

NORTH CENTRAL TEXAS COUNCIL OF GOVERNMENTS RAILROAD WARNING DEVICE INSTALLATION PROJECTS TYPICAL PROJECT PROGRESSION SEQUENCE

STEP 1

Receive and execute Local Project Advance Funding Agreement (LPAFA) from TxDOT district office. Return it to TxDOT district office for final execution.

STEP 2

Coordinate with TxDOT district office and the railroad company to schedule a diagnostic inspection of each site.

STEP 3

Prepare a set of plans (Exhibit A) for the installation of the warning devices based upon information gathered at the diagnostic inspection. Submit those plans to TxDOT district office for review and comment.

STEP 4

Prepare an agreement between the local authority and the railroad company for the installation of the warning devices. Submit two originals of this agreement along with the plans (Exhibit A) to the railroad for approval and execution. The cover letter to the railroad company with this submittal should include some phrasing stating that the railroad company is authorized to begin engineering of the project contingent upon approval of the agreement. The railroad company should return both originals to the local authority for final execution along with the circuit layout and estimate from the railroad's engineering contractor or department. Compile all of these documents together into a complete PS&E package (Exhibit B) and distribute copies to all parties involved (railroad original back to them, fully executed).

STEP 5

The railroad company will request a Work Order so that they may begin installation. The local authority should complete all curb work, median work, or other roadway modifications needed to accommodate the railroad force account work prior to issuing the work order to the railroad company.

STEP 6 (Recommended)

The local authority should have an inspector visit the site on a daily basis to insure that the installation is proceeding as planned and in a timely manner.

STEP 7 (Recommended)

When the work is completed, a final inspection of the site should be done. It is most efficient if this inspection can be accomplished at the time the signal is cut-over, or placed in service. However, if that cannot be done, the railroad should send an in-service notification to the local authority, which can then coordinate with the railroad company to schedule the final inspection.

Dallas District Contact People for NCTCOG Railroad Projects:

Anything to do with the wording or writing of the agreements:

Melanie Young		214-320-6229
Bitu Ramkar		214-320-424

Anything to do with funding or projects:

Wes McClure		214-320-4461
Melanie Young		214-320-6229

Anything to do with the P S& E preparation and submission;
Exhibit A drawings or
Construction

Dexter Hollabaugh		214-320-6232
Melanie Young		214-320-6229
Cheryl Cook		214-320-6291
Area Office	Number furnished on request	

Fort Worth District Contact People for NCTCOG Railroad Projects:

Anything to do with the wording or writing of the agreements, P S & E preparation and submission, Exhibit A drawings or Construction:

Tim Oster		817-370-6813
Jim Walling		817-370-6749

Anything to do with funding or projects:

Joe Mallard		817-370-6591
Judy Anderson		817-370-6710

**PRELIMINARY DOCUMENT
NOT FOR RELEASE PURSUANT TO 23 U.S.C. SECTION 409**

2

Southbound Southwest Bound Westbound

3

Westbound Northwest Bound Northbound

6

Crossing Surface

4

Northbound Northeast Bound Eastbound

5

Eastbound Southeast Bound Southbound

PROJECT INFORMATION

Location:
Street/Highway:
County:

Project No.:
CSJ:

Railroad:
DOT #
Milepost:

Date of Inspection:

Diagnostic Team:
TxDOT:
Railroad:
Others:

OFFICE USE ONLY
PRELIMINARY DOCUMENT
NOT FOR RELEASE PURSUANT TO 23 U.S.C. SECTION 409

MINIMUM WARNING TIME
20 seconds Minimum Time (MT)
seconds Clearance Time (CT)
seconds MINIMUM WARNING TIME (MWT)

_____ Average Daily Traffic (ADT)
_____ Special Vehicle moves
_____ MPH
_____ through trains at _____ mph per day
_____ switch moves at _____ mph per day

TOTAL WARNING TIME
seconds Minimum Warning Time (MWT)
seconds Buffer Time (BT)
seconds TOTAL WARNING TIME (TWT)

TOTAL APPROACH TIME
seconds Total Warning Time (TWT)
seconds Equipment Response Time (ERT)
seconds Advance Traffic Signal Preemption Time (APT)
seconds TOTAL APPROACH TIME

Salvaged equipment:

Total estimated cubic yards of fill material:

- This project is actual cost for reimbursement of payment to the Railroad Company as agreed to by:
 This project is lump sum cost for reimbursement of payment to the Railroad Company as agreed to by:

TxDOT:

Railroad Company:

- Existing cross bucks meet TMUTCD guidelines
 Existing cross bucks do not meet TMUTCD guidelines and need to be replaced repaired. If replacement or repair is needed the railroad company or its contractor will make necessary arrangements, within 30 days of diagnostic. **Notify TRF/RR when discrepancies are correct**

- RxxR pavement markings are to be installed, per the guidelines in the TMUTCD
 No RxxR pavement markings are to be installed because
 Stop bars are to be installed, per the guidelines in the TMUTCD
 No stop bars are to be installed because

- Side lights are to be installed at this location. (Crossing is 50 feet or less from the parallel roadway and no stop conditions exist)
 No side lights will be installed at this location. (Crossing is greater than 50 feet from the parallel roadway or stop conditions exist)

- AC power service is available at this location
 AC power service is not available at this location

- A signalized intersection is located _____ ft from crossing. Distance measured from the warning device to the edge of road/shoulder.
Attach copy of the preemption form
 No signalized intersection at this location

- Letter to proceed with project development was given to the Railroad Company
 No letter to proceed with project development was given to the Railroad Company because

- No yield or stop signs were installed by the State because
 Yield signs were recommended by the diagnostic team on an interim basis, per the guidelines in the TMUTCD. The local road authority will be notified in writing within 7 days. Yield signs to be placed within 30 days of diagnostic. **Notify TRF/RR when signs are installed**
 Stop signs were recommended by the diagnostic team on an interim basis, per the guidelines in the TMUTCD. The local road authority will be notified in writing within 7 days. Stop signs to be placed within 30 days of diagnostic. **Notify TRF/RR when signs are installed**
 Memo to install signs given to the district

**PRELIMINARY DOCUMENT
NOT FOR RELEASE PURSUANT TO 23 U.S.C. SECTION 409**

GENERAL NOTES

1. Signal circuits are designed to give 20 seconds Minimum Warning Time prior to the arrival of the fastest train at this crossing. See the railroad signal circuit layout for total approach time.
2. Constant warning Phase motion circuits are to be used at this location. Upgrades required at for circuit compatibility.
3. Conduit, fill dirt and crushed cover rock to be furnished in place by the Railroad Company or its Contractor.
4. The Railroad Company or its Contractor will remove the existing cross bucks mast flashers cantilevers and dispose of the foundations.
5. The State or its Contractor will furnish and install or replace the appropriate pavement markings as outlined on the attached layout and standard sheet and in accordance with the guidelines in the Texas Manual on Uniform Traffic Control Devices.
6. The State or its Contractor will furnish and install or replace the following signs in accordance with the guidelines in the Texas Manual on Uniform Traffic Control Devices (TMUTCD) and the Standard Highway Sign Designs Manual for Texas (SHSD): ea. (W10-1), ea. (W10-2), ea. (W10-3), ea. (W10-4), ea. (R15-4). Additional signs to be added
7. The State County City agrees to maintain the pavement markings and advance warning signs placed along the roadways under their jurisdiction in accordance with the guidelines in the Texas Manual on Uniform Traffic Control Devices and as shown on the layout and standard sheets as acknowledged on the Title Sheet.
8. The Railroad Company or its Contractor shall furnish, install and maintain sign mounting brackets for the malfunction sign (R15-4) at the States expense.
9. The Railroad Company or its Contractor shall stencil the DOT-AAR numbers on the signal masts facing the adjacent roadway in 2" black lettering.
10. The State County City agrees to trim and maintain trees and vegetation for adequate visibility of the crossing signals and advance warning signs as acknowledged on the Title Sheet.
11. The Railroad Company or its Contractor will provide traffic control in accordance with the guidelines in the Texas Manual on Uniform Traffic Control Devices.
12. The State Railroad Company or its Contractor will install metal beam guard fence as shown on the layout, at the State's Railroads expense.
13. The State Railroad Company or its Contractor will install retaining wall as shown on the layout, at the State's Railroads expense.
14. The Railroad or its Contractor will furnish and install a relay to provide simultaneous advance preemption to existing traffic signal proposed traffic signal advance flasher. Normally a closed circuit is required between the control relay of the grade crossing warning device and the traffic signal controller or flasher as stated in the Texas Manual on Uniform Traffic Control Devices.

ADDITIONAL NOTES ON BACK

DESCRIPTION OF PROJECT

complete gate assemblies
Complete cantilever assemblies with foot arm
ea. R15-2, (Tracks)

12" lamp housing shall be used and equipped with LED's (light emitting diodes) 25 watt incandescent lights, operated at not less than 8.5 volts under normal operating conditions.

JEFFERSON BLVD. DIV.		FEDERAL AID PROJECT NO.		CONTRACT NO.	
STATE	6	STP 2004 (503)RXH	JEFFERSON		
DISTRICT	TEXAS	DALLAS			
COUNTY	DAWSON	DALLAS			
SECTION	45	72D			
DATE	09/08	45	1		

STATE OF TEXAS
DEPARTMENT OF TRANSPORTATION

PLANS OF PROPOSED
STATE HIGHWAY IMPROVEMENT

PROJECT NUMBER: STP 2004 (503)RXH
CSJ NUMBER: 0918-45-720

JEFFERSON BLVD.

IN GRAND PRAIRIE, TEXAS
INSTALL GRADE CROSSING WARNING DEVICES
DALLAS COUNTY - DOT* 795462L
UNION PACIFIC RAILROAD

INDEX OF SHEETS

SHEET NO.	DESCRIPTION
1	TITLE SHEET
2	LAYOUT SHEET
3	GENERAL NOTES
4	SIGNING & STRIPING LAYOUT
5	WIRING DIAGRAM
6	RCSS-98
7	RCPL-96
8	WEIRING 15R(15) - 03



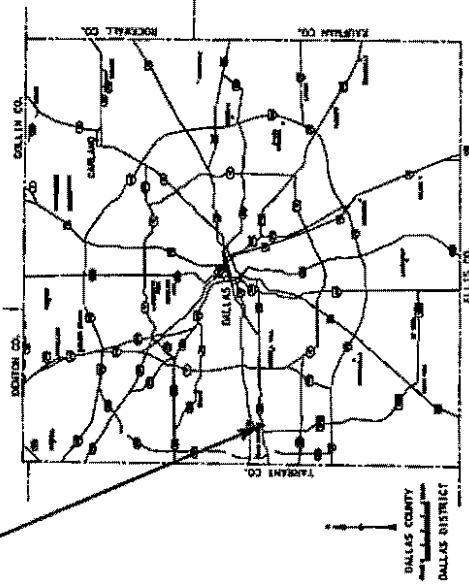
THE STANDARD SHEETS SPECIFICALLY IDENTIFIED ABOVE HAVE BEEN SELECTED BY ME OR UNDER MY RESPONSIBILITY SUPERVISION AS BEING APPLICABLE TO THIS PROJECT.

