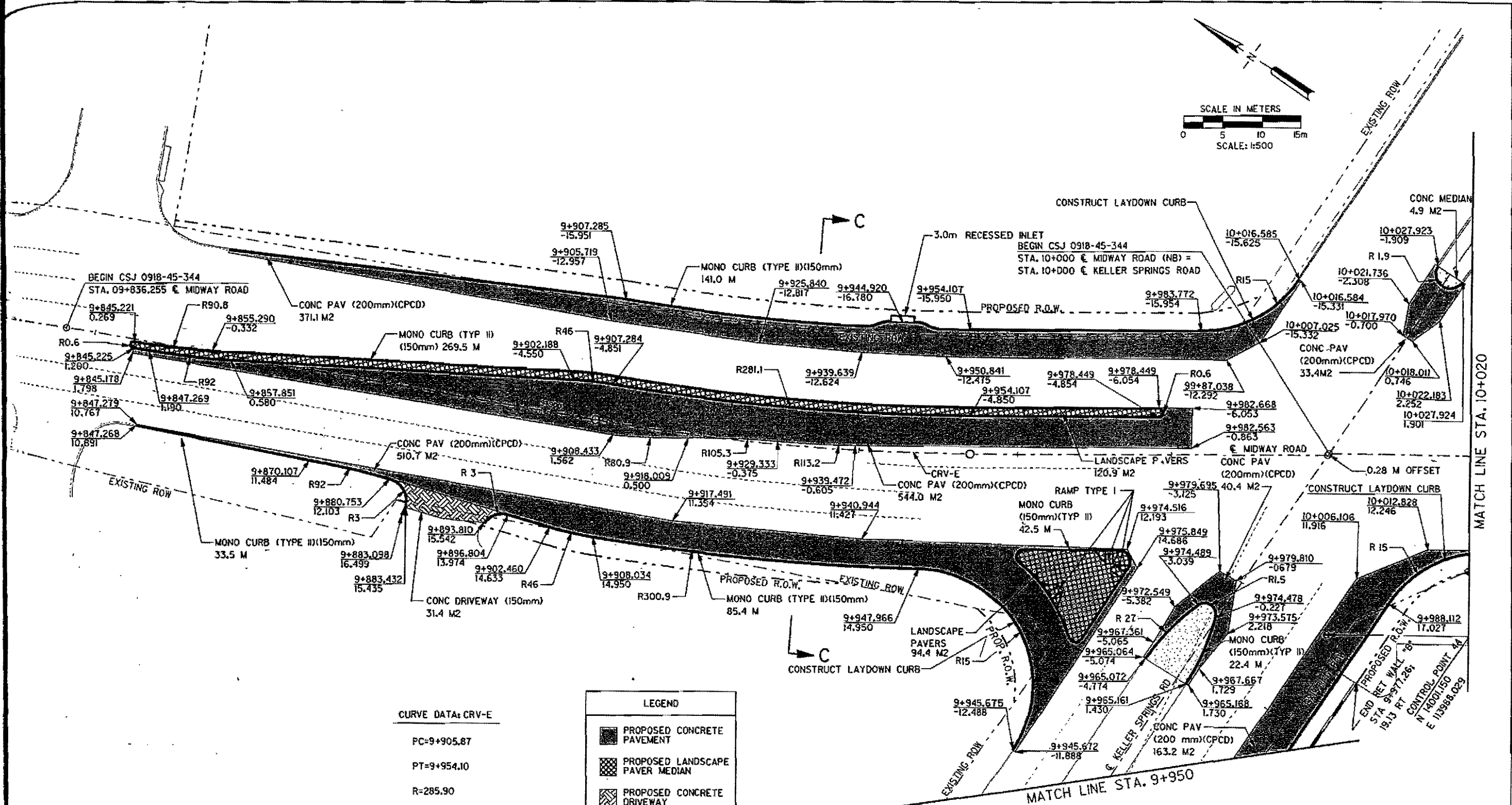
PAVING PLAN
KELLER SPRINGS ROAD
AT MIDWAY

SHEET 1 OF 3

DESIGNED: D&H	FED. RD DIST. No.	STATE	FEDERAL AID PROJECT No.	HIGHWAY No.
CHECKED: CES	6	TEXAS	CM 971 (87)	CG
DRAWN: P&S	STATE DIST. No.	COUNTY	DISTRICT No.	SECTION No.
CHECKED: CES	DALLAS	DALLAS	01B	45
				344



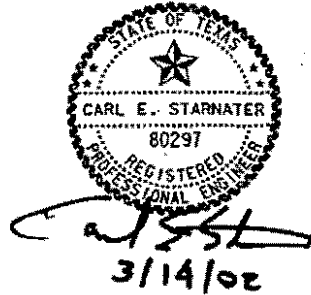
SCALE IN METERS
SCALE: 1:500



CURVE DATA: CRV-E
 PC=9+905.87
 PT=9+954.10
 R=285.90
 Δ=10° 44' 24" LT
 T=24.17
 L=48.23

LEGEND	
	PROPOSED CONCRETE PAVEMENT
	PROPOSED LANDSCAPE PAVER MEDIAN
	PROPOSED CONCRETE DRIVEWAY
	PROPOSED GRASS MEDIAN
	PROPOSED SIDEWALK
	PROPOSED CONCRETE MEDIAN

- NOTE:**
1. SEE SIGNAL DRAWING FOR DETAILS.
 2. SEE INLET EXTENSION DETAILS FOR PIPE SIZES AND ELEVATIONS.
 3. ALL CALLOUTS ARE TO FACE OF CURB UNLESS OTHERWISE NOTED.
 4. PAVER AND SUB-BASE TYPES TO BE APPROVED BY TOWN OF ADDISON.
 5. SEE MISC. DETAILS SHEET FOR LANDSCAPE PAVER DETAILS.

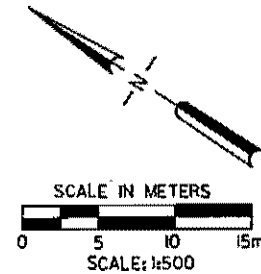


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 Texas Department of Transportation

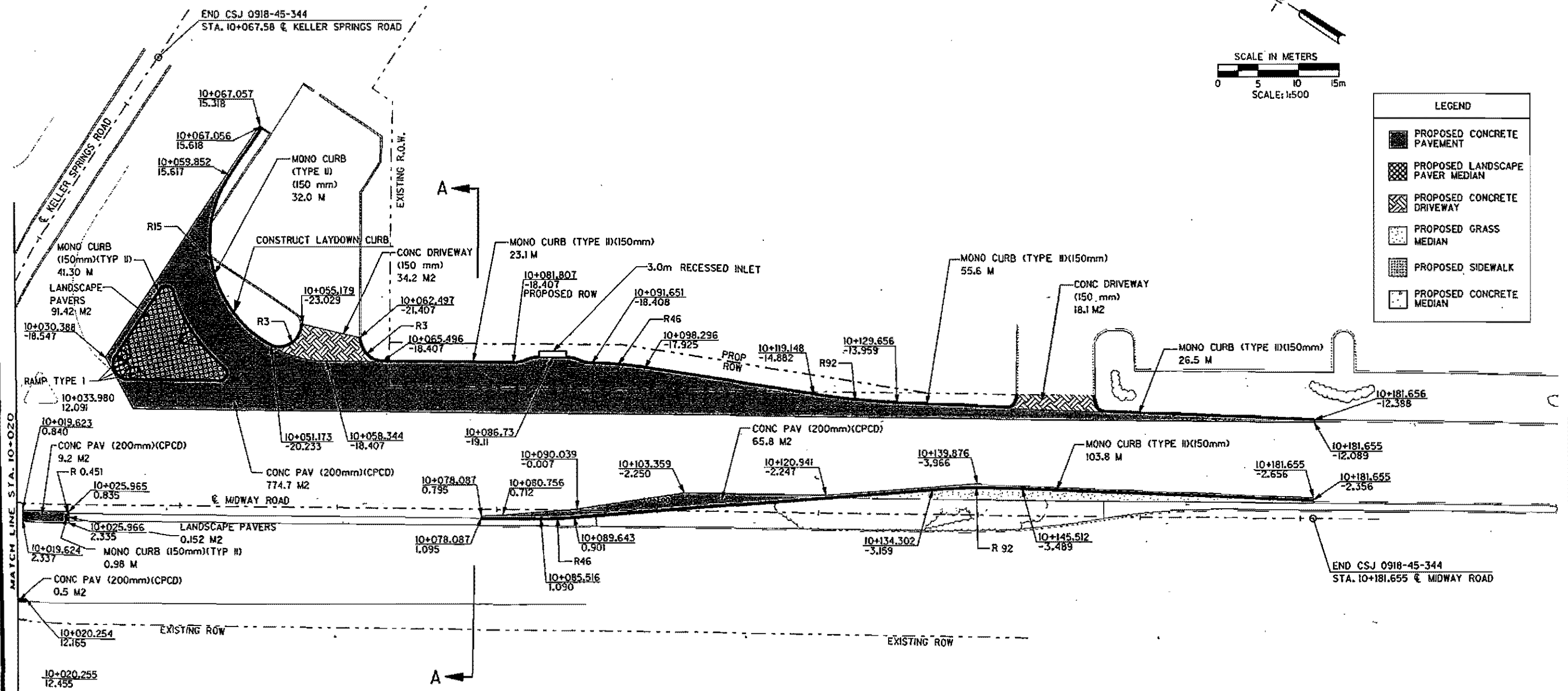
PAVING PLAN
 KELLER SPRINGS ROAD
 AT MIDWAY

SHEET 2 OF 3

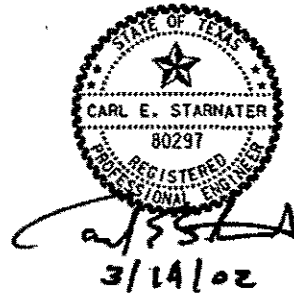
DESIGNED: DMH	FEA. NO. 6	STATE: TEXAS	FEDERAL AID PROJECT NO. CM 571 (P)	THROWBY NO. 05
DRAWN: CEG	STATE DIST. NO. 45	COUNTY: DALLAS	CONTROL NO. 828	JOB NO. 45 344
CHECKED: CEG	DALLAS	DALLAS	828	45 344



LEGEND	
	PROPOSED CONCRETE PAVEMENT
	PROPOSED LANDSCAPE PAVER MEDIAN
	PROPOSED CONCRETE DRIVEWAY
	PROPOSED GRASS MEDIAN
	PROPOSED SIDEWALK
	PROPOSED CONCRETE MEDIAN



- NOTE:
1. SEE SIGNAL DRAWING FOR DETAILS.
 2. SEE INLET EXTENSION DETAILS FOR PIPE SIZES AND ELEVATIONS.
 3. ALL CALLOUTS ARE TO FACE OF CURB UNLESS OTHERWISE NOTED.
 4. PAVER AND SUB-BASE TYPES TO BE APPROVED BY TOWN OF ADDISON.
 5. SEE MISC. DETAILS SHEET FOR LANDSCAPE PAVER DETAILS.



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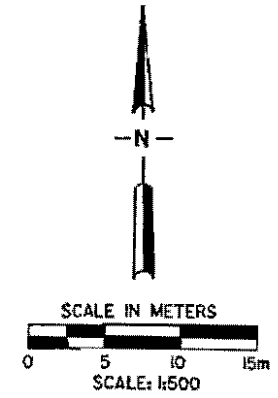
PAVING PLAN
KELLER SPRINGS ROAD
AT MIDWAY

SHEET 3 OF 3

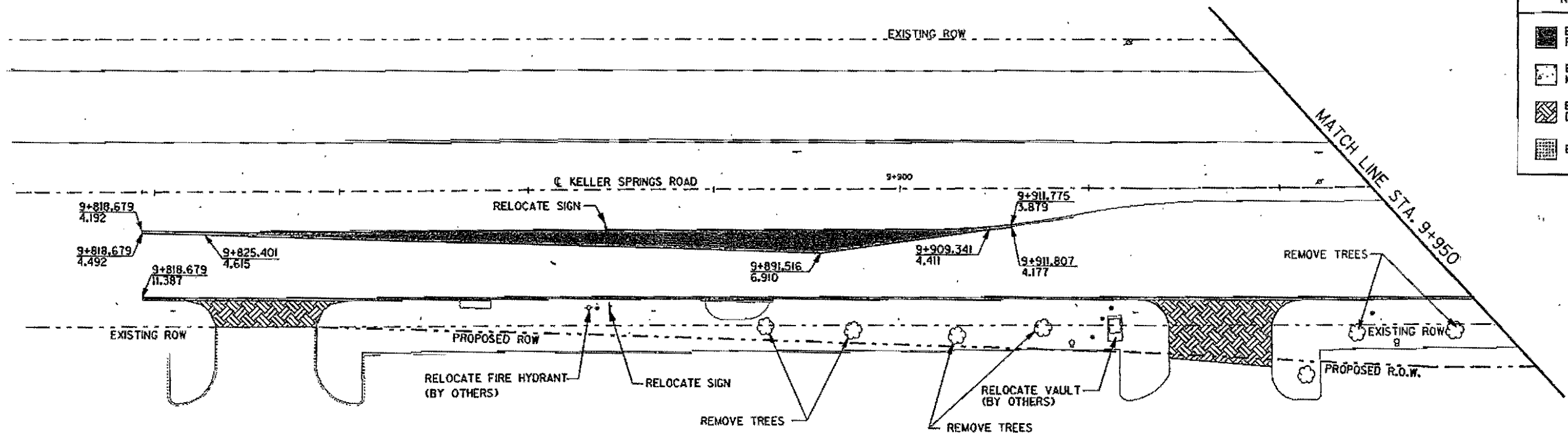
DESIGNED	CHKD	FED. RD. DIST. NO.	STATE	FEDERAL AID PROJECT NO.	HIGHWAY NO.
DESIGNED	DES	6	TEXAS	CM 571(87)	06
DRWN	FRS				
CHKD	DES	DALLAS	DALLAS	810	AS 344

REMOVAL NOTES

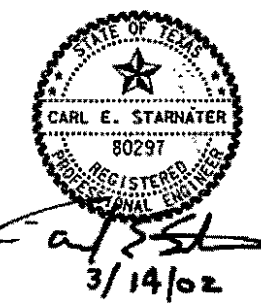
1. SEE SIGNING AND PAVEMENT MARKING SHEETS FOR SIGN RELOCATIONS.
2. SEE SIGNAL PLANS FOR SIGNAL RELOCATIONS.
3. EXISTING LANDSCAPE PAVERS IN MEOIANS SHALL BE SALVAGED AND USED IN PROPOSED MEDIANS.



