

D-

1996 DART LAP/CMS PROGRAM



1996

ARAPAHO ROAD PHASE I 1996-97 (9 MONTH ACTIVITY CALENDAR)

TASKS	JULY	AUGUST	SEPTEMBER	OCTOBER	NOVEMBER	DECEMBER	JANUARY	FEBRUARY	MARCH
B. Pinnel ROW	<div style="display: flex; align-items: center;"> <div style="width: 10%; border: 1px solid black; background-color: #e0e0e0; margin-right: 5px;">In Process</div> <div style="width: 90%; border: 1px solid black; background-color: #e0e0e0; position: relative;"> <div style="position: absolute; top: -10px; left: 50%; transform: translate(-50%, -100%);">Expected ROW acquisition</div> </div> </div>								
Southwestern Bell ROW	<div style="display: flex; align-items: center;"> <div style="width: 10%; border: 1px solid black; background-color: #e0e0e0; margin-right: 5px;"></div> <div style="width: 90%; border: 1px solid black; background-color: #e0e0e0; position: relative;"> <div style="position: absolute; top: -10px; left: 50%; transform: translate(-50%, -100%);">Negotiations run concurrent with Pinnel ROW negotiations</div> </div> </div>								
Auto One building ROW	<div style="display: flex; align-items: center;"> <div style="width: 10%; border: 1px solid black; background-color: #e0e0e0; margin-right: 5px;"></div> <div style="width: 90%; border: 1px solid black; background-color: #e0e0e0; position: relative;"> <div style="position: absolute; top: -10px; left: 50%; transform: translate(-50%, -100%);">Negotiations run concurrent with Pinnel ROW negotiations</div> </div> </div>								
DART Transit Center									
Utility relocation									
D. Snadon sidewalk issues									
75% Design Process									
Bid and construction dates	<div style="display: flex; justify-content: space-around; align-items: center;"> <div style="text-align: center;"> <p>3-1-97</p> <p>Out to bid 12/4/96</p> </div> <div style="text-align: center;"> <p>4-27-97</p> <p>Award bid 1/15/97</p> </div> <div style="text-align: center;"> <p>Start const.</p> </div> </div>								

Start west of Aurora

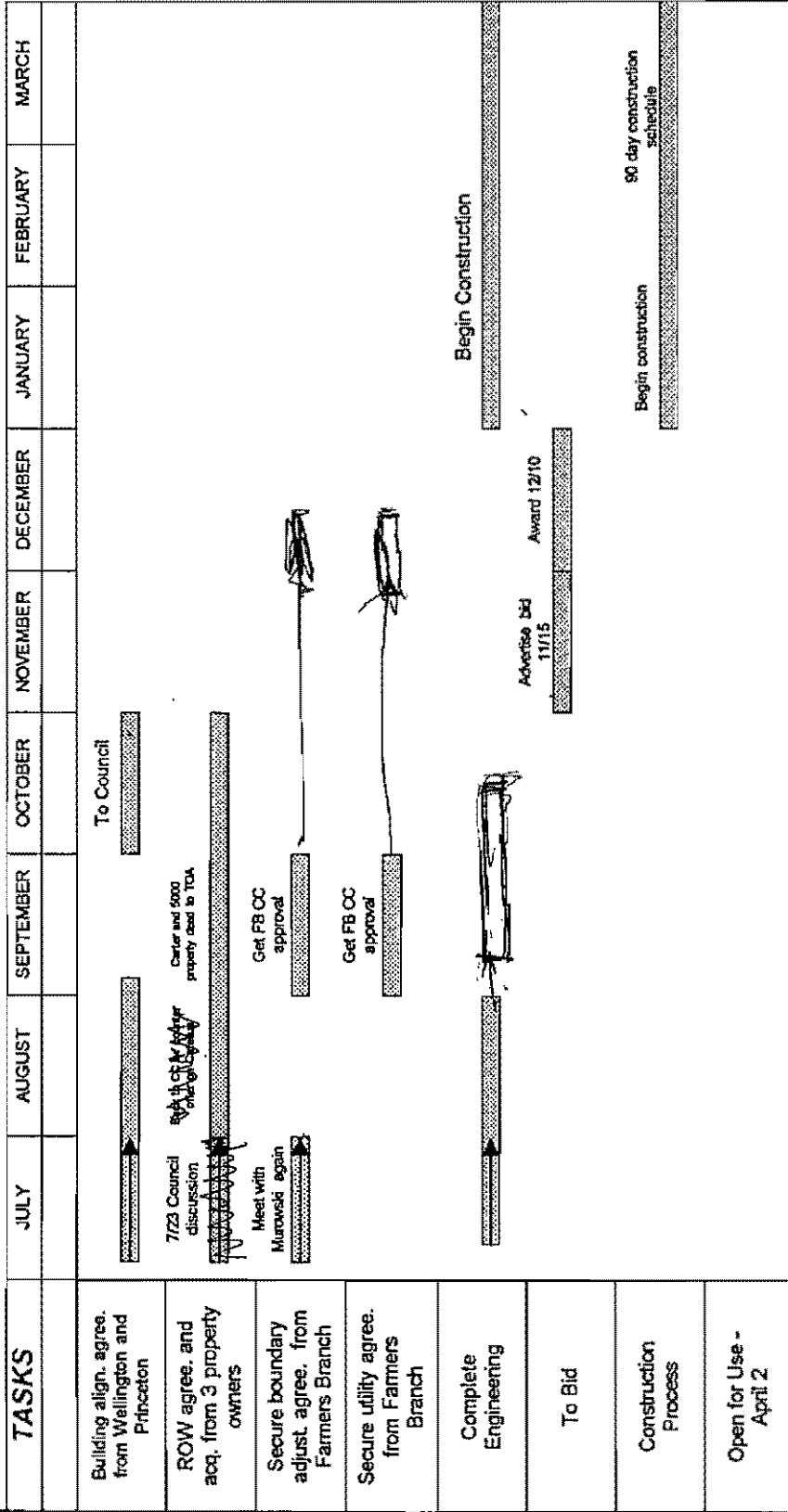
Start east of Aurora

Legend:		
	Scheduled	Current

SPECIAL NOTES / ISSUES

Demolition of existing structures must be added to the schedule.

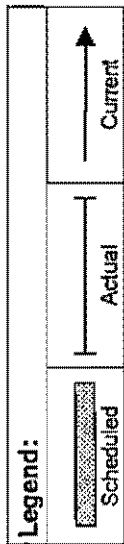
SOUTH QUORUM (North/South Phase) 1996-97 (9 MONTH ACTIVITY CALENDAR)



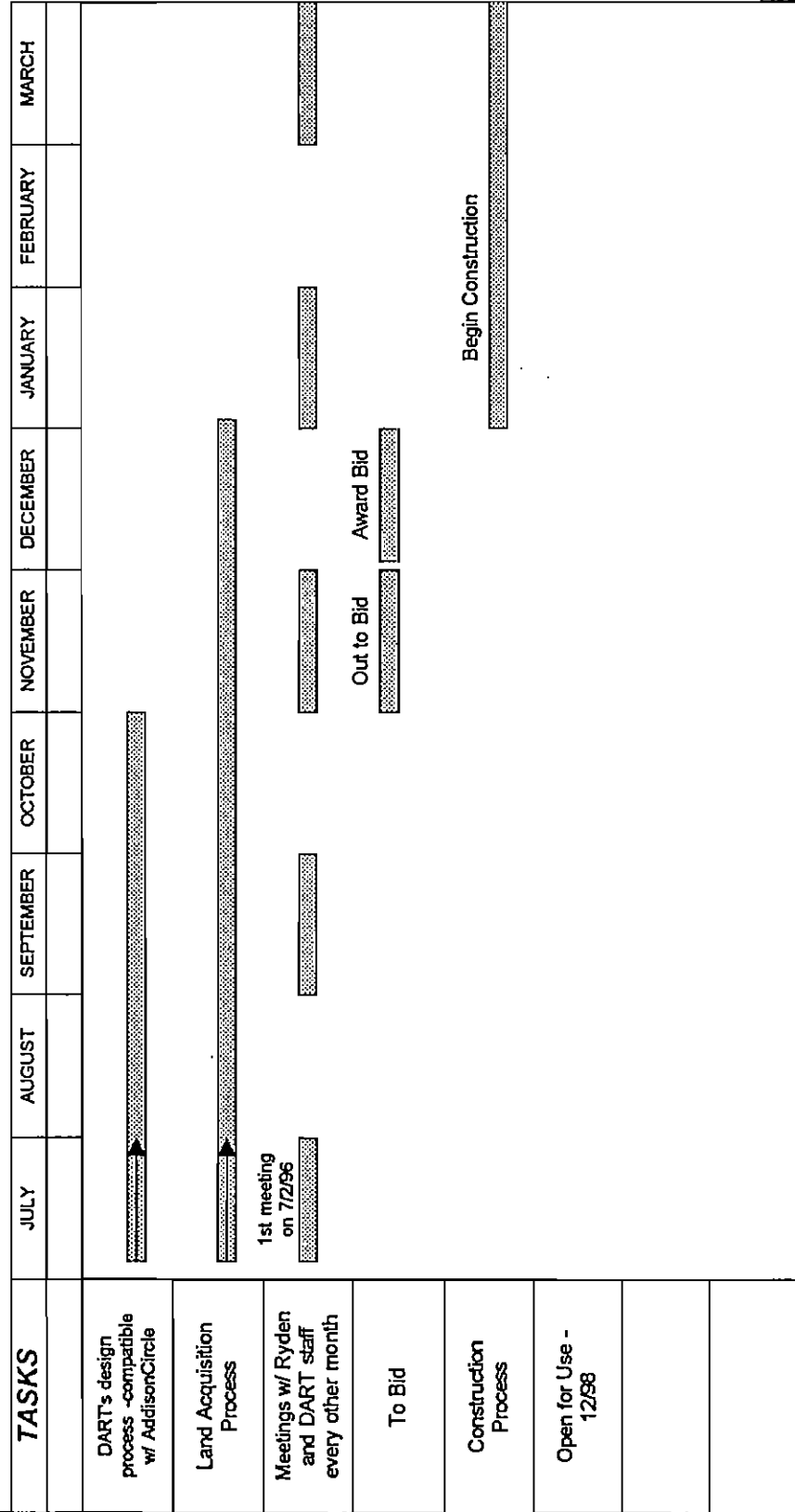
SPECIAL NOTES / ISSUES

Three (3) property owners impacted with North/South phase:

- Donald Carfer
- 5000 Quorum
- Catellus Inves.



DART TRANSIT CENTER 1996-97 (9 MONTH ACTIVITY CALENDAR)



SPECIAL NOTES / ISSUES

- TOA does not fully control development process. DART's ownership in the project may drive the development schedule. B. Pinnel negotiations may take DART longer than expected. We estimate Pinnel negotiations to take roughly 4 months.





Dallas Area Rapid Transit
P.O. Box 660163
Dallas, Texas 75266-0163
214/749-3278

November 18, 1996

Dave Nighswonger, P.E.
Assistant City Engineer
Town of Addison
P. O. Box 144
Addison, TX 75001

Re: LAP and LAP/CMS Projects

Dear Dave:

In response to your September 19, 1996 and October 16, 1996 letters, attached is the DART Resolution approving the Addison Road/Excel Way LAP project. Also attached is a check in the amount of \$134,000.00 for Technical Assistance (\$34,000) and Surveyor Road railroad crossing (\$100,000) projects. A copy of the signed LAP/CMS applications are attached.

In the future when referring to either of the above LAP/CMS projects, please use the project name and project number. This will help avoid confusion when referring to projects.

If you need additional information, please call me at 749-2913.

Sincerely yours,

A handwritten signature in black ink, appearing to read "Tom K. Ryden". The signature is fluid and cursive, with the first letters of the first and last names being capitalized and prominent.

Tom K. Ryden, P.E.
Sr. Manager Project Development

TKR:jr

Attachments



Dallas Area Rapid Transit

RESOLUTION

of the

960194

RESOLUTION

DALLAS AREA RAPID TRANSIT
(Executive Committee)

FY 1996 Local Assistance Program (LAP) Programming
Request from the Town of Addison

WHEREAS, the Town Addison is eligible for participation in the DART LAP program; and

WHEREAS, the activities requested are consistent with the intent of the LAP and LAP/CMS program guidelines; and

WHEREAS, funding for the activities requested is within current Budget and Financial Plan allocations.

NOW, THEREFORE, BE IT RESOLVED by the Dallas Area Rapid Transit Board of Directors that the FY 1996 LAP programming request from Addison, as contained in Attachment 1, is approved.

Sandy Greyson
Secretary

Billy J. Ratcliff
Chairman

APPROVED AS TO FORM:

ATTEST:

DART Counsel

Roger Snoble
President/Executive Director

October 22, 1996

Date

FUNDING APPLICATION FOR DART's LAP/CMS PROGRAM

DART RECEIVED

OCT 16 1996

LAP/CMS Project No. (for DART use only) 09 - 021 - 97001
 city no. project type project no.

PROJECT DEVELOPMENT

GENERAL INFORMATION		Date Submitted _____
Applicant City: <u>Town of Addison</u>	City CIP No. <u>N/A</u>	
Project Eligibility Type: (see list, reverse side) _____	Technical Assistance Program <u>21</u>	
Project Name: <u>Technical Assistance Program</u>		
Boundaries/short description _____		
Approx. Begin/End Dates: from <u>October 1996</u> to <u>September 1997</u>		

FUNDING		PROJECTED COST DISTRIBUTION	
Current LAP/CMS Request	\$ 34,000	Planning /Environmental Studies	\$
Previously Approved LAP/CMS Date(s)	\$ 0	Engineering/Design	\$
Federal (if applicable)	\$	Right-of-Way	\$
State (if applicable)	\$	Construction	\$
Local (if applicable)	\$	Testing/Inspection	\$
Other (if applicable)	\$	Operations/Maintenance	\$
TOTAL	\$ 34,000.	TOTAL	\$

DETAILED PROJECT DESCRIPTION

(List/Explain bus routes affected, intersections to improve, project scope, phases, etc. Attach project justification as needed.)
 (List/explain fund transfers, reprogramming requested if applicable.)

Funding for salaries of engineering staff responsible for coordinating and supervising the Town's capital improvement projects funded with LAP/CMS Funds.

John Bagat
 Authorized City Staff Representative

Tom R. Nolen
 DART Staff Concurrence

Public Works Director

Title

9-19-96

Date

10-23-96

Title

Date

FUNDING APPLICATION FOR DART's LAP/CMS PROGRAM

DART
RECEIVED

OCT 16 1996

LAP/CMS Project No. (for DART use only) 09 - 020 - 97001
 city no. project type project no.

PROJECT DEVELOPMENT

GENERAL INFORMATION		Date Submitted
Applicant City: <u>Town of Addison</u>	City CIP No. <u>N/A</u>	
Project Eligibility Type: (see list, reverse side) <u>20</u>		
Project Name: <u>Railroad crossing replacement on Surveyor Road</u>		
Boundaries/short description <u>Cottonbelt Railroad Crossing on Surveyor Road</u>		
Approx. Begin/End Dates: from _____ to _____		

FUNDING		PROJECTED COST DISTRIBUTION	
Current LAP/CMS Request	\$ 100,000.	Planning /Environmental Studies	\$
Previously Approved LAP/CMS Date(s)	\$ 0.	Engineering/Design	\$
Federal (if applicable)	\$	Right-of-Way	\$
State (if applicable)	\$	Construction	\$ 100,000.
Local (if applicable)	\$	Testing/Inspection	\$
Other (if applicable)	\$	Operations/Maintenance	\$
TOTAL	\$ 100,000.	TOTAL	\$ 100,000.

DETAILED PROJECT DESCRIPTION

(List/Explain bus routes affected, intersections to improve, project scope, phases, etc. Attach project justification as needed.)
 (List/explain fund transfers, reprogramming requested if applicable.)

Replace wood railroad tie crossing with precast concrete panels.

John Bangate
 Authorized City Staff Representative

Public Works Director
 Title

9-19-96
 Date

Jan K. Mylen
 DART Staff Concurrence

Title

10-23-96
 Date

960194

APPLICATION FOR LOCAL ASSISTANCE PROGRAM

DART RECEIVER

SEP 23 1996

PROJECT DEVELOPMENT

TYPE OF SUBMISSION (1) <input checked="" type="checkbox"/> Capital (C) <input type="checkbox"/> Technical (T) <input type="checkbox"/> Revision (R)	DATE SUBMITTED (2) September 18, 1996
CATEGORY CODE (see reverse) (3) <u>1</u> (4) <u>2</u> (5) _____ (6) _____ (7) _____ (8) _____	

APPLICANT INFORMATION	
LEGAL NAME (9) Town of Addison	ORGANIZATIONAL UNIT (10) Public Works
ADDRESS P.O. Box 144 City <u>Addison</u> (11) County <u>Dallas</u> (12) State <u>Texas</u> (13) Zip Code <u>75001</u> (14)	CONTACT PERSON Name: <u>John Baumgartner, P.E.</u> Telephone: <u>(972) 450-2871</u>

PROJECT TITLE (15)
Addison Road/Excel Way (formerly Bent Tree Plaza Drive) Intersection Improvements

PROJECT DESCRIPTION AND JUSTIFICATION (16)

The intersection will be widened to provide left turn lanes on Addison Road and install a traffic signal at the intersection.

Bus Route: 341

PROPOSED PROJECT Start Date <u>October, 1996</u> (17)	Ending Date <u>March, 1997</u> (18)
--	-------------------------------------

ESTIMATED FISCAL YEAR FUNDING		CONSTRUCTION COST DISTRIBUTION	
Federal \$ _____ (19)	Engineering/Design \$ 23,000 (26)	Right-of-Way \$ 23,000 (27)	
City \$ _____ (20)	Construction \$ 300,000 (28)	Other \$ _____ (29)	
State \$ _____ (21)	Total \$ 346,000 (30)		
Local \$ _____ (22)			
Other \$ _____ (23)			
DART \$ 346,000 (24)			
TOTAL \$ 346,000 (25)			

SIGNATURE OF AUTHORIZED REPRESENTATIVE	TITLE John Baumgartner, P.E. Director of Public Works	DATE SIGNED
--	---	-------------

FOR DART USE ONLY			
To DART Staff	Date _____	To P&D Committee	Date _____
To TASC	_____	P&D Review	_____
TASC Review	_____	Approved by P&D	_____



LETTER OF TRANSMITTAL

Public Works / Engineering
 16801 Westgrove • P.O. Box 144
 Addison, Texas 75001
 Telephone: [214] 450-2871 • Fax: [214] 931-6643

DATE	11/12/96	JOB NO.
ATTENTION	Ms. Jule Rodriguez	
RE:	LAP/CMS	

TO DART
P.O. Box 660163
Dallas, Tx. 75266-0163

GENTLEMAN:

- WE ARE SENDING YOU**
- Attached
 - Under separate cover via _____ the following items:
 - Shop Drawings
 - Prints
 - Plans
 - Samples
 - Specifications
 - Copy of letter
 - Change order
 - _____

COPIES	DATE	NO.	DESCRIPTION
1			Addison Ordinance No. 096-046 portion covering capital projects

THESE ARE TRANSMITTED as checked below:

- For approval
- For your use
- As requested
- For review and comment
- FOR BIDS DUE _____ 19_____
- Approved as submitted
- Approved as noted
- Returned for corrections
- _____
- Resubmit _____ copies for approval
- Submit _____ copies for distribution
- Return _____ corrected prints
- PRINTS RETURNED AFTER LOAN TO US

REMARKS _____

COPY TO _____

SIGNED: David Nighswonger

If enclosures are not as noted, please notify us at once.

TOWN OF
ADDISON

PUBLIC WORKS

To: Julie Rodriguez

Company: DART

FAX: 214-749-3662

Date: 11/1/96

of pages (including cover): 6

From: David Nighswonger, P.E.
Engineering
Phone: 214/450-2879
FAX: 214/450-2837

16801 Westgrove
P.O. Box 144
Addison, Texas 75001

Original in mail Original not to follow FYI Call me Per your request

Comments:

Supporting documents for
Surveyor Crossing LAP/CMS application.

opportunity to be heard for or against any item or the amount of any item contained in said budget, and all said persons were heard, after which said public hearing was closed; and

WHEREAS, the City Council, upon full consideration of the matter, is of the opinion that the budget hereinafter set forth is proper and should be approved and adopted:

NOW, THEREFORE, BE IT ORDAINED BY THE CITY COUNCIL OF THE TOWN OF ADDISON, TEXAS:

SECTION 1. The sum of \$49,509,310 is hereby appropriated for budget expenditures and that expenditures during the fiscal year shall be made in accordance with the budget approved by this ordinance unless otherwise authorized by a duly enacted ordinance of the City.

SECTION 2. The budget as adopted shall be deemed the official budget for the Town of Addison, Texas for the said fiscal year and a copy of the same marked "Exhibit A" shall be kept on file with the City Secretary and shall be open to inspection by any interested persons.

SECTION 3. That all ordinances of the City in conflict with the provisions of this ordinance be, and the same are hereby repealed and all other ordinances of the city not in conflict with the provisions of this ordinance shall remain in full force and effect.

SECTION 4. The importance of this ordinance creates an emergency and an imperative public necessity, and the ordinance shall take effect and be in force from and after its adoption.

DULY PASSED BY THE CITY COUNCIL OF THE TOWN OF ADDISON, TEXAS, on this the 24th day of September, 1996.



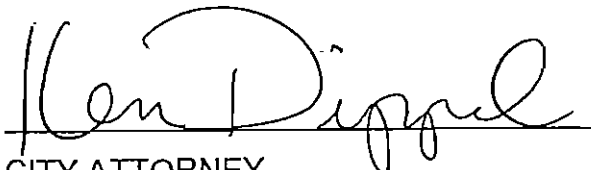
MAYOR

ATTEST:



CITY SECRETARY

APPROVED AS TO FORM:



CITY ATTORNEY

PUBLISHED IN THE METROCREST NEWS ON: _____

OFFICE OF THE CITY SECRETARY

ORDINANCE NO. 096-046

TOWN OF ADDISON
FIVE YEAR CAPITAL PROJECT FUNDING SUMMARY
Council Adopted Budget

	General	Hotel	Airport	Utility	CP Funds	DART	FAA	Other Funding	Developer	TOTAL
Arapaho Road Extension						1,500,000				1,500,000
ROW to Addison Rd Phase I						1,659,000				1,659,000
Arapaho Phase I Paving / Landscaping										2,462,000
Arapaho Phase II	2,462,000									2,462,000
Arapaho Phase III	8,591,000									8,591,000
Total										14,212,000
Urban District										
Phase I - Quorum Rotary Park*					965,000					965,000
Phase I - Bosque Park*					535,000					535,000
Phase I - Quorum Drive					928,000					928,000
Phase I - Mildred Drive					280,000					280,000
Phase I - Street R-3					445,000					445,000
Phase I - Street R-4					596,000					596,000
Phase I - Street M-1					780,000					780,000
Phase I - Storm Sewer Improvements					287,000					287,000
Phase I - Utility Improvements				69,000						69,000
Special Event Land Acquisition		5,490,000							1,100,000	6,590,000
Special Event Landscaping		4,360,000							53,500	4,360,000
Phase IIa - Quorum Drive	178,000									178,000
Phase IIa - Mildred Drive	449,000								134,700	583,700
Phase IIa - East Mildred Park	610,000									610,000
Phase IIa - Street M-2	362,000							108,600		470,600
Phase IIa - Street R-4	268,000							80,400		348,400
Phase IIa - Spectrum Drive	129,000							38,700		167,700
Phase IIb - Quorum North Park	295,000									295,000
Phase IIb - Mews Park	675,000									675,000
Phase IIb - Quorum Drive	342,000									342,000
Phase IIb - Spectrum Drive	235,000									235,000
Phase IIb - Street R-2	270,000									270,000
Phase IIb - Street R-3	205,000									205,000
Phase IIb - Street R-4	54,000									54,000
Phase IIb - Street R-5	166,000									166,000
Phase IIb - Street M-2	262,000									262,000
Pulse Point Acquisition						1,442,000				1,442,000
Pulse Point Parking/Paving						648,000				648,000
Total										22,840,900

TOWN OF ADDISON
FIVE YEAR CAPITAL PROJECT FUNDING SUMMARY
Council Adopted Budget

	General	Hotel	Airport	Utility	CP Funds	DART	FAA	Other Funding	Developer	TOTAL
Street Projects										
Midway/Beltline Intersection						1,000,000				1,000,000
Quorum/Beltline Intersection						750,000				750,000
Midway/Lindberg Intersection						150,000				150,000
South Quorum Access	2,250,000									2,250,000
Keller Springs/Addison Intersection	250,000									250,000
Keller Springs/Midway Intersection	125,000									125,000
Midway Road Rehabilitation Program	100,000							400,000		500,000
Surveyor R. Crossing Reconstruction	75,000									75,000
Airport Pkwy. Road Rehabilitation	88,000									88,000
Belt Line/Toll Road Pedestrian Bridges	6,000,000									6,000,000
Belt Line/Toll Road Urban Interchange						2,000,000				2,000,000
Belt Line Streetscape /TR to Addison Rd.	2,000,000									2,000,000
Addison Road Widening	1,500,000				1,400,000					1,500,000
Landmark/Wellington Extension										1,400,000
Brookhaven Club Drive Rehabilitation	2,500,000									2,500,000
Addison Rd./Bent Tree Pl. Intersection	25,000					225,000				250,000
Total										20,838,000
Park Projects										
Town Hall Creek Stabilization	1300,000									300,000
Upper Wynnwood Pond Desilting	125,000									125,000
Oaks North Drive Screen Wall	110,000									110,000
Pool facility	902,000									902,000
South Esarment Park Land Acquisition	225,000									225,000
South Esarment Park Construction	525,000									525,000
North Toll Road Landscaping-Phase II*					150,000					150,000
North Toll Road Landscaping-Phase III*	100,000								90,000	100,000
Total										2,527,000
Airport Projects										
Master Plan Update			15,000				135,000			150,000
Keller Springs/Dooley Rd. Land Acq. @		40,000					360,000	200,000		600,000
West-side Taxiway Rehabilitation		220,000					1,980,000			2,200,000
West-side Taxiway Extension		220,000					1,980,000			2,200,000
Total										5,150,000

TOWN OF ADDISON
 FIVE YEAR CAPITAL PROJECT FUNDING SUMMARY
 Council Adopted Budget

Utility Projects	General	Hotel	Airport	Utility	CP Funds	DART	FAA	Other Funding	Developer	TOTAL
Midway/Beltline Sewer Interceptor				2,900,000						2,900,000
Celestial Pump Station Improvements@				845,000						845,000
Sewer Line Replacement Program@				1,694,000						1,694,000
Water Line Replacement Program@				678,000						678,000
Total										6,117,000
Miscellaneous										0
TOTAL	\$32,753,000	\$9,850,000	\$495,000	\$6,186,000	\$6,366,000	\$9,374,000	\$4,455,000	\$600,000	\$1,605,900	\$71,684,900

NOTES:

The capital project summary identifies projects which may be included in a capital improvement plan over the next five years. The summary also identifies the likely sources of funding for each project. Projects which are shown as being funded through the Capital Project (CP) funds have funding which is already in place. Projects reflected in the other columns are likely to be funded within the five year planning horizon. Projects reflected in the General column have been identified as being possible candidates for application of General fund surpluses. Although some of these projects may be funded, funding of all listed projects is beyond the General fund's capacity within the five year planning horizon.

* Items funded in whole, or in part with General fund surpluses from prior fiscal years.

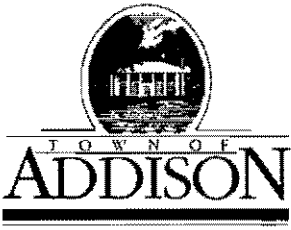
@ Items added to Capital Project List following FY1996 budget.

Projects in reverse type are proposed to be funded with FY1997 General fund surpluses.

Shaded items are proposed for funding should primary projects be delayed in past FY1997.