Andrew R. Oberlander
ANDREW R. OBERLANDER, P.E.
DATE: Dec. 28, 2004

NOTE:
THE RAILROAD COMPANY SHALL ERECT AND MAINTAIN SUCH BARRICADES AND WARNING SIGNS OR USE SUCH PRECAUTIONS AS MIGHT BE NECESSARY IN THE OPINION OF THE TADOT ENGINEER TO INSURE AND SAFEGUARD THE NORMAL USE OF THE HIGHWAY DURING THE INSTALLATION OF THE PROJECT.

ASSOCIATION OF AMERICAN RAILROADS SIGNAL SECTION SPECIFICATIONS SHALL GOVERN ON THIS PROJECT.

PROJECT LOCATION
IN GRAND PRAIRIE, TEXAS
JEFFERSON BLVD. & UP RAILROAD
DOT* 795462L - RRMP* 227.48



NOT FOR REPRODUCTION
PRELIMINARY

THESE PLANS WERE DEVELOPED BY MEMBERS OF THE DIAGNOSTIC TEAM IN ACCORDANCE WITH THE TERMS OF THE STATE-RAILROAD AGREEMENT AND ARE HEREBY AUTHORIZED FOR CONSTRUCTION.

TEXAS DEPARTMENT OF TRANSPORTATION

CORRECTY	Dec 29 2004	APPROVED	Nov 9 2004
<i>George W. Jones, P.E.</i>		<i>John B. Jones, P.E.</i>	
DIRECTOR OF TRANSPORTATION OPERATIONS		CITY OF GRAND PRAIRIE	
RECOMMENDED FOR APPROVAL	11/15/04	APPROVED FOR CONSTRUCTION	2004
<i>T. Jones, P.E.</i>		<i>T. Jones, P.E.</i>	
DISTRICT ENGINEER		DIRECTOR, TRAFFIC OPERATIONS DIVISION	

PRELIMINARY
NOT FOR REPRODUCTION

THE RAILROAD WILL FURNISH AND INSTALL A NEW CANTILEVER - GATE ASSEMBLY COMBINATION. LIGHT CONFIGURATION AS SHOWN. ONE BELL WILL BE INSTALLED AND ALL LIGHTS WILL BE L.E.D.'s.

EXISTING LANE MARKINGS (BY CITY OF GRAND PRAIRIE)

THE RAILROAD WILL FURNISH AND INSTALL A NEW FLASHING LIGHT - GATE ASSEMBLY WITH ONE BELL AND ALL LED LIGHTS. GATE LENGTH TO BE APPROX 22 FEET.

THE RAILROAD WILL FURNISH AND INSTALL A NEW CANTILEVER - GATE ASSEMBLY COMBINATION. LIGHT CONFIGURATION AS SHOWN. ONE BELL WILL BE INSTALLED AND ALL LIGHTS WILL BE L.E.D.'s.

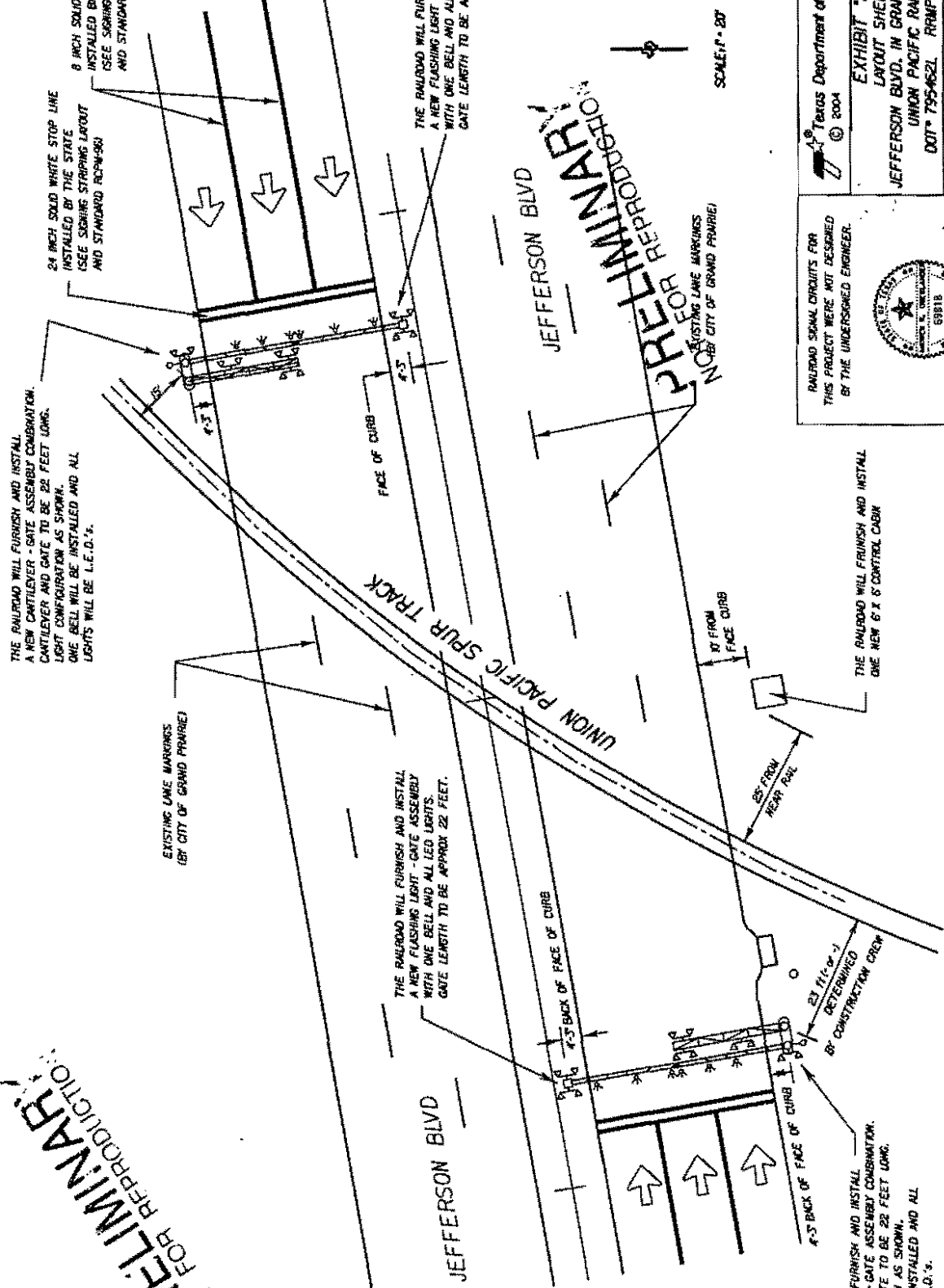
THE RAILROAD WILL AVOID ALL UNDERGROUND UTILITIES AND MAINTAIN MINIMUM CLEARANCES FROM OVERHEAD POWER LINES.

24 INCH SOLID WHITE STOP LINE INSTALLED BY THE STATE (SEE SIGNING STRIPING LAYOUT AND STANDARD RCPM-90)

8 INCH SOLID LANE LINES INSTALLED BY THE STATE (SEE SIGNING STRIPING LAYOUT AND STANDARD RCPM-90)

THE RAILROAD WILL FURNISH AND INSTALL A NEW FLASHING LIGHT - GATE ASSEMBLY WITH ONE BELL AND ALL LED LIGHTS. GATE LENGTH TO BE APPROX 22 FEET.

THE RAILROAD WILL FURNISH AND INSTALL ONE NEW 6' X 8' CONTROL CABIN



SCALE: 1" = 20'

Texas Department of Transportation
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EXHIBIT "A"
LAYOUT SHEET
JEFFERSON BLVD. IN GRAND PRAIRIE, TX
UNION PACIFIC RAILROAD
DOT# 795-462L RRP# 227.48

PROJECT NO.	795-462L	SHEET NO.	2
FEDERAL AID PROJECT NO.		DATE	4/1
STATE	TEXAS	CONTRACT NO.	0388
DISTRICT	DALLAS	SECTION	45
COUNTY	DALLAS	DATE	7/90
DESIGNED BY	STP 2004 (503) RPH	PROJECT NO.	795-462L
CHECKED BY	JEFFERSON	CONTRACT NO.	0388
DATE	BEVD	SECTION	45

RAILROAD SIGNAL CIRCUITS FOR THIS PROJECT WERE NOT DESIGNED BY THE UNDERSIGNED ENGINEER.



Charles W. Mc...
Professional Engineer
No. P.E. 12345
State of Texas

JEFFERSON BLVD.DGN

GENERAL NOTES

DESCRIPTION OF PROJECT

1. Signal circuits are designed to give 20 seconds Minimum Warning Time prior to the arrival of the fastest train at this crossing. See the railroad signal circuit layout for total approach time.
2. Constant warning circuits are to be used at this location.
3. Conduit, fill dirt and crushed cover rock to be furnished in place by the Railroad Company or its Contractor.
4. The Railroad Company or its Contractor will remove the existing crossbucks.
5. The State or its Contractor will furnish and install or replace the appropriate pavement markings as outlined on the attached layout and standard sheet and in accordance with the guidelines in the Texas Manual on Uniform Traffic Control Devices
6. The State or its Contractor will furnish and install or replace the following signs in accordance with the guidelines in the Texas Manual on Uniform Traffic Control Devices (TMUTCD) and the Standard Highway Sign Designs Manual for Texas (SHSD): 4 ea. (W 10-1), 2 ea. (R 5-4).
7. The City agrees to maintain the pavement markings and advance warning signs placed along the roadways under their jurisdiction in accordance with the guidelines in the Texas Manual on Uniform Traffic Control Devices and as shown on the layout and standard sheets as acknowledged on the Title Sheet.
8. The Railroad Company or its Contractor shall furnish and install sign mounting brackets for the malfunction sign (R 5-4) at the States expense.
9. The Railroad Company or its Contractor shall stencil the DOT-AAR numbers on the signal masts facing the adjacent roadway in 2" black lettering.
10. The City agrees to trim and maintain trees and vegetation for adequate visibility of the crossing signals and advance warning signs as acknowledged on the Title Sheet.
11. The Railroad Company or its Contractor will provide traffic control in accordance with the guidelines in the Texas Manual on Uniform Traffic Control Devices.
12. The Railroad will furnish and install a relay to provide advance preemption to the proposed traffic signal. Normally a closed circuit is required between the control relay of the grade crossing warning device and the traffic signal controller or flasher as stated in the Texas Manual on Uniform Traffic Control Devices.

