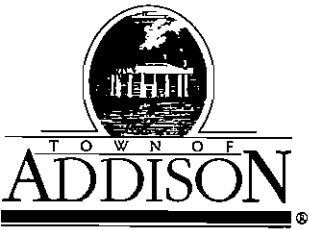


Cavanaugh Drive - Approach Reconstruction
2005

2005/11/12



Public Works / Engineering
 16801 Westgrove • P.O. Box 9010
 Addison, Texas 75001
 Telephone: (972) 450-2871 • Fax: (972) 450-2837

LETTER OF TRANSMITTAL

DATE	1-24-05	JOB NO.
ATTENTION		
RE:	Addison Airport	
	Cavanaugh Drive Approach	
	Reconstruction	

TO Carmen Moran

GENTLEMAN:

WE ARE SENDING YOU

- Shop Drawings
- Copy of letter
- Attached
- Prints
- Change order
- Under separate cover via _____ the following items:
- Plans
- Samples
- Specifications
- _____

COPIES	DATE	NO.	DESCRIPTION
1			Proposal from Mori's Engineering Inc for design of reconstruction of Drive Approach for Cavanaugh Museum

THESE ARE TRANSMITTED as checked below:

- For approval
- For your use
- As requested
- For review and comment
- FOR BIDS DUE _____ 19_____
- Approved as submitted
- Approved as noted
- Returned for corrections
- _____
- Resubmit _____ copies for approval
- Submit _____ copies for distribution
- Return _____ corrected prints
- PRINTS RETURNED AFTER LOAN TO US

REMARKS Drive approach connects To Taxiway
Uniform

COPY TO _____

SIGNED: [Signature]

If enclosures are not as noted, please notify us at once.

TOWN OF

ADDISON

PUBLIC WORKS

To: Mori Akhavan, PE

From: Jim Pierce, P.E.

Company: Mori's Engineering

Asst. Public Wks. Dir.

Phone: 972/450-2879

FAX #: 972-758-1838

FAX: 972/450-2837

jpierce@ci.addison.tx.us

Date: 1-24-05

16801 Westgrove

P.O.Box 9010

of pages (including cover): 2

Addison, TX 75001-9010

Re: Cavanaugh Drive Approach Reconstruction

Original in mail

Per your request

FYI

Call me

Comments: Your proposal of 1-3-05 has been accepted.

Please consider this your notice to proceed.

Call Dave Foster (cell) 214-683-7583

for airport access.

cc Lisa Pyles

Mark Alvedo

Addison!

JIM PIERCE, P.E.
Assistant Public Works Director
(972) 450-2879
(972) 450-2837 FAX
jpierce@ci.addison.tx.us

Town of Addison 16801 Westgrove Dr. P.O. Box 9010, Addison, Texas 75001-9010

1-11-05

Mark- Would like your
approval on this ... to re-
construct the drive approach
for Cavanaugh Hangars. Required
because of Keith Products building
expansion.

Jim

MORI'S ENGINEERING, INC.
Engineering - Surveying - Subsurface Utility
2616 Pickwick Lane
Plano, Texas 75093
mori@mce-us.com
PH: 972-816-2626 FAX: 972-758-1838

FAX TRANSMITTAL

To: Mr. Jim Pierce, P.E.

Fax No: (972) 450-2837

From: Mori Akhavan, P.E. Phone No. 972-816-2626

Subject: Addison Airport Drive Reconstruction

Date: January 3, 2005

Total Pages: 2(including cover)

Comments:

Thanks
Mori

MORI'S ENGINEERING, INC.
 Engineering • Surveying • Subsurface Utility
 2616 Pickwick Lane, Plano, Texas 75093
 Ph. 972-816-2626, Fax 972-758-1838
mori@nce-us.com

Date: January 3, 2005

Mr. Jim Pierce, P.E.
 Towne of Addison
 16801 Westgrove
 Addison, Texas 75248
 972-450-2871, 972-450-2837 Fax

RE: Addison Airport – Driveway Reconstruction
 Addison, Texas
 Surveying & Engineering Services Fee Proposal

Dear Jim:

I am very pleased to submit a fee proposal to provide Surveying and Engineering services for the subject project as follows:

1. FIELD: A topography survey of the subject area will be provided at 50' interval cross sections, including the finished floor elevations of all buildings. All the features within the project area will be collected, including pavement, pavement marking, joints, fences, buildings, walls, trees, bushes, meters, valves, fire hydrants, manholes, inlets, poles, posts, property corners, etc. All break lines including, retaining walls, curbs, edge of pavements, gravel lines, ditches with top, toe and centerlines, swales, etc. will be surveyed. Flow lines of all manholes and inlets within the project limits will be measured.
 FEE.....\$950

2. ENGINEERING: The subject driveway will be extended approximately 8' to create more turning space. The drive Storm sewer pipes will be removed and replaced. Construction document drawings and specification notes will be prepared to show all proposed features, including dimensions, grades and notes. Drawing will also show the plan/profile of the proposed drive culvert pipe.
 FEE.....\$2,350

- Plotting, printing, deliveries.....\$85

TOTAL ESTIMATED FEE.....\$3,385

Please review this proposal and do not hesitate to contact me with any question.
 I am anxiously looking forward in working with you toward a successful and complete project.

Sincerely,

 Mori Akhavan, P.E.



September 17, 2004

TOWN OF ADDISON
Mr. James C. Pierce, Jr., P.E.
Assistant City Engineer
P.O. Box 9010
Addison, Texas 75001-9010

In Account With

HNTB CORPORATION

Correspondence Address:
5910 West Plano Parkway, Suite 200
Plano, TX 75093

Remittance Address:
P.O. Box 802741
Kansas City, MO 64180-2741

Invoice No. 1-40608-DS-001

Professional services in connection with the Emergency Work at the Addison Airport.

For the period: 6/18/2004 through 8/20/2004

PAVEMENT REPAIR

LUMP SUM FEE: \$ 1,668.00

100.0% Complete x \$ 1,668.00 \$ 1,668.00

Total Amount Earned To Date \$ 1,668.00
Less Previously Invoiced -

AMOUNT DUE THIS INVOICE \$ 1,668.00

*Emergency repairs at Cavanaugh
Flight Museum Approach Entrance*

*OK to pay
Julian 9-27-04*

July 13, 2004

Mr. Jim Pierce
Asst. Public Works Director
Town of Addison
16801 Westgrove Drive
Addison, TX 75001-9010

SUBJECT: PAVEMENT REPAIR – CAVANAUGH FLIGHT MUSEUM ENTRANCE

Dear Mr. Pierce:

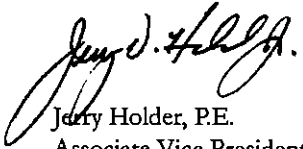
HNTB Corporation is pleased to submit the following proposal for Profession Engineering Services in connection with the subject. This Agreement is by and between HNTB Corporation and the Town of Addison. HNTB shall provide the following services:

- Assess pavement at location of failure
- Analyze video and recommend repair process
- Coordinate contractor and museum staff
- Site visits for construction inspection


Services shall be provided on a lump sum basis, in the amount of \$1,668.00.

If this proposal meets with your approval, please sign below and return one copy for our files.

Regards,


 Jerry Holder, P.E.
 Associate Vice President

Accepted: Town of Addison

By: 

Title: Asst Public Wks Director

Date: 7-19-04

40608-DS-001

SITE CONCRETE, INC.

Statement

To: Addison Airport
16051 Addison Rd., Ste. 220
Addison, TX 75001

06/11/04
Site Job# 24-053
Extra Work At Flight Museum

Attn: Lisa Pyles 972-392-4850 Fax 972-788-9334

We propose to furnish all labor, materials, and equipment necessary to construct, as an independent contractor, the following described work:

LOCATION: Addison Airport Museum Approach

<u>DESCRIPTION</u>					
1	Loader w/ Operator	APPROX.	2 DAYS @	\$1,200.00 DAY	\$2,400.00
2	Rubber Tire Excavator	APPROX.	3 DAYS @	\$1,400.00 DAY	\$4,200.00
3	Truck Rental (2 trucks)	APPROX.	2 DAYS @	\$900.00 DAY	\$1,800.00
4	Laborers	APPROX.	2 DAYS @	\$300.00 DAY	\$600.00
5	Supervision	APPROX.	3 DAYS @	\$450.00 DAY	\$1,350.00
6	P501 Conc. W/ Accelerator	APPROX.	80 CY @	\$125.00 CY	\$10,000.00
7	#5 Steel	APPROX.	1 LS @	\$1,000.00 LS	\$1,000.00
9	24" Class 4 RCP	APPROX.	192 LF @	\$50.00 LF	\$9,600.00
10	Flowable BackFill	APPROX.	32 CY @	\$65.00 CY	\$2,080.00
11	Delivery Fees	APPROX.	1 LS @	\$350.00 LS	\$350.00
12	Dump Fees	APPROX.	24 LOAD @	\$125.00 LOAD	\$3,000.00
Grand Total:					\$36,380.00

Note: We reduced the unit amount for Concrete to help with the additional amount of concrete needed for triple barrel headwalls and additional time to grade rock channel.

Per Bryan Piper.

SITE CONCRETE, INC.
3340 ROY ORR BOULEVARD
GRAND PRAIRIE, TEXAS 75050-4207
972-313-0733 FAX #972-513-0825
ESTIMATING FAX #972-513-0661

*OK to pay
John
6-15-04*

Clean mud out of eyes

To: Site Concrete
From: Town of Addison
Subject: Work Order CAV-1:

Date: May 19, 2004

defective

Work

This work shall consist of removal and replacement of existing concrete pavement, removal and replacement of existing concrete storm sewer pipe, traffic control devices, and all labor and material necessary to provide the services listed below. All work will be on a "time and materials basis" and will be under the supervision of the Addison Airport (Owner).

The concrete slab on the ramp between "Taxiway Uniform" and the "Cavanaugh Flight Museum Apron" will be removed starting with the two slabs on south end and the eastern most slab that runs from the ramp to taxiway. The exposed pipes will be removed. A minimum 95% Standard Proctor compaction at optimum moisture content will be obtained in all areas, including under the pipes.

The removed pipes will be examined for condition and be reused if possible. The decision to reuse the pipes will be agreed to by the Owner. Any of the existing pipe sections that are not reused will be replaced with new concrete pipe the Contractor will have on site. The pipe will be Reinforced Concrete Pipe C76- Class IV. Gasket material will be used to prevent infiltration at all joints. Pipe lengths will be 8-foot sections.

Split pipes

After proper compaction of the subgrade under the pipes has been accomplished, the pipes shall be installed with a 1-foot clear space between pipes. The pipes will be backfilled with flowable fill up to the springline. A concrete cradle will be poured above the pipe section, extending 1-foot outside of the exterior pipes. This cradle will be poured to the *bottom of the* final ramp elevation.

After the initial concrete slabs are removed, an examination of the subgrade under the northern slabs will be conducted and a determination of the necessity to remove these two slabs will be made by the Owner.

~~The new concrete pavement will consist of prepared subgrade (95% standard proctor) and an 8-inch slab with #5 rebar on 18-inch spacing in both directions. The concrete shall be 3,500 psi high early strength material and shall be doweled into the existing pavement with 1 1/4-inch bars, 20-inch long, on 15-inch centers (paint and oil one end, drill and epoxy other end). All joints shall be sealed with P605 Silicone Joint Sealant and a 1 1/4-inch backer rod.~~

Schedule

The construction schedule shall start at 7:00 a.m. on Thursday, May 20, 2004 and the ramp will be open to plane traffic on 7:00 a.m. on Wednesday, May 26, 2004.

All safety precautions necessary to meet FAA regulations will be strictly adhered to at all times.