REMOVAL LEGEND	
	EXISTING CONCRETE PAVEMENT
	EXISTING CONCRETE MEDIAN
	EXISTING CONCRETE DRIVEWAY
	EXISTING SIDEWALK



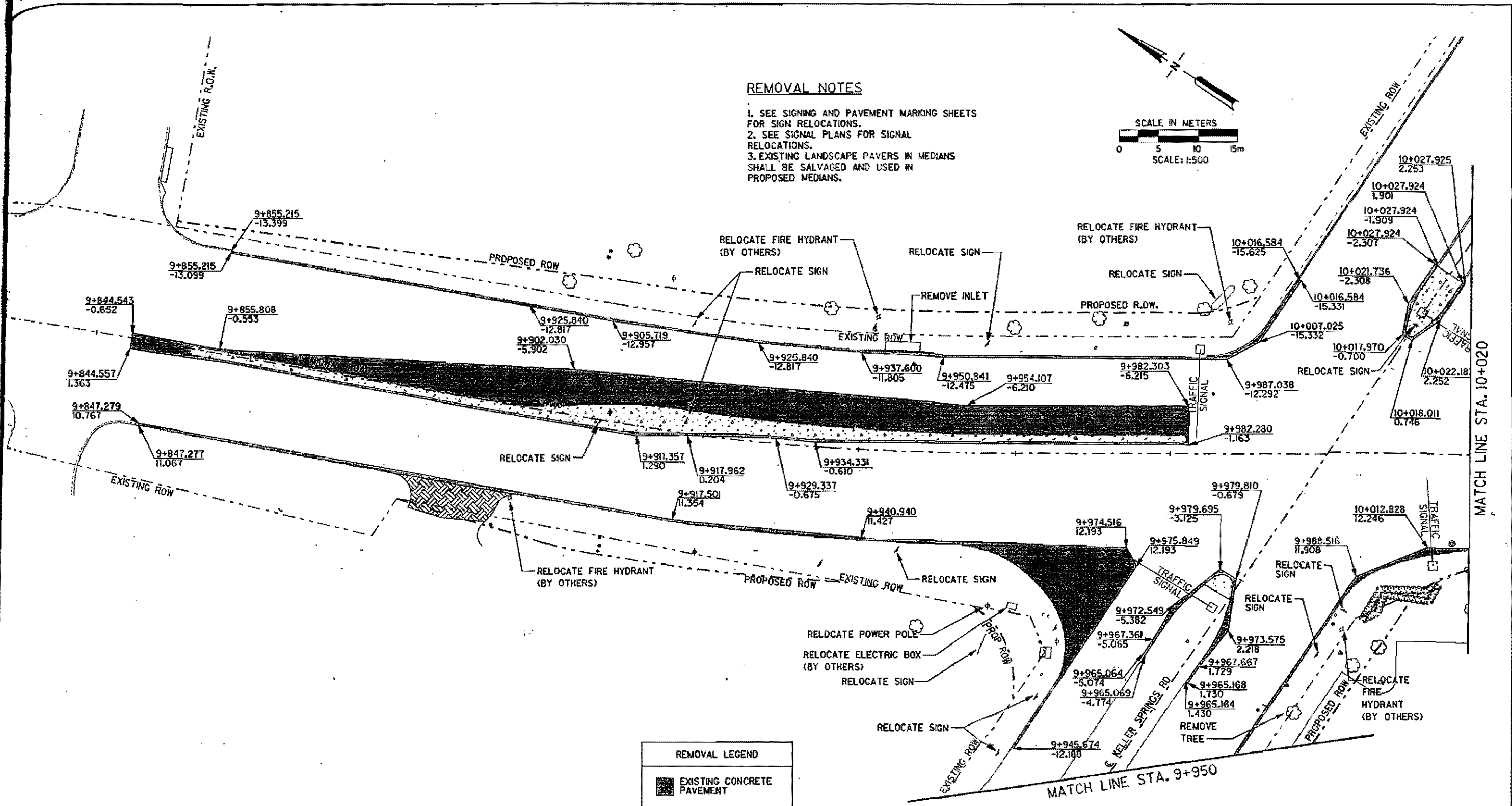
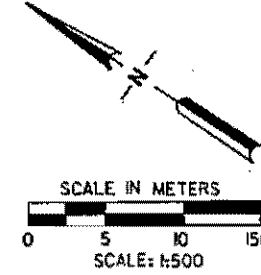
NOTE:
 EXISTING IRRIGATION IN CENTER MEDIANS SHALL BE CAPPED AND REMOVED AT THE MEDIAN REMOVAL LIMITS. IRRIGATION CAPPING SHALL BE SUBSIDIARY TO MEDIAN REMOVAL.



Texas Department of Transportation	
REMOVAL PLAN KELLER SPRINGS ROAD AT MIDWAY	
SHEET 1 OF 3	
DESIGNED: DMH	FED. AID PROJECT NO. STATE FEDERAL AID PROJECT NO. HIGHWAY NO.
CHECKED: CES	6 TEXAS CM 971 873 05
DRAWN: FRG	STATE DIST. NO. COUNTY CONTROL NO. SECTION JOB NO. SHEET NO.
CHECKED: CES	DALLAS DALLAS 26B 45 344 74

REMOVAL NOTES

1. SEE SIGNING AND PAVEMENT MARKING SHEETS FOR SIGN RELOCATIONS.
2. SEE SIGNAL PLANS FOR SIGNAL RELOCATIONS.
3. EXISTING LANDSCAPE PAVERS IN MEDIANS SHALL BE SALVAGED AND USED IN PROPOSED MEDIANS.



REMOVAL LEGEND

	EXISTING CONCRETE PAVEMENT
	EXISTING CONCRETE MEDIAN
	EXISTING CONCRETE DRIVEWAY
	EXISTING SIDEWALK

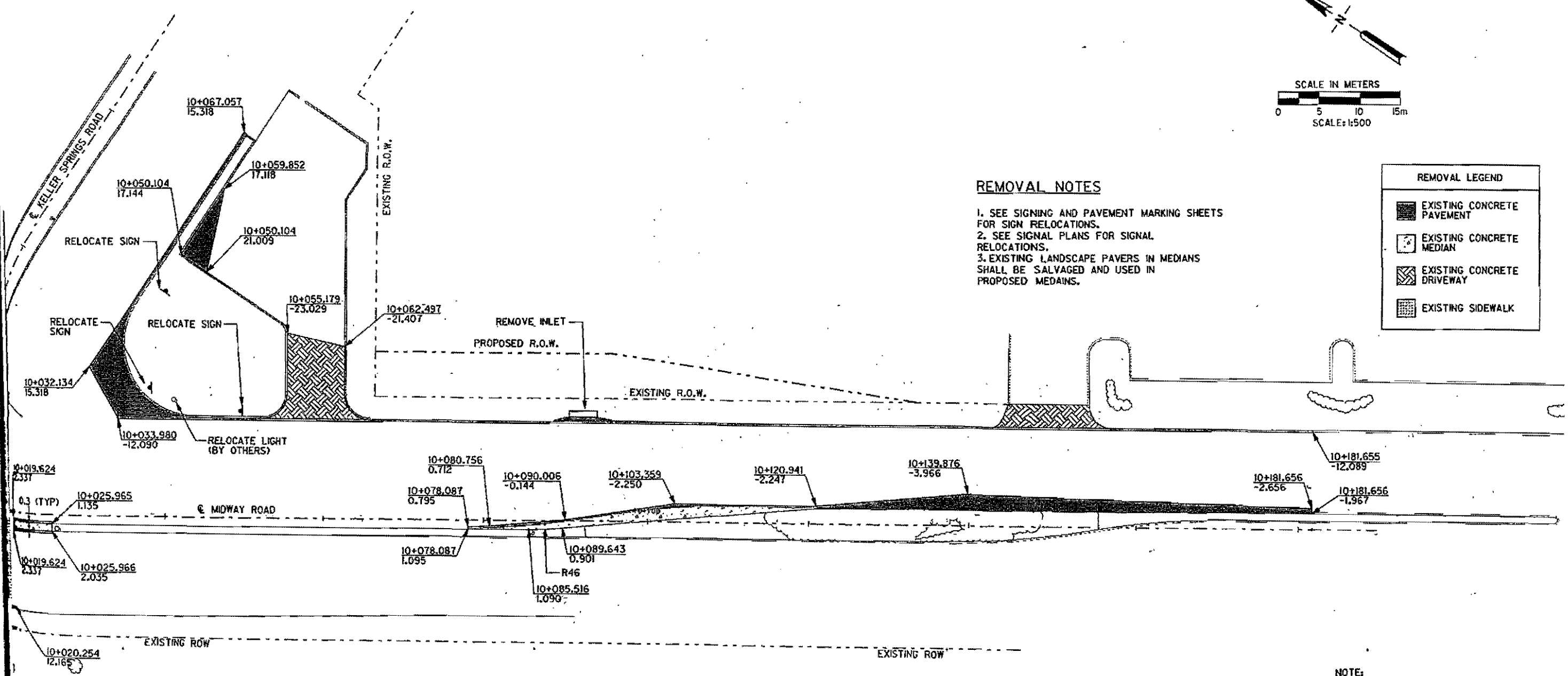
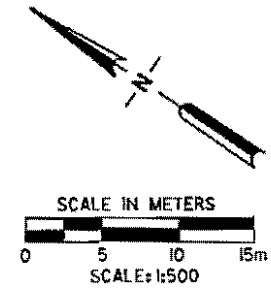
NOTE:
EXISTING IRRIGATION IN CENTER MEDIANS SHALL BE CAPPED AND REMOVED AT THE MEDIAN REMOVAL LIMITS. IRRIGATION CAPPING SHALL BE SUBSIDIARY TO MEDIAN REMOVAL.



Cartor-Burgess
Texas Department of Transportation

REMOVAL PLAN
KELLER SPRINGS ROAD
AT MIDWAY
SHEET 2 OF 3

DESIGNED: DMH	FILE NO. 6	STATE: TEXAS	FEDERAL AID PROJECT NO. CM 971(87)	SPRINT NO. 06
DRAWN: FRS	STATE DIST. NO.	COUNTY: DALLAS	CORREL. DIST. NO. 88B	SECTION NO. 45
CHECKED: CEG	DALLAS	DALLAS	88B	45 344

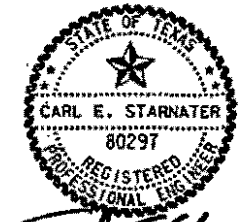


REMOVAL NOTES

1. SEE SIGNING AND PAVEMENT MARKING SHEETS FOR SIGN RELOCATIONS.
2. SEE SIGNAL PLANS FOR SIGNAL RELOCATIONS.
3. EXISTING LANDSCAPE PAVERS IN MEDIANS SHALL BE SALVAGED AND USED IN PROPOSED MEDIANS.

REMOVAL LEGEND	
	EXISTING CONCRETE PAVEMENT
	EXISTING CONCRETE MEDIAN
	EXISTING CONCRETE DRIVEWAY
	EXISTING SIDEWALK

NOTE:
EXISTING IRRIGATION IN CENTER MEDIANS SHALL BE CAPPED AND REMOVED AT THE MEDIAN REMOVAL LIMITS. IRRIGATION CAPPING SHALL BE SUBSIDIARY TO MEDIAN REMOVAL.



Carl E. Starnater
3/14/02

Carter-Burgess
Texas Department of Transportation

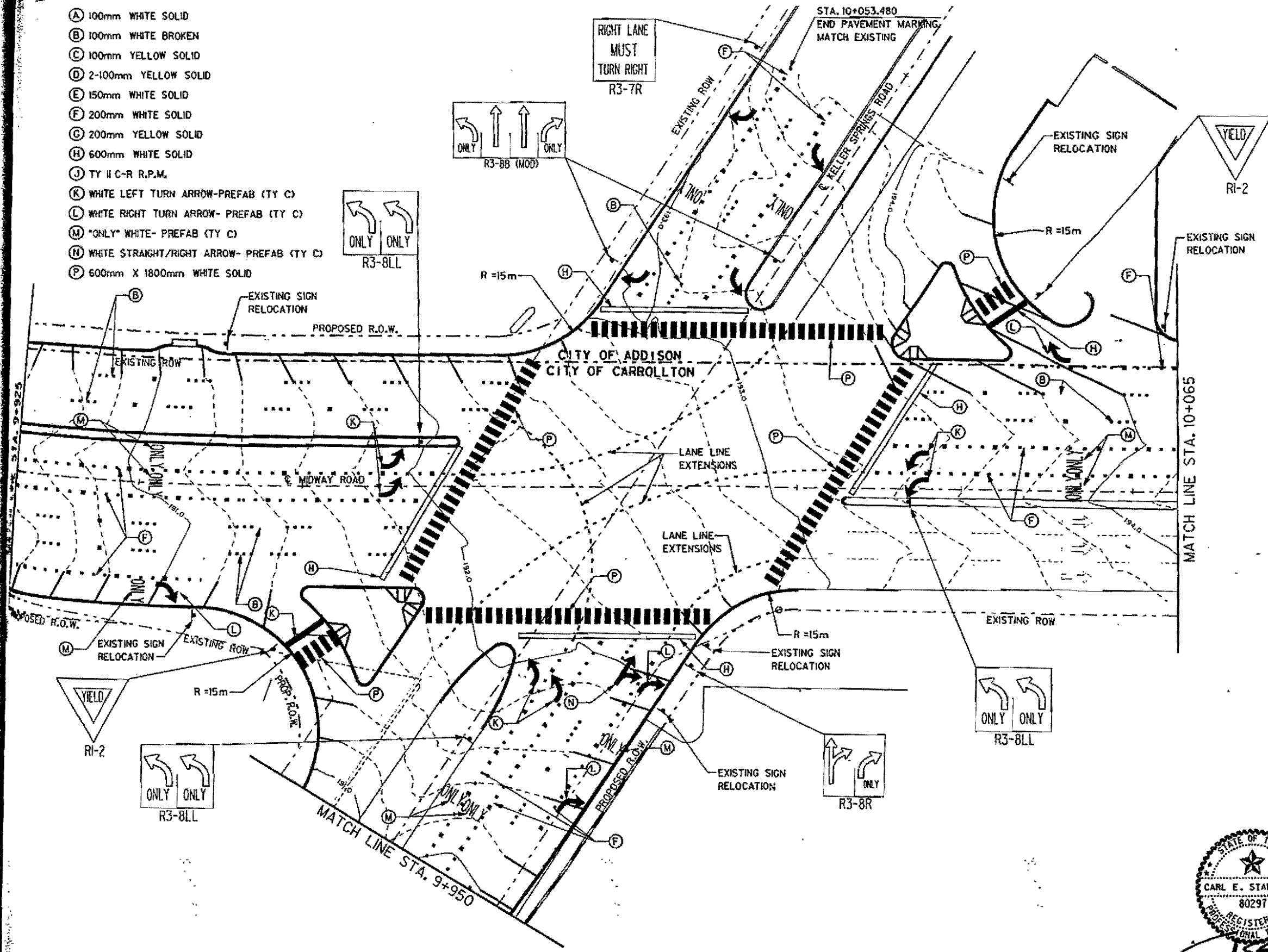
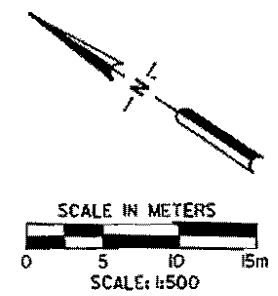
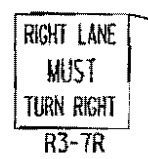
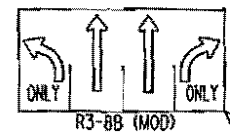
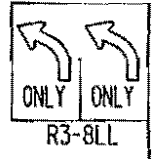
REMOVAL PLAN
KELLER SPRINGS ROAD
AT MIDWAY