OFFICE OF THE CITY SECRETARY

ORDINANCE NO. O96-046



LETTER OF TRANSMITTAL

Public Works / Engineering

16801 Westgrove • P.O. Box 144
Addison, Texas 75001
Telephone: (214) 450-2871 • Fax: (214) 931-6643

DATE	10/3/96	JOB NO.
ATTENTION	Tom Ryden	
RE:	LAP/CMS	

TO DART

GENTLEMAN:

WE ARE SENDING YOU

- Attached
- Under separate cover via _____ the following items:
- Shop Drawings
- Prints
- Plans
- Samples
- Specifications
- Copy of letter
- Change order
- _____

COPIES	DATE	NO.	DESCRIPTION
1			Executed LAP/CMS Interlocal Agreement

THESE ARE TRANSMITTED as checked below:

- For approval
- For your use
- As requested
- For review and comment
- FOR BIDS DUE _____ 19_____
- Approved as submitted
- Approved as noted
- Returned for corrections
- _____
- Resubmit _____ copies for approval
- Submit _____ copies for distribution
- Return _____ corrected prints
- PRINTS RETURNED AFTER LOAN TO US

REMARKS I will send you a Council Resolution for this item when the City Secretary has prepared it.

COPY TO _____

SIGNED: David Nijssen

If enclosures are not as noted, please notify us at once.

INTERLOCAL AGREEMENT

BETWEEN

DALLAS AREA RAPID TRANSIT

AND

**THE TOWN OF ADDISON, THE CITY OF CARROLLTON,
THE CITY OF COCKRELL HILL, THE CITY OF FARMERS BRANCH,
THE CITY OF GARLAND, THE CITY OF GLENN HEIGHTS,
THE CITY OF IRVING, THE CITY OF PLANO,
THE CITY OF RICHARDSON and THE CITY OF ROWLETT**

**ESTABLISHING GUIDELINES AND PROCEDURES FOR IMPLEMENTING A
LOCAL ASSISTANCE PROGRAM/CONGESTION MANAGEMENT SYSTEM PROGRAM**

WHEREAS, on or about April 11, 1989 and June 27, 1989, DALLAS AREA RAPID TRANSIT ("DART") established a program for the return of a portion of DART's sales tax income to its member cities known and identified as the "Local Assistance Program" ("Lap Program"); and,

WHEREAS, the Lap Program was continued in full force and effect until November 14, 1995 at which time the DART Board, in Resolution No. 950260 adopted a new Transit System Plan pursuant to which the Lap Program would terminate on June 30, 1996; and,

WHEREAS, on June 25, 1996, in Resolution No. 960117 the DART Board extended certain LAP ILA's to the end of Fiscal Year 1996, being September 30, 1996; and,

WHEREAS, the DART Board created a new program for returning a portion of DART' sales tax income to certain member cities, the "Local Assistance Program/Congestion Management System" ("LAP/CMS Program") effective on October 1, 1996, the first day of the 1997 fiscal year; and,

WHEREAS, on August 27, 1996, in Resolution No. 960153, the DART Board adopted a LAP/CMS Policy, including Guidelines for the LAP/CMS Program; and,

WHEREAS, eligibility for the LAP/CMS Program is limited to those member cities in which construction of light rail service has not yet commenced; and,

WHEREAS the parties desire to implement the LAP/CMS Program and to establish therefor guidelines and procedures for the creation, construction, completion, operation, maintenance and funding of projects; NOW THEREFORE,

WITNESSETH:

In consideration of the premises and the mutual promises and covenants contained herein, the parties agree as follows:

**ARTICLE ONE
Parties**

The parties to this Agreement are:

1.01. Dallas Area Rapid Transit ("DART"), a regional transportation authority organized pursuant to and in accordance with Chapter 452, Texas Transportation Code.

1.02. The Town of Addison; The City of Carrollton; The City of Cockrell Hill; The City of Farmers Branch; The City of Garland; The City of Glenn Heights; The City of Irving; The City of Plano; The City of Richardson; and The City of Rowlett.

ARTICLE TWO

Definitions

The following definitions shall apply to the terms used in this ILA, including without limitation the Guidelines:

2.01. "Allocations" shall refer to LAP/CMS Program fund allocations as determined in accordance with Section C of the Guidelines. The Allocation for the City of Irving shall be one half (1/2) that of other Eligible Member Cities Allocations because of the current work on Commuter Rail facilities in the City of Irving.

2.02. "Board" shall mean and refer to the Board of Directors of DART.

2.03. "City Council" shall mean and refer to the duly elected and acting City Council of each Eligible Member City.

2.04. "Commuter Rail" shall mean and refer to the self propelled diesel rail transit operations planned to connect the City of Dallas, the City of Fort Worth and DFW International Airport to be operated by DART and the "T" pursuant to a separate Interlocal Agreement with Railtran, on right-of-way owned by Railtran. Commuter Rail service is to operate under the assumed name of "Trinity Express."

2.05. "DART Enabling Legislation" means Chapter 452, Texas Transportation Code as the same may from time to time be amended.

2.06. "Eligible Member City (City/Cities) shall mean and refer to the Cities enumerated in Article 1.02 of this ILA.

2.07. "Eligible Projects" shall mean and include those listed in Section D of the Guidelines and any other project recommended by an Eligible Member City and specifically considered and approved by the Board.

2.08. "FTA" shall mean and refer to the Federal Transit Administration.

2.09. "Guidelines" shall mean and refer to the Guidelines for the LAP/CMS Program adopted by the Board on August 27, 1996 in Resolution No. 960153.

2.10. "ILA" shall mean and refer to this Interlocal Agreement.

2.11. "Light Rail" shall mean and refer to the DART electric powered rail system as distinguished from Commuter Rail.

2.12. "Railtran" shall mean and refer to DFW RAILTRAN a common administrative agency established by the Cities of Dallas and Fort Worth.

2.13. The "T" shall mean and refer to the Fort Worth Transportation Authority.

ARTICLE THREE
Incorporation of Guidelines

The Guidelines are hereby incorporated into this ILA as if they were set out verbatim at this point. A set of Guidelines is attached to each copy of this ILA.

ARTICLE FOUR
Purposes of ILA

4.01. The initial purpose of this ILA is to set forth the Eligible Member Cities' acceptance of and commitment to DART, and DART's commitment to the Eligible Member Cities, to jointly participate in and work together for the satisfactory implementation of the LAP/CMS Program.

4.02. By executing this ILA, each Eligible Member City accepts and approves the Guidelines as the basis for implementing the LAP/CMS Program.

4.03. It is the further purpose and intent of DART and the Eligible Member Cities to continue to explore other potential options which would permit continued and expanded revenue sharing with all of DART's member cities to meet their respective transportation requirements so long as such options do not adversely impact the DART Transit System Plan or the DART Service Plan or the DART Financial Plan.

ARTICLE FIVE
Term

The term of this ILA shall commence as of the first day of October, 1996 and shall terminate for each Eligible Member City as provided in §C. ALLOCATIONS of the Guidelines. Though terminated as to a specific Eligible Member City, the ILA shall remain in full force and effect until all Allocations for the affected Eligible Member City are finally funded and the activities of such Eligible Member City are wound up. It is understood by all of the parties hereto that the Allocations and program duration's shown in Table 1 of the Guidelines are projections only and that actual Allocations will be based on actual sales tax collections and actual program duration will be determined by the schedule of rail construction. If a contract for rail construction in a Member City is not awarded by the year shown in said Table 1, Allocations will continue through the year in which a rail construction contract is awarded, or through 2004, when all program Allocations shall end.

ARTICLE SIX
Administration and Procurement of Eligible Projects

It shall be the obligation of each Eligible Member City receiving funds under the LAP/CMS Program to supervise and administer the planning, design, land acquisition, construction, operation and maintenance of all Eligible Projects. Receipt of funds under the LAP/CMS Program shall not create an agency, partnership or joint venture relationship between DART and the Eligible Member City. Each Eligible Member City shall be responsible for handling all procurements needed for each Eligible Project in accordance with all applicable federal, state and local laws, codes, ordinances, rules and regulations of any governmental entity exercising jurisdiction over such Eligible Project, and the resolution of any protest, dispute, claim or demand arising out of any such action by the Eligible Member City shall be the sole responsibility of the Eligible Member City.

ARTICLE SEVEN
Indemnity

To the extent permitted by applicable law, each Eligible Member City agrees to defend, indemnify and hold DART, its directors, officers and employees free and harmless against any claims, demands, causes of action, costs and liabilities, including attorney fees, resulting from the performance, or failure of the Eligible Member City to perform any obligation or duty in connection with this ILA. Likewise, to the extent permitted by applicable law, DART hereby agrees to defend, indemnify and hold each Eligible Member City, its council, officers and employees free and harmless against any claims, demands, causes of action, costs and liabilities, including attorney fees resulting from the performance, or failure of DART to perform any obligation or duty in connection with this ILA.

ARTICLE EIGHT
Conflict of Interest

No City Council Member, officer, employee or agent of any Eligible Member City, and no Board Member, officer, employee or agent of DART shall have any financial interest, direct or indirect in this ILA or any Eligible Project.

ARTICLE NINE
Miscellaneous

10.01. **Notices.** Any notice, communication, invoice or report required or permitted pursuant to this ILA and the Guidelines shall be in writing and shall be effective when personally delivered or three (3) days after being mailed by United States Mail, certified, return receipt requested, to the respective parties at the address set forth for each party in the signature block of this ILA. Any party may change its address by giving notice to all other parties.

10.02. **Governing Law; Venue.** This ILA is performable in Dallas, Denton, Collin, Ellis and Rockwall Counties, Texas and exclusive venue for any legal action in connection with this ILA shall lie in Dallas County, Texas. This ILA shall be governed by and construed in accordance with the laws of the State of Texas.

10.03. **Construction.** In the event one or more of the provisions contained in this ILA shall for any reason be held to be invalid, illegal or unenforceable, such invalidity, illegality or unenforceability shall not affect any other provision hereof and this ILA shall be considered as if such invalid, illegal or unenforceable provision(s) had never been contained herein.

10.04. **Successors and Assigns.** This ILA shall be binding upon and inure to the benefit of each of the parties hereto and their respective successors and approved assigns.

10.05. **Captions.** The captions of articles and sections within this ILA are for convenience and information and shall not alter the substance of the terms and conditions of the ILA.

10.06. **Entire Agreement.** This ILA embodies the entire agreement of the parties hereto relating to the matters covered herein and cannot be modified or amended except by a written addendum or agreement of the parties. No interlocal agreement heretofore entered into between any Eligible Member City and DART shall be construed to apply to the subject matter(s) covered by this ILA and such other interlocal agreements, if any, shall continue in full force and effect in accordance with their respective terms.

EXECUTED by the parties effective as of October 1, 1996.

DALLAS AREA RAPID TRANSIT

By: _____
ROGER SNOBLE
President/Executive Director

P.O. Box 660163
Dallas, Texas 75266-7201

TOWN OF ADDISON

By: 
RON WHITEHEAD
City Manager

P.O. Box 144
Addison, Texas 75001

CITY OF CARROLLTON

By: _____
MARC GUY
City Manager

1945 Jackson Road
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4125 W. Clarendon
Dallas, Texas 75211

CITY OF FARMERS BRANCH

By: _____
RICHARD ESCALANTE
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P.O. Box 819010
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CITY OF GARLAND

By: _____
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P.O. Box 469002
Garland, Texas 75046

CITY OF GLENN HEIGHTS

By: _____
EARL KEATON
City Manager

1938 S. Hampton Road
Glenn Heights, Texas 75154

CITY OF IRVING

By: _____
STEVE McCULLOUGH
City Manager

825 W. Irving Boulevard
Irving, Texas 75060

CITY OF PLANO

By: _____
THOMAS MUEHLENBECK
City Manager

P.O. Box 860358
Plano, Texas 75086-0358

CITY OF RICHARDSON

By: _____
BILL KEFFLER
City Manager

P.O. Box 830309
Richardson, Texas 75083-0309

CITY OF ROWLETT

By: _____
MIKE GIBSON
City Manager

P.O. Box 99
Rowlett, Texas 75088-0099



PUBLIC WORKS DEPARTMENT

Post Office Box 144 Addison, Texas 75001

(214) 450-2871

16801 Westgrove

September 27, 1996

Mr. Tom Ryden
Dallas Area Rapid Transit
P.O. Box 660163
Dallas, Texas 75266-0163

Re: DART/LAP Quarterly Report - July - September 1996
Request for reimbursement

This report is intended to update the status of the DART/LAP projects and serve as a request for reimbursement.

- 1) Midway/Lindberg Intersection - Engineering for this project is 90% complete. We are in the process of reviewing plans and acquiring right-of-way.

DART Approved Project	\$ 150,000.00
Reimbursement by DART to date	\$ 10,830.00

- 2) Midway/Beltway Intersection - Engineering for this project is 90% complete. We are in the process of reviewing plans and acquiring right-of-way.

DART Approved Project	\$ 45,000.00
Reimbursement by DART to date	\$ 2,925.00

- 3) Addison/Westgrove Intersection - This project is complete. The remaining \$13,407.64 shall be reprogrammed.

DART Approved Project	\$ 125,000.00
Previous Requests:	
Engineering - EHA	\$ 8,500.00
Construction Jim Bowman Const.	\$ 96,208.23
Land Acquisition - AATI	\$ 3,180.18
Hall Financial	\$ 1,730.00
Reimbursement by DART to Date	\$ 109,618.41

New Invoices:	
Engineering - EHA	\$ 800.00
Engineering - EHA	\$ 233.95
Pat Haggerty	\$ 595.00
Pat Haggerty	\$ 85.00
Cowles & Thompson	\$ 260.00
Reimbursement this request	\$ 1,973.95

Total Project Costs	\$111,592.36
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4) Midway/Greenhill Intersection - This project is on hold.

DART Approved Project	\$ 75,000.00
Reimbursement by DART to date	\$ 4,280.00

5) Belt Line/Marsh Intersection - This project is complete. The remaining \$41,572.36 shall be reprogrammed.

Previous Requests:

DART Approved Project	\$ 800,000.00
-----------------------	---------------

Engineering - Espey Huston	\$ 19,590.00
SMR Landscaping	\$ 4,358.09
Barton-Aschman	\$ 2,836.15

Land Acquisition - Mobil	\$ 58,501.00
Mobil	\$ 5,000.00
TCB Trust	\$ 70,977.71
Watson and Taylor Realty Company	\$ 5,184.00
Actna	\$ 15,855.00
Pat Haggerty Co.	\$ 2,975.00
Cowles & Thompson	\$ 2,550.00

Construction - Ed Bell Construction	\$ 528,572.56
Valmont Industries	
(67% of \$29,778 for 4 signals poles at Belt Line and Marsh)	\$ 19,951.00
TU Electric	\$ 8,161.00
Groves Electric	\$ 2,036.03
Venice Ltd.	\$ 184.03
Hallas Landscape	\$ 755.50

Reimbursement by DART to date	\$ 747,487.07
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New Invoices:

Land - Legal Svcs, Cowles & Thompson	\$ 100.00
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Engineering - Espey Huston	\$ 2,939.01
Thomas Reprographics	\$ 48.58

Construction - Banner Express	\$ 237.60
Groves Electric	\$ 800.00
Milltronics	\$ 180.00
SEC Marsh/Belt Line	\$ 319.34
Newman Signs	\$ 329.82
Consolidated Traffic	\$ 179.90
Groves Electric	\$ 956.34
B & M Construction	\$ 950.00
Tree Source	\$ 720.00
Equipment - Durable Specialties	\$ 3150.00

Administrative - Dallas Ft. Worth	
Suburban News	\$ 172.80
Daily Commerical Record	\$ 69.68

Reimbursement this request	\$ 11,153.07
Total Project Costs	\$ 758,427.64

6) Keller Springs/Dallas North Tollway Intersection - This project is complete.

DART Approved Project	\$ 295,355.47
Engineering - Lichlighter/Jameson	\$ 35,825.00
Dowdey, Anderson & Assoc	\$ 1,730.00
Contractor - Gibson & Associates	\$ 254,818.40
City of Dallas	\$ 1,453.22
Precision Landscape Management	\$ 700.00
Dallas County	\$ 200.32
Cobb Enterprises	\$ 759.00
Reimbursement by DART to date	\$ 267,868.44
Reimbursement this request	\$ 27,617.50*
Total Project Costs	\$ 295,485.94

* Exceeds budget. Adjusted reimbursement request \$27,487.03

7) Belt Line/North Dallas Tollway Intersection - With the exception of restoration of the affected property, this project is complete. Restoration will be completed in January.

DART Approved Project	\$ 262,000.00
Previous Requests:	
Engineering Fee EIIA	\$ 13,770.00
SMR	\$ 1,932.29
Appraisal Fee	\$ 4,300.00
Land acquisition - Cowles & Thompson	\$ 120.00
Mobil	\$ 31,220.71
Trammel Crow	\$ 65,428.71
DISD Taxes	\$ 1,880.07
Dallas County Taxes	\$ 496.84
Construction - JRJ	\$ 86,296.31
Williams Landscape	\$ 11,674.75
Hencie International	\$ 37,684.32
(90% of \$41,871.47)	
Precision Landscape	\$ 975.00
Banner Express	\$ 198.72
Reimbursement by DART to Date	\$ 255,977.72
New Invoices:	
Land - Legal Svcs, Cowles & Thompson	
(4 invoices)	\$ 760.00
Consulting, Christon Co.	\$ 170.00
Pat Haggerty	\$ 127.50
Construction - Precision Landscape	\$ 56.52
Hencie Internat'l	
(10% on bal. on 1st request)	\$ 4,187.15
(Bal. on final bill not requested)	\$ 1,563.00
Reimbursement this request	\$ 6,864.17*
Total Project Costs	\$ 262,841.89

* Exceeds budget. Adjusted reimbursement request \$6,022.28

- 8) **Belt Line/Midway Intersection** - This project is a transit pass project. Engineering was started in late Spring 1994 by Barton-Aschman for DART and the environment assessment was submitted to the State for approval. We have started right-of-way acquisition.
- 9) **Belt Line/Quorum Intersection** - This project is a transit pass project. Engineering was started in late Spring 1994 by Barton Aschman for DART and the environment assessment was submitted to the State for approval. We have started right-of-way acquisition.
- 10) **Westgrove/Sojourn Intersection Improvement** - This project is complete. The remaining \$45,856.01 shall be reprogrammed.

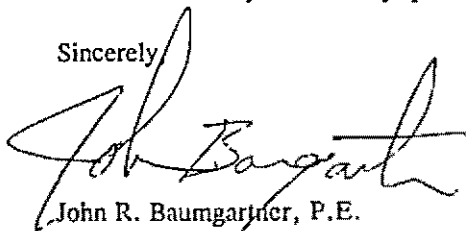
DART Approved Project	\$ 175,000.00
Previous Request:	
Engineering Fee - Barton Aschman	\$ 17,492.32
Construction - Innovated Systems Inc.	\$ 108,528.45
Reimbursement by DART to Date	\$ 126,020.77
New Invoices:	
Outdoor Reflections	\$ 350.00
Outdoor Reflections	\$ 2,540.42
Suburban Newspapers, Inc.	\$ 232.80
Reimbursement this request	\$ 3,123.22
Total Project Costs	\$ 129,143.99

- 11) **Arapaho Road Realignment/Extension** - The Town of Addison has authorized the engineer to proceed with the design of this project. Staff has started land acquisition.

DART Approved Project	\$2,903,000.00
Engineering Fee - Huitt - Zollar (90% of \$199,239)	\$ 179,315.00
Land Acquisition - Appraisal Consultation Group	\$ 5,400.00
LR Denton	\$ 9,500.00
MCM Tract	\$ 553,492.70
Total Project Costs	\$ 747,707.70
Reimbursement by DART to Date	\$ 747,707.70

Please call me if you have any questions or need additional information regarding these projects.

Sincerely



John R. Baumgartner, P.E.
Director of Public Works

JRB/amh

cc: Randy Moravec

Page Five
September 27, 1996
Mr. Tom Ryden

Attachments:

Addison/Westgrove Intersection: Espey-Huston, 2 invoices
Pat Haggerty, 2 invoices
Cowles & Thompson

Belt Line Marsh Intersection: Cowles & Thompson
Espey-Huston
Thomas Reprographics
Banner Express
Groves Electric
Milltronic
SEC Marsh/Belt Line
Newman Signs
Consolidated Traffic
Groves Electric
B & M Construction
Tree Source
Durable Specialties
Dallas Ft. Worth Suburban News
Daily Commercial Record

Keller Springs/Dallas North Tollway Intersection:
Lichtliter/Jameson, 2 invoices
Dowdey, Anderson & Assoc
Gibson & Associates
City of Dallas
Precision Landscape Management
Dallas County
Cobb Enterprises

Belt Line/North Dallas Tollway Intersection:
Cowles & Thompson, 4 invoices
Pat Haggerty, 2 invoices
Precision Landscape
Hencie International

**DART/LAP REIMBURSEMENT
REQUEST NUMBER 8
SEPTEMBER 27, 1996**

Addison/Westgrove Intersection	\$ 1,973.95
Belt Line/Marsh Intersection	\$11,153.07
Keller Springs/North Dallas Tollway Intersection	\$27,487.03
Belt Line/North Dallas Tollway Intersection	\$ 6,022.28
Westgrove/Sojourn Intersection	\$ 3,123.22
Total Reimbursement Requested	\$49,759.55



Dallas Area Rapid Transit
P.O. Box 660163
Dallas, Texas 75266-0163
214/749-3278

DATE: September 24, 1996

FROM: Tom K. Ryden

TO: Larry Cervenka, City of Garland; Dave Davis, City of Farmers Branch;
Jim Driscoll, City of Irving; Marcos Fernandez, City of Plano; George Human,
City of Richardson; John Godwin, City of Rowlett; Earl Keaton, City of Glenn
Heights; Sally McGlaun, City of Cockrell Hill; Dave Nighswonger, Town of
Addison; Cissy Sylo, City of Carrollton;

SUBJECT: LAP/CMS Project Application Form

Enclosed is a two-sided copy of a proposed project application form for the LAP/CMS Program as well as a copy of the current LAP application. Until the old LAP funds are programmed, please use the old form to apply for new or reprogrammed LAP projects. Once accepted, the new LAP/CMS form will be used for the new program.

The LAP/CMS form is similar to the one prepared by Marcos during the LAP/CMS meetings. We kept the Funding and Cost Distribution sections because they can provide useful details to the DART Board when considering projects. Also, the back side lists the eligible uses of the funds as stated in the Guidelines. The Project Description section should be large enough to explain the project, fund transfers/reprogramming, etc.

If you have suggestions on the new form, please call me at (214) 749-2913 or Mike Morgan at 214/749-2841 by Thursday, October 3, 1996.

A handwritten signature in black ink that reads "Tom K. Ryden". The signature is written in a cursive style and is positioned above a horizontal line.

Tom K. Ryden, P.E.
Sr. Manager Project Development

c: Mike Morgan

Attachment



Dallas Area Rapid Transit
P.O. Box 660163
Dallas, Texas 75266-0163
214/749-3278

DATE: September 20, 1996

FROM: Tom K. Ryden

TO: Larry Cervenka, City of Garland; Dave Davis, City of Farmers Branch;
Jim Driscoll, City of Irving; Marcos Fernandez, City of Plano; George Human,
City of Richardson; John Godwin, City of Rowlett; Earl Keaton, City of Glenn
Heights; Sally McGlaun, City of Cockrell Hill; Dave Nighswonger, Town of
Addison; Cissy Sylo, City of Carrollton;

SUBJECT: LAP/CMS Interlocal Agreement Modification

Since distribution of the Draft Interlocal Agreement (ILA) for the LAP/CMS program in early September, a clarification to the document has been requested. Article Five has been expanded to explain that LAP/CMS allocations may change from the future projections in Table 1 of the Guidelines, based on sales tax collections. Also, the new Article Five reinforces the point that program duration is a function of the rail construction schedule. The City LAP/CMS allocation end dates shown in Table 1 of the Guidelines are targets based on today's rail construction schedule. The last year of program allocations remains 2004.

This addition to the Interlocal Agreement is not a change in Board policy. It is being added for clarification purposes. The modified ILA document is being provided along with a signature page to allow all eligible cities to execute the same agreement.

A handwritten signature in black ink that reads 'Tom K. Ryden'.

Tom K. Ryden, P.E.
Sr. Manager Project Development

c: Paul Jones

INTERLOCAL AGREEMENT

BETWEEN

DALLAS AREA RAPID TRANSIT

AND

**THE TOWN OF ADDISON, THE CITY OF CARROLLTON,
THE CITY OF COCKRELL HILL, THE CITY OF FARMERS BRANCH,
THE CITY OF GARLAND, THE CITY OF GLENN HEIGHTS,
THE CITY OF IRVING, THE CITY OF PLANO,
THE CITY OF RICHARDSON and THE CITY OF ROWLETT**

**ESTABLISHING GUIDELINES AND PROCEDURES FOR IMPLEMENTING A
LOCAL ASSISTANCE PROGRAM/CONGESTION MANAGEMENT SYSTEM PROGRAM**

WHEREAS, on or about April 11, 1989 and June 27, 1989, DALLAS AREA RAPID TRANSIT ("DART") established a program for the return of a portion of DART's sales tax income to its member cities known and identified as the "Local Assistance Program" ("Lap Program"); and,

WHEREAS, the Lap Program was continued in full force and effect until November 14, 1995 at which time the DART Board, in Resolution No. 950260 adopted a new Transit System Plan pursuant to which the Lap Program would terminate on June 30, 1996; and,

WHEREAS, on June 25, 1996, in Resolution No. 960117 the DART Board extended certain LAP ILA's to the end of Fiscal Year 1996, being September 30, 1996; and,

WHEREAS, the DART Board created a new program for returning a portion of DART sales tax income to certain member cities, the "Local Assistance Program/Congestion Management System" ("LAP/CMS Program") effective on October 1, 1996, the first day of the 1997 fiscal year; and,

WHEREAS, on August 27, 1996, in Resolution No. 960153, the DART Board adopted a LAP/CMS Policy, including Guidelines for the LAP/CMS Program; and,

WHEREAS, eligibility for the LAP/CMS Program is limited to those member cities in which construction of light rail service has not yet commenced; and,

WHEREAS the parties desire to implement the LAP/CMS Program and to establish therefor guidelines and procedures for the creation, construction, completion, operation, maintenance and funding of projects; NOW THEREFORE,

WITNESSETH:

In consideration of the premises and the mutual promises and covenants contained herein, the parties agree as follows:

**ARTICLE ONE
Parties**

The parties to this Agreement are:

1.01. Dallas Area Rapid Transit ("DART"), a regional transportation authority organized pursuant to and in accordance with Chapter 452, Texas Transportation Code.

1.02. The Town of Addison; The City of Carrollton; The City of Cockrell Hill; The City of Farmers Branch; The City of Garland; The City of Glenn Heights; The City of Irving; The City of Plano; The City of Richardson; and The City of Rowlett.

ARTICLE TWO Definitions

The following definitions shall apply to the terms used in this ILA, including without limitation the Guidelines:

2.01. "Allocations" shall refer to LAP/CMS Program fund allocations as determined in accordance with Section C of the Guidelines. The Allocation for the City of Irving shall be one half (1/2) that of other Eligible Member Cities Allocations because of the current work on Commuter Rail facilities in the City of Irving.

2.02. "Board" shall mean and refer to the Board of Directors of DART.

2.03. "City Council" shall mean and refer to the duly elected and acting City Council of each Eligible Member City.

2.04. "Commuter Rail" shall mean and refer to the self propelled diesel rail transit operations planned to connect the City of Dallas, the City of Fort Worth and DFW International Airport to be operated by DART and the "T" pursuant to a separate Interlocal Agreement with Railtran, on right-of-way owned by Railtran. Commuter Rail service is to operate under the assumed name of "Trinity Express."

2.05. "DART Enabling Legislation" means Chapter 452, Texas Transportation Code as the same may from time to time be amended.