Agreed to by:

Site Concrete

Date

*pos set @
low grade
not 8" slab
on low
mud.
inf. of slab
n top of
pipes*



concrete to be added

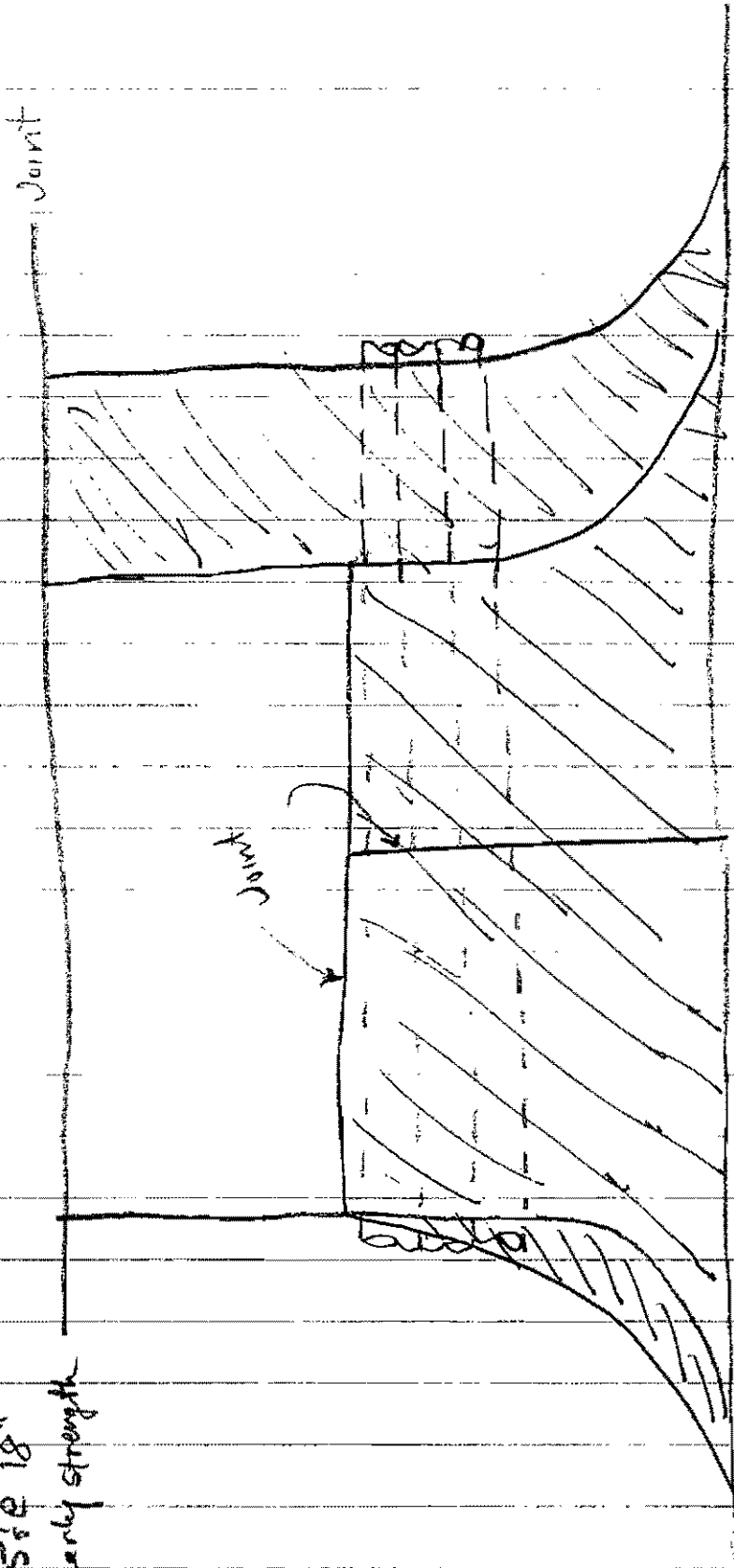
- ① Remove concrete
- ② Relay Pipes — use any good existing, replace those needing replacement
- ③ Relay pipes with 1 foot space between each pipe
- ④ Backfill with flowable fill
- ⑤ Reinf # 5 @ 18"
- ⑥ use hi-early strength concrete