SHEET 3 OF 3

DESIGNED: DMH	FED. RD. DIST. NO.	STATE	FEDERAL AID PROJECT NO.	HIGHWAY NO.
CHECKED: CEG	6	TEXAS	CA 971 (87)	CG
DRAWN: RHE	STATE DIST. NO.	COUNTY	CONTROL NO.	SECTION NO.
CHECKED: CEG	DALLAS	DALLAS	228	45

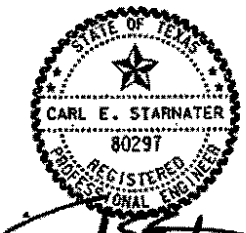
PAVEMENT MARKING LEGEND

- (A) 100mm WHITE SOLID
- (B) 100mm WHITE BROKEN
- (C) 100mm YELLOW SOLID
- (D) 2-100mm YELLOW SOLID
- (E) 150mm WHITE SOLID
- (F) 200mm WHITE SOLID
- (G) 200mm YELLOW SOLID
- (H) 600mm WHITE SOLID
- (J) TY II C-R R.P.M.
- (K) WHITE LEFT TURN ARROW-PREFAB (TY C)
- (L) WHITE RIGHT TURN ARROW-PREFAB (TY C)
- (M) *ONLY* WHITE- PREFAB (TY C)
- (N) WHITE STRAIGHT/RIGHT ARROW- PREFAB (TY C)
- (P) 600mm X 1800mm WHITE SOLID



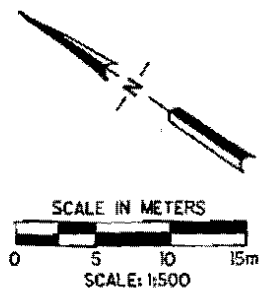
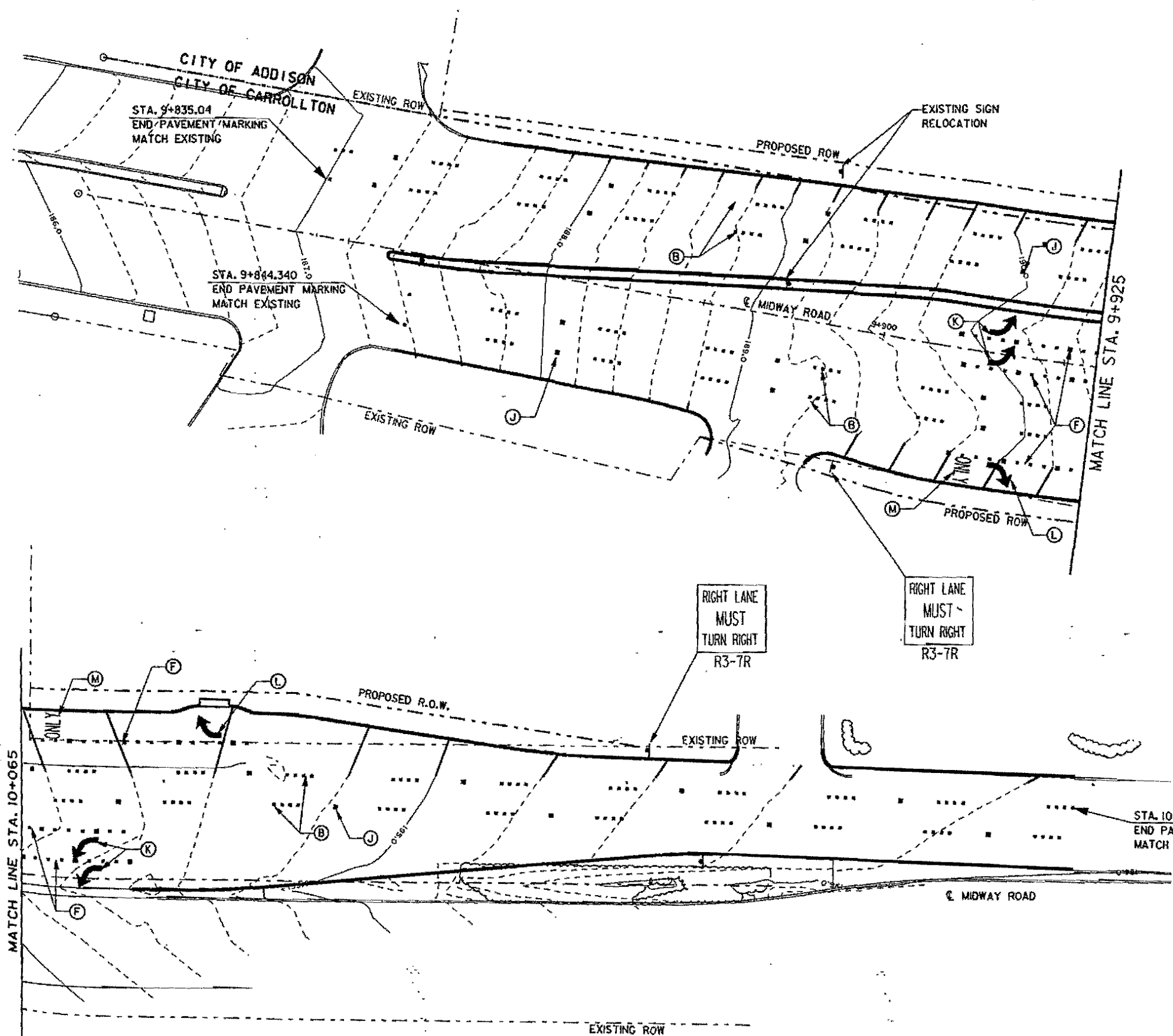
PAVEMENT MARKING NOTES

1. DIMENSIONS ARE SHOWN IN METERS UNLESS OTHERWISE NOTED.
2. REFER TO THE PAVEMENT MARKING DETAILS SHEET FOR MARKING DETAILS AND ADDITIONAL INFORMATION.
3. THE CONTRACTOR SHALL ALIGN AND MATCH PROPOSED PAVEMENT MARKINGS TO THE ORIGINAL EXISTING MARKING LOCATIONS AS DIRECTED BY THE ENGINEER.
4. CROSSWALK LOCATION SHALL BE ADJUSTED AT THE DIRECTION OF THE ENGINEER TO INSURE THAT HANDICAP RAMPS (EXCLUDING FLARED SIDES) ARE WHOLLY CONTAINED WITHIN THE CROSSWALK.
5. EXISTING ROADWAY SIGNS SHALL REMAIN IN PLACE AT THE END OF THE PROJECT EXCEPT WHERE REQUIRED TO BE REMOVED FOR THE CONSTRUCTION OF THIS PROJECT.
6. PROPOSED ROADWAY SIGNS ARE DESIGNATED BY TMUTCD NO.
7. FOR SIGN INSTALLATION ON SIGNAL POLES, SEE SIGNAL PLANS.
8. REFER TO *SUMMARY OF SMALL SIGNS* PAY ITEMS.
9. FOR ALL EXISTING SIGNS TO BE RELOCATED, CONTRACTOR SHALL FURNISH AND INSTALL NEW BREAKAWAY STUB POSTS ON A NEW FOUNDATION, REPLACE DAMAGED ATTACHMENT HARDWARE, AND REPAIR ALL DAMAGED GALVANIZING.
10. ALL BEGIN AND END STATIONS FOR PAVEMENT MARKINGS ARE APPROXIMATE. CONTRACTOR SHALL FIELD-VERIFY ALL.



[Handwritten signature]
3/14/02

Carter-Burgess	
Texas Department of Transportation	
SIGNING & PAVEMENT MARKING	
KELLER SPRINGS ROAD AT MIDWAY	
SHEET 1 OF 3	
DESIGNED: CEM	STATE: TEXAS
CHECKED: CES	FEDERAL AID PROJECT No.: CM 571 (71)
DRAWN: FHS	SECTION No.: 45
CHECKED: CES	JOB No.: 344
STATE: DALLAS	SHEET No.: 57



PAVEMENT MARKING NOTES

1. DIMENSIONS ARE SHOWN IN METERS UNLESS OTHERWISE NOTED.
2. REFER TO THE PAVEMENT MARKING DETAILS SHEET FOR MARKING DETAILS AND ADDITIONAL INFORMATION.
3. THE CONTRACTOR SHALL ALIGN AND MATCH PROPOSED PAVEMENT MARKINGS TO THE ORIGINAL EXISTING MARKING LOCATIONS AS DIRECTED BY THE ENGINEER.
4. CROSSWALK LOCATION SHALL BE ADJUSTED AT THE DIRECTION OF THE ENGINEER TO INSURE THAT HANDICAP RAMPS (EXCLUDING FLARED SIDES) ARE WHOLLY CONTAINED WITHIN THE CROSSWALK.
5. EXISTING ROADWAY SIGNS SHALL REMAIN IN PLACE AT THE END OF THE PROJECT EXCEPT WHERE REQUIRED TO BE REMOVED FOR THE CONSTRUCTION OF THIS PROJECT.
6. PROPOSED ROADWAY SIGNS ARE DESIGNATED BY TMUTCD NO.
7. FOR SIGN INSTALLATION ON SIGNAL POLES, SEE SIGNAL PLANS.
8. REFER TO "SUMMARY OF SMALL SIGNS" PAY ITEMS.
9. FOR ALL EXISTING SIGNS TO BE RELOCATED, CONTRACTOR SHALL FURNISH AND INSTALL NEW BREAKAWAY STUB POSTS ON A NEW FOUNDATION, REPLACE DAMAGED ATTACHMENT HARDWARE, AND REPAIR ALL DAMAGED GALVANIZING.
10. ALL BEGIN AND END STATIONS FOR PAVEMENT MARKINGS ARE APPROXIMATE. CONTRACTOR SHALL FIELD-VERIFY ALL.

PAVEMENT MARKING LEGEND

- (A) 100mm WHITE SOLID
- (B) 100mm WHITE BROKEN
- (C) 100mm YELLOW SOLID
- (D) 2-100mm YELLOW SOLID
- (E) 150mm WHITE SOLID
- (F) 200mm WHITE SOLID
- (G) 200mm YELLOW SOLID
- (H) 600mm WHITE SOLID
- (J) TY II C-R R.P.M.
- (K) WHITE LEFT TURN ARROW- PREFAB (TY C)
- (L) WHITE RIGHT TURN ARROW- PREFAB (TY C)
- (M) "ONLY" WHITE- PREFAB (TY C)
- (P) 600mm X 1800mm WHITE SOLID

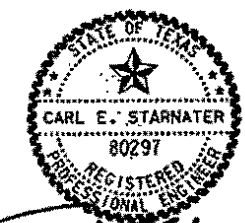
MATCH LINE STA. 10+065

MATCH LINE STA. 9+925

RIGHT LANE
MUST
TURN RIGHT
R3-7R

RIGHT LANE
MUST
TURN RIGHT
R3-7R

STA. 10+181.66
END PAVEMENT MARKING
MATCH EXISTING

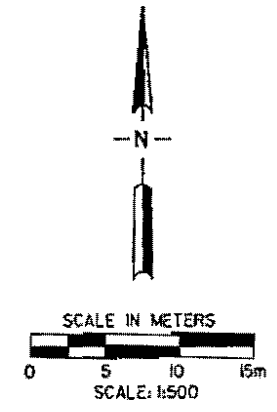


Carl E. Starnater
3/14/02

Cartor-Burgess
Texas Department of Transportation

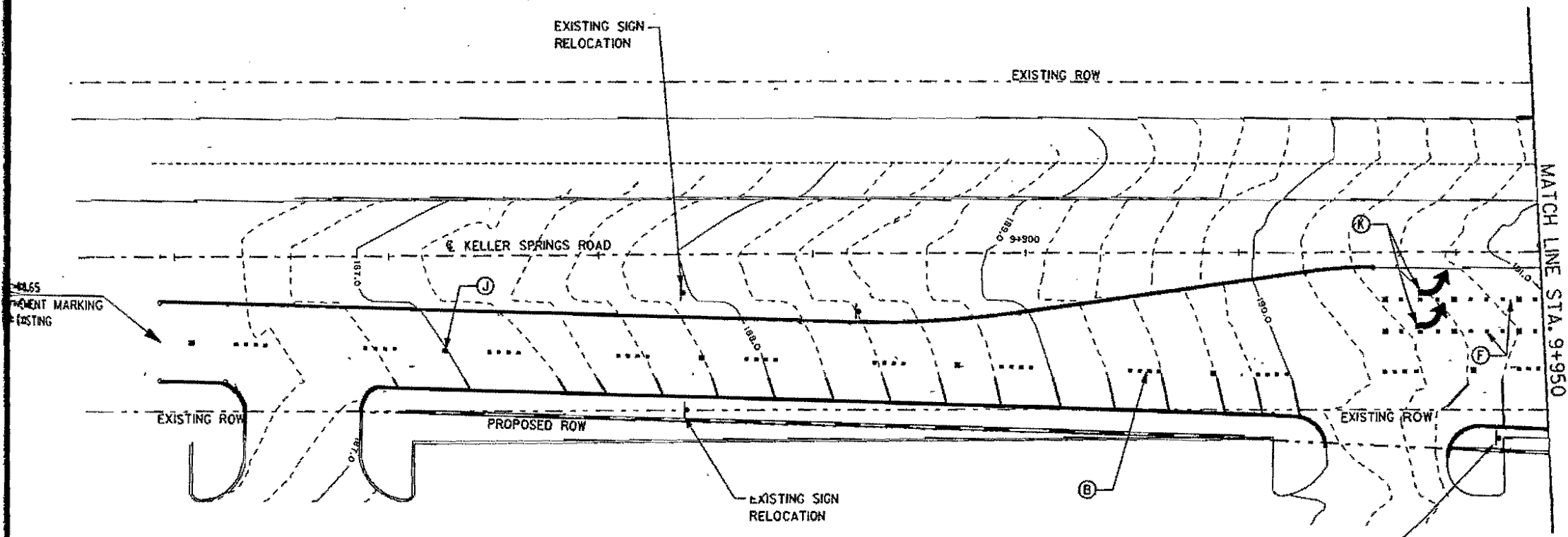
SIGNING & PAVEMENT MARKING
KELLER SPRINGS ROAD
AT MIDWAY
SHEET 2 OF 3

DESIGNED: DMH	FED. AID PROJ. NO.	STATE	FEDERAL AID PROJECT NO.	INTERSECT. NO.
CHECKED: CES	IS	TEXAS	ON 07/07/02	05
DRAWN: FR-ES	STATE DIST. NO.	COUNTY	SECTION	SHEET
CHECKED: CES	DALLAS	DALLAS	92B	45