- 2 - Gate / Cantilever Combination devices
gates and cantilever devices will be approx 22 feet in length
12 lamp housing shall be used and equipped with LED's (light emitting diodes), operated at not less than 8.5 volts under normal operating conditions.

PRELIMINARY
NOT FOR REPRODUCTION

RAILROAD SIGNAL CIRCUITS FOR
THIS PROJECT WERE NOT DESIGNED
BY THE UNDERSIGNED ENGINEER.



Robert O. ...
P. E.
Signature of Engineer

Texas Department of Transportation
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EXHIBIT "A"
GENERAL NOTES
JEFFERSON BLVD. IN GRAND PRAIRIE, TX
UNION PACIFIC RAILROAD
DOT 795-462L RRMP-227.48

SECTION	SHEET NO.	FEDERAL AID PROJECT NO.	ACCOUNT NO.
6	6	STP 2004 (603) RSH	JEFFERSON
COUNTY	DISTRICT	COUNTY	BLVD
TEXAS	DALLAS	DALLAS	
CITY	SECTION	DATE	SHEET
			7

SIGNS AND PAVEMENT MARKINGS SHOULD BE PLACED APPROX 445 FEET BACK FROM TRACK. ADJUST LOCATION TO FIT FIELD CONDITIONS

THE STATE WILL FURNISH AND INSTALL RR X-ING SYMBOL AND TRANSVERSE LINES AS SHOWN ON RCPM-96

THE STATE WILL FURNISH AND INSTALL W/O/S SIGN



ITEMS TO BE FURNISHED AND INSTALLED BY THE STATE

- 4 EA W/ING AND W/ING SIGNS
- 2 EA RR X-ING SIGNS
- 2 EA W/O/S SIGNS
- 1 EA W/ING SYMBOL (24" WHITE TRANSVERSE LINES AS SHOWN ON RCPM-96)

THE STATE OR ITS CONTRACTOR WILL FURNISH AND INSTALL 24 INCH WIDE SOLID WHITE STOP LINE 8 FT IN FRONT OF GATES

THE STATE OR ITS CONTRACTOR WILL FURNISH AND INSTALL 24 INCH WIDE SOLID WHITE STOP LINE 8 FT IN FRONT OF GATES

THE STATE OR ITS CONTRACTOR WILL FURNISH AND INSTALL W/O/S SIGN APPROX 370 LF (EACH) SEE STANDARD RCPM - 96 FOR DETAILS

SIGNS AND PAVEMENT MARKINGS SHOULD BE PLACED APPROX 445 FEET BACK FROM TRACK. ADJUST LOCATION TO FIT FIELD CONDITIONS

THE STATE WILL FURNISH AND INSTALL RR X-ING SYMBOL AND TRANSVERSE LINES AS SHOWN ON RCPM-96

THE STATE WILL FURNISH AND INSTALL W/O/S SIGN



THE STATE WILL FURNISH AND INSTALL W/O/S SIGN



JEFFERSON BLVD

JEFFERSON BLVD

JEFFERSON BLVD

JEFFERSON BLVD

THE STATE WILL FURNISH AND INSTALL W/O/S SIGN



THE STATE OR ITS CONTRACTOR WILL FURNISH AND INSTALL 8 INCH WIDE - SOLID WHITE LANE LINES APPROX 370 LF (EACH) SEE STANDARD RCPM - 96 FOR DETAILS



RRIS-4 SIGN



PLACEMENT OF THE RRIS-4 (UNALLOCATION) SIGN BY STATE FORCES ON BRACKET PROVIDED BY THE RAILROAD

GENERAL NOTES CONCERNING SIGNS & PAVEMENT MARKINGS

1. THIS PROJECT IS ON A CITY STREET. ALL WORK SHOULD BE COORDINATED WITH THE CITY OF GRAND PRairie
2. THE STATE WILL FURNISH AND INSTALL ALL SIGNS AND PAVEMENT MARKINGS SHOWN ON THE LAYOUT
3. THE CITY OF GRAND PRairie WILL BE RESPONSIBLE FOR MAINTENANCE OF SIGNS & PAVEMENT MARKINGS AFTER PROJECT COMPLETION

RAILROAD SIGNAL CATCHER
THIS PROJECT HAS BEEN REVIEWED BY THE STATE OF TEXAS



Robert A. P. E. 12/28/2004
Professional Engineer

Texas Department of Transportation
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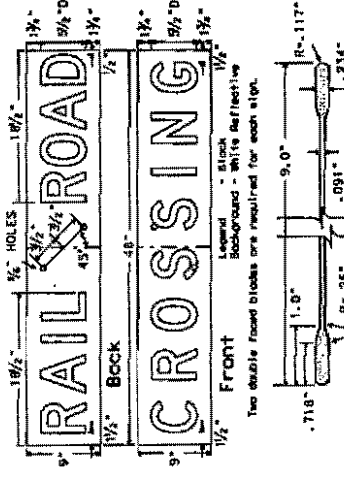
EXHIBIT "A"
SIGNING & STRIPING LAYOUT
JEFFERSON BLVD. IN GRAND PRairie, TX
UNION PACIFIC RAILROAD
DOT # 795-062L RSM# 227.48

SECTION	DATE	PROJECT NO.	TITLE
6	02/04	5603/RAH	JEFFERSON BLVD
COUNTY	STATE	CITY	SHEET
TARRANT	TEXAS	DALLAS	4
DATE	SCALE	SECTION	NO.
02/04	AS SHOWN	6 OF 7	4

NOT RECOMMENDED FOR PRELIMINARY USE

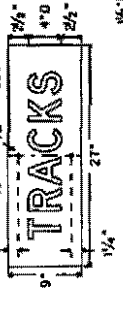
JEFFERSON BLVD.DWG

R15-1 Blade Detail

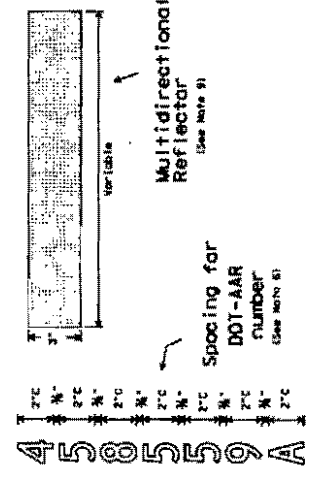
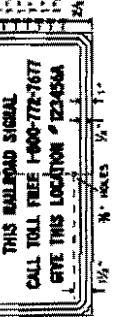


Blade cross section
 6081-15 or 6083-15

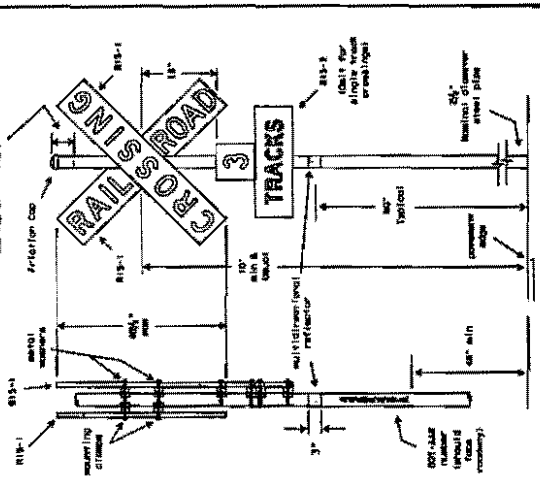
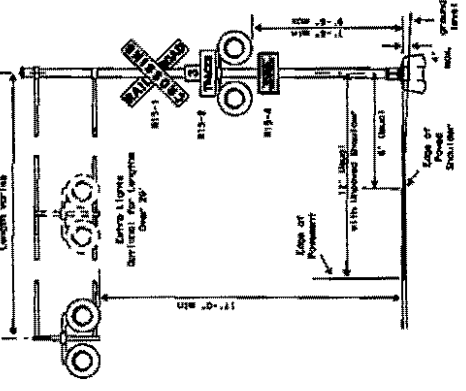
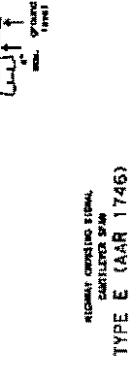
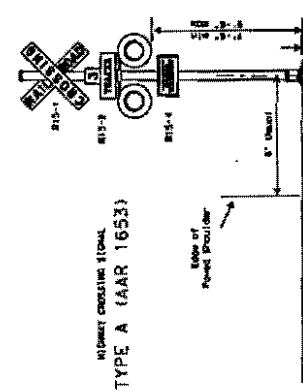
R15-2 Multiple track Sign Detail
 Legend - Black Background - Reflective



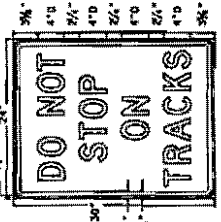
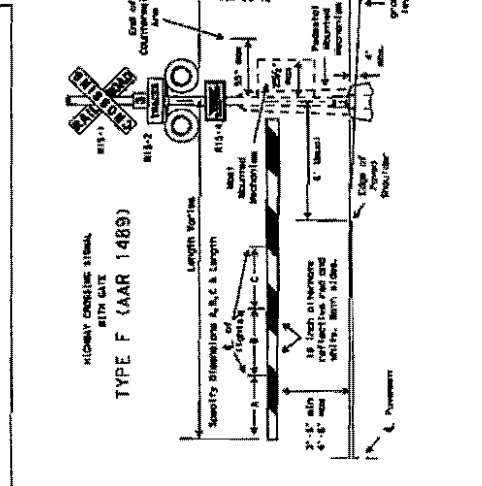
R15-4 Multifunction Sign
 Legend - Black Background - Reflective



LATERAL CLEARANCES FOR SIGNS



Assembled Elevations
RAILROAD CROSSING SIGN INSTALLATION (NEW POLE AND FOUNDATION) (R15-1)



RB-8

SPECIFICATION REFERENCE TABLE
 MINIMUM SIGN SIZE SPECIFICATIONS
 PLAT SURFACE REFLECTIVE SHEETING, TYPE C
 (HIGH SPECIFIC INTENSITY)

GENERAL NOTES

1. All signs and reflectors shall conform to the Association of American Railroads Signal Section Specifications.
2. All signs shall be made of aluminum or other material of equal or better quality than that specified. All signs shall be made of aluminum or other material of equal or better quality than that specified.
3. All signs shall be made of aluminum or other material of equal or better quality than that specified.
4. All signs shall be made of aluminum or other material of equal or better quality than that specified.
5. All signs shall be made of aluminum or other material of equal or better quality than that specified.
6. All signs shall be made of aluminum or other material of equal or better quality than that specified.
7. All signs shall be made of aluminum or other material of equal or better quality than that specified.
8. All signs shall be made of aluminum or other material of equal or better quality than that specified.
9. All signs shall be made of aluminum or other material of equal or better quality than that specified.
10. All signs shall be made of aluminum or other material of equal or better quality than that specified.