2.06. "Eligible Member City (City/Cities)" shall mean and refer to the Cities enumerated in Article 1.02 of this ILA.

2.07. "Eligible Projects" shall mean and include those listed in Section D of the Guidelines and any other project recommended by an Eligible Member City and specifically considered and approved by the Board.

2.08. "FTA" shall mean and refer to the Federal Transit Administration.

2.09. "Guidelines" shall mean and refer to the Guidelines for the LAP/CMS Program adopted by the Board on August 27, 1996 in Resolution No. 960153.

2.10. "ILA" shall mean and refer to this Interlocal Agreement.

2.11. "Light Rail" shall mean and refer to the DART electric powered rail system as distinguished from Commuter Rail.

2.12. "Railtran" shall mean and refer to DFW RAILTRAN a common administrative agency established by the Cities of Dallas and Fort Worth.

2.13. The "T" shall mean and refer to the Fort Worth Transportation Authority.

ARTICLE THREE
Incorporation of Guidelines

The Guidelines are hereby incorporated into this ILA as if they were set out verbatim at this point. A set of Guidelines is attached to each copy of this ILA.

ARTICLE FOUR
Purposes of ILA

4.01. The initial purpose of this ILA is to set forth the Eligible Member Cities' acceptance of and commitment to DART, and DART's commitment to the Eligible Member Cities, to jointly participate in and work together for the satisfactory implementation of the LAP/CMS Program.

4.02. By executing this ILA, each Eligible Member City accepts and approves the Guidelines as the basis for implementing the LAP/CMS Program.

4.03. It is the further purpose and intent of DART and the Eligible Member Cities to continue to explore other potential options which would permit continued and expanded revenue sharing with all of DART's member cities to meet their respective transportation requirements so long as such options do not adversely impact the DART Transit System Plan or the DART Service Plan or the DART Financial Plan.

ARTICLE FIVE
Term

The term of this ILA shall commence as of the first day of October, 1996 and shall terminate for each Eligible Member City as provided in §C. ALLOCATIONS of the Guidelines. Though terminated as to a specific Eligible Member City, the ILA shall remain in full force and effect until all Allocations for the affected Eligible Member City are finally funded and the activities of such Eligible Member City are wound up. It is understood by all of the parties hereto that the Allocations and program duration's shown in Table I of the Guidelines are projections only and that actual Allocations will be based on actual sales tax collections and actual program duration will be determined by the schedule of rail construction. If a contract for rail construction in a Member City is not awarded by the year shown in said Table I, Allocations will continue through the year in which a rail construction contract is awarded, or through 2004, when all program Allocations shall end.

ARTICLE SIX
Administration and Procurement of Eligible Projects

It shall be the obligation of each Eligible Member City receiving funds under the LAP/CMS Program to supervise and administer the planning, design, land acquisition, construction, operation and maintenance of all Eligible Projects. Receipt of funds under the LAP/CMS Program shall not create an agency, partnership or joint venture relationship between DART and the Eligible Member City. Each Eligible Member City shall be responsible for handling all procurements needed for each Eligible Project in accordance with all applicable federal, state and local laws, codes, ordinances, rules and regulations of any governmental entity exercising jurisdiction over such Eligible Project, and the resolution of any protest, dispute, claim or demand arising out of any such action by the Eligible Member City shall be the sole responsibility of the Eligible Member City.

ARTICLE SEVEN
Indemnity

To the extent permitted by applicable law, each Eligible Member City agrees to defend, indemnify and hold DART, its directors, officers and employees free and harmless against any claims, demands, causes of action, costs and liabilities, including attorney fees, resulting from the performance, or failure of the Eligible Member City to perform any obligation or duty in connection with this ILA. Likewise, to the extent permitted by applicable law, DART hereby agrees to defend, indemnify and hold each Eligible Member City, its council, officers and employees free and harmless against any claims, demands, causes of action, costs and liabilities, including attorney fees resulting from the performance, or failure of DART to perform any obligation or duty in connection with this ILA.

**ARTICLE EIGHT
Conflict of Interest**

No City Council Member, officer, employee or agent of any Eligible Member City, and no Board Member, officer, employee or agent of DART shall have any financial interest, direct or indirect in this ILA or any Eligible Project.

**ARTICLE NINE
Miscellaneous**

10.01. **Notices.** Any notice, communication, invoice or report required or permitted pursuant to this ILA and the Guidelines shall be in writing and shall be effective when personally delivered or three (3) days after being mailed by United States Mail, certified, return receipt requested, to the respective parties at the address set forth for each party in the signature block of this ILA. Any party may change its address by giving notice to all other parties.

10.02. **Governing Law; Venue.** This ILA is performable in Dallas, Denton, Collin, Ellis and Rockwall Counties, Texas and exclusive venue for any legal action in connection with this ILA shall lie in Dallas County, Texas. This ILA shall be governed by and construed in accordance with the laws of the State of Texas.

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EXECUTED by the parties effective as of October 1, 1996.

DALLAS AREA RAPID TRANSIT

By: _____
ROGER SNOBLE
President/Executive Director

P.O. Box 660163
Dallas, Texas 75266-7201

TOWN OF ADDISON

By: _____
RON WHITEHEAD
City Manager

P.O. Box 144
Addison, Texas 75001

CITY OF CARROLLTON

By: _____
MARC GUY
City Manager

1945 Jackson Road
Carrollton, Texas 75011-0535

CITY OF COCKRELL HILL

By: _____
LEO TREVINO LANDIN
Mayor

4125 W. Clarendon
Dallas, Texas 75211

CITY OF FARMERS BRANCH

By: _____
RICHARD ESCALANTE
City Manager

P.O. Box 819010
Farmers Branch, Texas 75381-9010

CITY OF GARLAND

By: _____
JEFF MUZZY
City Manager

P.O. Box 469002
Garland, Texas 75046

CITY OF GLENN HEIGHTS

By: _____
EARL KEATON
City Manager

1938 S. Hampton Road
Glenn Heights, Texas 75154

CITY OF IRVING

By: _____
STEVE McCULLOUGH
City Manager

825 W. Irving Boulevard
Irving, Texas 75060

CITY OF PLANO

By: _____
THOMAS MUEHLENBECK
City Manager

P.O. Box 860358
Plano, Texas 75086-0358

CITY OF RICHARDSON

By: _____
BILL KEFFLER
City Manager

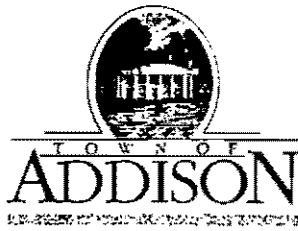
P.O. Box 830309
Richardson, Texas 75083-0309

CITY OF ROWLETT

By: _____
MIKE GIBSON
City Manager

P.O. Box 99
Rowlett, Texas 75088-0099

COPY



PUBLIC WORKS DEPARTMENT

Post Office Box 144 Addison, Texas 75001

(214) 450-2871

16801 Westgrove

September 16, 1996

MEMORANDUM

TO: Ron Whitehead, City Manager

FROM: John Baumgartner, Director of Public Works *JB*

SUBJECT: Interlocal Agreement for DART LAP/CMS Program

Attached is the Interlocal Agreement (ILA) between DART and the suburban cities for the new Local Assistance Program/Congestion Management System (LAP/CMS) program. The program guidelines are included with the ILA. The guidelines were negotiated between the staffs of the suburban cities and DART. The guidelines were approved by the DART board on August 27, 1996. Each suburban city must approve the ILA and guidelines for the program to begin.

The new LAP/CMS program is basically a continuation of the current LAP program, which ends on September 30, 1996. Some of the key points of the LAP/CMS program are as follows:

- The program provides that each eligible suburban city will receive an allocation of 15% of the DART sales taxes collected in the eligible city from FY 1997 through FY 2004. Addison is projected to receive approximately \$1,136,000 in FY 1997.
- The categories of eligible projects has been expanded to include projects such as the Arapaho Road Extension; projects that may not have bus service on them, but are significant to mobility and also provide benefit to transit.
- Technical Assistance funding remains at 3% of the city's annual program allocation.
- All unused existing LAP funds will be administered under the new LAP/CMS program. Interest will accrue on the unpaid LAP balance through September 30, 1997, at a fixed rate of 4%. No interest will accrue on the new LAP/CMS program allocations.
- DART staff will approve projects \$100,000 or less. DART Board must approve all projects over \$100,000.

- DART staff is authorized, without further Board approval, to pay at project closeout an amount up to 10% higher than the approved project amount.
- On projects of \$250,000 or less, DART will pay the City 100% of the project cost upon approval of the project by the City Council. On projects more than \$250,000, DART will pay the City 20% of the project cost upon approval of the project by the City Council and the remaining 80% of the cost upon Notice to Proceed. Upon project completion, the City shall return to DART any unspent funds, which will be placed in the Cities' unprogrammed funds account.

Overall, the new guidelines for the LAP/CMS should streamline the process for project approval and the transfer of funds. An important aspect of the new program is that the Town will be able to use its allocation to fund the Arapaho Road Extension project. Issues such as the percentage of the DART sales tax the cities would receive as their allocation and no accrued interest on the new allocations were previously agreed to by the DART Board and suburban Mayors and Councils. These items were not available for negotiation by the staffs.

Staff recommends that the City Council approve the Interlocal Agreement and guidelines for the LAP/CMS Program with DART.

INTERLOCAL AGREEMENT

BETWEEN

DALLAS AREA RAPID TRANSIT

AND

**THE TOWN OF ADDISON, THE CITY OF CARROLLTON,
THE CITY OF COCKRELL HILL, THE CITY OF FARMERS BRANCH,
THE CITY OF GARLAND, THE CITY OF GLENN HEIGHTS,
THE CITY OF IRVING, THE CITY OF PLANO,
THE CITY OF RICHARDSON and THE CITY OF ROWLETT**

**ESTABLISHING GUIDELINES AND PROCEDURES FOR IMPLEMENTING A
LOCAL ASSISTANCE PROGRAM/CONGESTION MANAGEMENT SYSTEM PROGRAM**

WHEREAS, on or about April 11, 1989 and June 27, 1989, DALLAS AREA RAPID TRANSIT ("DART") established a program for the return of a portion of DART's sales tax income to its member cities known and identified as the "Local Assistance Program" ("Lap Program"); and,

WHEREAS, the Lap Program was continued in full force and effect until November 14, 1995 at which time the DART Board, in Resolution No. 950260 adopted a new Transit System Plan pursuant to which the Lap Program would terminate on June 30, 1996; and,

WHEREAS, on June 25, 1996, in Resolution No. 960117 the DART Board extended certain LAP ILA's to the end of Fiscal Year 1996, being September 30, 1996; and,

WHEREAS, the DART Board created a new program for returning a portion of DART sales tax income to certain member cities, the "Local Assistance Program/Congestion Management System" ("LAP/CMS Program") effective on October 1, 1996, the first day of the 1997 fiscal year; and,

WHEREAS, on August 27, 1996, in Resolution No. 960153, the DART Board adopted a LAP/CMS Policy, including Guidelines for the LAP/CMS Program; and,

WHEREAS, eligibility for the LAP/CMS Program is limited to those member cities in which construction of light rail service has not yet commenced; and,

WHEREAS the parties desire to implement the LAP/CMS Program and to establish therefor guidelines and procedures for the creation, construction, completion, operation, maintenance and funding of projects; NOW THEREFORE,

WITNESSETH:

In consideration of the premises and the mutual promises and covenants contained herein, the parties agree as follows:

**ARTICLE ONE
Parties**

The parties to this Agreement are:

1.01. Dallas Area Rapid Transit ("DART"), a regional transportation authority organized pursuant to and in accordance with Chapter 452, Texas Transportation Code.

1.02. The Town of Addison; The City of Carrollton; The City of Cockrell Hill; The City of Farmers Branch; The City of Garland; The City of Glenn Heights; The City of Irving; The City of Plano; The City of Richardson; and The City of Rowlett.

ARTICLE TWO Definitions

The following definitions shall apply to the terms used in this ILA, including without limitation the Guidelines:

2.01. "Allocations" shall refer to LAP/CMS Program fund allocations as determined in accordance with Section C of the Guidelines. The Allocation for the City of Irving shall be one half (1/2) that of other Eligible Member Cities Allocations because of the current work on Commuter Rail facilities in the City of Irving.

2.02. "Board" shall mean and refer to the Board of Directors of DART.

2.03. "City Council" shall mean and refer to the duly elected and acting City Council of each Eligible Member City.

2.04. "Commuter Rail" shall mean and refer to the self propelled diesel rail transit operations planned to connect the City of Dallas, the City of Fort Worth and DFW International Airport to be operated by DART and the "T" pursuant to a separate Interlocal Agreement with Railtran, on right-of-way owned by Railtran. Commuter Rail service is to operate under the assumed name of "Trinity Express."

2.05. "DART Enabling Legislation" means Chapter 452, Texas Transportation Code as the same may from time to time be amended.

2.06. "Eligible Member City (City/Cities) shall mean and refer to the Cities enumerated in Article 1.02 of this ILA.

2.07. "Eligible Projects" shall mean and include those listed in Section D of the Guidelines and any other project recommended by an Eligible Member City and specifically considered and approved by the Board.

2.08. "FTA" shall mean and refer to the Federal Transit Administration.

2.09. "Guidelines" shall mean and refer to the Guidelines for the LAP/CMS Program adopted by the Board on August 27, 1996 in Resolution No. 960153.

2.10. "ILA" shall mean and refer to this Interlocal Agreement.

2.11. "Light Rail" shall mean and refer to the DART electric powered rail system as distinguished from Commuter Rail.

2.12 "Railtran" shall mean and refer to DFW RAILTRAN a common administrative agency established by the Cities of Dallas and Fort Worth.

2.13. The "T" shall mean and refer to the Fort Worth Transportation Authority.

ARTICLE THREE
Incorporation of Guidelines

The Guidelines are hereby incorporated into this ILA as if they were set out verbatim at this point. A set of Guidelines is attached to each copy of this ILA.

ARTICLE FOUR
Purposes of ILA

4.01. The initial purpose of this ILA is to set forth the Eligible Member Cities' acceptance of and commitment to DART, and DART's commitment to the Eligible Member Cities, to jointly participate in and work together for the satisfactory implementation of the LAP/CMS Program.

4.02. By executing this ILA, each Eligible Member City accepts and approves the Guidelines as the basis for implementing the LAP/CMS Program.

4.03. It is the further purpose and intent of DART and the Eligible Member Cities to continue to explore other potential options which would permit continued and expanded revenue sharing with all of DART's member cities to meet their respective transportation requirements so long as such options do not adversely impact the DART Transit System Plan or the DART Service Plan or the DART Financial Plan.

ARTICLE FIVE
Term

The term of this ILA shall commence as of the first day of October, 1996 and shall terminate for each Eligible Member City as provided in §C. ALLOCATIONS of the Guidelines. Though terminated as to a specific Eligible Member City, the ILA shall remain in full force and effect until all Allocations for the affected Eligible Member City are finally funded and the activities of such Eligible Member City are wound up .

ARTICLE SIX
Administration and Procurement of Eligible Projects

It shall be the obligation of each Eligible Member City receiving funds under the LAP/CMS Program to supervise and administer the planning, design, land acquisition, construction, operation and maintenance of all Eligible Projects. Receipt of funds under the LAP/CMS Program shall not create an agency, partnership or joint venture relationship between DART and the Eligible Member City. Each Eligible Member City shall be responsible for handling all procurements needed for each Eligible Project in accordance with all applicable federal, state and local laws, codes, ordinances, rules and regulations of any governmental entity exercising jurisdiction over such Eligible Project, and the resolution of any protest, dispute, claim or demand arising out of any such action by the Eligible Member City shall be the sole responsibility of the Eligible Member City.

ARTICLE SEVEN
Indemnity

To the extent permitted by applicable law, each Eligible Member City agrees to defend, indemnify and hold DART, its directors, officers and employees free and harmless against any claims, demands, causes of action, costs and liabilities, including attorney fees, resulting from the performance, or failure of the Eligible Member City to perform any obligation or duty in connection with this ILA. Likewise, to the extent permitted by applicable law, DART hereby agrees to defend, indemnify and hold each Eligible Member City, its council, officers and employees

free and harmless against any claims, demands, causes of action, costs and liabilities, including attorney fees resulting from the performance, or failure of DART to perform any obligation or duty in connection with this ILA.

ARTICLE EIGHT
Conflict of Interest

No City Council Member, officer, employee or agent of any Eligible Member City, and no Board Member, officer, employee or agent of DART shall have any financial interest, direct or indirect in this ILA or any Eligible Project.

ARTICLE NINE
Miscellaneous

10.01. **Notices.** Any notice, communication, invoice or report required or permitted pursuant to this ILA and the Guidelines shall be in writing and shall be effective when personally delivered or three (3) days after being mailed by United States Mail, certified, return receipt requested, to the respective parties at the address set forth for each party in the signature block of this ILA. Any party may change its address by giving notice to all other parties.

10.02. **Governing Law; Venue.** This ILA is performable in Dallas, Denton, Collin, Ellis and Rockwall Counties, Texas and exclusive venue for any legal action in connection with this ILA shall lie in Dallas County, Texas. This ILA shall be governed by and construed in accordance with the laws of the State of Texas.

10.03. **Construction.** In the event one or more of the provisions contained in this ILA shall for any reason be held to be invalid, illegal or unenforceable, such invalidity, illegality or unenforceability shall not affect any other provision hereof and this ILA shall be considered as if such invalid, illegal or unenforceable provision(s) had never been contained herein.

10.04. **Successors and Assigns.** This ILA shall be binding upon and inure to the benefit of each of the parties hereto and their respective successors and approved assigns.

10.05. **Captions.** The captions of articles and sections within this ILA are for convenience and information and shall not alter the substance of the terms and conditions of the ILA.

10.06. **Entire Agreement.** This ILA embodies the entire agreement of the parties hereto relating to the matters covered herein and cannot be modified or amended except by a written addendum or agreement of the parties. No interlocal agreement heretofore entered into between any Eligible Member City and DART shall be construed to apply to the subject matter(s) covered by this ILA and such other interlocal agreements, if any, shall continue in full force and effect in accordance with their respective terms.

EXECUTED by the parties effective as of October 1, 1996.

DALLAS AREA RAPID TRANSIT

By: _____
ROGER SNOBLE
President/Executive Director

P.O. Box 660163
Dallas, Texas 75266-7201

signature block and address for each Eligible Member City

**GUIDELINES FOR THE LAP/CMS
PROGRAM
August 27, 1996**

A. OBJECTIVE:

Provide continued financial assistance for projects consistent with these guidelines and DART's enabling legislation that enhance mobility in DART member cities that do not yet have rail construction in progress.

B. ELIGIBLE MEMBER CITIES:

The following cities are eligible to receive LAP/CMS funds as of October 1, 1996:

Addison, Carrollton, Cockrell Hill, Farmers Branch, Garland, Glenn Heights, Irving, Plano, Richardson, Rowlett.

C. ALLOCATIONS:

The LAP/CMS fund allocations will be made available for use by an eligible member city annually from FY 1997 through FY 2004 when the program allocations end. The annual allocation will be calculated by multiplying by 15% (7.5% for Irving) the DART sales taxes collected in an eligible member city for the previous twelve month period ending June 30. Each new annual allocation will be available for expenditure on October 1 (beginning in 1996).

The FY 1996 Financial Plan includes approximately \$87,000,000 for this program. Table 1 shows the allocations by DART member city, based on the 1996 projections and anticipated program duration. The time schedule in Table 1 is subject to change during DART's annual budget cycle. A city will no longer be eligible to receive program funds in the fiscal year after the Board awards a contract to initiate rail construction within that member city. In no case shall the DART Board award such a contract more than six months prior to construction. The budget for LAP/CMS will be revisited based on agency requirements to meet its obligations.

D. PROJECT ELIGIBILITY:

Eligible types of projects are shown below. In general, eligible projects are divided into two categories: 1) Transit related street improvements and 2) Transit supportive projects and programs.

Typically, funds approved for capital projects may be used for planning, environmental impact studies, engineering, final design, right-of-way acquisition, construction, and testing/inspection/surveying. Maintenance and operations may also be funded if approved by the DART Board in accordance with Section E of the guidelines.

Any project not explicitly listed but considered allowable under DART's enabling legislation may be proposed for DART consideration. Additional eligible projects include:

Transit related street improvements (streets with current or planned/previous bus routes).

1. Roadway improvements that add capacity
 - Road widening
 - Adding storage to turn lanes
 - Adding turn lanes
 - Adding/deleting access points
 - Adding bus priority lanes
 - HOV lanes
2. Grade separations
3. Matching funds for federal/state programs
4. Intersection improvements
 - New signals
 - Computerization of signals
 - Traffic surveillance equipment
 - Telecommunications
5. Cooperative funding of intercity capacity improvements
 - Signal progression
 - Bus only lanes
 - Bus signal preemption
 - Arterial street HOV lanes
 - Roadway and intersection improvements
6. Special design provisions along roadways and intersections with planned bus routes
 - Shelters
 - Sidewalks
 - Bus stops
 - Bus turnouts
 - Bus priority treatments
7. Roadway and intersection safety improvements

Transit supportive projects and programs

8. Additional service including non-productive service (previously eliminated or not meeting warrants)
9. Transportation services for the elderly and/or disabled
 - Emergency medical
 - Shuttle services

10. ADA compliance items related to the provision of transit service
11. Sidewalks related to providing access to transit facilities or services
12. Bus shelters
13. Bus benches
14. Travel demand management
 - Transportation Management Association operating funds
 - User subsidies for transit service
 - Vanpool programs
 - Rideshare promotion and incentives
 - Alternate work schedule subsidies
 - Employer trip reduction support
 - Emergency ride home funding taxi/loaner car
15. Vans for vanpool programs
16. Park and Ride lots
17. Mid-block bus turnouts
18. Incorporating private transit services/facilities into DART
19. Transportation enhancements at existing and planned DART transit centers, transfer centers and rail stations.
20. DART and Railtran related railroad crossing and right of way improvements to include street closings or grade separations.
21. Technical Assistance Program (TAP) funds may include, but are not limited to:
 - Transit planning, feasibility, design and engineering studies
 - Salaries for new or existing city staff time specifically allocated for transportation planning and/or studies, or interface with the DART staff on activities affecting the subject city
 - Technical Assistance seminars
 - Study and implementation of transit-supportive land use planning policies and other policies that benefit public transit
 - Minor capital support for technical staff
22. Other projects not meeting the test of past, present, or future bus service, but which are of significance to mobility as long as benefits to transit can be demonstrated. For example: A project not located on a DART fixed transit route that relieves traffic congestion on a parallel or intersecting DART route could be eligible for LAP/CMS funding.

E. PROJECT MAINTENANCE AND OPERATION:

Project maintenance is the responsibility of the project owner. If shared maintenance responsibility for a project is desired, the cost sharing arrangements must be approved by the DART Board and the eligible city as part of the project approval process. Operating costs of LAP/CMS projects may be eligible for LAP/CMS funding subject to DART Board approval.

F. PROJECT SUBMITTALS AND REPORTING:

Cities will prepare LAP/CMS applications for consideration and approval by DART. DART staff will have 30 days to review and respond to the applications. Once approved, the funding amount in the application becomes "programmed". Left over funding returned to DART after project closing becomes "unprogrammed". A city may simultaneously return and "reprogram" left over funds by submitting a project application for approval along with the left over monies.

DART will prepare, by September 1 of each year, a projected LAP/CMS allocation for each city for budget purposes consistent with the DART Financial Plan in effect at that time. DART will report quarterly (within 30 days after close of the quarter) on the status of the LAP/CMS fund including amounts allocated, amounts programmed, amounts expended, amounts available for reprogramming from completed projects, and a total amount available for programming for each eligible city.

Eligible cities will provide to DART notice of requests for LAP/CMS payment (at least 90 days in advance of a request for LAP/CMS payment) for any amount exceeding \$250,000.

Eligible cities will maintain accurate books and records of all approved projects. Cities will maintain separate records on each project including project budgets, budget amendments, revised budget balances, expenditures to date, estimated cost to complete, and DART payments to date. DART maintains the right to audit a city's books to ensure that funds are spent in accordance with the approved workplan and DART's enabling legislation. Cities maintain the right to audit DART's books to ensure that the fund allocations and disbursements are in accordance with these guidelines and other applicable laws.

Eligible cities will provide an Annual Cash Flow Projection report (See Table 2) for all approved projects by June 30 of each year beginning in 1997. DART may request updated cash flow projections from time to time to monitor its projected cash outflows.

G. CASH PAYMENTS:

DART will prefund Technical Assistance Program (TAP) costs on an annual basis after submittal of an annual work plan by the eligible member city and approval by DART. Technical Assistance funding will be 3% of an eligible city's annual program allocation.

DART will distribute LAP/CMS funds to the city for a City Council approved project according to the following schedule:

Project cost of \$250,000 or less

100% of the cost upon approval by the City Council

DART will pay within thirty (30) days of receipt of a request for payment with supporting documentation.

Project cost of over \$250,000

20% of the cost upon approval by the City Council.

80% of the cost upon Notice of Award by the city.

Provided that written notice is received from the city at least 90 days in advance, DART will pay within thirty (30) days of receipt of a request for payment over \$250,000 with supporting documentation.

Eligible cities will provide to DART a completion report for each project documenting final acceptance of the project and final project cost. Project documentation will consist of a statement of project expenditures attested by an authorized financial staff member, copies of paid invoices, or other similar documents.

DART staff is authorized without further Board approval to pay at project closeout an amount up to 10% higher than the approved project amount for properly documented eligible project costs. At project completion, cities will return any LAP/CMS funds distributed but unspent. These funds will be available for reprogramming.

Any unused funds allocated to an eligible city in any year will continue to be available to that city in succeeding years unless the city withdraws from DART. Interest will not be paid on allocated, but unpaid LAP/CMS balances.

H. OTHER FINANCIAL CONSIDERATIONS:

Since the LAP/CMS program allows for funding of operating and capital transit projects, member cities will be eligible to apply for and receive FTA funds with prior approval by the DART Board. Plano will continue to be eligible to receive funds for projects begun prior to FY 1989.

Member cities may pool their LAP/CMS allocations for approval and implementation of eligible projects.

Advanced (beyond 1 year) programming of funds is allowed subject to Board approval; however, cities may not program, in any future year, more than the current year's LAP/CMS allocation.

Board approval will be required for projects over \$100,000. DART staff will approve projects requiring LAP/CMS funding in amounts of \$100,000 or less. A city may appeal to the Board any DART staff decision regarding project approval.

Starting October 1, 1997, all unused existing LAP funds will be administered as part of the new LAP/CMS program. Interest will accrue on the unpaid LAP balance through September 30, 1997, at a fixed rate of 4%.

All DART payments for LAP or LAP/CMS projects will be drawn from the eligible city's existing LAP fund balance until it is exhausted. After all LAP funds allocated are disbursed, further payments will be drawn from that city's LAP/CMS allocation.

I. EXPANDED REVENUE SHARING

In accordance with Section 5 of DART Board Resolution No. 950260 of November 14, 1995, it is DART's intent to continue to explore options for expanded revenue sharing with member cities to meet the region's transportation needs. These guidelines do not reflect the process to be used to provide expanded revenue sharing.

J. WITHDRAWAL OF MEMBER CITIES

If a city withdraws from DART, no additional LAP/CMS funding will be allocated to that city. In the event of a withdrawal, only individual projects for which construction contracts have been executed or for which formal funding agreements have been executed will be funded by DART and allowed to proceed to completion, and the funds for completing these projects will be included as that city's specific obligation if that city withdraws from DART. All other funds revert to DART. Once a city calls a withdrawal election, no new LAP/CMS projects will be considered for approval until after the election is canvassed.

K. PROJECT IDENTIFICATION

Each construction project will display at least one sign on each approach to the construction site indicating funding for that project is provided through the DART LAP/CMS program. In addition, after implementation, if a project shall have a visible permanent marker, it shall indicate that funding for the project was provided through the DART LAP/CMS program.