PAVEMENT MARKING NOTES

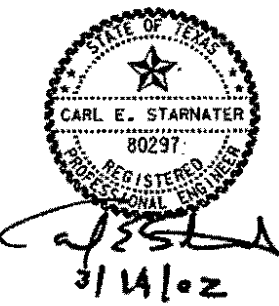
1. DIMENSIONS ARE SHOWN IN METERS UNLESS OTHERWISE NOTED.
2. REFER TO THE PAVEMENT MARKING DETAILS SHEET FOR MARKING DETAILS AND ADDITIONAL INFORMATION.
3. THE CONTRACTOR SHALL ALIGN AND MATCH PROPOSED PAVEMENT MARKINGS TO THE ORIGINAL EXISTING MARKING LOCATIONS AS DIRECTED BY THE ENGINEER.
4. CROSSWALK LOCATION SHALL BE ADJUSTED AT THE DIRECTION OF THE ENGINEER TO INSURE THAT HANDICAP RAMPS (EXCLUDING FLARED SIDES) ARE WHOLLY CONTAINED WITHIN THE CROSSWALK.
5. EXISTING ROADWAY SIGNS SHALL REMAIN IN PLACE AT THE END OF THE PROJECT EXCEPT WHERE REQUIRED TO BE REMOVED FOR THE CONSTRUCTION OF THIS PROJECT.
6. PROPOSED ROADWAY SIGNS ARE DESIGNATED BY TMTCD NO. AND NO. ON THE SHEET.
7. FOR SIGN INSTALLATION ON SIGNAL POLES, SEE SIGNAL PLANS.
8. REFER TO "SUMMARY OF SMALL SIGNS" PAY ITEMS.
9. FOR ALL EXISTING SIGNS TO BE RELOCATED, CONTRACTOR SHALL FURNISH AND INSTALL NEW BREAKAWAY STUB POSTS ON A NEW FOUNDATION, REPLACE DAMAGED ATTACHMENT HARDWARE, AND REPAIR ALL DAMAGED GALVANIZING.
10. ALL BEGIN AND END STATIONS FOR PAVEMENT MARKINGS ARE APPROXIMATE. CONTRACTOR SHALL FIELD-VERIFY ALL.



PAVEMENT MARKING LEGEND

- (A) 100mm WHITE SOLID
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- (C) 100mm YELLOW SOLID
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- (E) 150mm WHITE SOLID
- (F) 200mm WHITE SOLID
- (G) 200mm YELLOW SOLID
- (H) 600mm WHITE SOLID
- (J) TY II C-R R.P.M.
- (K) WHITE LEFT TURN ARROW
- (L) WHITE RIGHT TURN ARROW
- (M) "ONLY" WHITE
- (P) 600mm X 1800mm WHITE SOLID

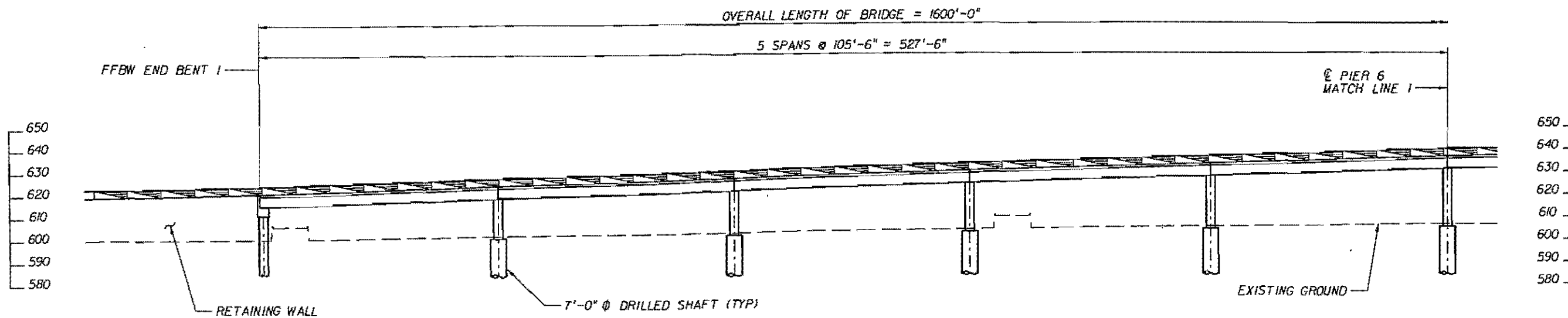
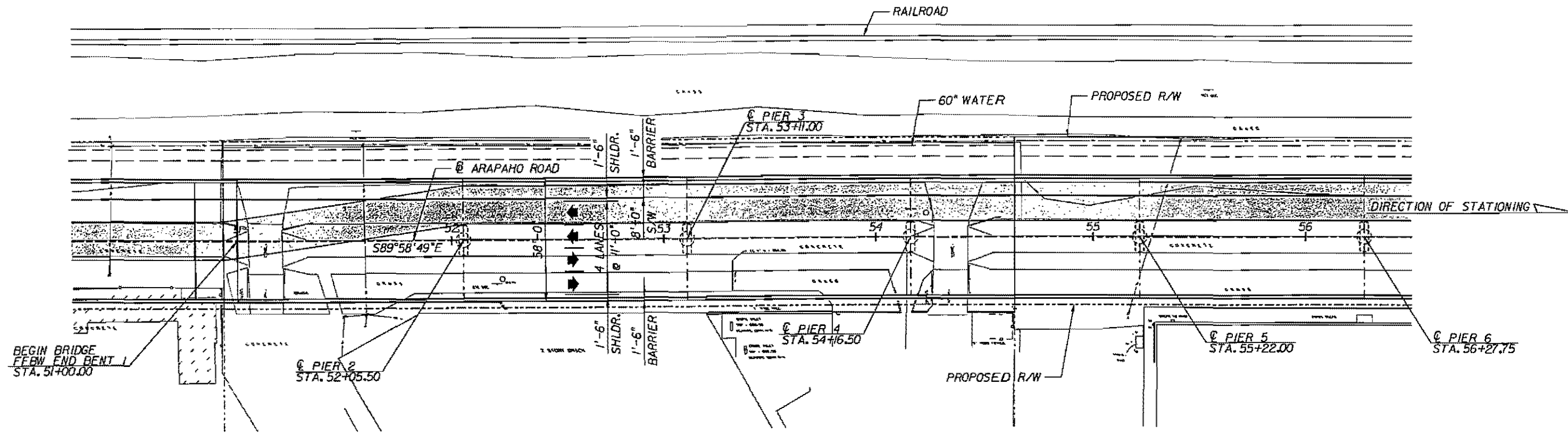
RIGHT LANE
MUST
TURN RIGHT
R3-7R

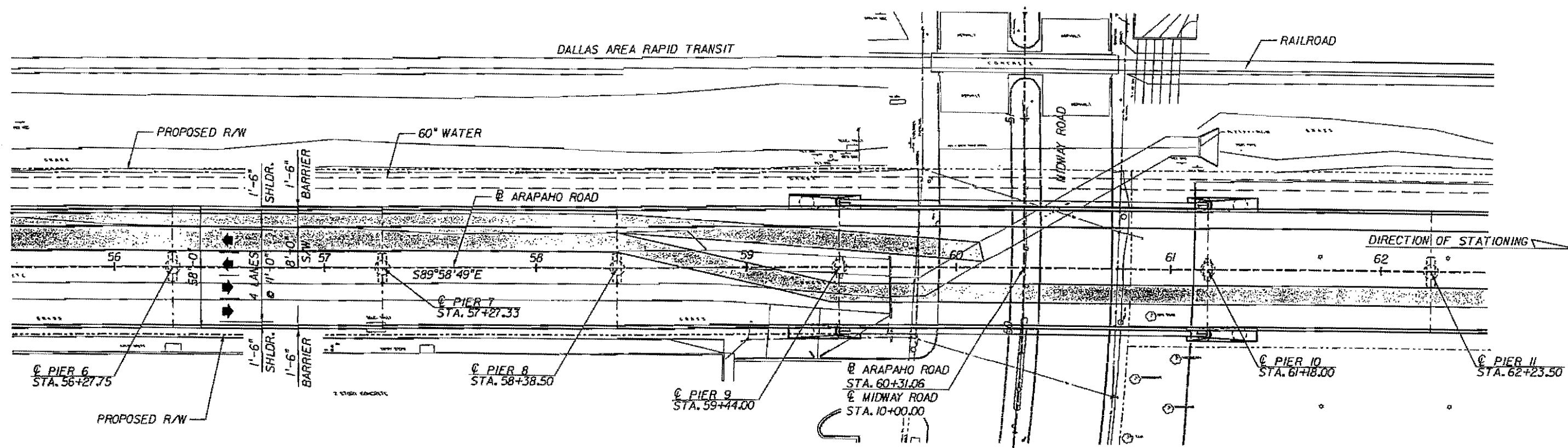


Carter-Burgess
Texas Department of Transportation

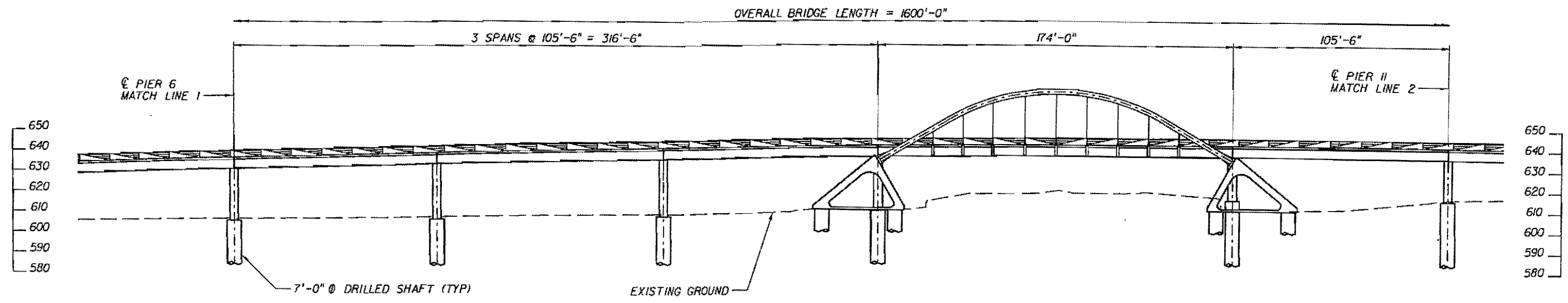
SIGNING & PAVEMENT MARKING
KELLER SPRINGS ROAD
AT MIDWAY
SHEET 3 OF 3

DESIGNED: CHA	FED. RD. DIST. NO.	STATE	FEDERAL AID PROJECT NO.	HIGHWAY NO.
DRAWN: CEG	6	TEXAS	CM 571 (87)	CS
EST. NO.	COUNTY	GENERAL DIST.	SECTION NO.	JOB SHEET NO.
DALLAS	DALLAS	928	45	344 89