STANDARD PLANS
 TEXAS DEPARTMENT OF TRANSPORTATION
 Traffic Operations Division

RAILROAD CROSSING
SIGNS and SIGNALS

RCSS-98

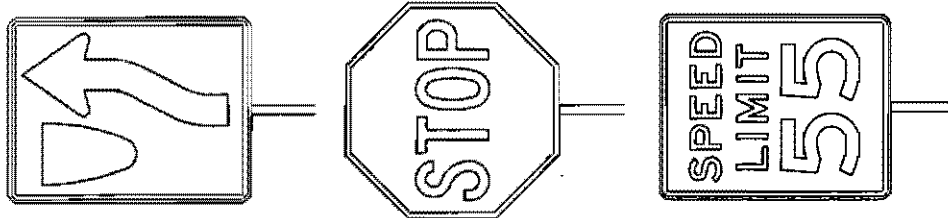
DATE	10/10/98	BY	W. J. ...
SCALE	AS SHOWN	CHECKED BY	...
PROJECT	...	APPROVED BY	...
...

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1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	16	17	18	19	20	21	22	23	24	25	26	27	28	29	30	31	32	33	34	35	36	37	38	39	40	41	42	43	44	45	46	47	48	49	50	51	52	53	54	55	56	57	58	59	60	61	62	63	64	65	66	67	68	69	70	71	72	73	74	75	76	77	78	79	80	81	82	83	84	85	86	87	88	89	90	91	92	93	94	95	96	97	98	99	100
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REQUIREMENTS FOR REGULATORY SIGNS

TYPICAL EXAMPLES



DEPARTMENT MATERIAL SPECIFICATIONS

PAINTED SIGN BLANKS
ALUMINUM SIGN BLANKS

DMS-7100
DMS-7110

Source Ft.	Min. Thickness
Less than 7.5	0.080
7.5 to 15	0.100
Greater than 15	0.125

PLAT SURFACE REFLECTIVE SHEETING
VINYL NON-REFLECTIVE DECAL SHEETING

DMS-4300
DMS-4320

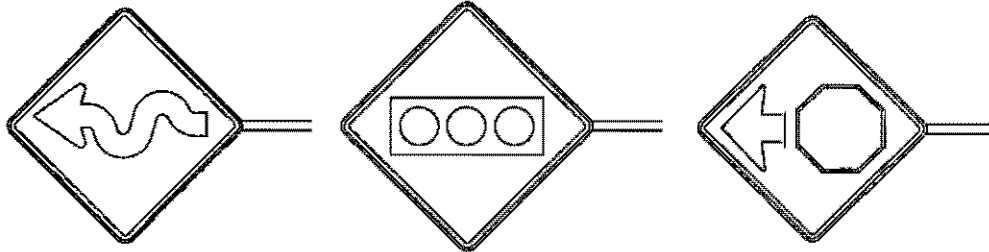
LEAGE COLOR SIGN SHEETING

BACKGROUND ALL TYPE C HIGH SPECIFIC INTERSTITIAL
LEGEND & BORDERS WHITE TYPE C HIGH SPECIFIC INTERSTITIAL
LEGEND & BORDERS BLACK VINYL NON-REFLECTIVE DECAL SHEETING

- GENERAL NOTES
- Signs to be furnished, should be as detailed elsewhere in the plans and shown on sign fabrication sheet. Standard sign designs and error dimensions can be found in the "Standard Highway Sign Designs for Texas" (1950).
 - Regulatory sign legends shall use the Federal Highway Administration (FHWA) Standard Highway Alphanumeric, C, D, E, and/or F1, with the exception of warning legends. Legend spacing of legends shall provide a balanced appearance when reading in cut-out vinyl non-reflective decal sheeting to white background sheeting, or combination thereof.
 - White legends shall be applied by screening process with transparent background sheeting or cut-out with sheeting to colored background sheeting, or combination thereof.
 - Colored legends shall be applied by screening process with transparent colored one for film or colored sheeting to colored ink, transparent colored one for film or colored sheeting to white background sheeting, or combination thereof.
 - Standard sign legends shall be applied by screening process with transparent colored one for film or colored sheeting to white background sheeting, or combination thereof.
 - Requirements for attachments are found on Standard Plan Sheet 158(1D).
 - Mounting details for roadblock mounted signs are shown in the "S&B series" Standard Plan Sheets.

REQUIREMENTS FOR WARNING SIGNS

TYPICAL EXAMPLES



DEPARTMENT MATERIAL SPECIFICATIONS

ALUMINUM SIGN BLANKS

DMS-7110

Source Ft.	Min. Thickness
Less than 7.5	0.080
7.5 to 15	0.100
Greater than 15	0.125

PLAT SURFACE REFLECTIVE SHEETING
VINYL NON-REFLECTIVE DECAL SHEETING

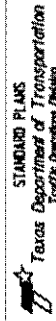
DMS-4300
DMS-4320

LEAGE COLOR SIGN SHEETING

BACKGROUND YELLOW TYPE E INTERESTING PROXIMITY
LEGEND & BORDERS WHITE VINYL NON-REFLECTIVE DECAL SHEETING
LEGEND ALL OTHER TYPE D NON-FLUORESCENT PROXIMITY
(Type VIII or Type IX only)

- GENERAL NOTES
- Signs to be furnished, should be as detailed elsewhere in the plans and shown on sign fabrication sheet. Standard sign designs and error dimensions can be found in the "Standard Highway Sign Designs for Texas" (1950).
 - Warning sign legends shall use the Federal Highway Administration (FHWA) Standard Highway Alphanumeric, C, D, E, and/or F1, with the exception of warning legends. Legend spacing of legends shall provide a balanced appearance when reading in cut-out vinyl non-reflective decal sheeting to white background sheeting, or combination thereof.
 - Colored legends shall be applied by screening process with transparent background sheeting or cut-out with sheeting to white background sheeting, or combination thereof.
 - Requirements for attachments are found on Standard Plan Sheet 158(1D).
 - Mounting details for roadblock mounted signs are shown in the "S&B series" Standard Plan Sheets.

The Standard Highway Sign Designs for Texas (1950) can be found at the following website:
<http://ftp.dot.state.tx.us/pub/texas-transportation/infocenter/infocenter/getinfo.pdf>



TYPICAL SIGN REQUIREMENTS

TSR(5) - 03

DATE	BY	CHKD	APP'D	REV
12-08	6	6	6	6
SIP 2004 13031 004				
DALLAS 13 780 82115818				

PRELIMINARY
NOT FOR REPRODUCTION

Morris County
CSJ 0946-02-019
Project STP 2001(936)RXH
FM 250 E. of Lone Star
DOT No. 840 753Y

STATE OF TEXAS §
COUNTY OF TRAVIS §

THIS AGREEMENT, made on the dates hereinafter shown, by and between the State of Texas, acting by and through the Texas Department of Transportation, hereinafter called the "State", and Texas & Northern Railroad, hereinafter called the "Railroad".

WITNESSETH

WHEREAS, the State and the Railroad propose to install a highway-railroad grade crossing warning system at the crossing shown on the layout marked Exhibit "A", attached hereto and made a part hereof.

NOW, THEREFORE, in consideration of the mutual undertakings as herein set forth, the parties hereto agree as follows:

AGREEMENT

1. RIGHTS. The Railroad and the State agree to the joint installation of a grade crossing warning system across Railroad property at the intersection of the track and highway listed on Exhibit "A".

2. CONDITIONS. These rights, given hereby, shall not in any way conflict or interfere with any prior or existing rights of the Railroad at the crossing. It is agreed that if there are future track modifications at the crossing, the Railroad will restore the highway to proper condition and restore the functional operation of the warning system.

3. DESIGN - PLANS AND ESTIMATE. The State will transmit to the Railroad, for approval, a plan layout for the proposed grade crossing warning system and the Railroad will transmit to the State a detailed cost estimate and circuit diagram plan. The estimate and diagram will reflect the same design as the State plan layout. After having been approved by the State, said plans and estimate are to be marked Exhibit "B" and, by reference, are to be made a part hereof. All traffic control devices installed herein shall comply with the latest edition of the Texas Manual on Uniform Traffic Control Devices for Streets and Highways. Highway design standards shall be in accordance with American Association of State Highway Officials (AASHTO), "A Policy on Geometric Design of Highways and Streets."

4. RESPONSIBILITIES. The Railroad, unless otherwise provided, shall make changes or alterations in the tracks, wire lines, drainage or other facilities located in the crossing right-of-way, which may be required by construction.

5. CONSTRUCTION. The State agrees to furnish such construction stakes and elevations as are required for the installation. The Railroad shall commence the installation within two weeks after receipt of Work Order from the State for such project, and shall proceed diligently to completion of the installation. Materials and labor required for the installation of the warning system are to be furnished by the Railroad. Materials are to be of the kind and quality detailed in the Exhibit "B". Assembly of materials should be undertaken sufficiently in advance to assure prompt delivery to the job site. Payment for any expense whatsoever in connection with the project will

be contingent upon issuance of a Work Order.

6. PAYMENT. Reimbursement to the Railroad will be made for work performed and materials furnished as set forth in the approved cost estimate in accordance with the provisions of the Federal-Aid Policy Guide, Subchapter B, Part 140, Subpart I, issued by the Federal Highway Administration on December 9, 1991 and amendments thereto except as modified by the provisions herein.

The Railroad may submit monthly bills of at least \$500.00 prepared in satisfactory form for work performed and materials installed. Payment will be made for as much as 95% of the costs detailed on the bill.

The Railroad will, in any event, submit a complete and final bill, including all eligible cost, when the project is completed and the State will pay to the Railroad as much as 95% of the cost detailed on the bill.

After the audit of Railroad and Railroad contractor's documentation of the final bill, the State will make a final payment of the remaining balance due the Railroad.

The Railroad may also request, under current State policies, payment for assembled materials designated for the project and delivered to the Railroad or Railroad's contractor, but not in place, by submitting proof of purchase such as an itemized bill from supplier, shipping invoice, or such other evidence of purchase as the State finds acceptable.

The Railroad and Railroad's contractor shall retain adequate cost accounting records for auditing purposes for three years after the final payment.