Table 1

**LAP/CMS PROJECTIONS
COMPARISON BY MEMBER CITY (1)**

(In Thousands of Dollars)

15% LAP TO CONSTRUCTION				
Projected				
CITY	FY 1997 Allocation	Total Program Cost	Fiscal Year for Cities	
Addison	\$ 1,136	\$11,599	2004	
Carrollton	1,908	17,313	2003	
Cockrell Hill	6	83	2004	
Dallas	0	0	na	
Farmers Branch	1,518	12,522	2003	
Garland	1,975	4,127	1998	
Glenn Heights	8	89	2004	
Highland Park	0	0	na	
Irving (2)	2,149	21,878	2004	
Plano	4,167	12,908	1999	
Richardson	2,768	4,971	1998	
Rowlett	188	1,835	2004	
University Park	0	0	na	
Total	\$15,823	\$87,324	2004	

1 Assumes program allocations end in FY 2004 for all cities. Allocations based on sales tax collections from July 1, 1995 through June 30, 1996.

2 Irving projected to receive 50% of the allocation of other cities for LAP/CMS.

ARTICLE FIVE
Term

The term of this ILA shall commence as of the first day of October, 1996 and shall terminate for each Eligible Member City as provided in C. ALLOCATIONS of the Guidelines. Though terminated as to a specific Eligible Member City, the ILA shall remain in full force and effect until all Allocations for the affected Eligible Member City are finally funded and the activities of such Eligible Member City are wound up.

The allocation amounts and program durations included in Table 1 of the Guidelines are projections. Actual allocations will be based on sales tax collections. Actual program duration will be determined by the schedule of rail construction. If a contract for rail construction in a city is not awarded by the year shown in Table 1, allocations will continue through the year in which a rail construction contract is awarded or through 2004 when all program allocations end.



Dallas Area Rapid Transit
P.O. Box 660163
Dallas, Texas 75266-0163
214/749-3278

DATE: September 4, 1996

FROM: Tom K. Ryden

TO: Larry Cervenka, City of Garland; Dave Davis, City of Farmers Branch;
Jim Driscoll, City of Irving; Marcos Fernandez, City of Plano; George Human,
City of Richardson; John Godwin, City of Rowlett; Earl Keaton, City of Glenn
Heights; Sally McGlaun, City of Cockrell Hill; Dave Nighswonger, Town of
Addison; Cissy Sylo, City of Carrollton

SUBJECT: LAP/CMS Interlocal Agreement

The DART Board of Directors approved guidelines for the new LAP/CMS program on August 27, 1996. The guidelines are attached. For your convenience, the document changes resulting from the DART/City staff meeting held August 21, 1996, are in bold type. Changes made by the DART Board are either underlined or crossed out.

To facilitate review and approval by each eligible member City Council, a draft Interlocal Agreement (ILA) is also enclosed. This ILA has, as an attachment, a 'clean' version of the guidelines. In other words, the guidelines attached to the ILA have no text bolding, underlining or cross out sections. It is the same document as approved by the Board.

Please review this ILA. If you have any questions, please contact either Paul Jones of DART's Legal department at 749-3088 or myself. The next step to get the LAP/CMS program underway is for each eligible member City Council to approve the ILA.

A handwritten signature in black ink that reads "Tom K. Ryden". The signature is written in a cursive style and is positioned above a horizontal line.

Tom K. Ryden

c: Paul Jones

**GUIDELINES FOR THE LAP/CMS
PROGRAM
August 27, 1996**

A. OBJECTIVE:

Provide continued financial assistance for projects consistent with these guidelines and DART's enabling legislation that enhance mobility in DART member cities that do not yet have rail construction in progress.

B. ELIGIBLE MEMBER CITIES:

The following cities are eligible to receive LAP/CMS funds as of October 1, 1996:

Addison, Carrollton, Cockrell Hill, Farmers Branch, Garland, Glenn Heights, Irving, Plano, Richardson, Rowlett.

C. ALLOCATIONS:

The LAP/CMS fund allocations will be made available for use by an eligible member city annually from FY 1997 through FY 2004 when the program allocations end. The annual allocation will be calculated by multiplying by 15% (7.5% for Irving) the DART sales taxes collected in an eligible member city for the previous twelve month period ending June 30. Each new annual allocation will be available for expenditure on October 1 (beginning in 1996).

The FY 1996 Financial Plan includes approximately \$87,000,000 for this program. Table 1 shows the allocations by DART member city, based on the 1996 projections and anticipated program duration. **The time schedule in Table 1 is subject to change during DART's annual budget cycle.** A city will no longer be eligible to receive program funds in the fiscal year after the Board awards a contract to initiate rail construction within that member city. In no case shall the DART Board award such a contract more than six months prior to construction. **The budget for LAP/CMS will be revisited based on agency requirements to meet its obligations.**

D. PROJECT ELIGIBILITY:

Eligible types of projects are shown below. **In general, eligible projects are divided into two categories: 1) Transit related street improvements and 2) Transit supportive projects and programs.**

Typically, funds approved for capital projects may be used for planning, environmental impact studies, engineering, final design, right-of-way acquisition, construction, and testing/inspection/surveying. Maintenance and operations may also be funded if approved by the DART Board in accordance with Section E of the guidelines.

Any project not explicitly listed but considered allowable under DART's enabling legislation may be proposed for DART consideration. Additional eligible projects include:

Transit related street improvements (streets with current or planned/previous bus routes).

1. Roadway improvements that add capacity
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 - Adding bus priority lanes
 - HOV lanes
2. Grade separations
3. Matching funds for federal/state programs
4. Intersection improvements
 - New signals
 - Computerization of signals
 - Traffic surveillance equipment
 - Telecommunications
5. Cooperative funding of intercity capacity improvements
 - Signal progression
 - Bus only lanes
 - Bus signal preemption
 - Arterial street HOV lanes
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 - Shelters
 - Sidewalks
 - Bus stops
 - Bus turnouts
 - Bus priority treatments
7. Roadway and intersection safety improvements

Transit supportive projects and programs

8. Additional service including non-productive service (previously eliminated or not meeting warrants)
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 - User subsidies for transit service
 - Vanpool programs
 - Rideshare promotion and incentives
 - Alternate work schedule subsidies
 - Employer trip reduction support
 - Emergency ride home funding taxi/loaner car
15. Vans for vanpool programs
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17. Mid-block bus turnouts
18. Incorporating private transit services/facilities into DART
19. Transportation enhancements at existing and planned DART transit centers, transfer centers and rail stations.
20. DART and Railtran related railroad crossing and right of way improvements to include street closings or grade separations.
21. Technical Assistance Program (TAP) funds may include, but are not limited to:
 - Transit planning, feasibility, design **and engineering** studies
 - Salaries for new or existing city staff time specifically allocated for transportation planning and/or studies, or interface with the DART staff on activities affecting the subject city
 - Technical Assistance seminars
 - Study and implementation of transit-supportive land use planning policies and other policies that benefit public transit
 - Minor capital support for technical staff
22. Other projects not meeting the test of past, present, or future bus service, but which are of significance to mobility as long as benefits to transit can be demonstrated. For example: A project not located on a DART **fixed transit** route that relieves traffic congestion on a parallel or intersecting DART route **could** be eligible for LAP/CMS funding.

E. PROJECT MAINTENANCE AND OPERATION:

Project maintenance is the responsibility of the project owner. If shared maintenance responsibility for a project is desired, the cost sharing arrangements must be approved by the DART Board and the eligible city as part of the project approval process. Operating costs of LAP/CMS projects may be eligible for LAP/CMS funding subject to DART Board approval.

F. PROJECT SUBMITTALS AND REPORTING:

Cities will prepare LAP/CMS applications for consideration and approval by DART. DART staff will have 30 days to review and respond to the applications. Once approved, the funding amount in the application becomes "programmed". Left over funding returned to DART after project closing becomes "unprogrammed". A city may simultaneously return and "reprogram" left over funds by submitting a project application for approval along with the left over monies.

DART will prepare, by September 1 of each year, a projected LAP/CMS allocation for each city for budget purposes consistent with the DART Financial Plan in effect at that time. DART will report quarterly (within 30 days after close of the quarter) on the status of the LAP/CMS fund including amounts allocated, amounts programmed, amounts expended, amounts available for reprogramming from completed projects, and a total amount available for programming for each eligible city.

Eligible cities will provide to DART notice of requests for LAP/CMS payment (at least 90 days in advance of a request for LAP/CMS payment) for any amount exceeding \$250,000.

Eligible cities will maintain accurate books and records of all approved projects. Cities will maintain separate records on each project including project budgets, budget amendments, revised budget balances, expenditures to date, estimated cost to complete, and DART payments to date. DART maintains the right to audit a city's books to ensure that funds are spent in accordance with the approved workplan and DART's enabling legislation. **Cities maintain the right to audit DART's books to ensure that the fund allocations and disbursements are in accordance with these guidelines and other applicable laws.**

Eligible cities will provide an Annual Cash Flow Projection report (See Table 2) for all approved projects by June 30 of each year **beginning in 1997**. DART may request updated cash flow projections from time to time to monitor its projected cash outflows.

G. CASH PAYMENTS:

DART will prefund Technical Assistance Program (TAP) costs on an annual basis after submittal of an annual work plan by the eligible member city and approval by DART. Technical Assistance funding will be 3% of an eligible city's annual program allocation.

DART will distribute LAP/CMS funds to the city for a City Council approved project according to the following schedule:

Project cost of \$250,000 or less

100% of the cost upon approval by the City Council

DART will pay within thirty (30) days of receipt of a request for payment with supporting documentation.

Project cost of over \$250,000

20% of the cost upon approval by the City Council.

80% of the cost upon Notice of Award by the city.

Provided that written notice is received from the city at least 90 days in advance, DART will pay within thirty (30) days of receipt of a request for payment over \$250,000 with supporting documentation.

Eligible cities will provide to DART a completion report for each project documenting final acceptance of the project and final project cost. Project documentation will consist of a statement of project expenditures attested by an authorized financial staff member, copies of paid invoices, or other similar documents.

DART staff is authorized without further Board approval to pay at project closeout an amount up to 10% higher than the approved project amount for properly documented eligible project costs. At project completion, cities will return any LAP/CMS funds distributed but unspent. These funds will be available for reprogramming.

Any unused funds allocated to an eligible city in any year will continue to be available to that city in succeeding years **unless the city withdraws from DART**. Interest will not be paid on allocated, but unpaid LAP/CMS balances.

H. OTHER FINANCIAL CONSIDERATIONS:

Since the LAP/CMS program allows for funding of operating and capital transit projects, member cities will be eligible to apply for and receive FTA funds with prior approval by **the DART Board**. Plano will continue to be eligible to receive funds for projects begun prior to FY 1989.

Member cities may pool their LAP/CMS allocations for approval and implementation of eligible projects.

Advanced (beyond 1 year) programming of funds is allowed subject to Board approval; however, cities may not program, in any future year, more than the current year's LAP/CMS allocation.

Board approval will be required for projects over \$100,000. DART staff will approve projects requiring LAP/CMS funding in amounts of \$100,000 or less. A city may appeal to the Board any DART staff decision regarding project approval.

Starting October 1, 1997, all unused existing LAP funds will be administered as part of the new LAP/CMS program. Interest will accrue on the unpaid LAP balance through September 30, 1997, at a fixed rate of 4%.

All DART payments for LAP or LAP/CMS projects will be drawn from the eligible city's existing LAP fund balance until it is exhausted. After all LAP funds allocated are disbursed, further payments will be drawn from that city's LAP/CMS allocation.

I. EXPANDED REVENUE SHARING

In accordance with Section 5 of DART Board Resolution No. 950260 of November 14, 1995, it is DART's intent to continue to explore options for expanded revenue sharing with member cities to meet the region's transportation needs. These guidelines do not reflect the process to be used to provide expanded revenue sharing.

J. WITHDRAWAL OF MEMBER CITIES

If a city withdraws from DART, no additional LAP/CMS funding will be allocated to that city. In the event of a withdrawal, only individual projects for which construction contracts have been executed or for which formal funding agreements have been executed will be funded by DART and allowed to proceed to completion, and the funds for completing these projects will be included as that city's specific obligation if that city withdraws from DART. All other funds revert to DART. **Once a city calls a withdrawal election, no new LAP/CMS projects can be submitted or approved will be considered for approval until after the election is canvassed.**

K. PROJECT IDENTIFICATION

Each construction project will display at least one sign on each approach to the construction site indicating funding for that project is provided through the DART LAP/CMS program. In addition, after implementation ~~and where appropriate, each~~ if a project shall have a visible permanent marker indicating it shall indicate that funding for the project was provided through the DART LAP/CMS program.

Table 1

**LAP/CMS PROJECTIONS
COMPARISON BY MEMBER CITY (1)**

(In Thousands of Dollars)

15% LAP TO CONSTRUCTION

CITY	FY 1997 Allocation	Projected	
		Total Program Cost	Fiscal Year for Cities
Addison	\$ 1,136	\$11,599	2004
Carrollton	1,908	17,313	2003
Cockrell Hill	6	83	2004
Dallas	0	0	na
Farmers Branch	1,518	12,522	2003
Garland	1,975	4,127	1998
Glenn Heights	8	89	2004
Highland Park	0	0	na
Irving (2)	2,149	21,878	2004
Plano	4,167	12,908	1999
Richardson	2,768	4,971	1998
Rowlett	188	1,835	2004
University Park	0	0	na
Total	\$15,823	\$87,324	2004

1 Assumes program allocations end in FY 2004 for all cities. Allocations based on sales tax collections from July 1, 1995 through June 30, 1996.

2 Irving projected to receive 50% of the allocation of other cities for LAP/CMS.

Table 2

EXAMPLE OF ANNUAL CASH FLOW PROJECTION WORKSHEET									
City of _____									
Date _____									
(a) Approved Project Description	(b) Project Number	(c) Approved Budget	(d) Budget Transfer	(c) + (d) Current Budget	(f) Payments Rec'd to Date	(g) (h) (i) Projected Payments from DART			(f+g+h+i) Total Payments
						FY _____	FY _____	FY _____	
Total									



Dallas Area Rapid Transit
P.O. Box 660163
Dallas, Texas 75266-0163
214/749-3278

DATE: August 23, 1996

FROM: Tom K. Ryden

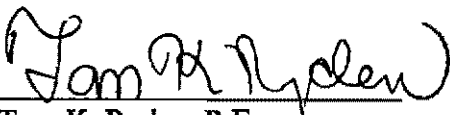
TO: Larry Cervenka, City of Garland; Dave Davis, City of Farmers Branch;
Jim Driscoll, City of Irving; Marcos Fernandez, City of Plano; George Human,
City of Richardson; John Godwin, City of Rowlett; Earl Keaton, City of Glenn
Heights; Sally McGlaun, City of Cockrell Hill; Dave Nighswonger, Town of
Addison; Cissy Sylo, City of Carrollton;

SUBJECT: LAP/CMS Guidelines

The DART Board of Directors is scheduled to consider for approval the LAP/CMS Guidelines on August 27, 1996. The attached LAP/CMS Guidelines are provided for your review and reflect comments received during the August 13, Board meeting and additional comments from member city staff since that meeting. Given everyone's involvement and hard work on the document, I wanted to let each of you have a copy for review in advance of the Board meeting.

The attached guidelines reflect an attempt to address a vast majority of issues raised at the staff level over the past several months. I know there are issues some of you feel are not yet addressed or resolved. The substantive changes from the version reviewed on August 13 based on Board and member city input are in bold type. I will be happy to discuss the new document at your convenience.

Your participation and support during this effort is very much appreciated.


Tom K. Ryden, P.E.

TKR:jr

Attachment

c: Roger Snoble
Doug Allen

**GUIDELINES FOR THE LAP/CMS
PROGRAM
August 27, 1996**

A. OBJECTIVE:

Provide continued financial assistance for projects consistent with these guidelines and DART's enabling legislation that enhance mobility in DART member cities that do not yet have rail construction in progress.

B. ELIGIBLE MEMBER CITIES:

The following cities are eligible to receive LAP/CMS funds as of October 1, 1996:

Addison, Carrollton, Cockrell Hill, Farmers Branch, Garland, Glenn Heights, Irving, Plano, Richardson, Rowlett.

C. ALLOCATIONS:

The LAP/CMS fund allocations will be made available for use by an eligible member city annually from FY 1997 through FY 2004 when the program allocations end. The annual allocation will be calculated by multiplying by 15% (7.5% for Irving) the DART sales taxes collected in an eligible member city for the previous twelve month period ending June 30. Each new annual allocation will be available for expenditure on October 1 (beginning in 1996).

The FY 1996 Financial Plan includes approximately \$87,000,000 for this program. Table 1 shows the allocations by DART member city, based on the 1996 projections and anticipated program duration. **The time schedule in Table 1 is subject to change during DART's annual budget cycle.** A city will no longer be eligible to receive program funds in the fiscal year after the Board awards a contract to initiate rail construction within that member city. **The budget for LAP/CMS will be revisited based on agency requirements to meet its obligations.**

D. PROJECT ELIGIBILITY:

Eligible types of projects are shown below. **In general, eligible projects are divided into two categories: 1) Transit related street improvements and 2) Transit supportive projects and programs.**

Typically, funds approved for capital projects may be used for planning, environmental impact studies, engineering, final design, right-of-way acquisition, construction, and testing/inspection/surveying. Maintenance and operations may also be funded if approved by the DART Board in accordance with Section E of the guidelines.

Any project not explicitly listed but considered allowable under DART's enabling legislation may be proposed for DART consideration. Additional eligible projects include:

Transit related street improvements (streets with current or planned/previous bus routes).

1. Roadway improvements that add capacity
 - Road widening
 - Adding storage to turn lanes
 - Adding turn lanes
 - Adding/deleting access points
 - Adding bus priority lanes
 - HOV lanes
2. Grade separations
3. Matching funds for federal/state programs
4. Intersection improvements
 - New signals
 - Computerization of signals
 - Traffic surveillance equipment
 - Telecommunications
5. Cooperative funding of intercity capacity improvements
 - Signal progression
 - Bus only lanes
 - Bus signal preemption
 - Arterial street HOV lanes
 - Roadway and intersection improvements
6. Special design provisions along roadways and intersections with planned bus routes
 - Shelters
 - Sidewalks
 - Bus stops
 - Bus turnouts
 - Bus priority treatments
7. Roadway and intersection safety improvements

Transit supportive projects and programs

8. Additional service including non-productive service (previously eliminated or not meeting warrants)

9. Transportation services for the elderly and/or disabled
 - Emergency medical
 - Shuttle services
10. ADA compliance items related to the provision of transit service
11. Sidewalks related to providing access to transit facilities or services
12. Bus shelters
13. Bus benches
14. Travel demand management
 - Transportation Management Association operating funds
 - User subsidies for transit service
 - Vanpool programs
 - Rideshare promotion and incentives
 - Alternate work schedule subsidies
 - Employer trip reduction support
 - Emergency ride home funding taxi/loaner car
15. Vans for vanpool programs
16. Park and Ride lots
17. Mid-block bus turnouts
18. Incorporating private transit services/facilities into DART
19. Transportation enhancements at existing and planned DART transit centers, transfer centers and rail stations.
20. DART and Railtran related railroad crossing and right of way improvements to include street closings or grade separations.
21. Technical Assistance Program (TAP) funds may include, but are not limited to:
 - Transit planning, feasibility, design **and engineering** studies
 - Salaries for new or existing city staff time specifically allocated for transportation planning and/or studies, or interface with the DART staff on activities affecting the subject city
 - Technical Assistance seminars
 - Study and implementation of transit-supportive land use planning policies and other policies that benefit public transit
 - Minor capital support for technical staff

22. Other projects not meeting the test of past, present, or future bus service, but which are of significance to mobility as long as benefits to transit can be demonstrated. For example: A project not located on a DART fixed transit route that relieves traffic congestion on a parallel or intersecting DART route could be eligible for LAP/CMS funding.

E. PROJECT MAINTENANCE AND OPERATION:

Project maintenance is the responsibility of the project owner. If shared maintenance responsibility for a project is desired, the cost sharing arrangements must be approved by the DART Board and the eligible city as part of the project approval process. Operating costs of LAP/CMS projects may be eligible for LAP/CMS funding subject to DART Board approval.

F. PROJECT SUBMITTALS AND REPORTING:

Cities will prepare LAP/CMS applications for consideration and approval by DART. DART staff will have 30 days to review and respond to the applications. Once approved, the funding amount in the application becomes "programmed". Left over funding returned to DART after project closing becomes "unprogrammed". A city may simultaneously return and "reprogram" left over funds by submitting a project application for approval along with the left over monies.

DART will prepare, by September 1 of each year, a projected LAP/CMS allocation for each city for budget purposes consistent with the DART Financial Plan in effect at that time. DART will report quarterly (within 30 days after close of the quarter) on the status of the LAP/CMS fund including amounts allocated, amounts programmed, amounts expended, amounts available for reprogramming from completed projects, and a total amount available for programming for each eligible city.

Eligible cities will provide to DART notice of requests for LAP/CMS payment (at least 90 days in advance of a request for LAP/CMS payment) for any amount exceeding \$250,000.

Eligible cities will maintain accurate books and records of all approved projects. Cities will maintain separate records on each project including project budgets, budget amendments, revised budget balances, expenditures to date, estimated cost to complete, and DART payments to date. DART maintains the right to audit a city's books to ensure that funds are spent in accordance with the approved workplan and DART's enabling legislation. **Cities maintain the right to audit DART's books to ensure that the fund allocations and disbursements are in accordance with these guidelines and other applicable laws.**

Eligible cities will provide an Annual Cash Flow Projection report (See Table 2) for all approved projects by June 30 of each year **beginning in 1997**. DART may request updated cash flow projections from time to time to monitor its projected cash outflows.

G. CASH PAYMENTS:

DART will prefund Technical Assistance Program (TAP) costs on an annual basis after submittal of an annual work plan by the eligible member city and approval by DART. Technical Assistance funding will be 3% of an eligible city's annual program allocation.

DART will distribute LAP/CMS funds to the city for a City Council approved project according to the following schedule:

Project cost of \$250,000 or less

100% of the cost upon approval by the City Council

DART will pay within thirty (30) days of receipt of a request for payment with supporting documentation.

Project cost of over \$250,000

20% of the cost upon approval by the City Council.

80% of the cost upon Notice of Award by the city.

Provided that written notice is received from the city at least 90 days in advance, DART will pay within thirty (30) days of receipt of a request for payment over \$250,000 with supporting documentation.

Eligible cities will provide to DART a completion report for each project documenting final acceptance of the project and final project cost. Project documentation will consist of a statement of project expenditures attested by an authorized financial staff member, copies of paid invoices, or other similar documents.

DART staff is authorized without further Board approval to pay at project closeout an amount up to 10% higher than the approved project amount for properly documented eligible project costs. At project completion, cities will return any LAP/CMS funds distributed but unspent. These funds will be available for reprogramming.

Any unused funds allocated to an eligible city in any year will continue to be available to that city in succeeding years **unless the city withdraws from DART**. Interest will not be paid on allocated, but unpaid LAP/CMS balances.

H. OTHER FINANCIAL CONSIDERATIONS:

Since the LAP/CMS program allows for funding of operating and capital transit projects, member cities will be eligible to apply for and receive FTA funds with prior approval by the DART Board. Plano will continue to be eligible to receive funds for projects begun prior to FY 1989.

Member cities may pool their LAP/CMS allocations for approval and implementation of eligible projects.

Advanced (beyond 1 year) programming of funds is allowed subject to Board approval; however, cities may not program, in any future year, more than the current year's LAP/CMS allocation.

Board approval will be required for projects over \$100,000. DART staff will approve projects requiring LAP/CMS funding in amounts of \$100,000 or less. A city may appeal to the Board any DART staff decision regarding project approval.

Starting October 1, 1997, all unused existing LAP funds will be administered as part of the new LAP/CMS program. Interest will accrue on the unpaid LAP balance through September 30, 1997, at a fixed rate of 4%.

All DART payments for LAP or LAP/CMS projects will be drawn from the eligible city's existing LAP fund balance until it is exhausted. After all LAP funds allocated are disbursed, further payments will be drawn from that city's LAP/CMS allocation.

I. EXPANDED REVENUE SHARING

In accordance with Section 5 of DART Board Resolution No. 950260 of November 14, 1995, it is DART's intent to continue to explore options for expanded revenue sharing with member cities to meet the region's transportation needs. These guidelines do not reflect the process to be used to provide expanded revenue sharing.

J. WITHDRAWAL OF MEMBER CITIES

If a city withdraws from DART, no additional LAP/CMS funding will be allocated to that city. In the event of a withdrawal, only individual projects for which construction contracts have been executed or for which formal funding agreements have been executed will be funded by DART and allowed to proceed to completion. All other funds revert to DART. **Once a city calls a withdrawal election, no new LAP/CMS projects can be submitted or approved until after the election is canvassed.**

K. PROJECT IDENTIFICATION

Each construction project will display at least one sign on each approach to the construction site indicating funding for that project is provided through the DART LAP/CMS program. In addition, after implementation and where appropriate, each project shall have a visible permanent marker indicating funding for the project was provided through the DART LAP/CMS program.

Table 1

**LAP/CMS PROJECTIONS
COMPARISON BY MEMBER CITY (1)**

(In Thousands of Dollars)

15% LAP TO CONSTRUCTION

CITY	FY 1997 Allocation	Projected	
		Total Program Cost	Fiscal Year for Cities
Addison	\$ 1,136	\$11,599	2004
Carrollton	1,908	17,313	2003
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Farmers Branch	1,518	12,522	2003
Garland	1,975	4,127	1998
Glenn Heights	8	89	2004
Highland Park	0	0	na
Irving (2)	2,149	21,878	2004
Plano	4,167	12,908	1999
Richardson	2,768	4,971	1998
Rowlett	188	1,835	2004
University Park	0	0	na
Total	\$15,823	\$87,324	2004

1 Assumes program allocations end in FY 2004 for all cities. Allocations based on sales tax collections from July 1, 1995 through June 30, 1996.

2 Irving projected to receive 50% of the allocation of other cities for LAP/CMS.



Dallas Area Rapid Transit
P.O. Box 660163
Dallas, Texas 75266-0163
214/749-3278

DATE: August 16, 1996

FROM: Tom K. Ryden

TO: George Human, City of Richardson; Marcos Fernandez, City of Plano; Brian Jahn, City of Carrollton; Dave Davis, City of Farmers Branch; Mike Gibson, City of Rowlett; Dave Nighswonger, Town of Addison; Larry Cervenka, City of Garland; Jim Driscoll, City of Irving

SUBJECT: LAP/CMS Guidelines

The DART Board of Directors is scheduled to review and approve the LAP/CMS Guidelines on August 27, 1996. The attached LAP/CMS Guidelines are provided for your review and represent the comments received during the August 13 Board meeting.

The unresolved issues from the last Board meeting were:

- use of FTA funds by eligible member cities;
- interest on the unused LAP balance at 4% for one year;
- definition of the city commitment on a project before funds are disbursed; and
- a moratorium on funds once a city calls a withdrawal election.

The attached guidelines reflect an attempt to address these issues. I suggest we meet as a group next Wednesday afternoon, August 22 or Thursday morning August 23.

A handwritten signature in black ink that reads "Tom K. Ryden". The signature is written in a cursive, flowing style with a large, prominent "T" and "R".

Tom K. Ryden, P.E.

TKR:jr

Attachments

**GUIDELINES FOR THE LAP/CMS
PROGRAM
August 14, 1996**

A. OBJECTIVE:

Provide continued financial assistance for projects consistent with these guidelines and DART's enabling legislation that enhance mobility in DART member cities that do not yet have rail construction in progress.

B. ELIGIBLE MEMBER CITIES:

The following cities are eligible to receive LAP/CMS funds as of October 1, 1996:

Addison, Carrollton, Cockrell Hill, Farmers Branch, Garland, Glenn Heights, Irving, Plano, Richardson, Rowlett.