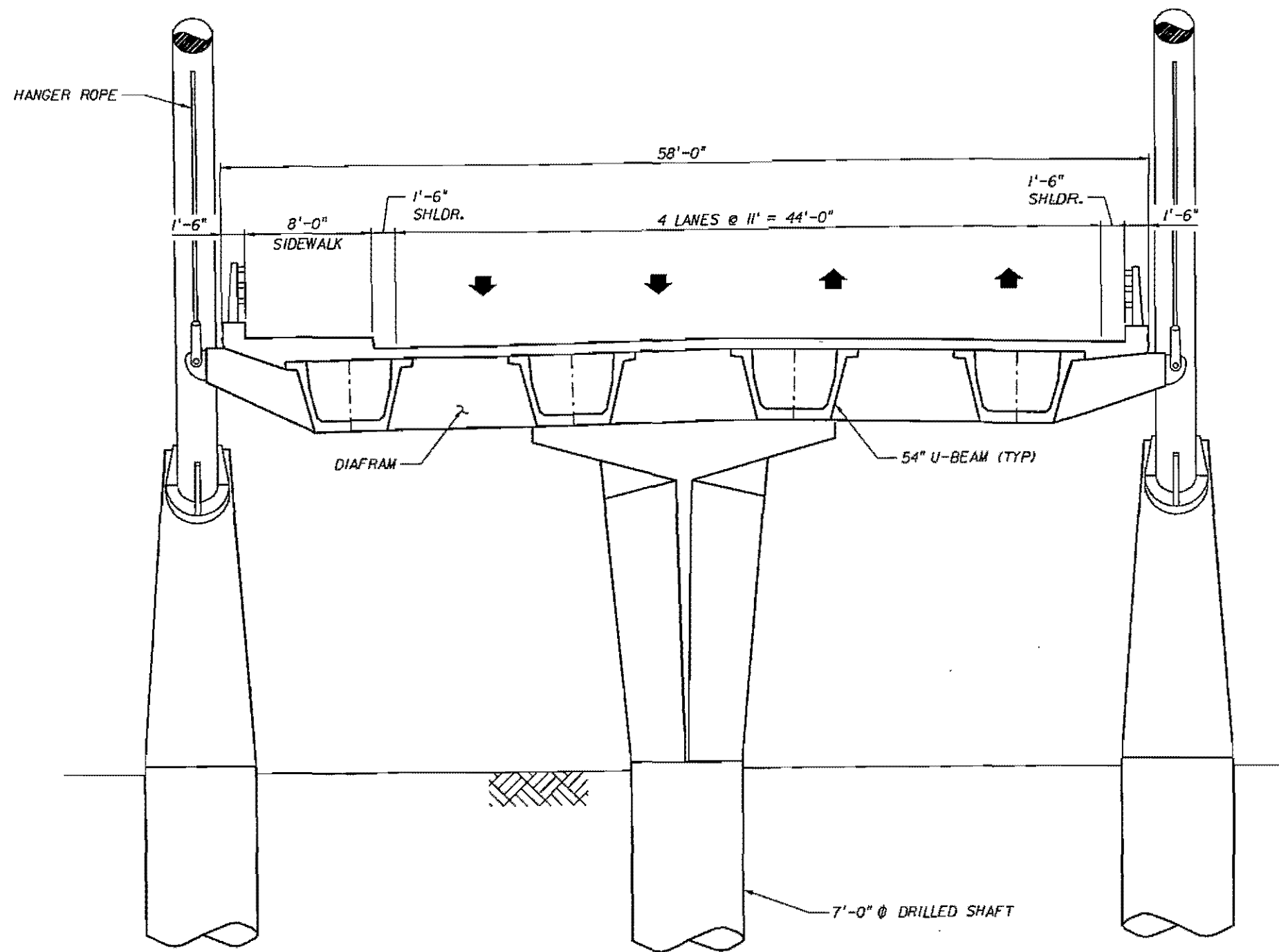




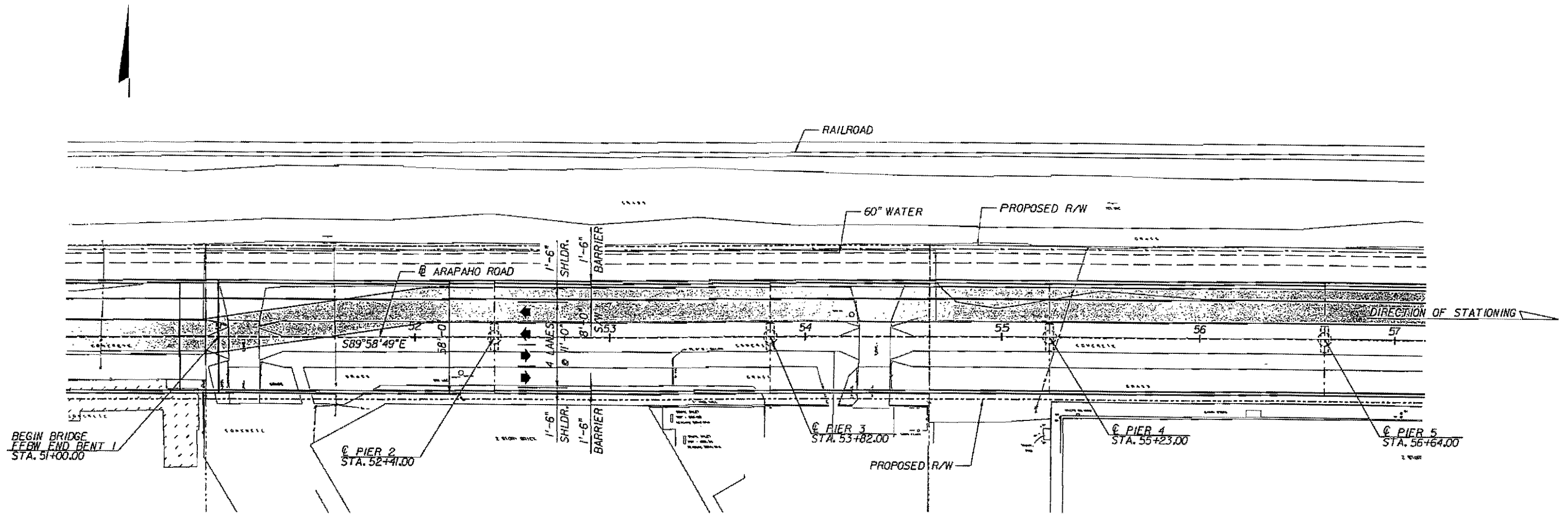
PLAN



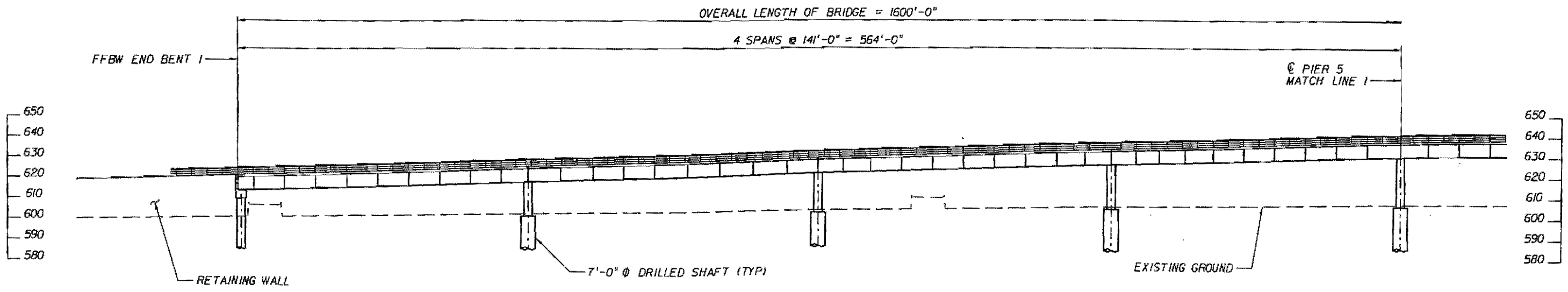
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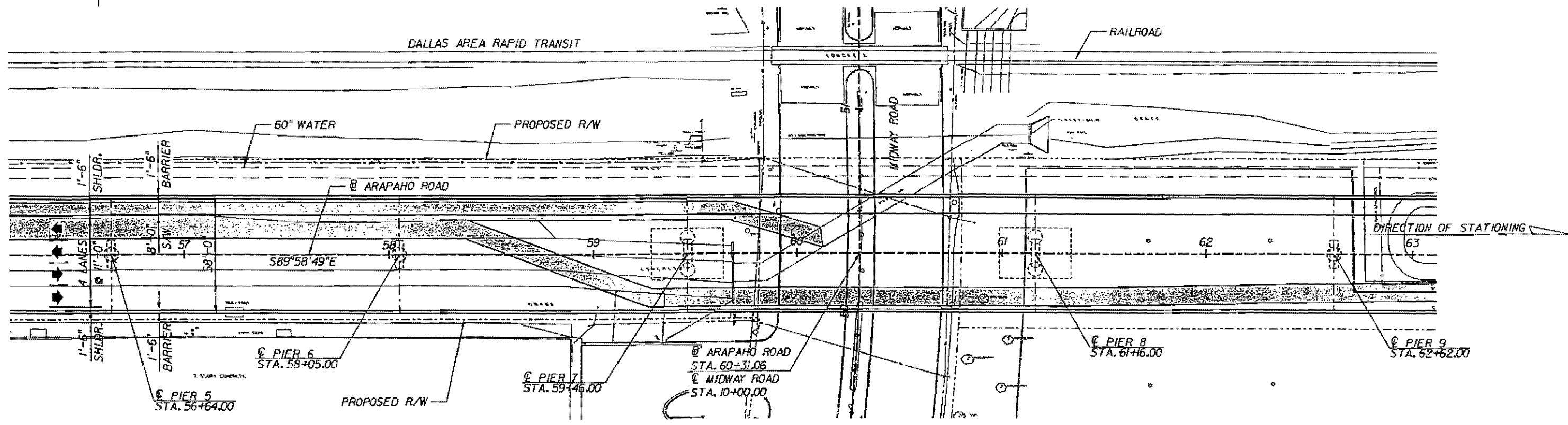
TYPICAL SECTION



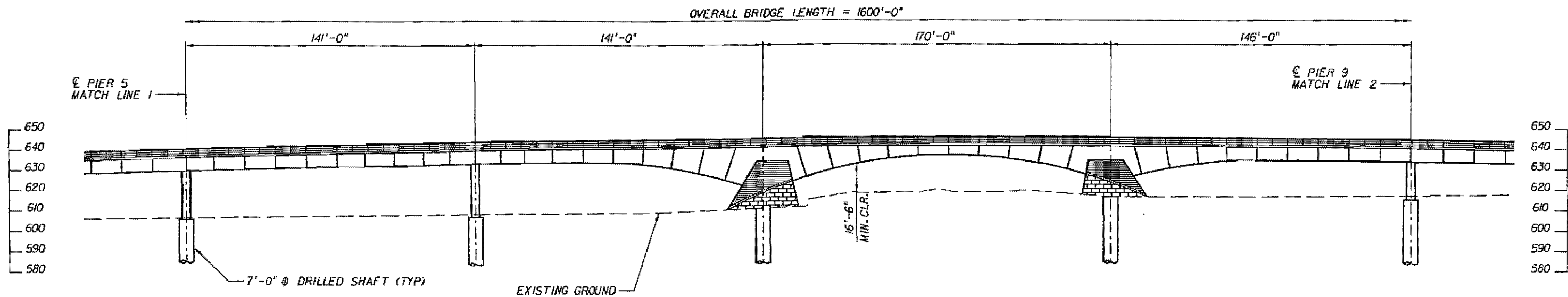
PLAN



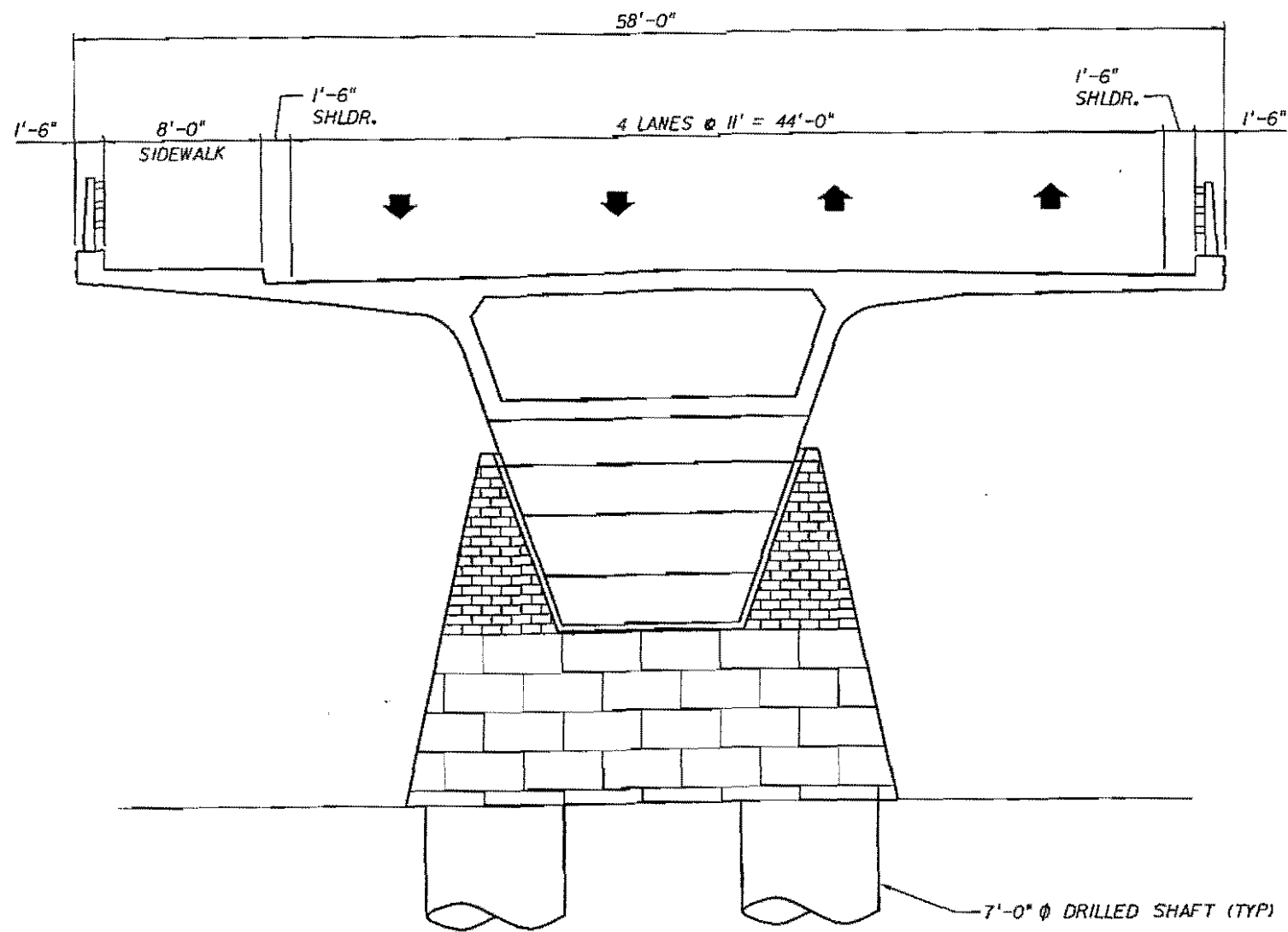
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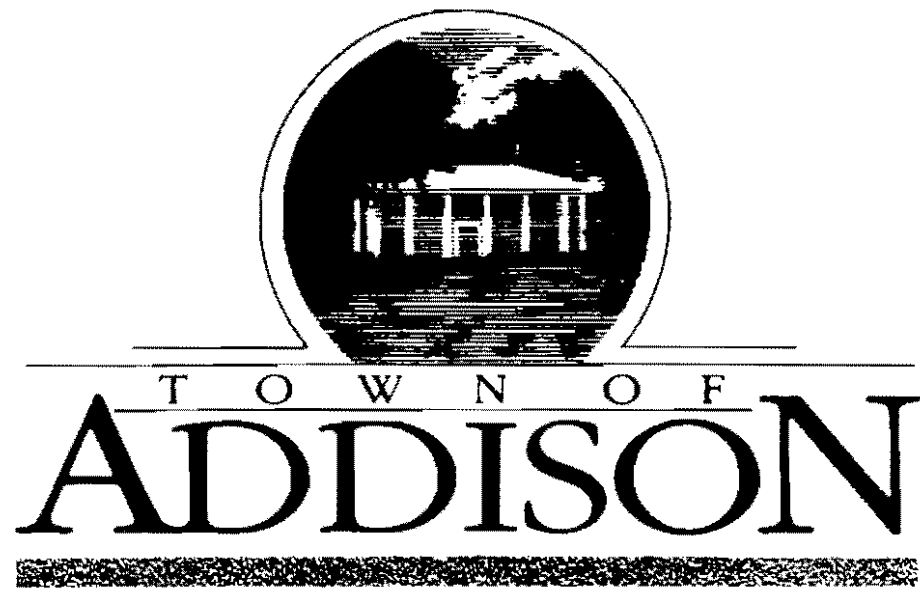
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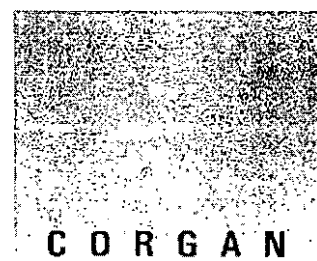
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TYPICAL SECTION