It has been determined that no benefits shall be construed as accruing to the Railroad and no contribution to this project will be required of the Railroad.

7. MAINTENANCE. The Railroad shall maintain and operate this highway-railroad grade crossing warning system as installed and in accordance with the design of operation as shown in the Exhibit B. No changes are to be made in the design, operation or location of the warning system without the written approval of the State. The State shall trim and maintain trees and vegetation for visibility of crossing signals and signs as well as maintain pavement markings and advance warning signs placed along the roadway.

8. TERMINATION. The State and Railroad mutually agree that this agreement shall be effective from its date, and thereafter until terminated by either party giving to the other eighteen (18) months written notice of its desire to terminate the same. Upon such or any other termination, State will surrender possession of the licensed premises to Railroad. However, it is understood that any prior agreements and/or rights of the parties will not be affected.

9. REGULATIONS. The State and Railroad will comply with all applicable statutes, rules and regulations enacted and promulgated by the United States Government and its Federal Highway Administration.

IN WITNESS WHEREOF, the State and the Railroad have executed duplicate counterparts of this agreement on the dates indicated below.

THE STATE OF TEXAS

RELIMINARY
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Executed for the Executive Director and approved for the Texas Transportation Commission for the purpose and effect of activating and/or carrying out the orders, established policies or work programs heretofore approved and authorized by the Texas Transportation Commission.

By _____ Date
Carlos A. Lopez, Executive Director, Traffic Operations Division

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TEXAS & NORTHERN RAILROAD

By _____ Date

Name and Title

Notices:

For the purposes of this agreement, all notices, correspondence, billings, and other documentation shall be mailed to the following addresses:

For the State of Texas

Railroad Liaison Engineer
Traffic Operations Division (TRF-RR)
125 E. 11th Street
Austin, TX 78701-2483

For Texas & Northern Railroad

Superintendent
Texas & Northern Railroad
Highway 259 South
Lone Star, TX 75668-0300

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AGENDA

**RAILROAD CROSSING RELIABILITY PARTNERSHIP PROGRAM
COORDINATION MEETING**

**THE TEXAS DEPARTMENT OF TRANSPORTATION
AND
THE NORTH CENTRAL TEXAS COUNCIL OF GOVERNMENTS
TRANSPORTATION DEPARTMENT**

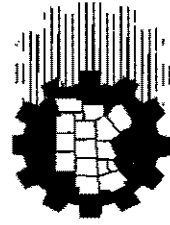
January 24, 2005, 1 p.m.

- 1. Welcome and Introductions: Mike Sims, NCTCOG**
- 2. Program Overview and Status: Mike Sims, NCTCOG**
- 3. The Texas Department of Transportation (TxDOT) Process**
 - a. State Rail Initiatives: Darin Kosmack, TxDOT
 - b. Developing and Executing a Project Agreement with TxDOT
 - c. Diagnostics and Plan Review
 - d. The Local Agreement with the Railroad
 - e. Construction Issues
 - f. Reimbursement and Long-Term Maintenance
- 4. Questions and Answers: Open Discussion**

RAILROAD CROSSING RELIABILITY PARTNERSHIP PROGRAM

Regional Transportation Council

August 12, 2004



**Transportation Department
North Central Texas Council of Governments**

RAILROAD CROSSING RELIABILITY PARTNERSHIP PROGRAM

Process

Internal Peer Reviews	March 12, 2002; April 5, 2002; August 29, 2002
Initial Mailout for Review by Partners	November 13, 2002
Public Meetings	December 4 & 9, 2002
Public Meetings	March 31 – April 2, 2003
Internal Peer Review	June 3, 2003
Project Selection Forum	June 20, 2003
Intermodal, Freight, and Safety Subcommittee Approval of Scoring Criteria	July 10, 2003
Public Meetings	July 14 & 15, 2003
Call-For-Projects Announced	August 8, 2003
Applicant Seminars	August 25, 2003; October 13, 2003
Public Meetings	October 8 & 9, 2003
Call-For-Projects Application Deadline	October 24, 2003
Meeting with Federal Railroad Administration	January 9, 2004
Internal Peer Review	February 10, 2004
External Peer Review	February 25, 2004
Public Meetings	March 8 & 9, 2004
Intermodal, Freight, and Safety Subcommittee	March 11, 2004
Intermodal, Freight, and Safety Subcommittee	May 13, 2004
Public Meetings	June 7 & 8, 2004
Regional Transportation Council (Information)	June 10, 2004
Letters sent to City Managers and County Administrators	June 15, 2004
Surface Transportation Technical Committee (Information)	June 18, 2004
Regional Transportation Council (Information)	July 8, 2004
Surface Transportation Technical Committee (Approval)	July 23, 2004
Regional Transportation Council (Possible Approval)	August 12, 2004

RAILROAD CROSSING RELIABILITY PARTNERSHIP PROGRAM

Call-For-Projects Rules

1. Eligible project types include closures, improved gates, structural barriers, channelization, Intelligent Transportation Systems, improvements to roadway geometrics or site lines, improved crossing surfaces, access roads, pedestrian or bicycle facilities, improved signs or warning devices, and other capital expenses necessary to improve the safety and/or reliability of an at-grade railroad crossing.
2. Project applicants may access the application on the NCTCOG website at <http://www.nctcog.org/crossings>.
3. All applicants must provide a signed paper copy of their complete application to NCTCOG no later than 5:00 p.m., Friday, October 24, 2003.
4. All projects require a minimum non-federal share of 20%.
5. Private entities may submit projects for consideration but funds shall only flow through local government entities.
6. Minimum federal award of \$100,000 per project. (Requires a minimum \$25,000 local match.)
7. Maximum federal award of \$800,000 per crossing. (Requires a minimum \$200,000 local match.)
8. Overmatching the projects with additional non-federal funds is allowed.
9. The federal share of submittals from any single entity may not exceed half of all available federal funds (\$4,695,000).
10. Right-of-way acquisition costs are not eligible for funding under this call-for-projects.
11. Given the limited availability of federal funds and the need to expedite improvements, project applicants are encouraged to cover the full cost of design, engineering and administration with non-federal funds.
12. Federal funds will not be eligible to fund cost overruns.
13. NCTCOG staff will generate final project rankings by benefit/cost comparison.
14. The Regional Transportation Council has final authority on project selection.

Railroad Crossing Reliability Partnership Program: Scoring Criteria *

	Criteria	Points Available	Scoring
1	Crossing Exposure Exposure based on average daily traffic, rail passengers, train counts, and accident history	0 – 40 Per Crossing	40 points Very High Exposure 20 points High Exposure 10 points Medium Exposure 5 points Low Exposure 0 points Very Low Exposure
2	Risk Reduction Strategy	0 –30 Per Crossing	30 points Closure, grade separation, quad gates, or dual gates with full channelization 15 points Dual Gates without full channelization 0 points Cantilevered or Mast Flashers, Stop Signs, Crossbucks
3	Site-Specific Community Benefits Problem types include: site distance, geometrics, emergency vehicle routing, vehicle access, pedestrian movements, blockage by stopped trains, noise, and other community factors	0 – 30 Per Crossing	30 points High Impact 15 points Medium Impact 7 points Low Impact 0 points Very Low Impact
4	Corridor Strategies	0 – 10 Once Per Crossing (for each crossing within the corridor)	10 points Consolidates two or more crossings or signals along a road or railroad (through closure or dynamically linking the operations) 5 points Makes other improvements at two or more crossings in a corridor

* Endorsed by the Intermodal, Freight, and Safety Subcommittee of the Regional Transportation Council on July 10, 2003.

RAILROAD CROSSING RELIABILITY PARTNERSHIP PROGRAM

Results Analysis

- 1. Improvements at a total of 61 crossings are recommended for funding**
- 2. All crossings with two or more accidents (based on Federal Railroad Administration data) were funded.
(Total of 8 crossings)**
- 3. All crossings with a benefit score of 75 points or greater were funded.
(Total of 11 crossings)**
- 4. All corridors which included closing at least one crossing were funded.
(Total of 7 crossings in 3 corridors)**

RAILROAD CROSSING RELIABILITY PARTNERSHIP PROGRAM

NCTCOG Staff Recommendation

- 1. Initial draft rankings were approved by the Surface Transportation Technical Committee on July 23, 2004.**
- 2. In TxDOT's Dallas District, utilize \$5,972,000 in federal funds to implement the top 15 ranked projects (shown on page 7).**
- 3. In TxDOT's Fort Worth District, utilize \$2,968,000 in federal funds to implement the top 7 ranked projects (shown on page 8).**
- 4. Consider railroad crossings along the Trinity Railway Express (\$5,969,942 in federal funds shown on page 9) as part of Partnership Program Two.**
- 5. Allocate \$450,000 for a multi-year regional safety program, including targeted education efforts at high risk crossing areas.**