C. ALLOCATIONS:

The LAP/CMS fund allocations will be made available for use by an eligible member city annually from FY 1997 through FY 2004 when the program allocations end. The annual allocation will be calculated by multiplying by 15% (7.5% for Irving) the DART sales taxes collected in a member city for the previous twelve month period ending June 30. Each new annual allocation will be available for expenditure on October 1 (beginning in 1996).

The FY 1996 Financial Plan includes approximately \$87,000,000 for this program. Table 1 shows the allocations by city, based on the 1996 projections and anticipated program duration. A city will no longer be eligible to receive program funds in the fiscal year after the Board awards a contract to initiate rail construction within that member city. If in the future, DART's sales tax is reduced, the Board reserves the right to re-examine the LAP/CMS budget.

D. PROJECT ELIGIBILITY:

Eligible types of projects are shown on Table 2. Any project not explicitly listed but considered allowable under DART's enabling legislation may be proposed for DART consideration. Additional eligible projects include:

- Provisions during design and construction of street improvement projects for future shelters, sidewalks, bus stops, bus turnouts, and bus priority treatments on streets where bus service is planned for the future. (Transit supportive category)

- Transportation enhancements at existing and planned DART transit centers, transfer locations and rail stations. (Transit supportive category)
- Improvements including grade separations and street closures at existing railroad crossings along DART and Railtran owned rail right of way. (Transit supportive category)
- Roadway and intersection safety improvements along eligible streets. (Transit related category)
- Other projects not meeting the test of past, present, or future bus service, but which are of significance to mobility as long as benefits to transit can be demonstrated. For example: A project not located on a DART route that relieves traffic congestion on a parallel or intersecting DART route would be eligible for LAP/CMS funding. (Category to be determined)
- Technical Assistance Program funds may include but are not limited to transit planning, feasibility and design studies; salaries for new or existing city staff time specifically allocated for transportation planning and/or studies, or interface with the DART staff on activities affecting the subject city; technical assistance seminars; study and implementation of transit-supportive land use planning policies and other policies that benefit public transit; and minor capital support for technical staff.

D. PROJECT ELIGIBILITY: (Proposed Replacement Section)

Eligible types of projects are shown below. Any project not explicitly listed but considered allowable under DART's enabling legislation may be proposed for DART consideration. Additional eligible projects include:

Transit related street improvements (streets with current or planned/previous bus routes).

1. Roadway improvements that add capacity
 - Road widening
 - Adding storage to turn lanes
 - Adding turn lanes
 - Adding/deleting access points
 - Adding bus priority lanes
 - HOV lanes
 - Final engineering and design
2. Grade separations
3. Matching funds for federal/state programs

4. Intersection improvements
 - New signals
 - Computerization of signals
 - Traffic surveillance equipment
5. Cooperative funding of intercity capacity improvements
 - Signal progression
 - Bus only lanes
 - Bus signal preemption
 - Arterial street HOV lanes
 - Roadway and intersection improvements
6. Final engineering and design including special design provisions along roadways and intersections with planned bus routes
 - Shelters
 - Sidewalks
 - Bus stops
 - Bus turnouts
 - Bus priority treatments
7. Roadway and intersection safety improvements

Transit supportive projects and programs

8. Additional service including non-productive service (previously eliminated or not meeting warrants)
9. Transportation services for the elderly and/or disabled
 - Emergency medical
 - Shuttle services
10. ADA compliance items related to the provision of transit service
11. Sidewalks related to providing access to transit facilities or services
12. Bus shelters
13. Bus benches
14. Travel demand management
 - Transportation Management Association operating funds
 - User subsidies for transit service
 - Vanpool programs
 - Rideshare promotion and incentives

Alternate work schedule subsidies
Employer trip reduction support
Emergency ride home funding taxi/loaner car

15. Vans for vanpool programs
16. Park and Ride lots
17. Mid-block bus turnouts
18. Incorporating private transit services/facilities into DART
19. Transportation enhancements at existing and planned DART transit centers, transfer centers and rail stations.
20. DART and Railtran related railroad crossing and right of way improvements to include street closings or grade separations.
21. Technical Assistance Program (TAP) funds may include, but are not limited to:
 - Transit planning, feasibility and design studies
 - Salaries for new or existing city staff time specifically allocated for transportation planning and/or studies, or interface with the DART staff on activities affecting the subject city
 - Technical Assistance seminars
 - Study and implementation of transit-supportive land use planning policies and other policies that benefit public transit
 - Minor capital support for technical staff
22. Other projects not meeting the test of past, present, or future bus service, but which are of significance to mobility as long as benefits to transit can be demonstrated. For example: A project not located on a DART route that relieves traffic congestion on a parallel or intersecting DART route would be eligible for LAP/CMS funding.

E. PROJECT MAINTENANCE AND OPERATION:

Project maintenance is the responsibility of the project owner. If shared maintenance responsibility for a project is desired, the cost sharing arrangements must be approved by the DART Board and the eligible city as part of the project approval process. Operating costs of LAP/CMS projects may be eligible for LAP/CMS funding subject to DART Board approval.

F. REPORTING:

DART will prepare, by September 1 of each year, a projected LAP/CMS allocation for each city for budget purposes consistent with the DART Financial Plan in effect at that time. DART will report quarterly (within 30 days after close of the quarter) on the status of the

LAP/CMS fund including amounts allocated, amounts programmed, amounts expended, amounts available for reprogramming from completed projects, and a total amount available for programming for each eligible city.

Eligible cities will provide to DART notice of requests for LAP/CMS payment (at least 90 days in advance of a request for LAP/CMS payment) for any amount exceeding \$250,000.

Eligible cities will maintain accurate books and records of all approved projects. Cities will maintain separate records on each project including project budgets, budget amendments, revised budget balances, expenditures to date, estimated cost to complete, and DART payments to date. DART maintains the right to audit a city's books to ensure that funds are spent in accordance with the approved workplan and DART's enabling legislation.

Eligible cities will provide an Annual Cash Flow Projection report (See Table 3) for all approved projects by June 30 of each year. DART may request updated cash flow projections from time to time to monitor its projected cash outflows.

G. CASH PAYMENTS:

DART will prefund Technical Assistance Program (TAP) costs on an annual basis after submittal of an annual work plan by the eligible member city and approval by DART. Technical Assistance funding will be 3% of an eligible city's annual program allocation.

DART will distribute LAP/CMS funds to the city for a City Council approved project according to the following schedule:

Project cost of \$250,000 or less

100% of the cost upon approval by the City Council.

DART will pay within thirty (30) days of receipt of a request for payment with supporting documentation.

Project cost of over \$250,000

20% of the cost upon approval by the City Council.

80% of the cost upon Notice To Proceed by the city.

Provided that written notice is received from the city at least 90 days in advance, DART will pay within thirty (30) days of receipt of a request for payment over \$250,000 with supporting documentation.

Eligible cities will provide to DART a completion report for each project documenting with final acceptance of the project and final project cost. Project documentation will consist of a statement of project expenditures attested by an authorized financial staff member, copies of paid invoices, or other similar documents.

DART staff is authorized without further Board approval to pay at project closeout an amount up to 10% higher than the approved project amount for properly documented

eligible project costs. At project completion, cities will return any LAP/CMS funds distributed but unspent. These funds will be available for reprogramming.

Any unused funds allocated to a city in any year will continue to be available to that city in succeeding years. Interest will not be paid on allocated, but unpaid LAP/CMS balances.

G. CASH PAYMENTS: (Proposed Replacement Section)

DART will prefund Technical Assistance Program (TAP) costs on an annual basis after submittal of an annual work plan by the eligible member city and approval by DART. Technical Assistance funding will be 3% of an eligible city's annual program allocation.

DART will distribute LAP/CMS funds to the city for a City Council approved project according to the following schedule:

Project cost of \$250,000 or less:

100% of the cost upon approval by the City Council of equipment purchases, right of way acquisition, or notice to proceed for project design, engineering, or construction

DART will pay within thirty (30) days of receipt of a request for payment with supporting documentation.

Project cost of over \$250,000:

20% of the cost upon approval by the City Council for equipment purchases, right of way acquisition or notice to proceed for engineering and design.

80% of the cost upon notice to proceed by the city for construction.

Provided that written notice is received from the city at least 90 days in advance, DART will pay within thirty (30) days of receipt of a request for payment over \$250,000 with supporting documentation.

(No change to remainder of Section G. above)

H. OTHER FINANCIAL CONSIDERATIONS:

Since the LAP/CMS fund allows for funding of operating and capital transit projects, member cities will be eligible to apply for and receive FTA funds with prior approval by DART. Plano will continue to be eligible to receive funds for projects begun prior to FY 1989.

Member cities may pool funds to create projects.

Advanced (beyond 1 year) programming of funds is allowed subject to Board approval; however, cities may not program, in any future year, more than the current year's LAP/CMS allocation.

*Plano
wants to
add \$100,000
threshold*

Board approval will be required for projects over \$100,000. DART staff will approve projects requiring LAP/CMS funding in amounts of \$100,000 or less. A city may appeal to the Board any DART staff decision regarding project approval.

Starting October 1, 1997, all unused existing LAP funds will be administered as part of the new LAP/CMS program. Interest will accrue on the unpaid LAP balance through September 30, 1997, at a fixed rate of 4%.

All DART payments for LAP or LAP/CMS projects will be drawn from the eligible city's existing LAP fund balance until it is exhausted. After all LAP funds allocated are disbursed, further payments will be drawn from that city's LAP/CMS allocation.

I. EXPANDED REVENUE SHARING

In accordance with Section 5 of DART Board Resolution No. 950260 of November 14, 1995, it is DART's intent to continue to explore options for expanded revenue sharing with member cities to meet the region's transportation needs. These guidelines do not reflect the process to be used to provide expanded revenue sharing.

J. WITHDRAWAL OF AND ADDITION OF MEMBER CITIES:

If a city withdraws from DART, no additional LAP/CMS funding will be allocated to that city. In the event of a withdrawal, individual projects for which construction contracts have been executed or for which formal funding agreements have been executed will be funded by DART and allowed to proceed to completion. All other funds revert to DART. If a city joins DART, total LAP/CMS funding will be increased by the joining city's annual LAP/CMS allocation. The joining city's allocation will be calculated based on the allocation methodology described in these guidelines.

cities agree to delete last 2 cities

J. WITHDRAWAL OF MEMBER CITIES (Proposed Replacement Section)

If a city withdraws from DART, no additional LAP/CMS funding will be allocated to that city. In the event of a withdrawal, only individual projects for which construction contracts have been executed or for which formal funding agreements have been executed will be funded by DART and allowed to proceed to completion. All other funds revert to DART. Once a city calls a withdrawal election, no new LAP/CMS projects can be submitted for approval and no new funding for existing LAP/CMS projects will be permitted until after the election is canvassed.

No provisions for addition of new cities into program

New

Cities object

Table 1

**LAP/CMS PROJECTIONS
COMPARISON BY MEMBER CITY (1)**

(In Thousands of Dollars)

15% LAP TO CONSTRUCTION

Projected

CITY	FY 1997 Allocation	Total Program Cost	Fiscal Year for Cities
Addison	\$ 1,136	\$11,599	2004
Carrollton	1,908	17,313	2003
Cockrell Hill	6	83	2004
Dallas	0	0	na
Farmers Branch	1,518	12,522	2003
Garland	1,975	4,127	1998
Glenn Heights	8	89	2004
Highland Park	0	0	na
Irving (2)	2,149	21,878	2004
Plano	4,167	12,908	1999
Richardson	2,768	4,971	1998
Rowlett	188	1,835	2004
University Park	0	0	na
Total	\$15,823	\$87,324	2004

1 Assumes program ends in FY 2004 for all cities. Allocations based on sales tax collections from July 1, 1995 through June 30, 1996.

2 Irving projected to receive 50% of the allocation of other cities for LAP/CMS.

PROPOSED USES OF LAP/CMS FUNDS

TRANSIT RELATED STREET IMPROVEMENTS

(Streets with current or planned/previous bus routes)

1. Roadway improvements - adding capacity -

- Road widening
- Adding storage to turn lanes
- Adding turn lanes
- Adding/defining access points
- Adding bus priority lanes
- HOV Lanes
- Final Engineering and Design

2. Grade Separations

3. Matching funds for federal/state programs

4. Intersection Improvements

- New Signals
- Computerization of Signals
- Traffic Surveillance Equipment

5. Cooperative funding of intercity capacity improvements

- Signal Progression
- Bus only lanes
- Bus signal preemption
- Arterial Street HOV lanes

TRANSIT SUPPORTIVE PROJECTS AND PROGRAMS

- 1. Additional Service**
non-productive services (previously eliminated or not meeting warrants)
- 2. Transportation services for the elderly and/or disabled**
emergency medical
shuttle services
- 3. ADA compliance items related to the provision of transit services**
- 4. Sidewalks related to providing access to transit facilities or services**
- 5. Bus Shelters**
- 6. Bus Benches**
- 7. Travel Demand Management**
Transportation Management Association operating funds
User subsidies for transit services
Vaspool Programs
RideShare promotion and incentives
Alternative work schedules subsidies
Employer trip reduction support
Emergency ride home funding and/or car
- 8. Vans for vaspool programs**
- 9. Park and Ride Lots**
- 10. Mid-block bus turnouts**
- 11. Incorporating private transit services/facilities into DART**
- 12. Technical Assistance Program (TAP)**

All project requests will require Board approval. Additional projects not explicitly listed above but considered allowable under DART's enabling legislation may be proposed for DART consideration.

MEMBER CITY	<u>CAP</u>	UNPAID BALANCE	<u>TAP</u>	UNPAID BALANCE
	UNPROGRAMMED BALANCE		UNPROGRAMMED BALANCE	
ADDISON	\$ 334,555.00	\$ 4,286,360	\$0	\$0

As of July 31, 1996

ADDISON CAPITAL ASSISTANCE PROGRAM

PROJECT #	PROJECT NAME	DATE APPROVED	APPROVED AMOUNT	AMOUNT TO BE REPROGRAMMED	EXPENDITURES DURING				EXPENDITURES DURING FY96	TOTAL AMOUNT PAID PER PROJECT	PROGRAMMED FUNDS NOT YET PAID
					FY89-90	FY91-92	FY93-94	FY95			
	TRANSFER TO TAP ACCOUNT FOR PAVEMENT MGMT. & THOROUGH-FARE PLAN STAFF SALARIES Project Completed	12/19/89	\$18,712.40 \$5,859.60		\$24,572.00				\$24,572.00	\$0.00	
	TRANSFER TO TAP ACCOUNT TO HELP FUND SALARY FOR CITY ENGINEER Project Completed	10/23/90	\$4,672.93		\$4,672.93				\$4,672.93	\$0.00	
	BELT LINE/QUORUM INTERSECTION	10/23/90	\$187,000.00						\$0.00	\$187,000.00	
	BELT LINE/MIDWAY INTERSECTION	10/23/90	\$78,606.00						\$0.00	\$78,606.00	
	Right-of-Way Acquisition	04/11/95	\$508,000.00						\$0.00	\$508,000.00	
	BELT LINE/ADDISON RD INTERSECT ADDITIONAL FUNDS	10/23/90 11/26/91	\$137,960.00 \$63,040.00								
	Payment 03/17/93				\$12,605.00						
	Payment 11/01/93				\$151,277.36						
	Payment 04/11/95				\$170.00						
	Project Completed Under Budget and funds transferred to the Keller Springs/N. Dallas Tollway Project	04/11/95	(\$31,947.64)						\$184,052.36	\$0.00	
	MONTFORT/BELT LINE INTERSECT	10/23/90	\$125,000.00								
	Payment 03/17/93				\$19,928.00						
	Payment 02/10/94				\$56,064.42						
	Payment 10/04/94				\$8,128.66						
	Payment 04/11/95				\$3,622.75						
	Project Completed Under Budget and funds transferred to the Keller Springs/N. Dallas Tollway Project	04/11/95	(\$37,258.17)						\$87,741.83	\$0.00	
	TRAFFIC SIGNAL IMPROVEMENTS	10/23/90	\$20,400.00						\$20,400.00	\$0.00	
	Payment 10/04/94				\$20,400.00						
	Project Completed 10/4/94										
	SIDEWALK/BUS BENCH PADS	11/26/91	\$41,000.00								
	Project Completed Under Budget	04/11/95	(\$3,211.70)		\$3,211.70				\$37,788.30	\$0.00	
	ADISON/WESTGROVE INTERSECT.	11/26/91	\$125,000.00								
	Payment 03/17/93				\$8,500.00						
	Payment 04/11/95				\$86,955.18				\$109,618.41	\$15,381.59	
	Payment 03/14/96				\$14,163.23						
	MIDWAY/LINDBERG INTERSECT.	11/26/91	\$150,000.00								
	Payment 03/17/93				\$10,830.00				\$10,830.00	\$139,170.00	
	MIDWAY/BELTWAY INTERSECT.	11/26/91	\$45,000.00								
	Payment 03/17/93				\$2,925.00				\$2,925.00	\$42,075.00	
	MIDWAY/GREENHILL INTERSECT.	11/26/91	\$75,000.00								
	Payment 03/17/93				\$4,280.00				\$4,280.00	\$70,720.00	
	BELT LINE/MARSH LN. INTERSECT. Additional Funds	11/26/91	\$550,000.00								
	Payment 06/14/94		\$250,000.00		\$19,590.00						
	Payment 09/30/94				\$635,526.80				\$747,487.07	\$52,512.93	
	Payment 04/11/95				\$5,525.00						
	Payment 03/14/96				\$86,845.27						

ADDISON CAPITAL ASSISTANCE PROGRAM

PROJECT #	PROJECT NAME	DATE APPROVED	APPROVED AMOUNT	AMOUNT TO BE REPROGRAMMED	EXPENDITURES DURING				EXPENDITURES DURING FY96	TOTAL AMOUNT PAID PER PROJECT	PROGRAMMED FUNDS NOT YET PAID
					FY89-90	FY91-92	FY93-94	FY95			
	TRANSFER TO TAP FOR CITY ENGINEER/PLANNER POSITION	11/26/91 09/30/94	\$8,701.00				\$8,701.00		\$8,701.00	\$0.00	
	THOROUGHFARE PLAN Payment Payment	2/25/92 03/17/93 11/01/93	\$37,500.00			\$33,750.00 \$3,750.00			\$37,500.00	\$0.00	
	BELT LINE/DALLAS NORTH TOLLWAY Payment Payment Payment Payment	06/22/93 6/22/93 11/01/93 02/10/94 04/11/95 03/14/95	\$282,000.00			\$13,770.00 \$3,870.00	\$120.00	\$238,217.72	\$255,977.72	\$6,022.28	
	KELLER SPRINGS/DALLAS NORTH TOLLWAY INTERSECTION Payment Additional funds transferred from Belt Line/Monfort and Belt Line/Addison Rd. Projects Payment	6/22/93 04/11/95 04/11/95 03/14/95	\$226,150.00 \$69,205.47			\$35,825.00		\$232,043.44	\$267,868.44	\$27,487.03	
	EXT ARAPAHO FROM NO DALLAS TOLLWAY TO MARSH LANE PHASE I Payment	10/26/93 02/10/94	\$255,000.00			\$255,000.00			\$255,000.00	\$0.00	
	WEST GROVE/SOJOURN INTER. IMP Payment Payment Payment	02/15/94 09/30/94 04/11/95 03/14/95	\$175,000.00			\$15,570.00	\$93,958.00	\$16,492.77	\$126,020.77	\$48,979.23	
	ARAPAHO REALIGNMENT/EXTENSIO Additional Funds Additional Funds Payment Payment Additional Funds Payment	02/15/94 08/13/94 10/01/94 04/11/95 03/14/95 10/01/95 05/20/96	\$803,000.00 \$1,200,000.00 \$900,000.00 \$620,559.00			\$184,715.00		\$553,492.70	\$747,707.70	\$2,775,851.30	
	BELT LINE RD / BUSINESS DR. Additional Funds Payment Payment (and Project Complete)	02/15/94 08/14/94 10/04/94 04/12/95	\$200,000.00 \$50,000.00			\$225,000.00	\$25,000.00		\$250,000.00	\$0.00	
	TOTAL		\$7,114,948.89	\$3,211.70	\$24,572.00	\$4,672.93	\$1,547,252.54	\$435,890.93	\$3,163,143.53	\$3,951,805.36	
	TOTAL DISBURSEMENTS ALL YEARS			\$3,163,143.53							

Project(s) Closed

Funds Allocated thru FY 96 \$6,163,588.84
 Interest Earned on Unpaid Balance \$1,285,934.26
 Total Funds Available \$7,449,504.20
 Funds Programmed \$7,114,948.89
 Funds Unprogrammed \$334,555.31
 Cash Payments Made \$3,163,143.53

ADDISON CAPITAL ASSISTANCE PROGRAM

PROJECT #	PROJECT NAME	DATE APPROVED	APPROVED AMOUNT	AMOUNT TO BE REPROGRAMMED	EXPENDITURES DURING FY89-90	EXPENDITURES DURING FY91-92	EXPENDITURES DURING FY93-94	EXPENDITURES DURING FY95	EXPENDITURES DURING FY96	TOTAL AMOUNT PAID PER PROJECT	PROGRAMMED FUNDS NOT YET PAID
	TRANSFER TO TAP ACCOUNT FOR PAVEMENT MGMT. & THOROUGH-FARE PLAN STAFF SALARIES Project Completed	12/19/89	\$18,712.40 \$5,659.60		\$24,572.00					\$24,572.00	\$0.00
	TRANSFER TO TAP ACCOUNT TO HELP FUND SALARY FOR CITY ENGINEER Project Completed	10/23/90	\$4,672.93			\$4,672.93				\$4,672.93	\$0.00
	BELT LINE/QUORUM INTERSECTION	10/23/90	\$187,000.00							\$0.00	\$187,000.00
	BELT LINE/MIDWAY INTERSECTION	10/23/90	\$78,606.00							\$0.00	\$78,606.00
	Right-of-Way Acquisition	04/11/95	\$508,000.00							\$0.00	\$508,000.00
	BELT LINE/ADDISON RD INTERSECT ADDITIONAL FUNDS Payment 03/17/93 Payment 11/01/93 Project Completed Under Budget and funds transferred to the Keller Springs/N. Dallas Tollway Project	10/23/90 11/26/91 03/17/93 11/01/93 04/11/95	\$132,960.00 \$63,040.00		\$12,605.00 \$151,277.36			\$170.00		\$164,052.36	\$0.00
	MONTFORT/BELT LINE INTERSECT Payment 03/17/93 Payment 02/10/94 Payment 10/04/94 Payment 04/11/95 Project Completed Under Budget and funds transferred to the Keller Springs/N. Dallas Tollway Project	10/23/90 03/17/93 02/10/94 10/04/94 04/11/95	\$125,000.00		\$19,926.00 \$56,064.42 \$8,128.66			\$3,622.75		\$87,741.83	\$0.00
	TRAFFIC SIGNAL IMPROVEMENTS Payment Project Completed 10/4/94	10/23/90 10/04/94	\$20,400.00		\$20,400.00					\$20,400.00	\$0.00
	SEWALK/BUS BENCH PADS Project Completed Under Budget	11/26/91 04/11/95	\$41,000.00 (\$3,211.70)	\$3,211.70	\$34,404.00 \$3,384.30					\$37,788.30	\$0.00
	ADDISON/WESTGROVE INTERSECT. Payment 03/17/93 Payment 04/11/95 Payment 03/14/96	11/26/91 03/17/93 04/11/95 03/14/96	\$125,000.00		\$8,500.00	\$86,955.18		\$14,163.23		\$109,618.41	\$15,381.59
	MIDWAY/INDBERG INTERSECT. Payment	11/26/91 03/17/93	\$150,000.00		\$10,830.00					\$10,830.00	\$139,170.00
	MIDWAY/BELTWAY INTERSECT. Payment	11/26/91 03/17/93	\$45,000.00		\$2,925.00					\$2,925.00	\$42,075.00
	MIDWAY/GREENHILL INTERSECT. Payment	11/26/91 03/17/93	\$75,000.00		\$4,280.00					\$4,280.00	\$70,720.00
	BELT LINE/MARSH LN. INTERSECT. Additional Funds Payment 09/30/94 Payment 04/11/95 Payment 03/14/96	11/26/91 06/14/94 09/30/94 04/11/95 03/14/96	\$550,000.00 \$250,000.00		\$19,590.00 \$635,526.80			\$5,525.00	\$86,845.27	\$747,487.07	\$52,512.93

ADDISON CAPITAL ASSISTANCE PROGRAM

PROJECT #	PROJECT NAME	DATE APPROVED	APPROVED AMOUNT	AMOUNT TO BE REPROGRAMMED	EXPENDITURES DURING FY89-90	EXPENDITURES DURING FY91-92	EXPENDITURES DURING FY93-94	EXPENDITURES DURING FY95	EXPENDITURES DURING FY96	TOTAL AMOUNT PAID PER PROJECT	PROGRAMMED FUNDS NOT YET PAID
	TRANSFER TO TAPFOR CITY ENGINEER/PLANNER POSITION	11/26/91 09/30/94	\$8,701.00				\$8,701.00			\$8,701.00	\$0.00
	THOROUGHFARE PLAN	2/25/92 03/17/93 11/01/93	\$37,500.00				\$33,750.00 \$3,750.00			\$37,500.00	\$0.00
	BELT LINE/DALLAS NORTH TOLLWAY	06/22/93 6/22/93 11/01/93 02/10/94 04/11/95 03/14/96	\$262,000.00				\$13,770.00 \$3,870.00	\$120.00	\$238,217.72	\$255,977.72	\$6,022.28
	KELLER SPRINGS/DALLAS NORTH TOLLWAY INTERSECTION	6/22/93 04/11/95	\$226,150.00					\$35,825.00		\$267,868.44	\$27,487.03
	Additional funds transferred from Belt Line/Monfort and Belt Line/Addison Rd. Projects	04/11/95 03/14/96	\$69,205.47					\$232,043.44			
	EXT ARAPAHO FROM NO DALLAS TOLLWAY TO MARSH LANE PHASE I	10/28/93 02/10/94	\$255,000.00				\$255,000.00			\$255,000.00	\$0.00
	WEST GROVE/ SOJOURN INTER. IMP	02/15/94 09/30/94 04/11/95 03/14/96	\$175,000.00				\$15,570.00	\$93,958.00	\$16,492.77	\$126,020.77	\$48,979.23
	ARAPAHO REALIGNMENT/EXTENSIO	02/15/94 09/13/94 10/01/94 04/11/95 03/14/96 10/01/95 05/20/96	\$803,000.00 \$1,200,000.00 \$900,000.00					\$184,715.00	\$9,500.00 \$553,492.70	\$747,707.70	\$2,775,851.30
	BELT LINE RD. / BUSINESS DR.	02/15/94 06/14/94 10/04/94 04/12/95	\$200,000.00 \$50,000.00				\$225,000.00			\$250,000.00	\$0.00
	TOTAL		\$7,114,948.89	\$3,211.70	\$24,572.00	\$4,672.93	\$1,547,252.54	\$435,890.93	\$1,150,755.13	\$3,163,143.53	\$3,951,805.36
	TOTAL DISBURSEMENTS ALL YEARS					\$3,163,143.53					

Project(s) Closed

Funds Allocated thru FY 96
Interest Earned on Unpaid Balance
Total Funds Available
Funds Programmed
Funds Unprogrammed
Cash Payments Made

\$6,163,569.94
\$1,285,934.26
\$7,449,504.20
\$7,114,948.89
\$334,555.31
\$3,163,143.53

**LOCAL ASSISTANCE PROGRAM
 BASED ON SALES TAX COLLECTIONS
 JULY 1995 THROUGH JUNE 1996**

CITY	DART ALLOCATION	LAP AT 15% (Irving 7.5%)
ADDISON	7,570,612	1,135,592
CARROLLTON	12,724,873	1,908,731
COCKRELL HILL	41,786	6,268
DALLAS	151,002,330	0
FARMERS BRANCH	10,122,734	1,518,410
GARLAND	13,165,488	1,974,823
GLENN HEIGHTS	55,495	8,324
HIGHLAND PARKS	995,436	0
IRVING	28,650,492	2,148,787
PLANO	27,780,189	4,167,028
RICHARDSON*	18,451,642	2,767,746
ROWLETT	1,256,044	188,407
UNIVERSITY PARK	1,667,149	0
TOTAL DART	273,484,269	15,824,116