ARAPAHO ROAD BRIDGE PRESENTATION HANDOUT



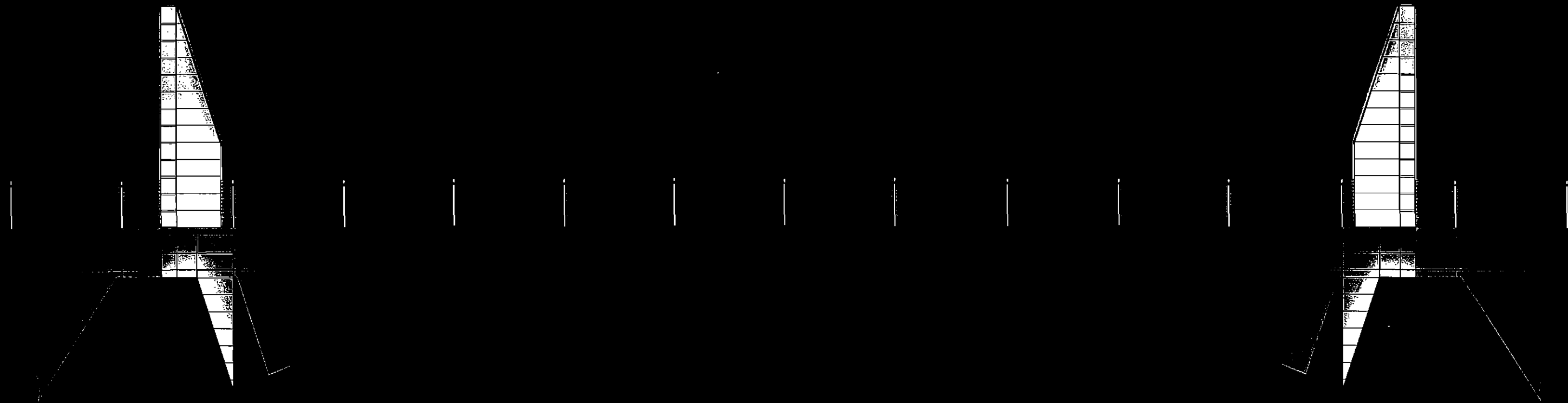


ARAPAHO BRIDGE

TOWN OF ADDISON, TEXAS



Brandon Partnership Inc.
Lighting Design



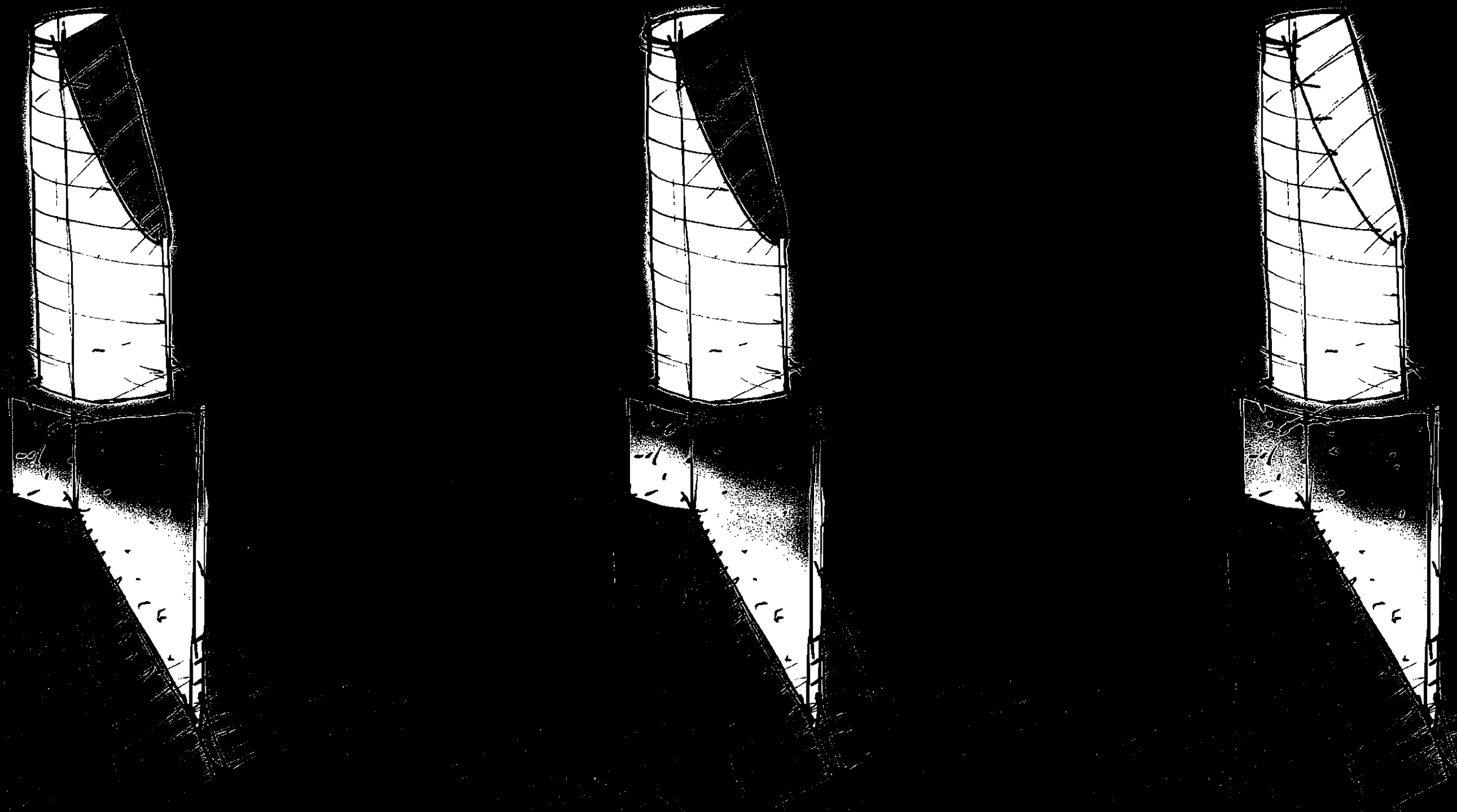
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TOWN OF ADDISON, TEXAS



Brandon Partnership Inc.

Lighting Design



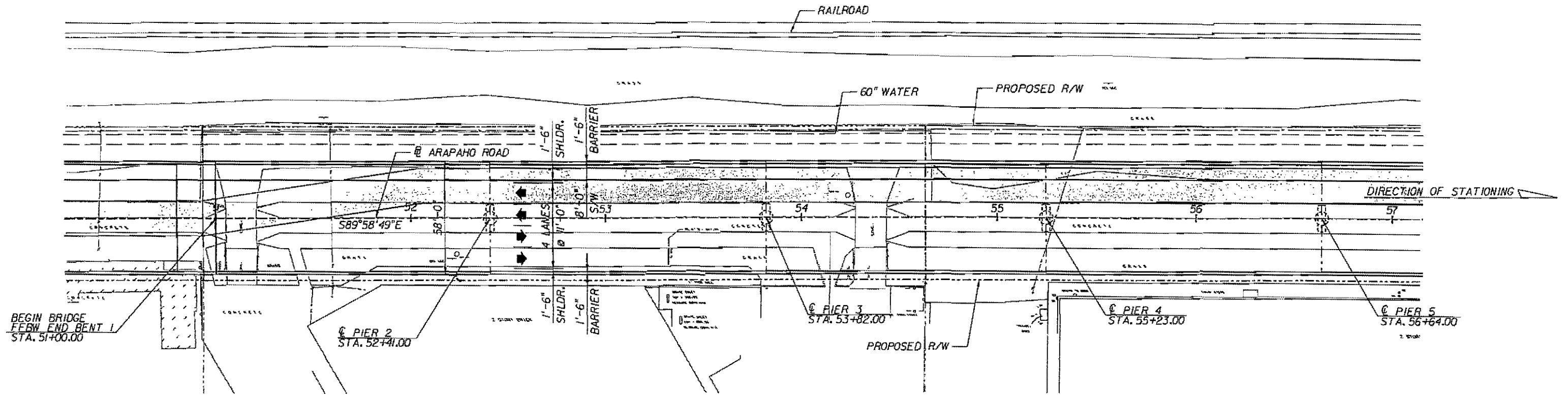
ARAPAHO BRIDGE

TOWN OF ADDISON, TEXAS

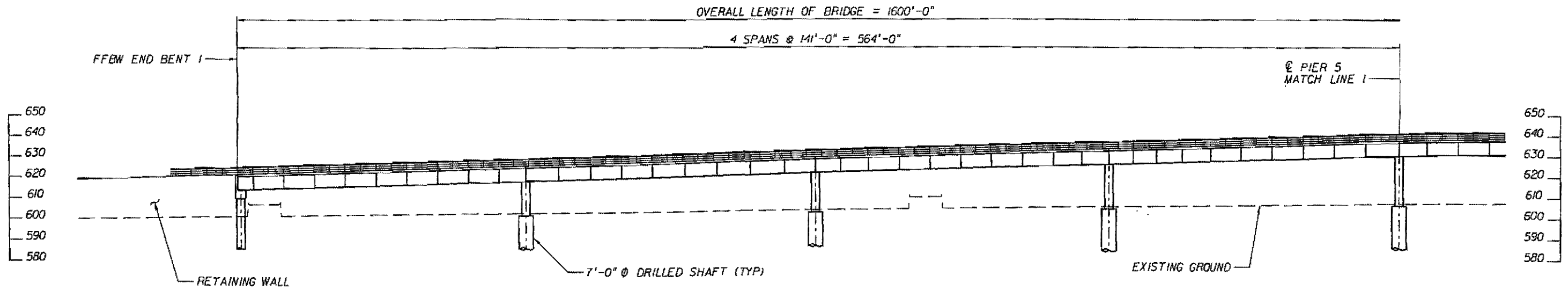


Branden Partnership Inc.

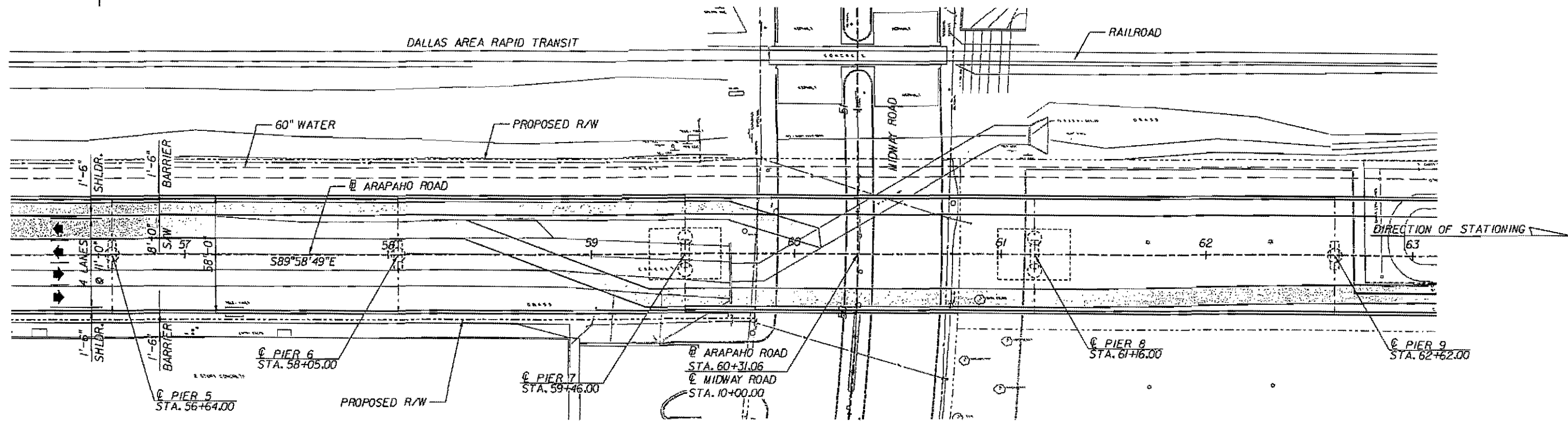
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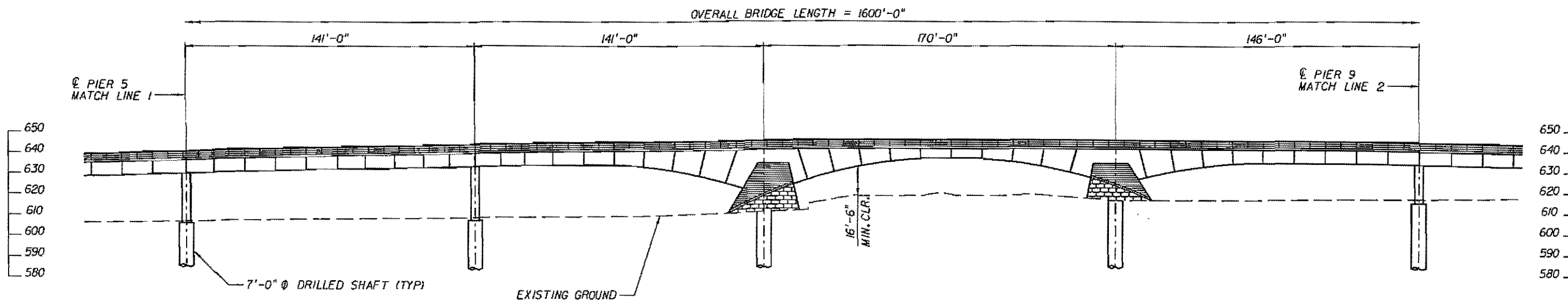
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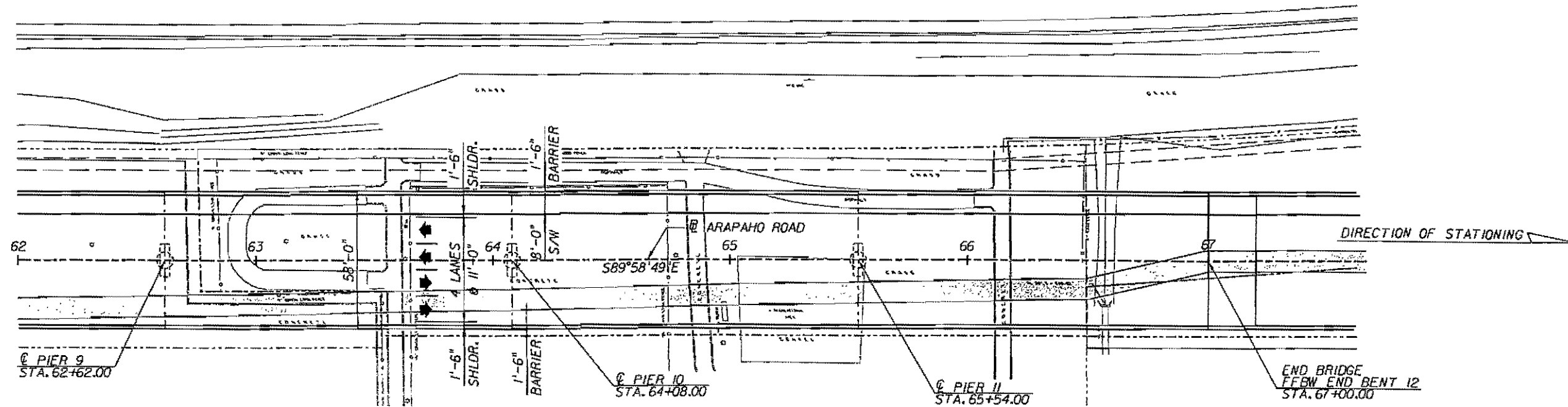
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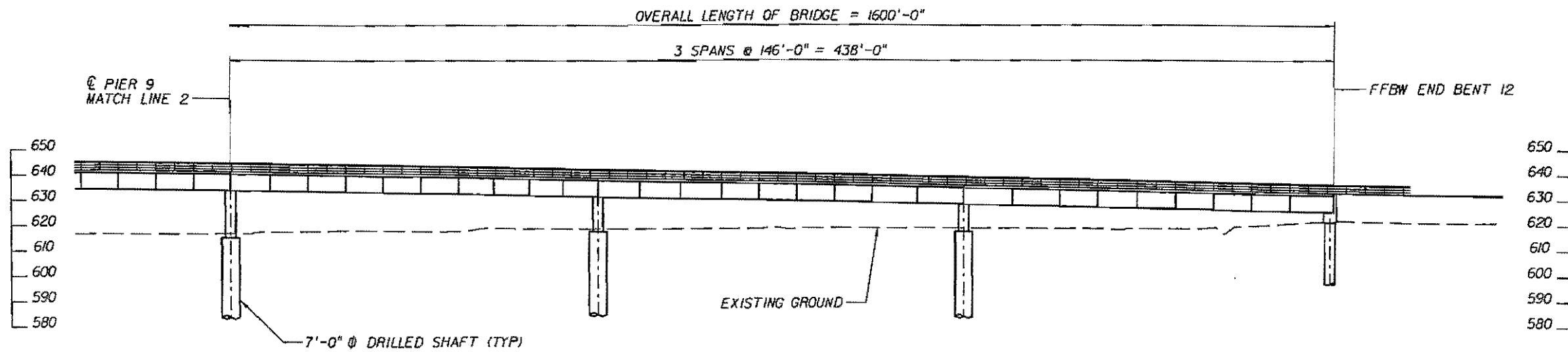
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ELEVATION