JUNE 2004
Railroad Crossing Reliability Partnership Program

RANK	PROJECT LOCATION	APP #	STREET	RR OWNER	APPLICANT	PROJECT EXPOSURE SCORE	PROJECT PROTECTION SCORE	COMMUNITY BENEFITS SCORE	PROJECT CORRIDOR SCORE	TOTAL PROJECT POINTS	TOTAL PROJECT COST	PROJECT BENEFIT INDEX	REQUESTED FEDERAL FUNDS*	RECOMMENDED FEDERAL FUNDS	IMPLEMENTATION YEAR	FEDERAL FUNDS REMAINING	PROJECT DESCRIPTION	
FEDERAL FUNDS AVAILABLE																		
1	SOUTH GARLAND KCS RAILROAD CROSSINGS	284	KINGSLEY RD	KCS	Grand	15	75	87	30	187	\$145,000	12.90	\$116,000	\$116,000	FY 2005	\$5,850,000	INSTALL ITS TRAIN DETECTION RE-SURFACE	
2	COIT AT KCS	203	LEON RD	KCS	Grand	40	15	7	0	62	\$125,000	4.08	\$100,000	\$100,000	FY 2005	\$5,750,000	RE-SURFACE	
3	US 75 NB FRONTAGE ROAD AT DART	131	SHILOH RD	KCS	Plano	40	15	7	0	62	\$125,000	4.08	\$100,000	\$100,000	FY 2005	\$5,650,000	RE-SURFACE	
4	KEALY STREET AT KCS	111	COIT (E OF PLANO PKWY)	KCS	Plano	10	90	15	0	55	\$190,000	2.88	\$152,000	\$152,000	FY 2005	\$5,501,000	INSTALL DUAL GATES AND ADD PAVEMENT MARKINGS	
5	PLANO PKWY AT DART	207	US 75 FRONTAGE (NB)	DART	Lewisville	10	15	7	0	32	\$125,000	2.56	\$100,000	\$100,000	FY 2005	\$5,401,000	RE-SURFACE	
6	19TH STREET AT UPRR	133	19TH ST (SW)	UP	Grand Prairie	40	30	15	5	90	\$350,000	2.37	\$304,000	\$304,000	FY 2005	\$5,100,000	INSTALL DUAL GATES, MEDIAN, AND WAYSIDE HORN	
7	SOUTHERN DALLAS RAILROAD CROSSINGS	282	CLEVELAND RD	BNSF	Dallas	0	30	14	10	54	\$250,000	2.16	\$200,000	\$200,000	FY 2005	\$4,900,000	RE-SURFACE	
8	US 75 SB FRONTAGE ROAD AT DART	132	WITT RD	BNSF	Dallas	5	15	7	0	27	\$125,000	2.16	\$100,000	\$100,000	FY 2005	\$4,800,000	RE-SURFACE	
9	ADDISON RAILROAD CROSSINGS	185	US 75 FRONTAGE (SB)	DART	Plano	25	90	37	10	132	\$450,000	2.10	\$504,000	\$504,000	FY 2005	\$4,296,000	INSTALL DUAL GATES (ALSO RE-SURFACE AT ADDISON RD)	
10	EAST DALLAS / RAILROAD CROSSINGS	255	SURVEYOR BLVD	DART	Addicks													UPGRADE GATES AND INSTALL MEDIANS AT ALL LOCATIONS. INSTALL SIGNAGE AT PEAY, GUS THOMASSON, LAKELAND, HIGHLAND, AND ST FRANCIS RE-SURFACE AT LAKELAND AND ST FRANCIS
11	BAGDAD ROAD AT UPRR	149	PEAY RD	KCS	Dallas	20	30	7	5	62	\$300,000	1.63	\$304,000	\$304,000	FY 2005	\$2,184,000	INSTALL DUAL GATES, MEDIAN, AND WAYSIDE HORN	
12	CR 605 AT KCS	140	GUS THOMASSON RD	KCS	Dallas	20	30	7	0	57	\$332,000	1.62	\$281,500	\$281,500	FY 2005	\$1,902,400	INSTALL DUAL GATES, IMPROVE DRAINAGE	
13	14TH STREET SW AT UPRR	108	BARNES BRIDGE RD	KCS	Dallas	10	30	15	5	60	\$390,000	1.56	\$304,000	\$304,000	FY 2005	\$1,598,400	INSTALL DUAL GATES, MEDIAN, AND WAYSIDE HORN	
14	MACON AT UPRR	275	LAKELAND DR	KCS	Dallas	10	30	30	0	70	\$443,000	1.58	\$354,400	\$354,400	FY 2005	\$1,244,000	INSTALL MEDIANS AND FENCING	
15	NORTH DALLAS / DART RAILROAD CROSSINGS	238	SANTA ANNA AVE	KCS	Dallas	50	285	70	50	455	\$2,800,000	1.57	\$2,320,000	\$1,244,000	FY 2005	\$0	UPGRADE GATES AND EXTEND CROSSING SURFACE TO ACCOMMODATE PEDESTRIANS AT ALL LOCATIONS. UPGRADE TO DUAL GATES AND WIDEN ROADWAY APPROACHES AT ALL LOCATIONS. RE-SURFACE PEAY, GUS THOMASSON, LAKELAND, HIGHLAND, AND ST FRANCIS. RE-SURFACE AT DART CROSSING AT DAVENPORT (EAST-WEST PORTION), KNOLL TRAIL, MEANDERING WAY, AND DAVENPORT (NORTH-SOUTH PORTION)	
16	VALLEY VIEW LANE AT DART	112	MACON AT UPRR	UP	Dallas	10	30	7	0	47	\$320,000	1.47	\$259,000	\$0	N/A	\$0	RE-SURFACE, UPGRADE GATES	
17	2ND STREET SW AT UPRR	135	CAMPBELL RD	DART	Dallas	5	30	7	5	47	\$325,000	1.45	\$200,000	\$0	N/A	\$0	INSTALL DUAL GATES AND MEDIAN	
18	5TH STREET SE AT UPRR	143	HILLCREST	DART	Dallas	10	30	7	5	52	\$390,000	1.37	\$304,000	\$0	N/A	\$0	INSTALL DUAL GATES, MEDIAN, AND WAYSIDE HORN	
19	5TH STREET SW AT UPRR	148	COIT	DART	Dallas	10	30	7	5	52	\$390,000	1.37	\$304,000	\$0	N/A	\$0	INSTALL DUAL GATES, MEDIAN, AND WAYSIDE HORN	
20	CENTER STREET AT UPRR	147	KNOLL TRAIL DR	DART	Dallas	5	30	7	5	47	\$390,000	1.24	\$304,000	\$0	N/A	\$0	INSTALL DUAL GATES, MEDIAN, AND WAYSIDE HORN	
21	JOSEPH HARDIN SPUR LINE (SOUTH)	286	MEANDERING WAY	DART	Dallas	0	0	7	5	12	\$125,000	0.96	\$100,000	\$0	N/A	\$0	RE-SURFACE	
22	JOSEPH HARDIN ROAD AT BNSF	287	DAVENPORT (NORTH-SOUTH PORTION)	DART	Dallas	0	0	7	5	12	\$125,000	0.96	\$100,000	\$0	N/A	\$0	RE-SURFACE	
23	HART WAY AT BNSF	288	DICKERSON	DART	Dallas	0	0	7	0	7	\$125,000	0.56	\$100,000	\$0	N/A	\$0	RE-SURFACE	
24	COCKRELL HILL ROAD AT BNSF	289	DALLAS PKWY	DART	Dallas	5	0	7	0	12	\$250,000	0.46	\$200,000	\$0	N/A	\$0	RE-SURFACE	

* All projects require a minimum non-federal share of 20%. Right-of-way acquisition costs are not eligible for funding under this rail-for-projects. Federal funds will not be eligible to fund cost overruns.

JUNE 2004
Railroad Crossing Reliability Partnership Program

RANK	PROJECT LOCATION	APP #	STREET	RR OWNER	PROJECT EXPOSURE SCORE	PROJECT PROTECTION SCORE	COMMUNITY BENEFITS SCORE	PROJECT CORRIDOR SCORE	TOTAL PROJECT POINTS	TOTAL PROJECT COST	PROJECT BENEFIT INDEX	REQUESTED FEDERAL FUNDS	RECOMMENDED FEDERAL FUNDS	IMPLEMENTATION YEAR	FEDERAL FUNDS REMAINING	PROJECT DESCRIPTION
FEDERAL FUNDS AVAILABLE																
1	STAMORE SCHOOL ROAD CROSSINGS	154	STAMORE SCHL RD	BNSF	15	60	14	10	89	\$148,000	8.64	\$119,200	\$119,200	FY 2005	\$2,945,900	INSTALL/UPGRADE MEDIANS, INSTALL SIDEWALK SURFACE, UPGRADE SIGNAL, ADD ITS CAMERA
2	HALTOM CITY RAILROAD CROSSINGS	103	HALTOM RD	UP	15	75	75	25	180	\$420,000	4.52	\$336,000	\$800,000	FY 2005	\$2,448,800	INSTALL GATES AT HALTOM ROAD AND DART RAILROAD CROSSINGS AT FORT WORTH LOCATIONS IN PREPARATION FOR A QUIET ZONE
3	HEMPHILL WEST RAILROAD CROSSINGS	174	HEMPHILL ST	BNSF	25	130	105	50	330	\$620,000	4.02	\$494,000	\$494,000	FY 2005	\$1,892,600	CLOSE CAPPS AND JENNINGS, INSTALL ADDITIONAL GATES AND SIDEWALKS AT THREE LOCATIONS, ADD ITS CAMERA
4	MAGNOLIA AVENUE RAILROAD CROSSINGS	172	JENNINGS AVE (SOUTH)	BNSF	13	60	60	20	153	\$465,000	3.83	\$384,000	\$384,000	FY 2003	\$1,669,800	CLOSE BOTH CROSSINGS, BUILD ALTERNATE ACCESS ROUTE
5	UP MAIN E/W LINE THROUGH ARLINGTON	151	DAVIS DR	UP	115	150	75	25	365	\$1,023,700	3.57	\$618,900	\$618,900	FY 2005	\$849,840	INSTALL CONSTANT WARNING TIME DEVICE AND HIGH INTENSITY LED LIGHTS AT ALL LOCATIONS, INSTALL SIDEWALKS AT GREAT SOUTHWEST PARKWAY AND DAMAS, INSTALL CONCRETE SURFACE AT GREAT SOUTHWEST PARKWAY, INSTALL MEDIAN BARRIERS AT STADIUM, COLLINS, AND BOWEN LOWER TRACK AT COLLINS TO IMPROVE SURFACE
6	AVENUE E EAST AT UPRR	156	AVE E EAST	UP	10	30	7	0	47	\$180,000	2.94	\$138,000	\$138,000	FY 2005	\$721,840	INSTALL WAYSIDE HORN AND CONSTANT WARNING TIME DEVICE
7	PEACH STREET RAILROAD CROSSINGS	179	PEACH (WEST)	BNSF	15	105	127	50	297	\$1,060,000	2.80	\$548,000	\$721,840	FY 2005	\$0	CLOSE THE CROSSING OF 1300 EAST 1ST STREET AND THE UPRR LINE, INSTALL A MEDIAN AT 1000 PEACH STREET AND THE BNSF LINE, INSTALL A MEDIAN AT 800 EAST 1ST STREET AND THE BNSF LINE, MODIFY STREET ACCESS AT 1500 EAST 1ST STREET AND THE UPRR LINE, IMPROVE THE CROSSING SURFACE AT SAMUELS AVENUE AND THE UPRR LINE, IMPROVED PEDESTRIAN IMPROVEMENTS
8	OAKLAND AT UPRR	166	OAKLAND	UP	5	30	30	5	70	\$273,000	2.55	\$220,000	\$0	N/A	\$0	UPGRADE TO QUAD GATES, INSTALL SIDEWALK SURFACE AND ITS CAMERA
9	SIX FLAGS DRIVE AT UPRR	163	SIX FLAGS DR	UP	0	30	15	0	45	\$165,500	2.43	\$145,400	\$0	N/A	\$0	INSTALL MEDIAN, LED WARNING LIGHTS, AND CONSTANT WARNING TIME DEVICE, RAISE TRACK TO IMPROVE SURFACE
10	BASSWOOD BOULEVARD AT UPRR	165	BASSWOOD BLVD	UP	20	30	7	5	62	\$260,000	2.38	\$208,000	\$0	N/A	\$0	UPGRADE TO QUAD GATES, INSTALL SIDEWALK SURFACE
11	HUDSON CEMETARY ROAD AT UPRR	122	HUDSON CEMETARY RD	UP	5	15	15	0	35	\$175,000	2.00	\$140,000	\$0	N/A	\$0	INSTALL GATES AND LIGHTS
12	TIERNEY ROAD AT UPRR	180	TIERNEY RD	UP	10	30	7	5	52	\$260,000	2.00	\$208,000	\$0	N/A	\$0	UPGRADE TO QUAD GATES, INSTALL SIDEWALK SURFACE
13	AYERS AVENUE AT UPRR	187	AYERS	UP	5	30	7	5	47	\$260,000	1.81	\$208,000	\$0	N/A	\$0	UPGRADE TO QUAD GATES, INSTALL SIDEWALK SURFACE
14	WINNIE STREET AT UPRR	181	WINNIE	UP	5	30	7	5	47	\$260,000	1.81	\$208,000	\$0	N/A	\$0	UPGRADE TO QUAD GATES, INSTALL SIDEWALK SURFACE
15	TARRANT PARKWAY NORTH AT UPRR	186	TARRANT PARKWAY (NORTH)	UP	5	30	7	5	47	\$260,000	1.81	\$208,000	\$0	N/A	\$0	UPGRADE TO QUAD GATES, INSTALL SIDEWALK SURFACE
16	KATY ROAD / TIMBERLAND DRIVE AT UPRR	123	KATY RD / TIMBERLAND DR	UP	5	30	0	0	35	\$225,000	1.56	\$180,000	\$0	N/A	\$0	INSTALL GATES AND LIGHTS
17	AVENUE H AT UPRR	166	AVE H	UP	0	15	7	0	22	\$160,000	1.36	\$128,000	\$0	N/A	\$0	INSTALL CONSTANT WARNING TIME DEVICE AND WAYSIDE HORN
18	23RD STREET SW AT UPRR	134	23RD ST (SW)	UP	10	30	7	5	52	\$360,000	1.37	\$304,000	\$0	N/A	\$0	INSTALL QUAD GATES, MEDIAN, AND WAYSIDE HORN
19	ERIE STREET AT UPRR	240	ERIE	UP	10	30	7	5	52	\$445,000	1.17	\$368,000	\$0	N/A	\$0	UPGRADE TO QUAD GATES, INSTALL SIDEWALK SURFACE
20	HANDLEY DRIVE AT UPRR	239	HANDLEY DR	UP	10	30	7	5	52	\$450,000	1.16	\$380,000	\$0	N/A	\$0	UPGRADE TO QUAD GATES, INSTALL SIDEWALK SURFACE AND ITS CAMERA