*Including allocation for Buckingham

**ALLOCATION OF DART SALES TAX
BASED ON COLLECTIONS FOR MEMBER CITIES
(JULY 1995 THRU JUNE 1996)**

MEMBER CITY	ACTUAL CITY SALES TAX COLLECTION	ALLOCATION OF DART COLLECTION
ADDISON	8,056,403	7,570,612
BUCKINGHAM	335,994	313,775
CARROLLTON	13,563,256	12,724,873
COCKRELL HILL	44,380	41,786
DALLAS	161,092,651	151,002,330
FARMERS BRANCH	10,776,205	10,122,734
GARLAND	14,013,954	13,165,488
GLENN HEIGHTS	59,710	55,495
HIGHLAND PARKS	1,063,938	995,436
IRVING	30,592,472	28,650,492
PLANO	29,649,132	27,780,189
RICHARDSON	19,262,574	18,137,867
ROWLETT	1,342,206	1,256,044
UNIVERSITY PARK	1,789,593	1,667,149
TOTALS	291,642,469	273,484,269

**BASIS FOR MONTHLY ALLOCATION
JULY 1995 THROUGH JUNE 1996**

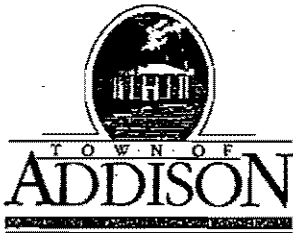
MONTH	TOTAL FOR MEMBER CITIES	TOTAL FOR DART	MONTHLY PERCENTAGE
JULY 95	18,866,446	17,881,968	94.78%
AUG 95	20,468,224	19,350,287	94.54%
SEP 95	30,049,237	25,901,871	86.20%
OCT 95	18,623,764	17,817,428	95.67%
NOV 95	20,274,596	19,664,539	96.99%
DEC 95	34,182,157	31,379,182	91.80%
JAN 96	21,418,481	20,196,145	94.29%
FEB 96	21,872,614	20,771,068	94.96%
MAR 96	29,883,850	26,501,530	88.68%
APR 96	22,615,962	21,275,935	94.07%
MAY 96	21,891,920	20,435,584	93.35%
JUN 96	31,495,219	32,308,732	102.58%
TOTALS	291,642,469	273,484,269	93.77%

Jim

ADDISON CAPITAL ASSISTANCE PROGRAM

PROJECT #	PROJECT NAME	DATE APPROVED	APPROVED AMOUNT	AMOUNT TO BE REPROGRAMMED	EXPENDITURES DURING FY88-89	EXPENDITURES DURING FY91-92	EXPENDITURES DURING FY93-94	EXPENDITURES DURING FY95	EXPENDITURES DURING FY96	TOTAL AMOUNT PAID PER PROJECT	PROGRAMMED FUNDS NOT YET PAID
	TRANSFER TO TAP ACCOUNT FOR PAVEMENT MGMT. & THOROUGH-FARE PLAN STAFF SALARIES Project Completed	12/19/89	\$18,712.40 \$5,859.60		\$24,572.00					\$24,572.00	\$0.00
	TRANSFER TO TAP ACCOUNT TO HELP FUND SALARY FOR CITY ENGINEER Project Completed	10/23/90	\$4,672.93			\$4,672.93				\$4,672.93	\$0.00
	BELT LINE/QUORUM INTERSECTION Additional Funding	10/23/90 04/11/95	\$187,000.00 \$153,000.00							\$0.00	\$340,000.00
	BELT LINE/MIDWAY INTERSECTION Additional Funding	10/23/90 04/11/95	\$78,806.00 \$355,000.00							\$0.00	\$433,806.00
	BELT LINE/ADDISON RD INTERSECT ADDITIONAL FUNDS Payment Payment Payment Project Completed Under Budget	10/23/90 11/26/91 03/17/93 11/01/93 04/11/95 04/11/95	\$132,960.00 \$63,040.00			\$12,605.00 \$151,277.36	\$170.00			\$164,052.36	\$0.00
	MONTFORT/BELT LINE INTERSECT Payment Payment Payment Payment Project Completed Under Budget	10/23/90 03/17/93 02/10/94 10/04/94 04/11/95 04/11/95	\$125,000.00			\$19,926.00 \$56,064.42 \$8,128.66	\$3,622.75			\$87,741.83	\$0.00
	TRAFFIC SIGNAL IMPROVEMENTS Payment Project Completed 10/4/94	10/23/90 10/04/94	\$20,400.00			\$20,400.00				\$20,400.00	\$0.00
	SIDEWALK/BUS BENCH PADS Project Completed Under Budget	11/26/91 04/11/95	\$41,000.00			\$34,404.00 \$3,384.30				\$37,788.30	\$0.00
	ADDISON/WESTGROVE INTERSECT. Payment Payment Payment	11/26/91 03/17/93 04/11/95 03/14/96	\$125,000.00			\$8,500.00	\$86,955.18	\$14,163.23		\$109,618.41	\$15,381.59
	MIDWAY/LINDBERG INTERSECT. Payment	11/26/91 03/17/93	\$150,000.00			\$10,830.00				\$10,830.00	\$139,170.00
	MIDWAY/BELTWAY INTERSECT. Payment	11/26/91 03/17/93	\$45,000.00			\$2,925.00				\$2,925.00	\$42,075.00
	MIDWAY/GREENHILL INTERSECT. Payment	11/26/91 03/17/93	\$75,000.00			\$4,280.00				\$4,280.00	\$70,720.00
	BELT LINE/MARSH INTERSECT. (& first Additional Funds Payment Payment Payment	11/26/91 06/14/94 09/30/94 04/11/95 03/14/96	\$550,000.00 \$250,000.00			\$19,590.00 \$635,526.80	\$5,525.00	\$86,845.27		\$747,467.07	\$52,512.93
	TRANSFER TO TAP FOR CITY ENGINEER/PLANNER POSITION	11/26/91 09/30/94	\$8,701.00			\$8,701.00				\$8,701.00	\$0.00

THOROUGHFARE PLAN	2/25/92 03/17/93 11/01/93	\$37,500.00				\$37,500.00	\$0.00
Payment		\$33,750.00					
Payment		\$3,750.00					
BELT LINE/DALLAS NORTH TOLLWAY	06/22/93 6/22/93 11/01/93 02/10/94 04/11/95 03/14/96	\$262,000.00				\$238,217.72	\$6,022.28
Payment		\$13,770.00					
Payment		\$3,670.00			\$120.00		
Payment							
Payment							
KELLER SPRINGS/DALLAS NORTH TOLLWAY INTERSECTION	6/22/93 04/11/95 04/11/95	\$226,150.00					
Payment		\$35,825.00					
Additional funds transferred from Bellline Quorum and Beltline/Midway Projects		\$68,205.47					
Payment	03/14/96				\$232,043.44		\$27,487.03
EXT ARAPAHO FROM NO DALLAS TOLLWAY TO MARSH LANE PHASE I	10/28/93 02/10/94	\$255,000.00					\$0.00
Payment		\$255,000.00					
WEST GROVE/SOJOURN INTER. IMP	02/15/94 09/30/94 04/11/95 03/14/96	\$175,000.00				\$16,492.77	\$48,979.23
Payment		\$15,570.00					
Payment					\$93,958.00		
Payment							
ARAPAHO REALIGNMENT/EXTENSIO	02/15/94 09/13/94 10/01/94 04/11/95 03/14/96 10/01/95 05/20/96	\$803,000.00 \$1,200,000.00 \$900,000.00				\$9,500.00	\$2,775,851.30
Additional Funds		\$803,000.00					
Additional Funds		\$1,200,000.00					
Payment		\$900,000.00					
Payment		\$184,715.00					
Payment							
Additional Funds		\$520,559.00					
Payment					\$553,492.70		
BELT LINE RD. 7 BUSINESS DR.	02/15/94 06/14/94 10/04/94 04/12/95	\$200,000.00 \$50,000.00					\$0.00
Additional Funds		\$200,000.00					
Payment		\$50,000.00					
Payment (and Project Complete)					\$25,000.00		
TOTAL		\$72,417.51	\$24,572.00	\$4,672.93	\$1,547,252.54	\$435,890.93	\$1,150,755.13
TOTAL DISBURSEMENTS ALL YEARS			\$3,163,143.53				\$3,951,805.36



PUBLIC WORKS DEPARTMENT

Post Office Box 144 Addison, Texas 75001

(214) 450-2871

16801 Westgrove

March 13, 1995

Mr. Tom Ryden
Assistant Director
Bus Project Development
1401 Pacific Ave.
P.O. Box 66013
Dallas, Texas 75266-7212

Re: DART/LAP Quarterly Report - September 1994/December 1994
Request for reimbursement

This report is intended to update the status of the DART/LAP projects and serve as a request for reimbursement.

1) Midway/Lindberg Intersection - Engineering for this project is 90% complete. We are in the process of reviewing plans and acquiring right-of-way.

DART Approved Project	\$ 150,000.00
Reimbursement to date	\$ 10,830.00

2) Midway/Beltway Intersection - Engineering for this project is 90% complete. We are in the process of reviewing plans and acquiring right-of-way.

DART Approved Project	\$ 45,000.00
Reimbursement to date	\$ 2,925.00

3) Addison/Westgrove Intersection - This project is substantially complete.

DART Approved Project	\$ 125,000.00
Engineering - EHA	\$ 8,500.00
Construction Jim Bowmann Const. (90% of \$91,160.75)	\$ 82,045.00
Land Acquisition - AATI	\$ 3,180.18
Hall Financial	\$ 1,730.00
Reimbursement to Date	\$ 8,500.00
Reimbursement requested this report	\$ 86,955.18

4) Midway/Greenhill Intersection - This project is on hold.

DART Approved Project	\$ 75,000.00
Reimbursement By DART to date	\$ 4,280.00

5) Belt Line/Addison Intersection - This project is complete and all outstanding invoices have been paid.

DART Approved Project	\$ 196,000.00
Engineering - Espey Huston	\$ 12,605.00
Construction - Gibson & Assoc.	\$ 149,599.00
Cobb Enterprises	\$ 1,678.00
Land Acquisition - Pat Haggerty Co.	\$ 170.00
Total Project Costs	\$ 164,052.00
Reimbursements by DART to date	\$ 163,882.36
Reimbursement requested this report	\$ 170.00

This leaves approximately \$ 31,948.64 for this project. Please transfer these funds to the Keller Springs/North Dallas Tollway Intersection project.

6) Belt Line/Marsh Intersection - This project is substantially complete.

DART Approved Project	\$ 800,000.00
Engineering - Espey Huston	\$ 19,590.00
SMR Landscaping	\$ 4,358.09
Land Acquisition - Mobil	\$ 58,501.00
TCB Trust	\$ 70,977.71
Watson and Taylor Realty Company	\$ 5,184.00
Aetna	\$ 15,855.00
Pat Haggerty Co.	\$ 2,975.00
Cowles & Thompson	\$ 2,550.00
Construction - Ed Bell Construction (90% of \$511,889)	\$ 460,700.00
Valmont Industries (67% of \$29,778 for 4 signals poles at Belt Line and Marsh)	\$ 19,951.00
Reimbursement by DART to date	\$ 655,116.80
Reimbursement requested this report.	\$ 5,525.00

7) **Belt Line/Montfort Intersection** - This project is complete. The remaining reimbursement consists of costs associated with land acquisition.

DART Approved Project	\$ 125,000.00
Engineering - Barton-Aschman	\$ 19,926.30
Contractors - Bowman Construction	\$ 60,409.97
Hoover Landscape	\$ 1,377.75
SMR Landscaping	\$ 2,405.06
Land Acquisition - Bernie, Maynard & Parsons, Ltd.	\$ 2,687.75
Pat Haggerty Co. Inc.	\$ 935.00
Total Project Costs	\$ 87,741.83
Reimbursement by DART to date	\$ 84,119.08
Reimbursements requested this report	\$ 3,622.75

Please release the remaining \$37,258.17 for use the Keller Springs/North Dallas Tollway project.

8) **Keller Springs/Dallas North Tollway Intersection** - We are currently negotiating the engineering contract and discussing the right-of-way acquisition with the two affected property owners. Anticipate design starting in September and construction in January.

DART Approved Project	\$ 226,150.00
Engineering - Lichlighter/Jameson	\$ 35,825.00
Reimbursement to Date	\$ 0.00
Reimbursement requested this report	\$ 35,825.00

Please increase the DART approved project by the \$31,948.64 remaining from the Belt Line/Addison Road Intersection and \$37,258.17 from the Belt Line/Montfort Intersection.

9) **Belt Line/North Dallas Tollway Intersection** - Engineering is complete. We are in the process of acquiring the right-of-way and letting the project.

DART Approved Project	\$ 262,000.00
Engineering Fee (90% of \$ 15,300)	\$ 13,770.00
Appraisal Fee (90% of \$ 4,300)	\$ 3,870.00
Land acquisition - Cowles & Thompson	\$ 120.00
Reimbursement to Date	\$ 17,640.00
Reimbursement Requested this report	\$ 120.00

10) **Belt Line/Midway Intersection** - This project is a transit pass project. Engineering was started in late Spring 1994 by Barton-Aschman for DART and the environment assessment was submitted to the State for approval. We hope to start right-of-way acquisition in the spring.

11) **Belt Line/Quorum Intersection** - This project is a transit pass project. Engineering was started in late Spring 1994 by Barton Aschman for DART and the environment assessment was submitted to the State for approval. We hope to start right-of-way acquisition in the spring.

12) **Business/Belt Line Intersection Improvements** - This project is substantially complete.

DART Approved Project	\$ 250,000.00
Construction - Ed Bell Construction	\$ 225,000.00
Reimbursement to date	\$ 225,000.00
Reimbursement Requested this report	\$ 25,000.00

13) **Westgrove/Sojourn Intersection Improvement** - This project was awarded in January with construction anticipated to start in February.

DART Approved Project	\$ 175,000.00
Engineering Fee - Barton Aschman (90% of 17,300)	\$ 15,570.00
Construction - Innovated Systems Inc. (90% of \$103,819.60)	\$ 93,958.00
Reimbursement to Date	\$ 15,570.00
Reimbursement Requested this report	\$ 93,958.00

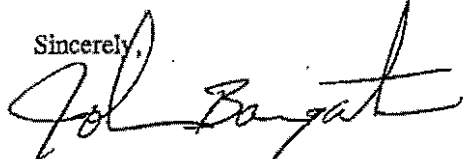
Page 5
March 13, 1995
Mr. Tom Ryden

14) **Arapaho Road Realignment/Extension** - The Town of Addison has authorized the engineer to proceed with the design of this project.

DART Approved Project	\$ 2,003,000.00
Engineering Fee - Huitt - Zollar (90% of \$199,239)	\$ 179,315.00
Land Acquisition - Appraisal Consultation Group	\$ 5,400.00
Reimbursement to Date	\$ 0.00
Reimbursement Requested this report	\$ 184,715.00

Please call me if you have any questions or need additional information regarding these projects.

Sincerely,



John R. Baumgartner, P.E.
Director of Public Works

JRB/amh

Attachments: Addison/Westgrove
- Jim Bowman Contract
- AATI Deed
- Hall Financial Deed

Belt Line/Addison
- Pat Haggerty Co. Invoice

Belt Line/Marsh
- EHA Agreement
- SMR Invoice
- Mobil Deed
- TCB Trust Deed/Closing statement
- AETNA Deed
- Watson and Taylor Deed
- Ed Bell Construction contract with unit prices
- Valmont Invoice
- Cowles & Thompson Invoice - 6
- Pat Haggerty Company Invoice

Belt Line/Montfort
- Barton/Aschman Invoices - 8
- SMR Invoice
- Bowman Construction Final Invoice
- Hoover Landscape Invoice
- Pat Haggerty Company Invoice - 3
- Bernie, Maynard & Parsons Ltd. Invoice

Keller Springs/Dallas North Tollway
- Lichlighter Jameson Agreements - 2

Belt Line/North Dallas Tollway
- Cowles & Thompson Invoice

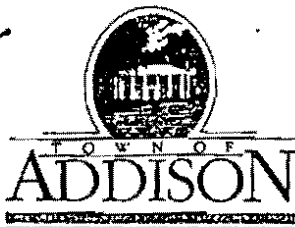
Belt Line/Business
- Ed Bell Construction Invoice #7

Westgrove/Sojourn
- Barton Aschman Engineering Agreement
- Innovated Systems Inc. Agreement

Arapaho Road Realignment
- Huitt-Zollar Contract
- Appraisal Consultation Group Invoice

**DART/LAP REIMBURSEMENT
REQUEST NUMBER 5
MARCH 13, 1995**

Addison/Westgrove Intersection	\$ 86,955.18
Belt Line/Addison Intersection	\$ 170.00
Belt line/Marsh Intersection	\$ 5,525.00
Belt Line/Montfort Intersection	\$ 3,622.75
Keller Springs/Dallas North Tollway	\$ 35,825.00
Belt Line/North Dallas Tollway	\$ 120.00
Belt Line/Business Intersection	\$ 25,000.00
Westgrove/Sojourn Intersection	\$ 93,958.00
Arapaho Road Realignment	\$ 184,715.00
 Total Reimbursement Requested	 \$ 435,890.93



PUBLIC WORKS DEPARTMENT

Post Office Box 144 Addison, Texas 75001

February 13, 1995

Mr. Tom Ryden
Assistant Director - Bus Project Development
Dallas Area Rapid Transit
P.O. Box 660163
Dallas, Texas 75266-0163

Re: 1993 - 1994 DART - TAP Report
Proposed 1994 -- 1995 DART - TAP Program

Dear Tom:

Attached for your review/approval is a copy of Addison's proposed Technical Assistance Program for the fiscal year 1994 - 1995.

The only programmed expenditure of the 1993 - 94 TAP funds was the support of the Town's transportation/engineering staff. Both positions were utilized in planning and implementing the DART - LAP work program during the 1993 - 94 fiscal year.

Please call me if you have any questions or need additional information.

Sincerely,

John R. Baumgartner, P.E.
Director of Public Works

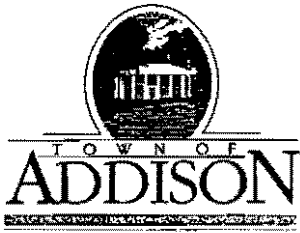
JRB/amh
Attachment

JRB copy

*Mailed
Attachment
for
File*

APPLICATION FOR LOCAL ASSISTANCE PROGRAM

TYPE OF SUBMISSION (1) <input type="checkbox"/> Capital (C) <input checked="" type="checkbox"/> Technical (T) <input type="checkbox"/> Revision (R)		DATE SUBMITTED (2) February 13, 1995 <i>May 7, 1996</i>	
APPLICANT INFORMATION		CATEGORY CODE (see reverse) (3) _____ (4) _____ (5) _____ (6) _____ (7) _____ (8) _____	
LEGAL NAME (9) Town of Addison		ORGANIZATIONAL UNIT (10) Town of Addison-Engineering Department	
ADDRESS P. O. Box 144 City <u>Addison</u> (11) County <u>Dallas</u> (12) State <u>Texas</u> (13) Zip Code <u>75001</u> (14)		CONTACT PERSON Name: <u>John Baumgartner</u> Telephone: <u>(214) 450-2886 450-2871</u>	
PROJECT TITLE (15) Technical Assistance Program City Engineer/Planner Position			
PROJECT DESCRIPTION AND JUSTIFICATION (16) Partial funding of the City Engineer and a <i>Capital Projects Manager</i> Civil Engineer position. The duties of these positions include the following: <ol style="list-style-type: none"> 1.) Town of Addison DART-LAP coordinator 2.) Plan, administer and implement DART-LAP program. <p>NOTE: These funds provide approximately ¹⁷20% of the funding of the salary of these positions.</p> <p style="text-align: center; font-size: 1.2em;"><i>19,956.85 + accrued interest</i></p>			
PROPOSED PROJECT Start Date _____ (17) Ending Date _____ (18)			
ESTIMATED FISCAL YEAR FUNDING		CONSTRUCTION COST DISTRIBUTION	
Federal \$ _____ (19)	Engineering/Design \$ _____ (26)		
City \$ <u>98,043.15</u> (20)	Right-of-Way \$ _____ (27)		
State \$ <u>77,325.00</u> (21)	Construction \$ _____ (28)		
Local \$ _____ (22)	Other \$ _____ (29)		
Other \$ _____ (23)	Total \$ _____ (30)		
DART \$ <u>27,675.00</u> (24)			
TOTAL \$ <u>105,000.00 118,000</u> (25)			
SIGNATURE OF AUTHORIZED REPRESENTATIVE <i>John Baumgartner</i>		TITLE Director Public Works	DATE SIGNED <u>2/13/95</u>
FOR DART USE ONLY			
To DART Staff _____	Date _____	To P&D Committee _____	Date _____
To TASC _____	_____	P&D Review _____	_____
TASC Review _____	_____	Approved by P&D _____	_____



PUBLIC WORKS DEPARTMENT

Post Office Box 144 Addison, Texas 75001

(214) 450-2871

16801 Westgrove

August 17, 1994

Mr. Tom Ryden
Assistant Director
Bus Project Development
Dallas Area Rapid Transit
P.O. Box 66013
Dallas, Texas 75266-7212

Re: Arapaho Road
DART-LAP Allocation

Dear Tom:

As we have discussed, the town of Addison wishes to utilize its DART-LAP funds on our Arapaho realignment/extension project. Addison's single most important mobility need is relief to the congestion on Belt Line Road. We have studied several alternatives for relieving the congestion. The Arapaho Road realignment /extension is clearly the most cost effective based on the impact the improvements would have on adjacent properties, and on the benefits realized by users of mass transit.

Reducing the congestion on Belt Line Road can be accomplished through two approaches, increasing the capacity of the roadway or reducing the number of vehicles using the roadway. The three methods to increase the capacity are signal/intersection improvements, grade separated intersections, and additional through lanes. The Town is in the process of initiating improvement projects at the major intersections on Belt Line Road. These improvements include new signals and additional right and left turn lanes. The Town feels the improvements are the most that can be done to increase capacity given the constraints of the developed properties along Belt Line.

Grade separated intersections and additional through lanes on Belt Line Road are unacceptable options from a cost standpoint, and the impact the improvements would have on the adjacent properties. Both options would require the acquisition of large amounts of right-of-way. The construction of additional through lanes would eliminate parking and some of the landscape buffer areas along Belt Line Road. Parking for most of the businesses and restaurants along Belt Line Road is already a precious commodity. The elimination of parking areas would likely cause the Town to pay damages in addition to the cost of the land to many of the businesses. Addison has high standards and prides itself on the landscaping that accents the streets and businesses.

Grade separated intersections would devastate the existing businesses and restaurants along Belt Line Road. Frontage roads would be required to provide for turning movements at the intersections. In some instances the taking would require the Town to acquire the entire parcel and improvements possibly causing that business to relocate to another community. The cost of the right-of-way would be several times more than the cost of the roadway improvements. The goal of any project that Addison would support must be to enhance and benefit the adjacent properties and businesses, not to negatively impact them.

Page Two
Tom Ryden
August 17, 1993

If the capacity of Belt Line Road cannot be further increased, then the other option is to reduce the number of vehicles using the roadway. This can be accomplished through the increased use of mass transit and the construction of an alternate east-west route. The locations for additional east-west routes is limited by the Addison Airport and the railroad spur lying parallel to Inwood Road, which divide the Town.

Addison supports mass transit and hopes that commuters will increasingly use the systems presently available. Obviously, DART is attempting to improve the availability and access to mass transit in this area through its proposed DART Addison Transit Center, which will be located on Quorum Drive just north of Arapaho Road. The commuters and buses travelling to and from the transit center will contribute to the need for an additional east-west route near Belt Line Road.

Arapaho Road when completed will provide an alternate east-west route immediately north of Belt Line Road from Marsh Lane to east of Central Expressway. It will provide another route for those traveling to the Tollway and those circulating within Addison. Arapaho will directly benefit mass transit users living in Addison, Farmers Branch, Carrollton, and north Dallas by providing another route to the proposed transit center and by reducing the congestion on Belt Line Road. Attached is a report prepared by Barton-Aschman & Associates that supports the project and identifies the benefits realized by the transit vehicles.

The Arapaho Road realignment/extension is planned in three phases:

Phase I	Dallas North Tollway to Addison Road
Phase II	Addison Road to Midway Road
Phase III	Midway Road to Marsh Lane

The realignment/extension of Arapaho Road is not the construction of a complete new road. Approximately, one-third of the road will occupy existing rights-of-way for Arapaho Road, Centurion Way, and Realty Drive.

We have retained Huitt-Zollars, Inc. to design phase I. DART has approved \$1,058,000 in LAP funding for half of phase I, from the Dallas North Tollway to Quorum Drive. Attached is an application for \$1,200,000 of DART-LAP funding for the remainder of phase I, from Quorum Drive to Addison Road. It is our understanding that the \$1,200,000 is the unprogrammed amount of our LAP allocation.

We are also requesting DART agree that the funding of the second and third phases of the Arapaho Road Project with our DART-LAP allocations for fiscal years 1995 & 1996 is an appropriate use for DART LAP funds. We understand that Addison will have to submit separate LAP applications for the future phases as the allocations become available.

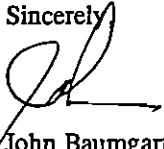
The estimated construction and right-of-way costs for phases two and three total approximately \$11,000,000. Assuming Addison's 1995 and 1996 LAP allocations total approximately \$1,800,000, Addison will still be required to fund approximately \$9,200,000, or 83% of the entire project, through bonds, landowner dedications, and other grants. We are continuing to obtain right-of-way dedications as property along the route develops.

Page Three
Tom Ryden
August 17, 1994

In summary, Addison feels the most cost effective solution to reducing congestion on Belt Line Road is the realignment/extension of Arapaho Road from the Dallas North Tollway to Marsh Lane. Arapaho Road will reduce the congestion on Belt Line Road by providing an alternate east-west route. The reduced congestion will directly benefit the DART bus routes by reducing the travel time on Belt Line Road. Arapaho will also provide better access to the proposed Addison Transit Center and provide routing flexibility for existing or proposed bus routes. The use of DART-LAP funds for this project is justified by the benefits it provides those traveling on Belt Line Road and the commuters using DART.

If you have any questions or need additional information please call me.