PLAN



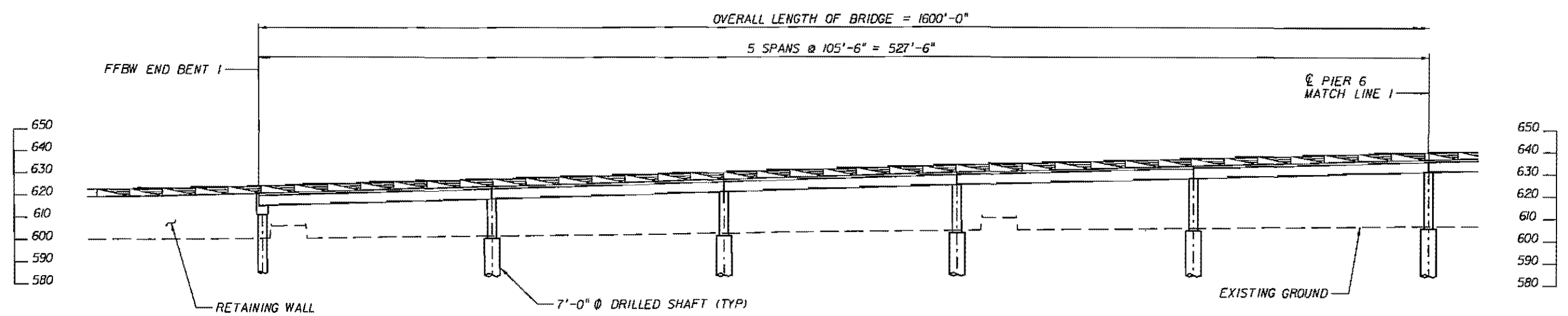
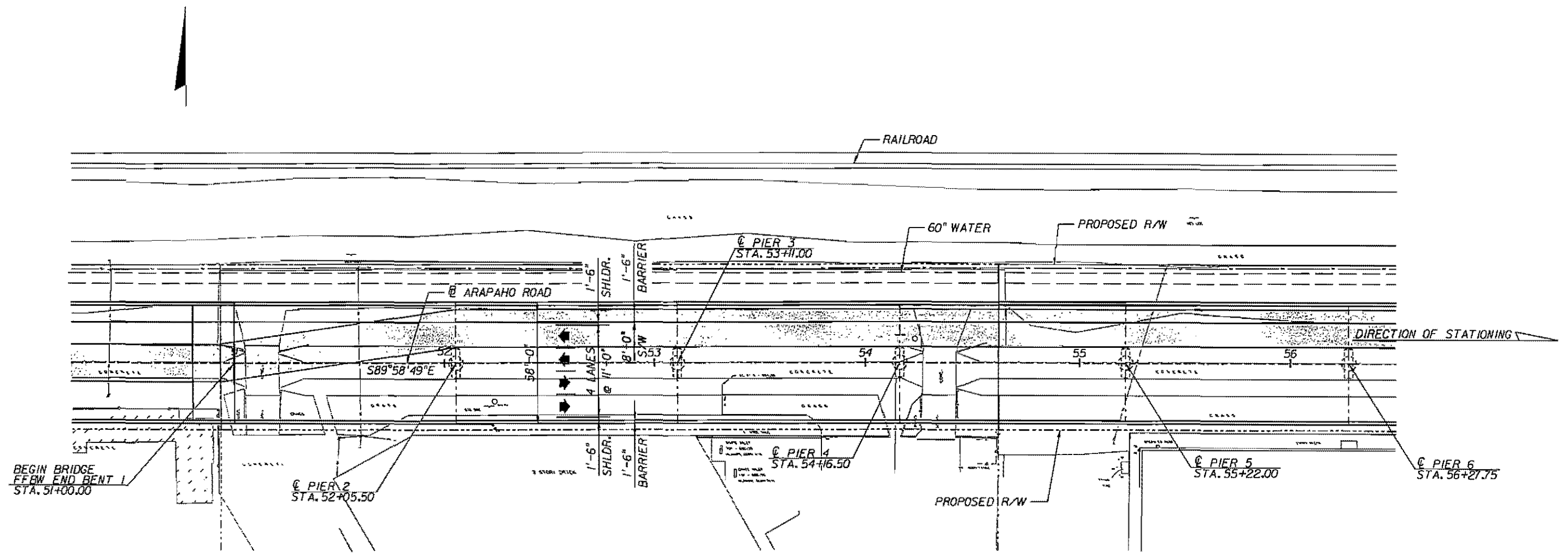
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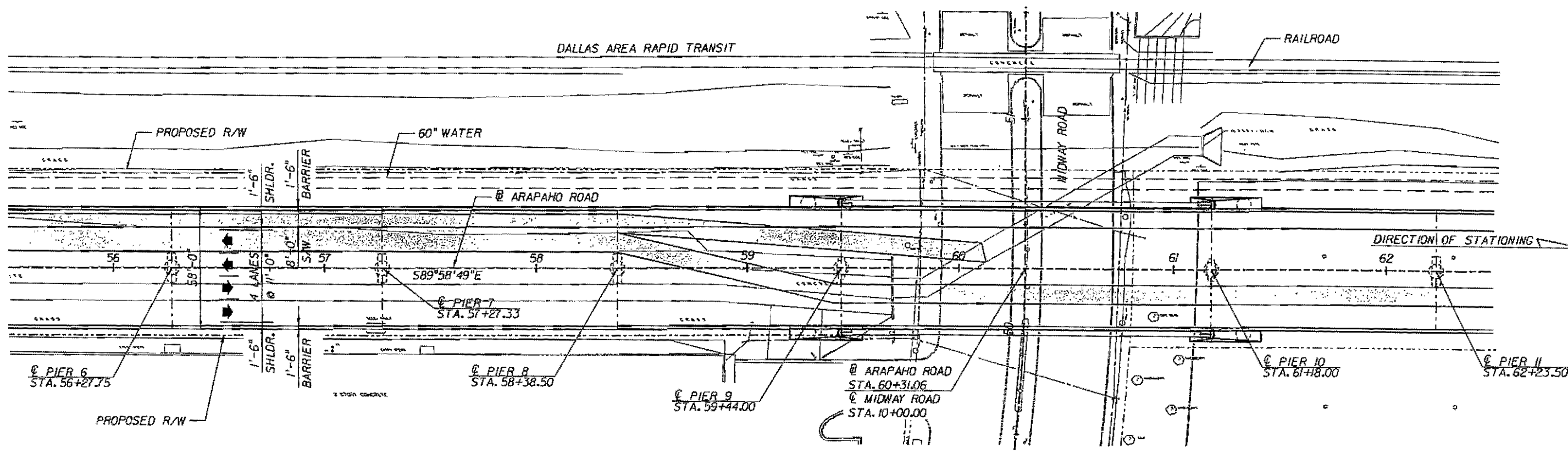


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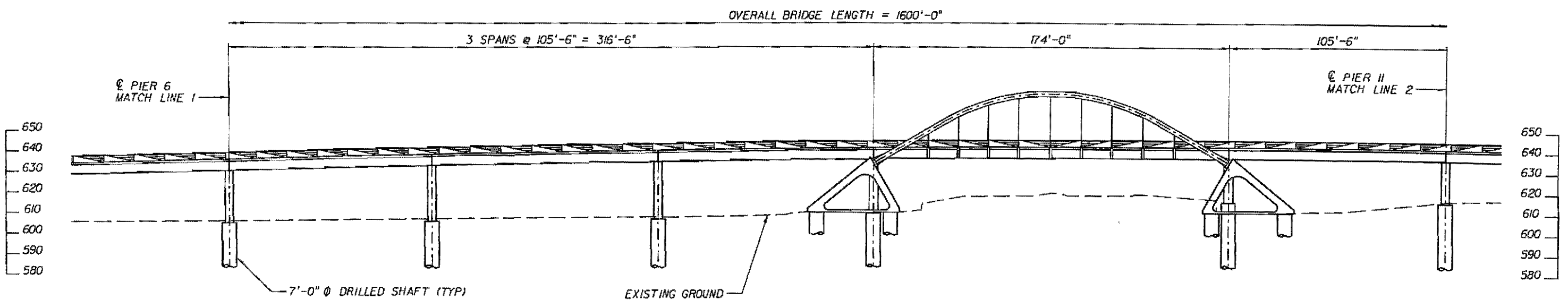
TOWN OF ADDISON, TEXAS