concrete to be removed

12,000
10,000
11,000

192 = 30,000
20,000 + pipe



Taxi way Uniform

CAVANAUGH RAMP

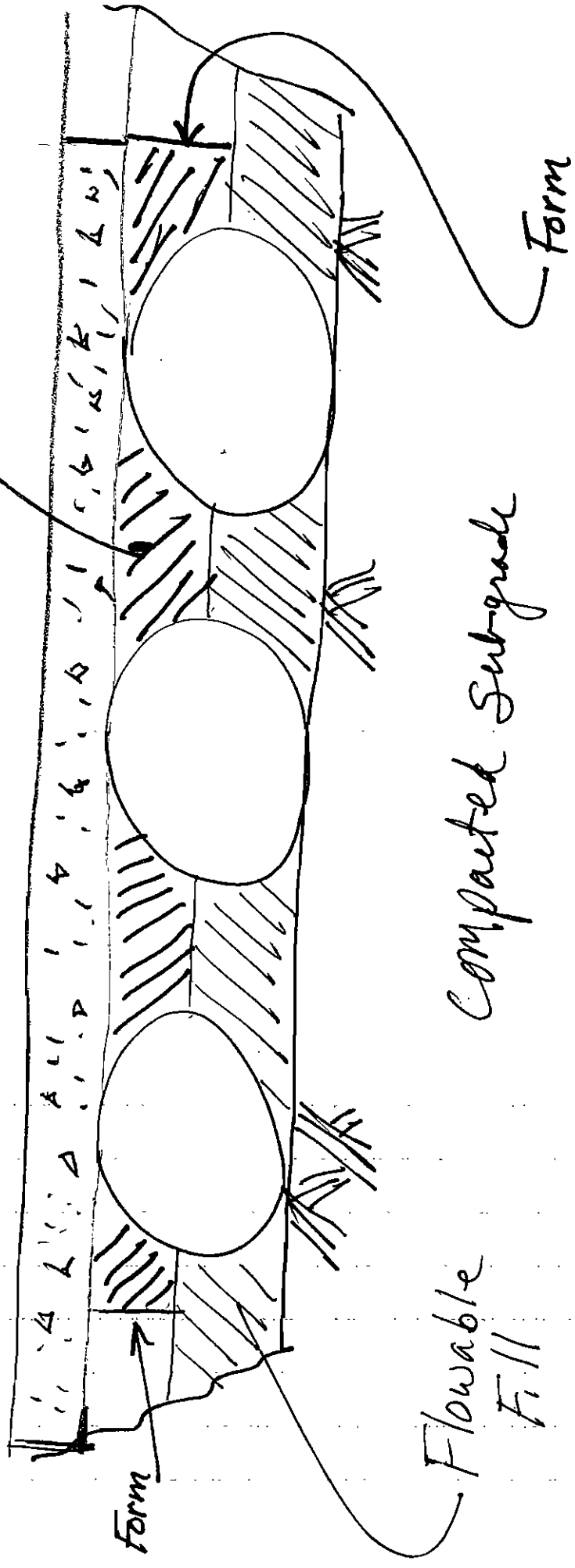
5-18-04

Well pipe float?

$$A = 785 \times \left(\frac{32}{12}\right)^2 = 785 \times 7.11 = 5.58 \text{ ft}^2, \quad \frac{1}{2} \times 2 = 2.79 \text{ ft}^2$$

$$2.79 \text{ ft}^2 \times 150 \text{ lb/ft}^3 = 418 \text{ lb}$$

Pipe weighs 366 lb/ft
Pipe and float
UMC Cradle



To: Site Concrete
From: Town of Addison
Subject: Work Order CAV-1:

Date: May 19, 2004

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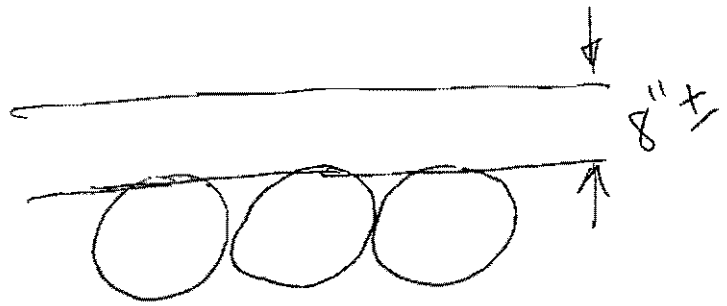
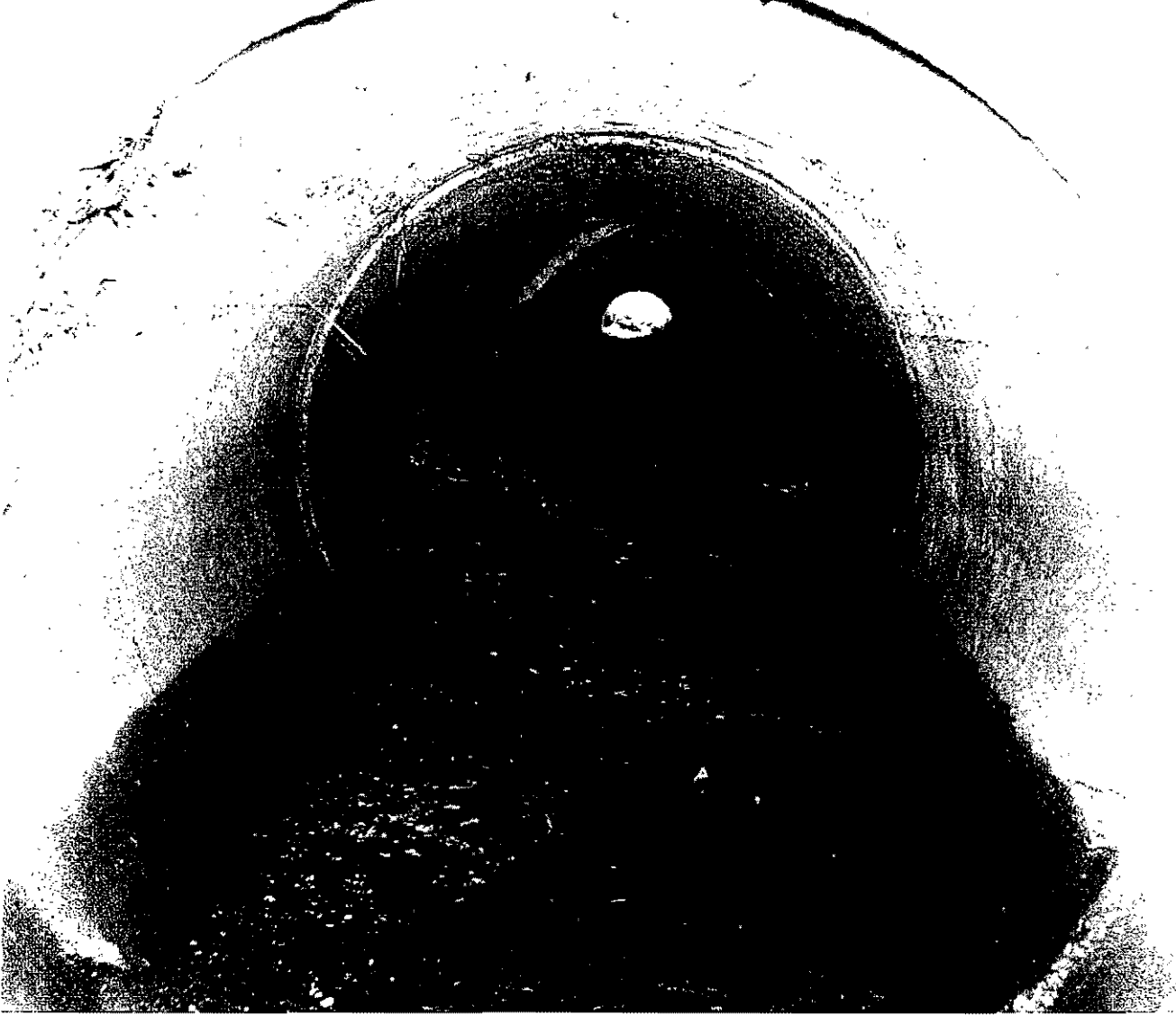
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Agreed to by:

Site Concrete

Date





MA