Sincerely



John Baumgartner, P.E.
Director of Public Works

DCN

B:\Ryden.Jtr

APPLICATION FOR LOCAL ASSISTANCE PROGRAM

TYPE OF SUBMISSION (1) <input checked="" type="checkbox"/> Capital (C) <input type="checkbox"/> Technical (T) <input type="checkbox"/> Revision (R)	DATE SUBMITTED (2) June 24, 1994 CATEGORY CODE (see reverse) (3) <u> 2 </u> (4) <u> 14 </u> (5) <u> </u> (6) <u> </u> (7) <u> </u> (8) <u> </u>
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APPLICANT INFORMATION	
LEGAL NAME (9) Town of Addison	ORGANIZATIONAL UNIT (10) Town of Addison - Engineering Dept.
ADDRESS P.O. Box 144 City <u> Addison </u> (11) County <u> Dallas </u> (12) State <u> Texas </u> (13) Zip Code <u> 75001 </u> (14)	CONTACT PERSON Name: <u> John Baumgartner </u> Telephone: <u> (214) 450-2886 </u>

PROJECT TITLE (15)
 Arapaho Road Extension, from Quorum Drive to Addison Road

PROJECT DESCRIPTION AND JUSTIFICATION (16)

- This project is an extension of the Arapaho Road realignment project. It is a continuation of Phase I of the extension of Arapaho Road from the Dallas North Tollway to Marsh Lane.
- See attached report from Barton - Aschman & Associates.
- Funding requested for engineering, and construction costs, and right-of-way.
- DART Bus Routes: 400 & 333
- Request DART agree that the use of Addison's '95 & '96 DART LAP allocations to fund phases II & III of the Arapaho Project is an acceptable use of DART LAP funds.
- Future LAP requests for phases II & III (estimated)
 1995 - \$900,000 1996 - \$900,000

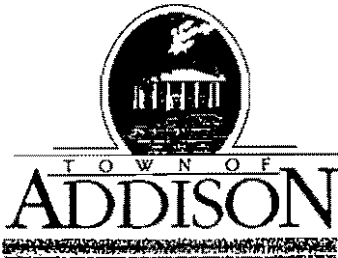
PROPOSED PROJECT
 Start Date September, 1994 (17) Ending Date March, 1996 (18)

ESTIMATED FISCAL YEAR FUNDING		CONSTRUCTION COST DISTRIBUTION	
Federal	\$ (19)	Engineering/Design	\$ (26)
City	\$ (20)	Right-of-Way	\$ 600,000 (27)
State	\$ (21)	Construction	\$ 600,000 (28)
Local	\$ (22)	Other	\$ (29)
Other	\$ (23)	Total	\$ 1,200,000 (30)
DART	\$ 1,200,000 (24)		
TOTAL	\$ 1,200,000 (25)		

SIGNATURE OF AUTHORIZED REPRESENTATIVE 	TITLE Director of Public Works	DATE SIGNED 8/17/94
---	--	-------------------------------

FOR DART USE ONLY

To DART Staff _____ To TASC _____ TASC Review _____	To P&D Committee _____ P&D Review _____ Approved by P&D _____
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FINANCE DEPARTMENT

(214) 450-7051

Post Office Box 144 Addison, Texas 75001

5350 Belt Line Road

August 2, 1994

Christopher A. Poinsatte
Chief Financial Officer
Dallas Area Rapid Transit
P.O. Box 660163
Dallas, Texas 75266-0163

Dear Chris:

Please find enclosed the diskette with a completed cash flow schedule for Addison's Capital Assistance Program. The changes were made to addcap.wk4. Also enclosed is a copy of the printed schedules. Please be advised the amounts reported on the schedule are estimates and subject to change. Any material changes will be brought to your attention on revised schedules. If you have any questions concerning this information, please contact me.

Sincerely yours,


Randolph C. Moravcc, CGFO
Finance Director

RCM:rm

Enclosure: Diskette, Copies of Schedules

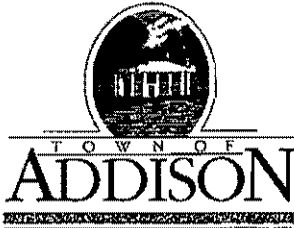
Copies (schedules only): Ron Whitehead, City Manager
John Baumgartner, Public Works Director

ADDISON - CAPITAL ASSISTANCE PROGRAM

Report thru March 31, 1994

Schedule 1 - Annual Cash Flow Summary	3/31/94	9/30/94	9/30/95	9/30/96	9/30/97	9/30/98	9/30/99	Out Years
Balance Forward	5,384,919	4,721,748	3,522,085	1,652,422	906,962	1	1	1
Add:								
Future Allocations (Note 1)	n/a	n/a	862,400	646,800	n/a	n/a	n/a	n/a
Future Interest Earnings at 5%	n/a	103,048	129,363	63,985	22,674	0	0	unknown
Less:								
Payments on Approved Projects (Sch 2)	(663,171)	(1,057,711)	(1,706,426)	(56,245)	0	0	0	0
Payments on Future Projects (Sch 3)	n/a	(245,000)	(1,155,000)	(1,400,000)	(929,635)	0	0	0
Total Available	4,721,748	3,522,085	1,652,422	906,962	1	1	1	1

(1) Per DART Estimate of Allocation letter dated 05/19/94



PUBLIC WORKS DEPARTMENT

(214) 450-2871

Post Office Box 144 Addison, Texas 75001

16801 Westgrove

June 24, 1994

Mr. Tom Ryden
Assistant Director
Bus Project Development
Dallas Area Rapid Transit
P.O. Box 66013
Dallas, Texas 75266-7212

Re: Arapaho Road
DART-LAP Allocation

Dear Tom:

As we have discussed, the town of Addison wishes to utilize its DART-LAP funds on our Arapaho realignment/extension project. Addison's single most important mobility need is relief to the congestion on Belt Line Road which adversely affects the transit operations. We have studied several alternatives for relieving the congestion. The Arapaho Road realignment /extension is clearly the most advantageous from a cost standpoint and the impact the improvements would have on adjacent properties.

Reducing the congestion on Belt Line Road can be accomplished by one of two ways, increasing the capacity of the roadway or reducing the number of vehicles using the roadway. The three methods to increase the capacity are signal/intersection improvements, grade separated intersections, and additional through lanes. The Town is in the process of initiating improvement projects at the major intersections on Belt Line Road. These improvements include new signals and additional right and left turn lanes. The Town feels that capacity of Belt Line Road will have been maximized with the completion of these intersection improvements.

Grade separated intersections and additional through lanes on Belt Line Road are unacceptable options from a cost standpoint, and the impact the improvements would have on the adjacent properties. Both options would require the acquisition of large amounts of right-of-way. In almost all cases, the needed right-of-way is presently used as a landscape buffer and for parking areas. Unfortunately, the space does not exist between the existing buildings and the roadway to relocate the landscaping and parking areas. The Town has high standards and prides itself on the landscaping that accents the streets and businesses. The elimination of the greenspace between the roadway and businesses is unacceptable.

Parking for most of the businesses and restaurants along Belt Line Road is already a precious commodity. The elimination of parking areas would likely cause the Town to pay damages in addition to the cost of the land to many of the businesses. In some instances the taking would require the Town to acquire the entire parcel and improvements. The cost of the right-of-way would be several times more than the cost of the improvements.

Belt Line Road is the center of the Town's commerce. The goal of any project that Addison would support must be to enhance and benefit the adjacent properties and businesses, not to harm them. The addition of lanes or the construction of grade separated intersections would eliminate many businesses. The improvements would also destroy the aesthetic aspects of Belt Line Road that distinguishes it from the extensions of the road in the adjoining cities.

Page Two
Tom Ryden
June 22, 1994

The Arapaho Road Project does present an option that will reduce the number of cars on Belt Line Road. Arapaho Road when completed to Marsh Lane will provide an alternate east-west route north of Belt Line Road. It will provide another route for those traveling to the Tollway and those circulating within Addison. Arapaho will also provide easy access to the proposed DART Addison Transit Center for those living in Addison, Farmers Branch, and Carrollton. Attached is a report prepared by Barton-Aschman & Associates that supports the project and identifies the benefits realized by the transit vehicles.

The Arapaho Road realignment/extension is planned in three phases:

Phase I	Dallas North Tollway to Addison Road
Phase II	Addison Road to Midway Road
Phase III	Midway Road to Marsh Lane


We have retained Huitt-Zollars, Inc. to design phase I. Design will start shortly. DART has approved \$1,058,000 in LAP funding for half of phase I, from the Dallas North Tollway to Quorum Drive. Attached is an application for \$1,200,000 of DART-LAP funding for the remainder of phase I, from Quorum Drive to Addison Road. It is our understanding that the \$1,200,000 is the unprogrammed amount of our LAP allocation.

We are also requesting DART to approve the use of our DART-LAP allocations for fiscal years 1995 & 1996 to partially fund the second and third phases of the Arapaho Project. The estimated construction and right-of-way costs for phases two and three total approximately \$11,000,000. Assuming Addison's 1995 and 1996 LAP allocations total approximately \$1,800,000, Addison will still be required to fund approximately \$9,200,000, or 83% of the entire project, through bonds, landowner dedications, and other grants. We are continuing to obtain right-of-way dedications as property along the route develops.

In summary, Addison feels the most cost efficient solution to reducing congestion on Belt Line Road is the realignment/extension of Arapaho Road to Marsh Lane. Arapaho Road will reduce the number of cars on Belt Line Road by providing an alternative east-west route. The reduced congestion will directly benefit the DART bus routes by reducing the travel time on Belt Line Road. Arapaho will also provide better access to the proposed Addison Transit Center. The use of DART-LAP funds for this project is justified by the benefits it provides those traveling on Belt Line Road and the commuters using DART.

If you have any questions or need additional information please call me.

Sincerely,




John Baumgartner, P.E.
Director of Public Works

DCN

B:\Ryden.ltr

TECHNICAL MEMORANDUM

To: John Baumgartner
From: Gary Jost 
Karen George
Date: May 27, 1994
Subject: Analysis of the Extension of Arapaho Road

This memorandum presents the findings of an evaluation of the extension of Arapaho Road in Addison, Texas. This study evaluates the benefits of the extension of Arapaho Road to DART service in the area.

BACKGROUND

The following sections provide information on existing travel demand, alternatives for enhancing east-west capacity, and existing transit routes.

EXISTING TRAVEL DEMAND

Presently, the demand for east-west capacity is served predominantly by Belt Line Road. Three factors have resulted in a heavily traveled arterial with significant congestion during the peak hours:

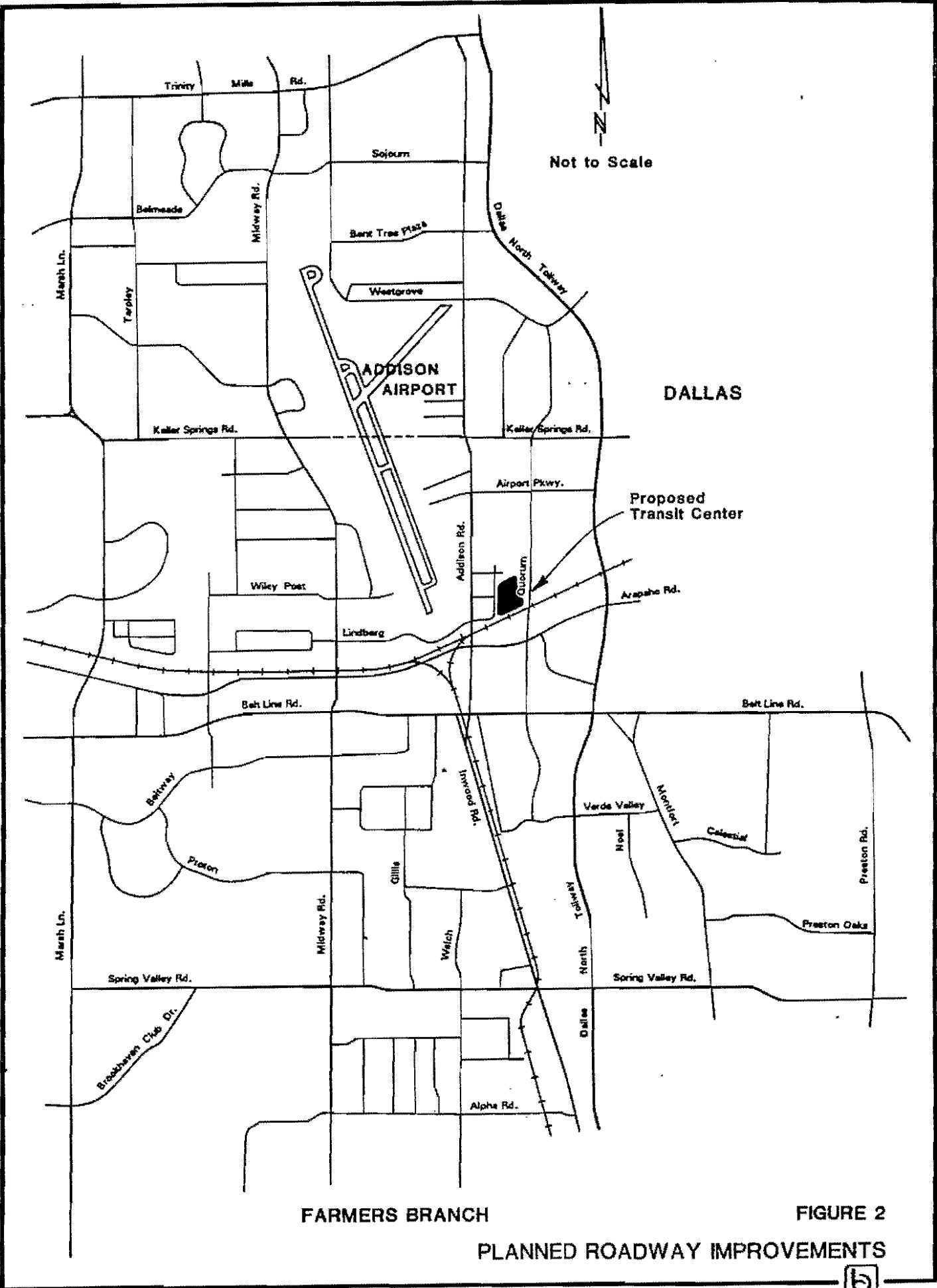
- (1) The combination of continuity from I-35E on the west to US 75 on the east,
- (2) a concentration of employment, retail, and residential centers along its route, and
- (3) an interchange with the Dallas North Tollway (DNT).

Recent traffic counts conducted along Belt Line Road in November 1994 revealed 24-hour volumes in excess of 50,000 vehicles, well above the design capacity of the roadway. Traffic volumes have increased along the roadway at annual rate of over three percent (3%).

ADDITIONAL EAST-WEST CAPACITY PROJECTS

The existing roadway system is shown in Figure 1. There have been several alternatives discussed for providing additional east-west capacity within the Town. These alternatives, as well as other transportation needs, were analyzed in December 1992, as part of the development of a comprehensive transportation plan to meet the existing and future needs of the Town. This plan was adopted by the Town's Council in January, 1993.

In order to provide additional east-west capacity, the Thoroughfare Plan includes both the planned Keller Springs Toll Tunnel and the extension of Arapaho Road from the DNT to Marsh Lane. Existing traffic volumes on Belt Line Road within the Town limits have prompted the



FARMERS BRANCH

FIGURE 2

PLANNED ROADWAY IMPROVEMENTS



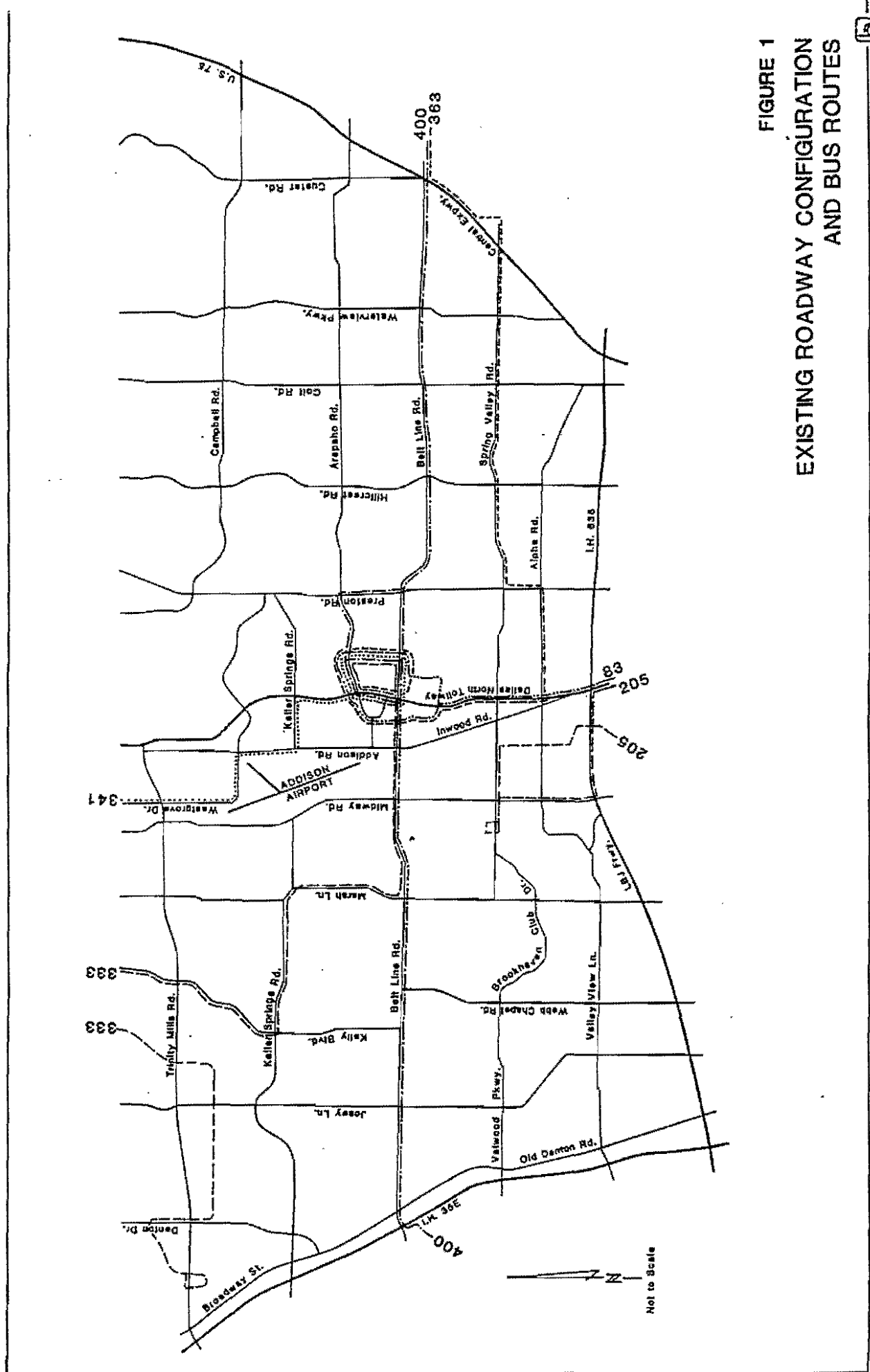


FIGURE 1
 EXISTING ROADWAY CONFIGURATION
 AND BUS ROUTES

construction of additional turning lanes at several of Belt Line Road's major intersections. Projected traffic demand will necessitate improvements of Belt Line Road beyond the implementation of additional turning lanes. The cost of improvements on Belt Line Road necessary to meet projected east-west demand would easily exceed the cost of Arapaho Road extension. Because of this, as well as the impact to adjacent properties and aesthetic considerations, the Town elected not to proceed with the additional improvements of Belt Line Road. Figure 2 illustrates the planned improvements in the area which were approved by Town Council.

Keller Springs

Keller Springs Road currently extends to the east and west from Addison Airport. Keller Springs Road extends from the Airport past Preston Road to Campbell Road on the east side of the Airport. On the west, the road continues through Carrollton to I-35E. Tunnelling under the Airport has been identified as the only feasible way to connect the eastern and western sections of Keller Springs while maintaining Airport operations. Because of the substantial cost of such a project, funding is proposed through tolls. A two-lane (one lane in each direction) tunnel with a \$.50 toll is planned. At this time, the bonds for design and construction of this facility are being sold. The planned Keller Springs Toll Tunnel chiefly links areas west of the Airport to the DNT.

Arapaho Road

Arapaho Road is a continuous six-lane divided arterial from the DNT to US 75. West of the DNT, Arapaho intersects with Spectrum at a four-way stop, right angle intersection. The planned Arapaho extension would extend Arapaho as a four-lane divided arterial west from the DNT to Marsh Lane. The extension of Arapaho is scheduled to occur in three phases. The first phase, the realignment of Arapaho from DNT to Addison, was recently let for design. The second phase, extending Arapaho from Addison to Midway is planned for letting within the next three to four years. The third phase, extending Arapaho from Midway to Marsh is planned within the next five to ten years. The realignment and extension of Arapaho Road will serve as a relief facility for Belt Line Road and will provide a continuous circulation route for adjacent land uses.

EXISTING TRANSIT SERVICE

Currently, several bus routes traverse the study area, including routes 83, 205, 333, 341, 363 and 400. Significant transfer activity is made in the area of the existing Prestonwood Pulse Point on Prestonwood Boulevard. These transferring activities are scheduled to be relocated to a new Transfer Center located north of the Cotton Belt Rail Line and west of Quorum Drive, just north of the proposed Arapaho Road alignment. The land acquisition process for the new transfer center is currently underway. The existing bus routes in the study area and the proposed transfer center in are shown in Figure 1.

BENEFITS OF EXTENDING ARAPAHO ROAD

There are three primary benefits to transit service resulting from the extension of Arapaho Road. These three benefits: level of service (LOS) improvement, decrease in delay, and increased continuity are discussed below.

LOS IMPROVEMENTS

An important element of the Thoroughfare Plan Development process was to provide necessary roadway capacity to meet existing and future travel demand within the Town. The effective capacity of any particular roadway is dependent on several factors, including horizontal and vertical curvature, signal spacing, signal operation, and level of access control to adjacent properties.

Considering these elements, the North Central Texas Council of Governments has developed capacities for various roadway classifications for use in their transportation planning efforts throughout the region. These planning capacities were used in determining the Town's Thoroughfare Plan. For Belt Line Road, as a six-lane divided, a daily capacity of 43,500 vehicles per day was assumed.

LOS is a qualitative measure of identifying how effectively traffic is managed along a roadway link and is defined by categories A through F. A comparison of volumes and LOS under different scenarios are shown in Table 1.

	Daily Volume Just West of Addison Rd	Link LOS	V/C	Daily Volume Just East of Marsh Ln	Link LOS	V/C
Existing Count	54,199	F	1.25	41,054	E	.95
Yr 2010 No-Build	61,000	F	1.40	41,000	E	.94
Yr 2010 w/Arapaho to Midway Rd	50,000	F	1.15	41,000	E	.94
Yr 2010 w/Arapaho to Marsh Ln	49,000	F	1.13	33,000	D	.76

The Arapaho Road extension has a clear impact in reducing the projected growth of traffic on Belt Line Road. This is important because the roadway is currently over capacity and additional growth will only increase the level of congestion.

In the vicinity of Addison Road, the Arapaho project will divert 11,000 to 12,000 vehicles per day from Belt Line Road resulting in a net improvement in LOS from the existing condition. Near Marsh Lane, the impact of the Arapaho Road extension depends on whether the extension is completed to Marsh Lane. With a Marsh Lane connection, the model predicts a 20% net diversion of traffic from Belt Line Road of about 8,000 vehicles per day with an expected improvement in the level of service. All of these reductions would have an appreciable positive impact on traffic congestion compared to conditions without the Arapaho Road project.

DECREASE IN DELAY

The existing travel time on Belt Line Road from the DNT to Marsh Lane was obtained from Town staff. The noon peak travel time by intersection is shown in Table 2. The average travel time between DNT and Marsh is 5 minutes and 52 seconds. The average speed obtained during the Noon peak over the last year is 25 miles per hour.

TABLE 2 Belt Line Road Travel Time		
Segment*	Westbound	Eastbound
Dallas Parkway - Quorum	48 secs	1 min 18 secs
Quorum - Addison	1 min 2 secs	43 secs
Addison - Beltway	33 secs	28 secs
Beltway - Midway	1 min 5 secs	55 secs
Midway - Runyon	31 secs	1 min 43 secs
Runyon - Surveyor	20 secs	17 secs
Surveyor - Commercial	17 secs	16 secs
Commercial - Marsh	55 secs	32 secs
TOTAL	5 min 31 secs	6 min 12 secs
* From one intersection through the next		

Without the extension of Arapaho Road, the volume on Belt Line Road will increase by about 7,000 vehicles per day or 700 vehicles per hour near Addison Road as shown in Table 1. This increase in traffic volume on Belt Line Road will increase the travel time on Belt Line Road. The current cycle length is 120 seconds (2 minutes) during all peaks. With the increase in volume, delay will also increase. Buses and vehicles will likely have to wait more than one cycle before clearing major intersections. This would increase the delay by two minutes for each cycle missed and also decrease the average travel speed. With Arapaho Road extended to Marsh Lane, the volume on Belt Line Road is substantially decreased (by approximately 10,000 vehicles per day), thus allowing buses to travel on Belt Line Road with less delay and at higher speeds under projected conditions without the Arapaho Road project.

ALTERNATE ROUTE

The Arapaho Road extension will provide additional opportunity for bus ridership as well as an alternative route for buses. Although there is currently not a large number of east-west bus routes servicing along Belt Line Road, the new Transfer Center, future rail service along the corridor, future development, and increases in transit usage all provide the ingredients for increased bus service in the area. The Arapaho Road extension offers an additional or alternative route for this increased service. Also, the proximity of the proposed Transfer Center to the Arapaho Road extension will allow for a more direct and less congested route to the proposed Transfer Center.

CONCLUSIONS

Based on this evaluation, the following conclusions can be drawn.

- The extension of Arapaho Road diverts traffic from Belt Line Road with associated increases in LOS and decreases in delay. This effect will be beneficial to all traffic, including DART transit vehicles using Belt Line Road.
- With the extension of Arapaho Road, DART buses will have a more convenient and less congested alternative for existing and future routes; including a more direct route to the new Transfer Center.



Dallas Area Rapid Transit
P.O. Box 660163
Dallas, Texas 75266-0163
214/749-3278

March 7, 1994

Mr. John R. Baumgartner, P.E.
Director of Public Works
City of Addison
P.O. Box 144
Addison, Texas 75001

Dear Mr. Baumgartner:

On February 15, 1994, the DART Board authorized projects proposed by your city under the LAP Program. A copy of the signed resolution and "application" form for each project are enclosed for your files.

If you need additional information, please call me at 749-2913.

Sincerely,

A handwritten signature in black ink that reads "Tom K. Ryden". The signature is written in a cursive, flowing style.

Tom K. Ryden
Manager, Project Development

TKR/tw

Enclosure



Dallas Area Rapid Transit

940029

RESOLUTION

**RESOLUTION
of the
DALLAS AREA RAPID TRANSIT
(Executive Committee)**

FY 1994 Local Assistance Program Activities for Addison, Carrollton and Glenn Heights

WHEREAS, Addison, Carrollton and Glenn Heights are eligible for participation in DART Local Assistance Program (LAP) projects; and

WHEREAS, these projects are within current Budget and Financial Plan allocations.

NOW, THEREFORE, BE IT RESOLVED by the Dallas Area Rapid Transit Board of Directors that:

- Section 1. The FY 1994 Local Assistance Program requests by the Town of Addison, as shown in Attachment 1, are approved for funding under the LAP.
- Section 2. The FY 1994 Local Assistance Program requests for the City of Carrollton, as shown in Attachment 2, are approved for funding under the LAP.
- Section 3: The FY 1994 Local Assistance project for the City of Glenn Heights, as shown in Attachment 3, is approved for funding under the LAP.

Norma Stanton
Secretary

Kathy Angle
Chairman

APPROVED AS TO FORM:

ATTEST:

DART Counsel

Roger Snoble
Executive Director

February 15, 1994
Date

940029

APPLICATION FOR LOCAL ASSISTANCE PROGRAM

TYPE OF SUBMISSION (1) <input checked="" type="checkbox"/> Capital (C) <input type="checkbox"/> Technical (T) <input type="checkbox"/> Revision (R)		DATE SUBMITTED (2) December 16, 1993	
		CATEGORY CODE (see reverse)	
		(3) <u>1</u>	(4) <u>3</u>
		(5) <u>14</u>	(6) <u> </u>
		(7) <u> </u>	(8) <u> </u>

APPLICANT INFORMATION	
LEGAL NAME (9) Town of Addison	ORGANIZATIONAL UNIT (10) Town of Addison - Engineering Dept.
ADDRESS P.O. Box 144 City <u>Addison</u> (11) County <u>Dallas</u> (12) State <u>Texas</u> (13) Zip Code <u>75001</u> (14)	CONTACT PERSON Name: <u>John Baumgartner</u> Telephone: <u>(214) 450-2886</u>

PROJECT TITLE (15) Westgrove/Sojourn Intersection Improvements

PROJECT DESCRIPTION AND JUSTIFICATION (16)

The intersection improvements entail installing a traffic signal, and restriping the intersection.