* All projects require a minimum non-federal share of 20%. Right-of-way acquisition costs are not eligible for funding under this cost-for-project. Federal funds will not be eligible to fund cost overruns.

** Reflects funding received in May 2004 from TxDOT's Section 130 Program.

RAILROAD CROSSING RELIABILITY PARTNERSHIP PROGRAM

NCTCOG Staff Recommendation

Trinity Railway Express Railroad Crossings
To Be Considered Under Partnership Program Two
(Shown in order from West to East)

Judkins	Greenbelt
Galvez	Motocross
Riverside	Mosier Valley
Beach	Calloway Cemetary
Haltom	Tarrant Main
Elliot Reeder	Valley View
Carson	Irving Yard Way
Minnis	Gilbert
Handley-Ederville	Irby
Precinct Line	Rogers
Norwood	MacArthur
Bell Spur	
Total Project Cost:	\$7,462,427
Federal Funds Requested:	\$5,969,942
Local Matching Funds Required:	\$1,492,485

RAILROAD CROSSING RELIABILITY PARTNERSHIP PROGRAM

Requested Action

- 1. Approve staff recommended projects**
- 2. Place projects in the TIP**

RAILROAD CROSSING RELIABILITY PARTNERSHIP PROGRAM

For more information:

**Mike Sims
(817) 695-9226
msims@nctcog.org**

**Rachel Harshman
(817) 608-2395
rharshman@nctcog.org**

RAILROAD CROSSING RELIABILITY PARTNERSHIP PROGRAM

Regional Transportation Council

August 12, 2004



**Transportation Department
North Central Texas Council of Governments**

RAILROAD CROSSING RELIABILITY PARTNERSHIP PROGRAM

Process

Internal Peer Reviews	March 12, 2002; April 5, 2002; August 29, 2002
Initial Mailout for Review by Partners	November 13, 2002
Public Meetings	December 4 & 9, 2002
Public Meetings	March 31 – April 2, 2003
Internal Peer Review	June 3, 2003
Project Selection Forum	June 20, 2003
Intermodal, Freight, and Safety Subcommittee Approval of Scoring Criteria	July 10, 2003
Public Meetings	July 14 & 15, 2003
Call-For-Projects Announced	August 8, 2003
Applicant Seminars	August 25, 2003; October 13, 2003
Public Meetings	October 8 & 9, 2003
Call-For-Projects Application Deadline	October 24, 2003
Meeting with Federal Railroad Administration	January 9, 2004
Internal Peer Review	February 10, 2004
External Peer Review	February 25, 2004
Public Meetings	March 8 & 9, 2004
Intermodal, Freight, and Safety Subcommittee	March 11, 2004
Intermodal, Freight, and Safety Subcommittee	May 13, 2004
Public Meetings	June 7 & 8, 2004
Regional Transportation Council (Information)	June 10, 2004
Letters sent to City Managers and County Administrators	June 15, 2004
Surface Transportation Technical Committee (Information)	June 18, 2004
Regional Transportation Council (Information)	July 8, 2004
Surface Transportation Technical Committee (Approval)	July 23, 2004
Regional Transportation Council (Possible Approval)	August 12, 2004

RAILROAD CROSSING RELIABILITY PARTNERSHIP PROGRAM

Call-For-Projects Rules

1. Eligible project types include closures, improved gates, structural barriers, channelization, Intelligent Transportation Systems, improvements to roadway geometrics or site lines, improved crossing surfaces, access roads, pedestrian or bicycle facilities, improved signs or warning devices, and other capital expenses necessary to improve the safety and/or reliability of an at-grade railroad crossing.
2. Project applicants may access the application on the NCTCOG website at <http://www.nctcog.org/crossings>.
3. All applicants must provide a signed paper copy of their complete application to NCTCOG no later than 5:00 p.m., Friday, October 24, 2003.
4. All projects require a minimum non-federal share of 20%.
5. Private entities may submit projects for consideration but funds shall only flow through local government entities.
6. Minimum federal award of \$100,000 per project. (Requires a minimum \$25,000 local match.)
7. Maximum federal award of \$800,000 per crossing. (Requires a minimum \$200,000 local match.)
8. Overmatching the projects with additional non-federal funds is allowed.
9. The federal share of submittals from any single entity may not exceed half of all available federal funds (\$4,695,000).
10. Right-of-way acquisition costs are not eligible for funding under this call-for-projects.
11. Given the limited availability of federal funds and the need to expedite improvements, project applicants are encouraged to cover the full cost of design, engineering and administration with non-federal funds.
12. Federal funds will not be eligible to fund cost overruns.
13. NCTCOG staff will generate final project rankings by benefit/cost comparison.
14. The Regional Transportation Council has final authority on project selection.

Railroad Crossing Reliability Partnership Program: Scoring Criteria *

	Criteria	Points Available	Scoring
1	Crossing Exposure Exposure based on average daily traffic, rail passengers, train counts, and accident history	0 – 40 Per Crossing	40 points Very High Exposure 20 points High Exposure 10 points Medium Exposure 5 points Low Exposure 0 points Very Low Exposure
2	Risk Reduction Strategy	0 – 30 Per Crossing	30 points Closure, grade separation, quad gates, or dual gates with full channelization 15 points Dual Gates without full channelization 0 points Cantilevered or Mast Flashers, Stop Signs, Crossbucks
3	Site-Specific Community Benefits Problem types include: site distance, geometrics, emergency vehicle routing, vehicle access, pedestrian movements, blockage by stopped trains, noise, and other community factors	0 – 30 Per Crossing	30 points High Impact 15 points Medium Impact 7 points Low Impact 0 points Very Low Impact
4	Corridor Strategies	0 – 10 Once Per Crossing (for each crossing within the corridor)	10 points Consolidates two or more crossings or signals along a road or railroad (through closure or dynamically linking the operations) 5 points Makes other improvements at two or more crossings in a corridor

* Endorsed by the Intermodal, Freight, and Safety Subcommittee of the Regional Transportation Council on July 10, 2003.

RAILROAD CROSSING RELIABILITY PARTNERSHIP PROGRAM

Results Analysis

- 1. Improvements at a total of 61 crossings are recommended for funding**
- 2. All crossings with two or more accidents (based on Federal Railroad Administration data) were funded.
(Total of 8 crossings)**
- 3. All crossings with a benefit score of 75 points or greater were funded.
(Total of 11 crossings)**
- 4. All corridors which included closing at least one crossing were funded.
(Total of 7 crossings in 3 corridors)**

RAILROAD CROSSING RELIABILITY PARTNERSHIP PROGRAM

NCTCOG Staff Recommendation

- 1. Initial draft rankings were approved by the Surface Transportation Technical Committee on July 23, 2004.**
- 2. In TxDOT's Dallas District, utilize \$5,972,000 in federal funds to implement the top 15 ranked projects (shown on page 7).**
- 3. In TxDOT's Fort Worth District, utilize \$2,968,000 in federal funds to implement the top 7 ranked projects (shown on page 8).**
- 4. Consider railroad crossings along the Trinity Railway Express (\$5,969,942 in federal funds shown on page 9) as part of Partnership Program Two.**
- 5. Allocate \$450,000 for a multi-year regional safety program, including targeted education efforts at high risk crossing areas.**