PLAN

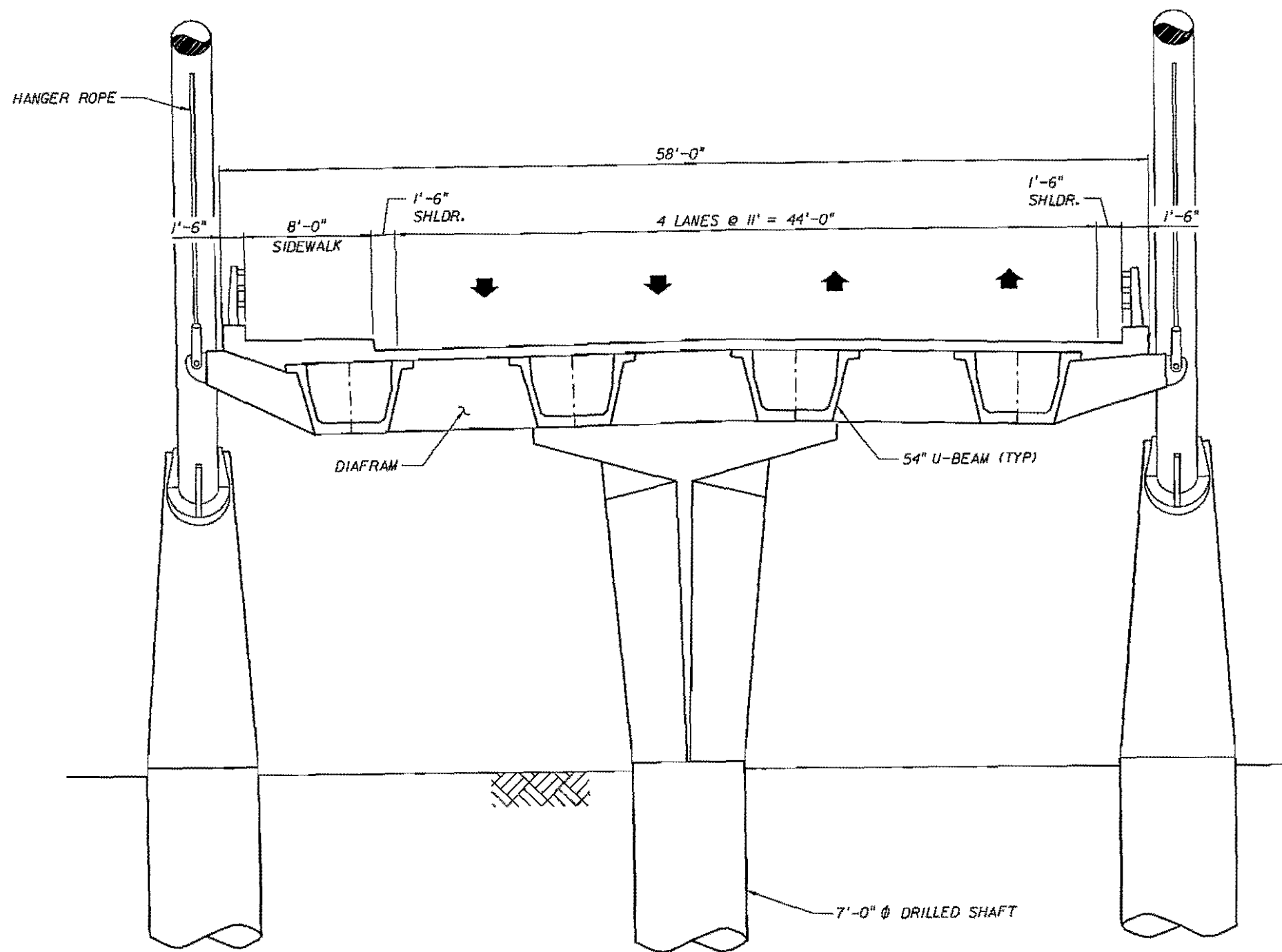


ELEVATION

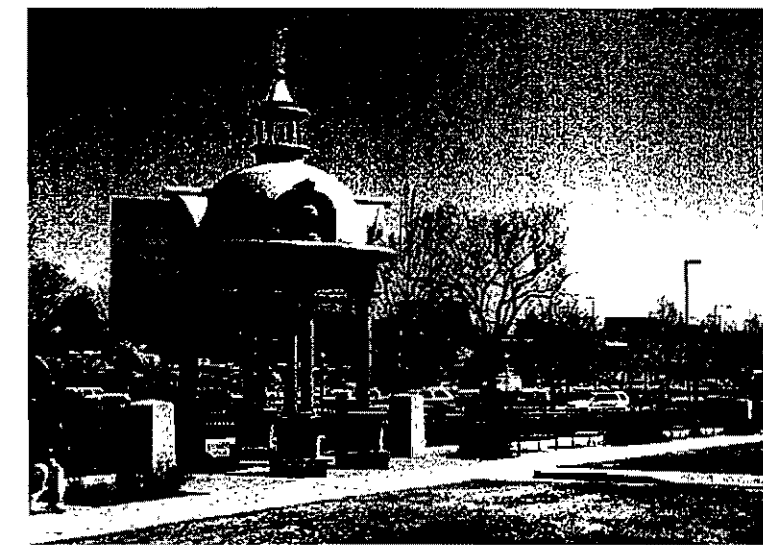
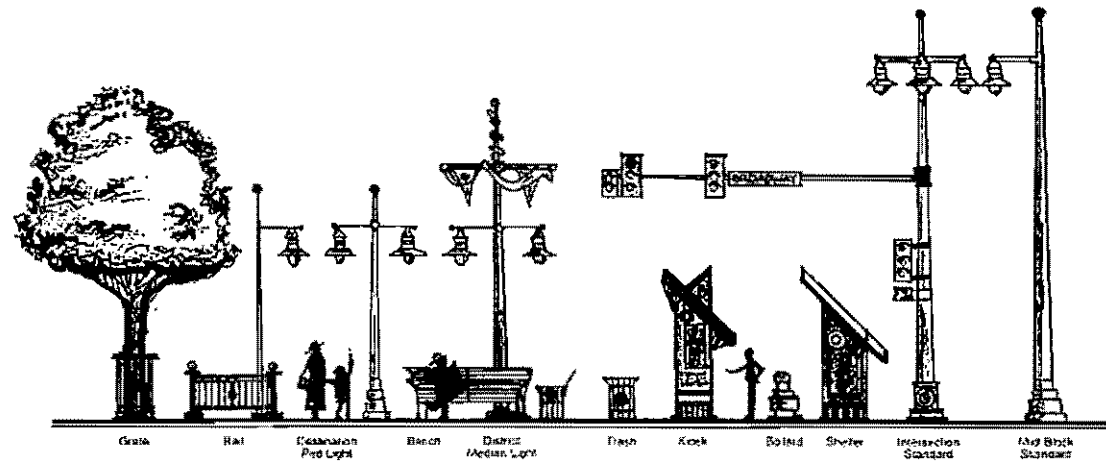
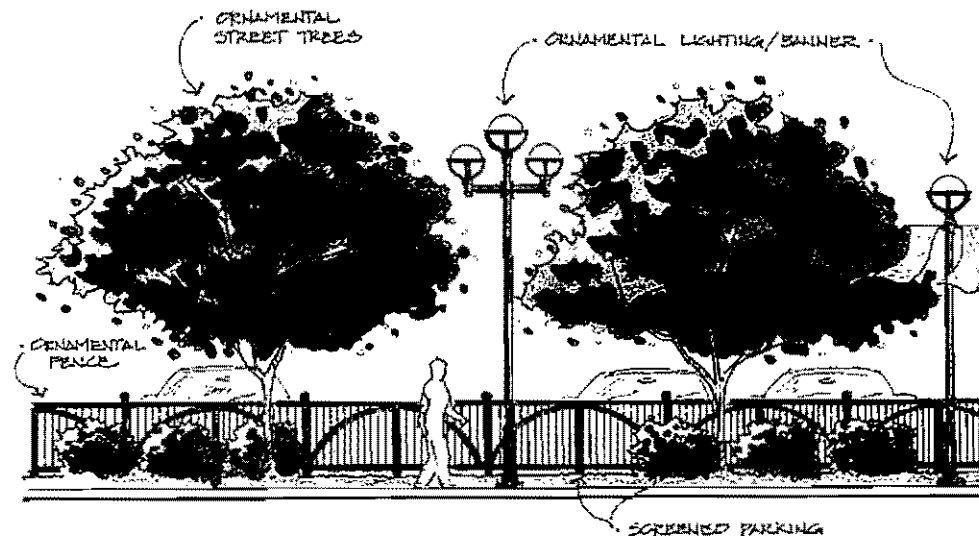
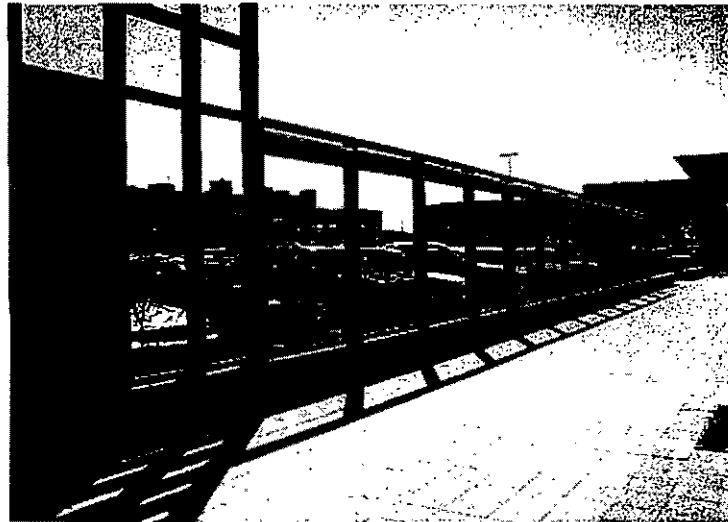
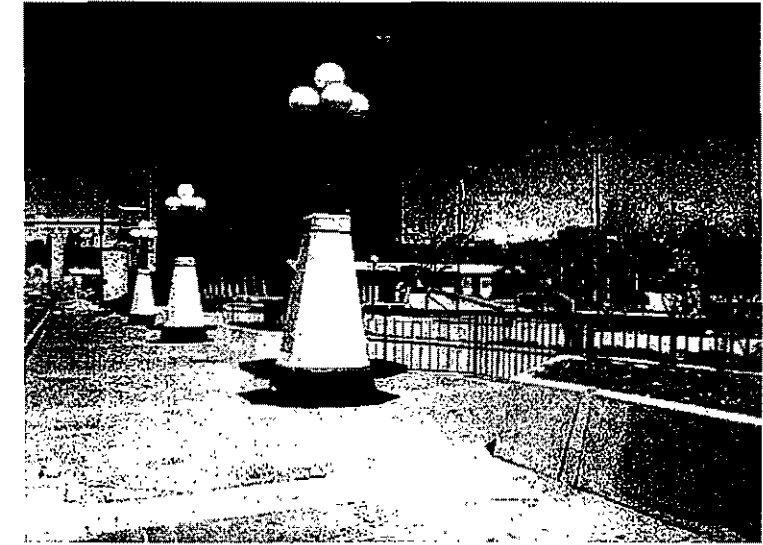
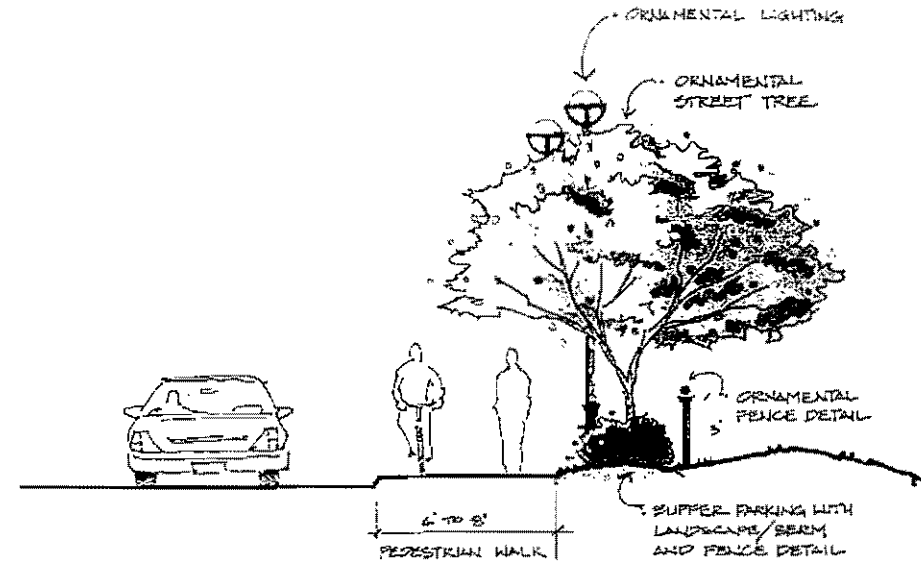
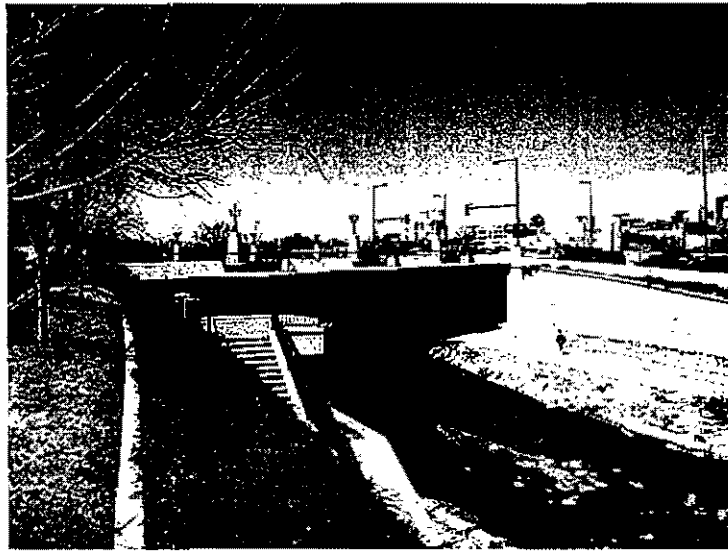
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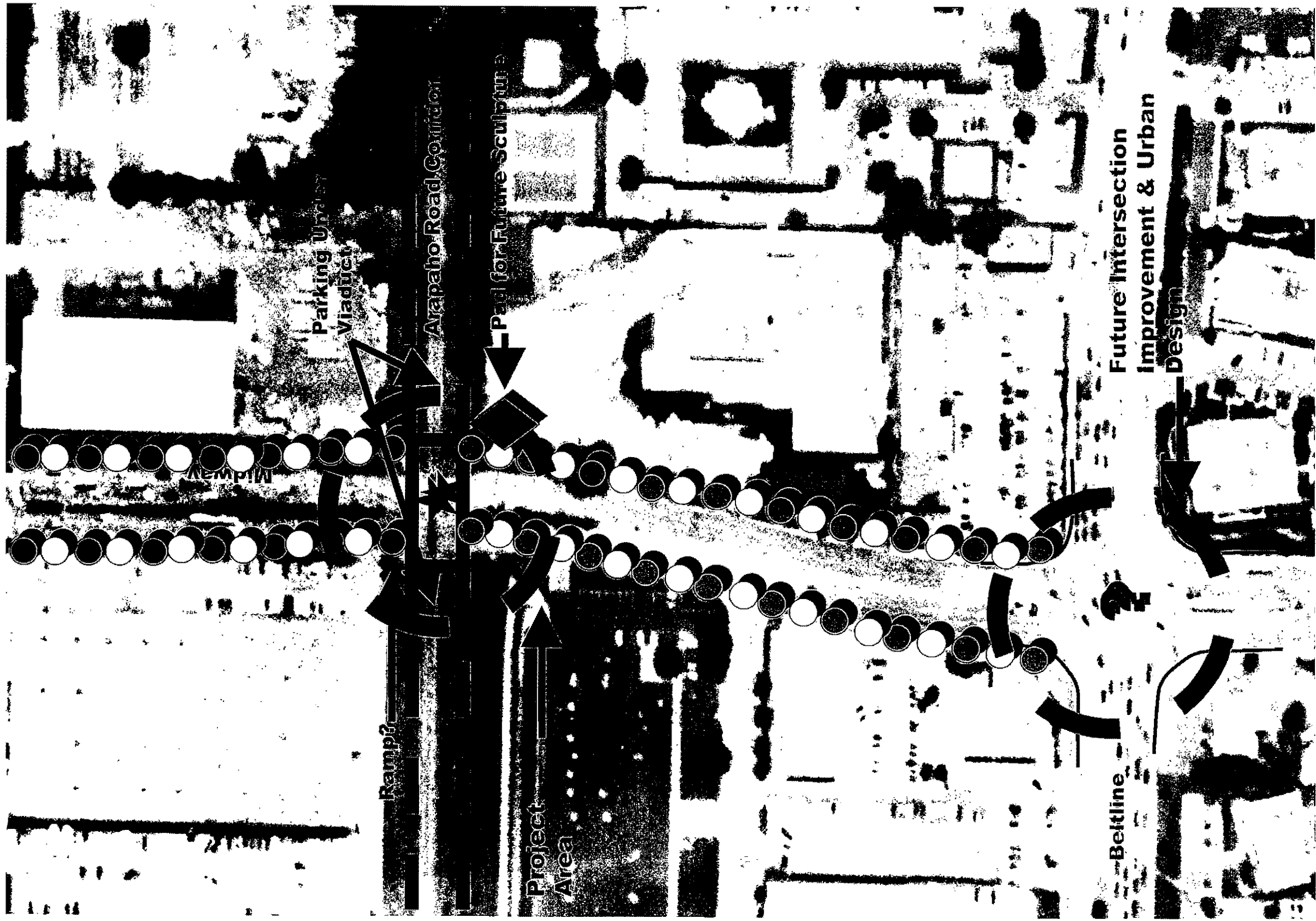
TOWN OF ADDISON, TEXAS





TYPICAL SECTION





Project Area

ARAPAHO BRIDGE

TOWN OF ADDISON, TEXAS