Funding requested for engineering and construction costs.

DART Bus Route: 341

PROPOSED PROJECT
 Start Date February, 1994 (17) Ending Date December, 1994 (18)

ESTIMATED FISCAL YEAR FUNDING		CONSTRUCTION COST DISTRIBUTION	
Federal \$	(19)	Engineering/Design \$	20,000.00 (26)
City \$	(20)	Right-of-Way \$	(27)
State \$	(21)	Construction \$	155,000.00 (28)
Local \$	(22)	Other \$	(29)
Other \$	(23)	Total \$	175,000.00 (30)
DART \$	175,000.00 (24)		
TOTAL \$	175,000.00 (25)		

SIGNATURE OF AUTHORIZED REPRESENTATIVE 	TITLE Director of Public Works	DATE SIGNED 12/16/93
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FOR DART USE ONLY

To DART Staff	Date	To P&D Committee	Date
To TASC	_____	P&D Review	_____
TASC Review	_____	Approved by P&D	_____

940029

APPLICATION FOR LOCAL ASSISTANCE PROGRAM

TYPE OF SUBMISSION (1) <input type="checkbox"/> Capital (C) <input checked="" type="checkbox"/> Technical (T) <input type="checkbox"/> Revision (R)	DATE SUBMITTED (2) January 6, 1994
	CATEGORY CODE (see reverse) (3) _____ (4) _____ (5) _____ (6) _____ (7) _____ (8) _____

APPLICANT INFORMATION	
LEGAL NAME (9) Town of Addison	ORGANIZATIONAL UNIT (10) Town of Addison-Engineering Dept.
ADDRESS P.O. Box 144 City <u>Addison</u> (11) County <u>Dallas</u> (12) State <u>Texas</u> (13) Zip Code <u>75001</u> (14)	CONTACT PERSON Name: <u>John Baumgartner</u> Telephone: <u>(214) 450-2886</u>

PROJECT TITLE (15) Technical Assistance Program City Engineer/Planner Position

PROJECT DESCRIPTION AND JUSTIFICATION (16)

Partial funding of the City Engineer and a Civil Engineer position. The duties of these positions include the following:

1. Town of Addison DART-LAP Coordinator.
2. Plan, administer and implement DART-LAP program.

NOTE: The 1993-94 DART-TAP fund includes \$17,046 of unprogrammed proceeds for 1992-93 and \$27,485.64 from 1993-94 for a total of \$44,531.64. These funds provide approximately 45% of the funding of the salary of these positions.

PROPOSED PROJECT
Start Date _____ (17) Ending Date _____ (18)

ESTIMATED FISCAL YEAR FUNDING		CONSTRUCTION COST DISTRIBUTION	
Federal	\$ _____ (19)	Engineering/Design	\$ _____ (26)
City	\$ _____ (20)	Right-of-Way	\$ _____ (27)
State	\$ _____ (21)	Construction	\$ _____ (28)
Local	\$ 55,468.36 (22)	Other	\$ _____ (29)
Other	\$ _____ (23)	Total	\$ _____ (30)
DART	\$ 44,531.64 (24)		
TOTAL	\$100,000 (25)		

SIGNATURE OF AUTHORIZED REPRESENTATIVE <i>John Baumgartner</i>	TITLE Director of Public Works/City Engineer	DATE SIGNED 1-6-94
--	--	------------------------------

FOR DART USE ONLY			
To DART Staff	_____	To P&D Committee	_____
To TASC	_____	P&D Review	_____
		Approved by P&D	_____

940029

APPLICATION FOR LOCAL ASSISTANCE PROGRAM

TYPE OF SUBMISSION (1) <input checked="" type="checkbox"/> Capital (C) <input type="checkbox"/> Technical (T) <input type="checkbox"/> Revision (R)	DATE SUBMITTED (2) December 16, 1993
	CATEGORY CODE (see reverse) (3) <u>1</u> (4) <u>3</u> (5) <u>14</u> (6) <u> </u> (7) <u> </u> (8) <u> </u>

APPLICANT INFORMATION	
LEGAL NAME (8) Town of Addison	ORGANIZATIONAL UNIT (10) Town of Addison - Engineering Dept.
ADDRESS P.O. Box 144 City <u>Addison</u> (11) County <u>Dallas</u> (12) State <u>Texas</u> (13) Zip Code <u>75001</u> (14)	CONTACT PERSON Name: <u>John Baumgartner</u> Telephone: <u>(214) 450-2886</u>

PROJECT TITLE (15) Belt Line Rd. / Business Drive Intersection Improvements

PROJECT DESCRIPTION AND JUSTIFICATION (16)

The intersection improvements entail:

1. A traffic signal
2. Belt Line westbound lanes - dual left turn lanes, and three thru lanes.
3. Belt Line eastbound lanes - single left turn lane, three thru lanes, and a right turn lane.

The additional turn lanes and signal will provide for a more efficient movement of vehicles at this intersection; thus reducing the delay to thru traffic on Belt Line Rd.

Funding requested for engineering, and construction costs.

All right-of-way is being donated by the property owners.

DART Bus Routes: 333, 400

PROPOSED PROJECT
 Start Date December, 1993 (17) Ending Date December, 1994 (18)

ESTIMATED FISCAL YEAR FUNDING		CONSTRUCTION COST DISTRIBUTION	
Federal	\$ (18)	Engineering/Design	\$ 33,000.00 (26)
City	\$ 200,000.00 (20)	Right-of-Way	\$ (27)
State	\$ (21)	Construction	\$ 367,000.00 (28)
Local	\$ (22)	Other	\$ (29)
Other	\$ (23)	Total	\$ 400,000.00 (30)
DART	\$ 200,000.00 (24)		
TOTAL	\$ 400,000.00 (25)		

SIGNATURE OF AUTHORIZED REPRESENTATIVE 	TITLE Director of Public Works	DATE SIGNED 12/16/93
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FOR DART USE ONLY

To DART Staff	Date _____	To P&D Committee	Date _____
To TASC	_____	P&D Review	_____
		Approved by P&D	_____

940029

APPLICATION FOR LOCAL ASSISTANCE PROGRAM

TYPE OF SUBMISSION (1) <input checked="" type="checkbox"/> Capital (C) <input type="checkbox"/> Technical (T) <input type="checkbox"/> Revision (R)	DATE SUBMITTED (2) December 16, 1993
	CATEGORY CODE (see reverse) (3) <u>1</u> (4) <u>2</u> (8) <u>3</u> (6) <u>14</u> (7) <u> </u> (9) <u> </u>

APPLICANT INFORMATION	
LEGAL NAME (9) Town of Addison	ORGANIZATIONAL UNIT (10) Town of Addison - Engineering Dept.
ADDRESS P.O. Box 144 City <u>Addison</u> (11) County <u>Dallas</u> (12) State <u>Texas</u> (13) Zip Code <u>75001</u> (14)	CONTACT PERSON Name: <u>John Baumgartner</u> Telephone: <u>(214) 450-2886</u>

PROJECT TITLE (16) Arapaho Realignment , from North Dallas Tollroad to Quorum

PROJECT DESCRIPTION AND JUSTIFICATION (16)

This project is the first phase of Addison's plan to extend Arapaho Rd. from the North Dallas Tollroad to Marsh Lane.

Funding requested for engineering, and construction costs.

DART Bus Route: 83

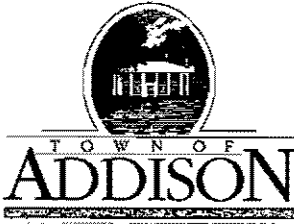
PROPOSED PROJECT
 Start Date February, 1994 (17) Ending Date March, 1996 (18)

ESTIMATED FISCAL YEAR FUNDING		CONSTRUCTION COST DISTRIBUTION	
Federal \$	(19)	Engineering/Design \$	90,000.00 (26)
City \$	(20)	Right-of-Way \$	(27)
State \$	(21)	Construction \$	713,000.00 (28)
Local \$	(22)	Other \$	(29)
Other \$	(23)	Total \$	803,000.00 (30)
DART \$	803,000.00 (24)		
TOTAL \$	803,000.00 (25)		

SIGNATURE OF AUTHORIZED REPRESENTATIVE 	TITLE Director of Public Works	DATE SIGNED 12/16/93
---	--	--------------------------------

FOR DART USE ONLY

To DART Staff	Date _____	To P&D Committee	Date _____
To TASC	_____	P&D Review	_____
TASC Review	_____	Approved by P&D	_____



PUBLIC WORKS DEPARTMENT

Post Office Box 144 Addison, Texas 75001

(214) 450-2871

16801 Westgrove

January 10, 1994

Mr. Tom Ryden
Assistant Director
Bus Project Development
1401 Pacific Avenue
P.O. Box 66013
Dallas, Texas 75266-7212

Re: DART/LAP Quarterly Report - December 1993
Request for reimbursement

This report is intended to update the status of the DART/LAP projects and serve as a request for reimbursement.

1) **Midway/Lindberg Intersection** - Engineering for this project is 90% complete. We are in the process of reviewing plans and acquiring right-of-way.

DART Approved Project \$ 150,000.00

Reimbursement by DART to date \$ 10,830.00

2) **Midway/Beltway Intersection** - Engineering for this project is 90% complete. We are in the process of reviewing plans and acquiring right-of-way.

DART Approved Project \$ 45,000.00

Reimbursements by DART to date \$ 2,925.00

3) **Addison/Westgrove Intersection** - Engineering for this project is 90% complete. We are in the process of reviewing plans and acquiring right-of-way.

DART Approved Project \$125,000.00

Reimbursements by DART to date \$ 8,500.00

4) **Midway/Greenhill School Intersection** - Engineering for this project is 90% complete. We are in the process of reviewing plans and acquiring right-of-way.

DART Approved Project \$ 75,000.00

Reimbursements by DART to date \$ 4,280.00

5) **Beltline/Addison Intersection** - This project is complete and all outstanding invoices have been paid.

DART Approved Project	\$ 196,000.00
Engineering - Epsey Huston	12,605.00
Construction - Gibson & Associate	149,599.36
Cobb Enterprises	1,678.00
Total Project Costs	\$ 163,882.36
Reimbursements by DART to date	\$ 163,882.36

This leaves approximately \$ 32,117.64 for this project. Currently we owe the Southern Pacific Railroad approximately \$ 20,000 for signal work associated with this project. We have not received the invoice.

6. **Beltline/Marsh Intersection** - Engineering for this project is 90% complete. We are in the process of reviewing plans and acquiring right-of-way.

DART Approved Project	\$ 550,000.00
Reimbursements by DART to date	\$ 19,590.00

7. **Beltline/Montfort Intersection** - Engineering for this project is complete. The bid was awarded in November of 1993 and construction is scheduled to start January 11, 1994.

DART Approved Project	\$ 125,000.00
Engineering - Barton-Aschman (90% of \$22,140.00)	19,926.00
Contractor - Bowman Construction (90% of \$62,293.80)	56,064.42
Reimbursements by DART to date	19,926.00

Reimbursements requested this report \$ 56,064.42

8. **Keller Springs/Dallas North Tollway Intersection** - Working through the proposal regarding a Keller Springs toll tunnel under Addison Airport. No action regarding this project.

DART Approved Project	\$ 226,150.00
Reimbursements to Date	0.00

9. **Beltline/North Dallas Tollway Intersection** - Engineering is nearing completion. We are in the process of acquiring the right-of-way.

DART Approved Project	\$ 262,000.00
Engineering Fee (90% of \$15,300)	13,770.00
Appraisal Fee (90% of \$4,300)	3,870.00
Reimbursements to Date	13,770.00

Reimbursement Requested this report \$ 3,870.00

10. **Arapaho Realignment/Right-of-Way** - acquired in December of 1993.

DART Approved Project	\$ 255,000.00
Reimbursement to Date	0.00

Reimbursement Requested this report \$ 255,000.00

11. **Beltline/Midway Intersection** - This project is a transit pass project. The engineering contract is currently being negotiated between Barton-Aschman and DART.

12. **Beltline/Quorum Intersection** - This project is a transit pass project. The engineering contract is currently being negotiated between Barton-Aschman and DART.

13. **Business/Beltline Intersection Improvements** - Applied for approval of partial funding from DART-LAP in December 1993. Began engineering with the Town's contribution.

Request for DART-LAP Participation \$ 200,000.00

14. **Westgrove/Sojourn Intersection Improvement** - Applied for approval of funding from DART-LAP

Request for DART-LAP Participation \$ 175,000.00

Page Four
January 10, 1994

15. **Arapaho Road Realignment** - Applied for approval of funding from DART-LAP.

Request for DART-LAP Participation \$ 803,000.00

Please call me if you have any questions of need additional information regarding these projects.

Sincerely,

John R. Baumgartner, P.E.
Director of Public Works

JRB/gmk

cc: Randy Moravac - Finance Director
(No attachments)

Attachments:

1. Agenda and minutes from the November 23, 1993
City Council Meeting - Montfort/Beltline Intersection
2. Appraisal contracts for two parcels on Beltline Road -
North Dallas Tollway Intersection
3. Warranty deed and closing statement for 6.23 acres tract -
Arapaho Realignment - Right-Of-Way Acquisition

**DART/LAP REIMBURSEMENT
REQUEST NUMBER 3
January 7, 1994**

Project	Reimbursement Request
Beltline/Montfort Intersection	\$ 56,064.42
Beltline/North Dallas Tollway	3,870.00
Arapaho Realignment -R.O.W. Acquisition	\$ 255,000.00
Total Reimbursement Requested	\$ 314,934.42

To: David
8-1-94

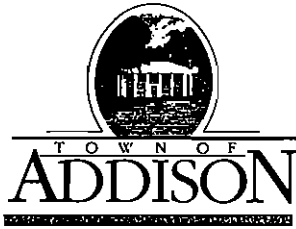
ADDISON - CAPITAL ASSISTANCE PROGRAM

Report thru March 31, 1994

Schedule 1 - Annual Cash Flow Summary

	3/31/94	9/30/94	9/30/95	9/30/96	9/30/97	9/30/98	9/30/99	Out Years
Balance Forward	5,384,919	4,721,748	4,841,286	5,974,069	6,943,815	7,299,906	7,674,257	8,067,804
Add:								
Future Allocations (Note 1)	n/a	n/a	862,400	646,800	n/a	n/a	n/a	n/a
Future Interest Earnings at 5%	n/a	119,538	270,384	322,947	356,093	374,354	393,552	unknown
Less:								
Payments on Approved Projects (Sch 2)	(663,171)	0	0	0	0	0	0	0
Payments on Future Projects (Sch 3)	n/a	0	0	0	0	0	0	0
Fiscal Year End Balance	4,721,748	4,841,286	5,974,069	6,943,815	7,299,906	7,674,257	8,067,804	8,067,804

(1) Per DART Estimate of Allocation letter dated 05/19/94



PUBLIC WORKS DEPARTMENT

Post Office Box 144 Addison, Texas 75001

(214) 450-2871

16801 Westgrove

January 6, 1994

Mr. Tom Ryden
Assistant Director
Dallas Area Rapid Transit
P.O. Box 660163
Dallas, Texas 75266-0163

Re: 1992-93 DART-TAP Report
Proposed 1993-94 DART-TAP Program

Dear Tom:

Attached for your review/approval is a copy of Addison's proposed Technical Assistance Program for the fiscal year 1993-94.

The only programmed expenditure of the 1992-93 TAP funds was the Town of Addison's 1993 city wide traffic counts. Barton-Aschman completed the counts in November of 1993 for a cost of \$9,750. The remaining \$17,046 of the 1992-93 allocation is included in the 1993-94 program.

Please call me if you have any questions or need additional information.

Sincerely,

John R. Baumgartner, P.E.
Director of Public Works

JRB/gmk
Attachment

APPLICATION FOR LOCAL ASSISTANCE PROGRAM

TYPE OF SUBMISSION (1) <input type="checkbox"/> Capital (C) <input checked="" type="checkbox"/> Technical (T) <input type="checkbox"/> Revision (R)	DATE SUBMITTED (2) January 6, 1994 CATEGORY CODE (see reverse) (3) _____ (4) _____ (5) _____ (6) _____ (7) _____ (8) _____
---	--

APPLICANT INFORMATION	
LEGAL NAME (9) Town of Addison	ORGANIZATIONAL UNIT (10) Town of Addison-Engineering Dept.
ADDRESS P.O. Box 144 City Addison (11) County Dallas (12) State Texas (13) Zip Code 75001 (14)	CONTACT PERSON Name: John Baumgartner Telephone: (214) 450-2886

PROJECT TITLE (15) Technical Assistance Program City Engineer/Planner Position

PROJECT DESCRIPTION AND JUSTIFICATION (16)

Partial funding of the City Engineer and a Civil Engineer position. The duties of these positions include the following:

1. Town of Addison DART-LAP Coordinator.
2. Plan, administer and implement DART-LAP program.

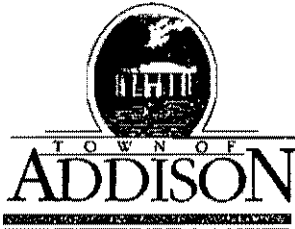
NOTE: The 1993-94 DART-TAP fund includes \$17,046 of unprogrammed proceeds for 1992-93 and \$27,485.64 from 1993-94 for a total of \$44,531.64. These funds provide approximately 45% of the funding of the salary of these positions.

PROPOSED PROJECT	
Start Date _____ (17)	Ending Date _____ (18)

ESTIMATED FISCAL YEAR FUNDING		CONSTRUCTION COST DISTRIBUTION	
Federal	\$ _____ (19)	Engineering/Design	\$ _____ (26)
City	\$ _____ (20)	Right-of-Way	\$ _____ (27)
State	\$ _____ (21)	Construction	\$ _____ (28)
Local	\$ 55,468.36 (22)	Other	\$ _____ (29)
Other	\$ _____ (23)	Total	\$ _____ (30)
DART	\$ 44,531.64 (24)		
TOTAL	\$ 100,000 (25)		

SIGNATURE OF AUTHORIZED REPRESENTATIVE 	TITLE Director of Public Work/City Engineer DATE SIGNED 1-6-94
--	---

FOR DART USE ONLY			
	Date		Date
To DART Staff	_____	To P&D Committee	_____
To TASC	_____	P&D Review	_____
TASC Review	_____	Approved by P&D	_____



PUBLIC WORKS DEPARTMENT

(214) 450-2871

Post Office Box 144 Addison, Texas 75001

16801 Westgrove

October 01, 1993

Mr. Tom K. Ryden
Assistant Director
Bus Project Development
Dallas Area Rapid Transit
1401 Pacific Avenue
P.O. Box 66013
Dallas, Texas 75266-7212

Re: DART/LAP Quarterly Report - September 1993
Request for reimbursement

This report is intended to update the status of the DART/LAP projects and serve as a request for reimbursement.

1) Miscellaneous Sidewalk & Bench Pads - September 1992 - This project is complete with this report.

DART Approved Project	\$ 41,000.00
Contractor	
Nelson & Associates	\$ 33,368.05
Daniel's Dirt	750.00
Jim Bowman's Construction Co.	3,670.25
Total Construction	\$ 37,788.30
Reimbursement by DART to date	\$ 34,404.00
Reimbursement requested this report	\$ 3,384.30

This leaves \$ 3,211.70 remaining in this project that was used . Please free these monies for use on another project.

2) Midway/Lindberg Intersection - Engineering for this project is 90% complete. We are in the process of reviewing plans and acquiring right-of-way.

DART Approved Project	\$150,000.00
Reimbursement by DART to date	\$ 10,830.00

3) Midway/Beltway Intersection - Engineering for this project is 90% complete. We are in the process of reviewing plans and acquiring right-of-way.

DART Approved Project	\$ 45,000.00
Reimbursement by DART to date	\$ 2,925.00

4) Addison/Westgrove Intersection - Engineering for this project is 90% complete. We are in the process of reviewing plans and acquiring right-of-way.

DART Approved Project	\$125,000.00
Reimbursements by DART to date	\$ 8,500.00

5) Midway/Greenhill School Intersection - Engineering for this project is 90% complete. We are in the process of reviewing plans and acquiring right-of-way.

DART Approved Project	\$ 75,000.00
Reimbursements by DART to date	\$ 4,280.00

6) Beltline/Addison Intersection

This project is complete and all outstanding invoices have been paid.

DART Approved Project	\$ 196,000.00
Engineering - Epsey Huston	12,605.00
Construction - Gibson & Associates	149,599.36
Cobb Enterprises	1,678.00
Total Project Costs	\$ 163,882.36
Reimbursements by DART to date	12,605.00
Reimbursements requested this report	\$ 151,277.36

This leaves approximately \$ 32,117.64 for this project. Currently we owe the Southern Pacific Railroad approximately \$ 20,000 for signal work associated with this project. We have not received the invoice.

7) Beltline/Marsh Intersection - Engineering for this project is 90% complete. We are in the process of a reviewing plans and acquiring right-of-way.

DART Approved Project	\$ 550,000.00
Reimbursements by DART to date	\$ 19,590.00

8) Beltline/Montfort Intersection - Engineering for this project is 90% complete. We are in the process of a reviewing plans and acquiring right-of-way.

DART Approved Project	\$ 125,000.00
Reimbursements by DART to date	\$ 19,926.00

Thoroughfare Plan Development - This project is complete and the Council accepted the plan.

DART Approved Project	\$ 37,500.00
Engineering Fee	37,508.17
Reimbursements by DART to date	33,500.00
Reimbursements requested this report	\$ 3,750.00

9) Keller Springs/Dallas North Tollway Intersection - Working through the proposal regarding a Keller Springs toll tunnel under Addison Airport. No action regarding this project.

DART Approved Project	\$ 226,150.00
Reimbursement to Date	\$ 0

10) Belt Line/North Dallas Tollway Intersection - We have just hired an engineer to proceed with the design.

DART Approved Project	\$ 262,000.00
Engineering Fee (90% of \$15,300)	\$ 13,770.00
Reimbursement to Date	\$ 0
Reimbursement requested this report.	\$ 13,770.00

Page Four
Tom Ryden

11) Arapaho Realignment

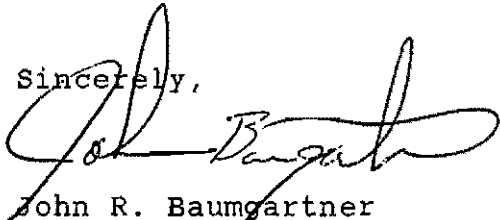
Request from DART - LAP	\$ 255,000.00
Reimbursement to Date	\$ 0

12) Beltline/Midway Intersection - This project is a transit pass project. The engineering contract is currently being negotiated between Barton-Aschman and DART.

13) Beltline/Quorum Intersection - This project is a transit pass project. The engineering contract is currently being negotiated between Barton-Aschman and DART.

Please call me if you have any questions or need additional information regarding these projects.

Sincerely,



John R. Baumgartner
Director of Public Works

JRB/gmk

cc: Randy Moravec - Director of Finance
(No attachments)

Attachments: 1) DART/LAP Reimbursement Request No. 2
2) 8 invoices for the sidewalk project
3) 3 invoices for the Addison Road project
4) 12 invoices for the thoroughfare project
5) Agreement with Epsley, Huston & Associates for the Beltline/Tollway project.

DART/LAP REIMBURSEMENT
REQUEST NUMBER 2
OCTOBER 01, 1993

Project	Reimbursement Request
Miscellaneous Sidewalks - 1992	\$ 3,384.30
Beltline/Addison Intersection	151,277.36
Thoroughfare Plan	3,750.00
Beltline/North Dallas Tollway Intersection	<u>13,770.00</u>
Total Reimbursement Requested	\$ 172,181.66

9-30-93



from the desk of...

Tom K. Ryden, P.E.
Assistant Director
Bus Project Development

Copies to:

Ron W
Randy M
FY 1993 RB

Attached is the year end summary (FY 1993) for LAP expenditures. If you have any questions, please call me.

Tom.

Interer? ?
7/4/93

CAPITAL ASSISTANCE PROGRAM

September 30, 1993

City	FY89-93			Cash Payments
	Allocation	Programmed	Unprogrammed	Made
Addison	4,399,665	2,156,602	2,243,063	176,055
Buckingham	92,896	0	92,896	0
Carrollton	6,648,818	6,494,467	154,351	2,945,911
Cockrell Hill	61,103	20,000	41,103	18,000
Coppell	20,027	19,175	852	17,257
Dallas	N/A	N/A	N/A	N/A
Dallas County	N/A	N/A	N/A	N/A
Farmers Branch	5,819,245	4,046,043	1,773,202	3,471,043
Garland	7,963,386	6,315,341	1,648,045	2,581,505
Glenn Heights	32,711	0	32,711	0
Highland Park	N/A	N/A	N/A	N/A
Irving	15,287,038	12,448,655	2,838,383	4,892,397
Plano	11,373,638	11,055,377	318,261	5,489,090
Richardson	8,751,791	8,292,091	459,700	5,152,123
Rowlett	528,616	100,868	427,748	100,868
University Park	N/A	N/A	N/A	N/A
Total	60,978,934	50,948,619	10,030,315	24,844,249

TECHNICAL ASSISTANCE PROGRAM

September 30, 1993

City	FY89-93		Cash Payments	
	Allocation	Programmed	Unprogrammed	Made
Addison	109,822	92,776	17,046	109,822
Buckingham	2,729	0	2,729	0
Carrollton	172,272	91,357	80,915	172,272
Cockrell Hill	1,862	0	1,862	0
Coppell	528	528	0	528
Dallas	2,429,258	2,429,258	0	2,429,258
Dallas County	169,528	89,816	79,712	169,528
Farmers Branch	150,580	147,949	2,631	150,580
Garland	210,665	196,550	14,115	210,665
Glenn Heights	939	0	939	0
Highland Park	20,891	11,892	8,999	20,891
Irving	389,060	386,984	2,076	389,060
Plano	301,618	301,618	0	301,618
Richardson	231,877	231,877	0	231,877
Rowlett	14,241	0	14,241	3,805
University Park	37,043	22,197	14,846	37,043
Total	4,242,913	4,002,802	240,111	4,226,947

APPLICATION FOR LOCAL ASSISTANCE PROGRAM

TYPE OF SUBMISSION (1) <input checked="" type="checkbox"/> Capital (C) <input type="checkbox"/> Technical (T) <input type="checkbox"/> Revision (R)	DATE SUBMITTED (2) October 28, 1991 CATEGORY CODE (see reverse) (3) _____ (4) _____ (5) _____ (6) _____ (7) _____ (8) _____
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APPLICANT INFORMATION	
LEGAL NAME (9) Town of Addison	ORGANIZATIONAL UNIT (10) Town of Addison-Engineering Department
ADDRESS P. O. Box 144 City <u>Addison</u> (11) County <u>Dallas</u> (12) State <u>Texas</u> (13) Zip Code <u>75001</u> (14)	CONTACT PERSON Name: <u>John Baumgartner</u> Telephone: <u>(214) 450-2886</u>

PROJECT TITLE (15)

PROJECT DESCRIPTION AND JUSTIFICATION (16)

PROPOSED PROJECT
 Start Date _____ (17) Ending Date _____ (18)

ESTIMATED FISCAL YEAR FUNDING		CONSTRUCTION COST DISTRIBUTION	
Federal	\$ _____ (19)	Engineering/Design	\$ _____ (26)
City	\$ _____ (20)	Right-of-Way	\$ _____ (27)
State	\$ _____ (21)	Construction	\$ _____ (28)
Local	\$ _____ (22)	Other	\$ _____ (29)
Other	\$ _____ (23)	Total	\$ _____ (30)
DART	\$ _____ (24)		
TOTAL	\$ _____ (25)		

SIGNATURE OF AUTHORIZED REPRESENTATIVE 	TITLE City Engineer	DATE SIGNED October 28, 1991
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FOR DART USE ONLY

To DART Staff _____ Date _____ To TASC _____ TASC Review _____	To P&D Committee _____ Date _____ P&D Review _____ Approved by P&D _____
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