JUNE 2004
Railroad Crossing Reliability Partnership Program

RANK	PROJECT LOCATION	APP #	STREET	RD OWNER	APPLICANT	PROJECT EXPOSURE SCORE	PROJECT PROTECTION SCORE	COMMUNITY BENEFITS SCORE	PROJECT CORRIDOR SCORE	TOTAL PROJECT POINTS	TOTAL PROJECT COST	REQUESTED FEDERAL FUNDS*	RECOMMENDED FEDERAL FUNDS	IMPLEMENTATION YEAR	FEDERAL FUNDS REMAINING	PROJECT DESCRIPTION
FEDERAL FUNDS AVAILABLE																
FEDERAL FUNDS AVAILABLE																
1	SOUTH GARLAND KCS RAILROAD CROSSINGS	284	LEON RD	KCS	Grand Prairie	15	75	67	30	187	\$145,000	\$116,000	\$116,000	FY 2003	\$5,859,000	INSTALL ITS / TRAIN DETECTION RE-SURFACE
2	COIT AT KCS	203	SHILOH RD	KCS	Philo	40	15	7	0	62	\$123,000	\$100,000	\$100,000	FY 2003	\$5,759,000	RE-SURFACE
3	US 75 NB FRONTAGE ROAD AT DART	131	US 75 FRONTAGE (NB)	DART	Philo	40	15	7	0	62	\$125,000	\$100,000	\$100,000	FY 2003	\$5,659,000	RE-SURFACE
4	NEALY STREET AT KCS	111	NEALY (NORTH)	KCS	Lewisville	10	30	15	0	55	\$160,000	\$152,000	\$152,000	FY 2003	\$5,504,000	INSTALL DUAL GATES AND ADD PAVEMENT MARKINGS
5	PLANO PKWAY AT DART	207	PLANO PKWAY	DART	Philo	10	15	7	0	32	\$125,000	\$100,000	\$100,000	FY 2003	\$5,404,000	RE-SURFACE
6	16TH STREET AT UPRR	133	16TH ST (SW)	UP	Grand Prairie	40	30	15	5	90	\$300,000	\$304,000	\$304,000	FY 2003	\$5,100,000	INSTALL QUAD GATES, MEDIAN, AND WAYSIDE HORN
7	SOUTHERN DALLAS RAILROAD CROSSINGS	262	CLEVELAND RD	BNSF	Dallas	0	30	14	10	54	\$250,000	\$200,000	\$200,000	FY 2003	\$4,900,000	RE-SURFACE
8	US 75 SB FRONTAGE ROAD AT DART	132	US 75 FRONTAGE (SB)	DART	Philo	5	15	7	0	27	\$125,000	\$100,000	\$100,000	FY 2003	\$4,800,000	RE-SURFACE
9	ADDISON RAILROAD CROSSINGS	165	ADDISON RD	DART	Addition	25	60	37	10	132	\$430,000	\$504,000	\$504,000	FY 2003	\$4,296,000	INSTALL QUAD GATES (ALSO RE-SURFACE AT ADDISON RD)
10	EAST DALLAS / KCS RAILROAD CROSSINGS	255	PEAY RD	KCS	Dallas	35	240	79	40	394	\$2,260,000	\$1,868,000	\$1,868,000	FY 2003	\$2,428,000	UPGRADE GATES AND INSTALL MEDIANS AT ALL CROSSINGS. RE-SURFACE AT PEAY, BARNES BRIDGE RD, CENTREVILLE RD, LAKELAND DR, HIGHLAND DR, SANTA ANNA AVE AND ST FRANCIS. RE-SURFACE AT LABEL AND AND ST FRANCIS
11	BAGDAD ROAD AT UPRR	148	BAGDAD RD	UP	Grand Prairie	20	30	7	5	62	\$390,000	\$304,000	\$304,000	FY 2003	\$2,164,000	INSTALL QUAD GATES, MEDIAN, AND WAYSIDE HORN
12	CR 802 AT KCS	140	CR 802	KCS	Collin County	20	30	7	0	57	\$352,000	\$281,600	\$281,600	FY 2003	\$1,902,400	INSTALL QUAD GATES, IMPROVE DRAINAGE
13	14TH STREET SW AT UPRR	108	14TH ST (SW)	UP	Grand Prairie	10	30	15	5	60	\$360,000	\$304,000	\$304,000	FY 2003	\$1,598,400	INSTALL QUAD GATES, MEDIAN, AND WAYSIDE HORN
14	MACON AT UPRR	275	MACON	UP	Dallas	10	30	30	0	70	\$443,000	\$354,400	\$354,400	FY 2003	\$1,244,000	INSTALL MEDIANS AND FENCING
15	NORTH DALLAS / DART RAILROAD CROSSINGS	236	MCCALLUM	DART	Dallas	50	285	70	50	455	\$2,900,000	\$2,320,000	\$2,320,000	FY 2003	\$0	UPGRADE GATES AND EXTEND CROSSING SUPPLY TO NEIGHBORING MEDIANS AT ALL CROSSINGS. UPGRADE TO QUAD GATES AND WIDEN ROADWAY APPROACHES AT DICKERSON RE-SURFACE VEHICLE CROSSING AT COIT AND DICKERSON. INSTALL MEDIANS AT DAVENPORT (EAST-WEST PORTION), KNOLL TRAIL, MEANDERING WAY, AND DAVENPORT (NORTH-SOUTH PORTION)
16	VALLEY VIEW LANE AT DART	112	VALLEY VIEW LN	DART	Farmers Branch	10	30	7	0	47	\$320,000	\$256,000	\$256,000	N/A	\$0	RE-SURFACE, UPGRADE GATES
17	2ND STREET SW AT UPRR	135	2ND ST (SW)	UP	Grand Prairie	5	30	7	5	47	\$225,000	\$200,000	\$200,000	N/A	\$0	INSTALL QUAD GATES AND MEDIAN
18	5TH STREET SE AT UPRR	145	5TH (SE)	UP	Grand Prairie	10	30	7	5	52	\$390,000	\$304,000	\$304,000	N/A	\$0	INSTALL QUAD GATES, MEDIAN, AND WAYSIDE HORN
19	5TH STREET SW AT UPRR	148	5TH (SW)	UP	Grand Prairie	10	30	7	5	52	\$390,000	\$304,000	\$304,000	N/A	\$0	INSTALL QUAD GATES, MEDIAN, AND WAYSIDE HORN
20	CENTER STREET AT UPRR	147	CENTER ST	UP	Grand Prairie	5	30	7	5	47	\$390,000	\$304,000	\$304,000	N/A	\$0	INSTALL QUAD GATES, MEDIAN, AND WAYSIDE HORN
21	JOSEPH HARDIN ROAD AT BNSF	268	JOSEPH HARDIN DR	BNSF	Dallas	0	0	7	5	12	\$125,000	\$100,000	\$100,000	N/A	\$0	RE-SURFACE
22	ROAD AT BNSF	267	JOSEPH HARDIN RD	BNSF	Dallas	0	0	7	5	12	\$125,000	\$100,000	\$100,000	N/A	\$0	RE-SURFACE
23	MANT WAY AT BNSF SPUR	266	MANT	BNSF	Dallas	0	0	7	0	7	\$125,000	\$100,000	\$100,000	N/A	\$0	RE-SURFACE
24	COCKRELL HILL ROAD AT BNSF SPUR	269	COCKRELL HILL RD	BNSF	Dallas	5	0	7	0	12	\$250,000	\$200,000	\$200,000	N/A	\$0	RE-SURFACE

* All projects require a minimum non-federal share of 20%. Right-of-way acquisition costs are not eligible for funding under this call-for-proposals. Federal funds will not be eligible to fund cost overruns.

RANK	PROJECT LOCATION	APP #	STREET	ITR OWNER	APPLICANT	PROJECT EXPIRE SCORE	PROJECT PROTECTION SCORE	COMMUNITY BENEFITS SCORE	PROJECT CORP. SCORE	TOTAL PROJECT POINTS	TOTAL PROJECT COST	PROJECT BENEFIT INDEX	REQUESTED FEDERAL FUNDS*	RECOMMENDED FEDERAL FUNDS	IMPLEMENTATION YEAR	FEDERAL FUNDS REMAINING	PROJECT DESCRIPTION
FEDERAL FUNDS AVAILABLE																	
1	SYCAMORE SCHOOL CROSSINGS	184	SYCAMORE SCHL RD	BNSF	Fort Worth	15	80	14	10	09	\$148,000	6.84	\$110,200	\$119,200	FY 2005	\$2,848,800	INSTALL/IMPROVE MEDIANS, INSTALL SIDEWALK SURFACE, UPGRADE SIGNAL, ADD ITS CAMERA
2	HALTOM CITY RAILROAD CROSSINGS	183	SYCAMORE SCHL RD	UP	Fort Worth	15	75	75	25	100	\$420,000	4.52	\$206,000	\$200,000**	FY 2005	\$2,848,800	INSTALL GATES AT HALTOM ROAD AND DART UPGRADE ELECTRONICS AT ALL OTHER LOCATIONS IN PREPARATION FOR A QUIET ZONE
3	HEMPHILL WEST RAILROAD CROSSINGS	174	HEMPHILL ST	BNSF	Fort Worth	25	150	103	50	330	\$820,000	4.02	\$254,000	\$458,000	FY 2005	\$1,892,800	CLOSE CAPPS AND JENNINGS, INSTALL ADDITIONAL GATES AND SIDEWALKS AT THREE LOCATIONS, ADD ITS CAMERA
4	MAGNOLIA AVENUE RAILROAD CROSSINGS	177	MAGNOLIA (EAST)	BNSF	Fort Worth	15	60	80	20	155	\$405,000	3.83	\$224,000	\$324,000	FY 2005	\$1,892,800	CLOSE BOTH CROSSINGS, BUILD ALTERNATE ACCESS ROUTE
5	UP MAIN E-W LINE THROUGH ARLINGTON	141	BOHLEN RD	UP	Arlington												INSTALL CONSTANT WARNING TIME DEVICE AND HIGH INTENSITY LED LIGHTS AT ALL LOCATIONS UPGRADE TO QUAD GATES AT GREAT SOUTHWEST PARKWAY AND DAVIS INSTALL CONCRETE SURFACE AT GREAT SOUTHWEST PARKWAY, INSTALL MEDIAN BARRIERS AT STADIUM COLLINS, AND BOWEN, LOWER TRACK AT COLLINS TO IMPROVE SURFACE
6	AVENUE E EAST AT UPRR	156	AVE E EAST	UP	Arlington	10	30	7	0	47	\$160,000	2.94	\$128,000	\$128,000	FY 2005	\$721,840	INSTALL WAYSIDE HORN AND CONSTANT WARNING TIME DEVICE
7	PEACH STREET RAILROAD CROSSINGS	181	151 ST	BNSF	Fort Worth												CLOSE THE CROSSING OF 1500 EAST 1ST STREET AND THE UPRR LINE, INSTALL A MEDIAN AT 1000 PEACH STREET AND THE BNSF LINE, INSTALL A MEDIAN AT 800 EAST 1ST STREET AND THE UPRR LINE, IMPROVE STREET ACCESS AT 1500 EAST 1ST STREET AND THE UPRR LINE, IMPROVE THE CROSSING SURFACE AT SAMUELS AVENUE AND THE BNSF LINE, ASSOCIATED PEDESTRIAN IMPROVEMENTS
8	OKLAND BOULEVARD AT UPRR	188	OKLAND	UP	Fort Worth	5	30	30	5	70	\$275,000	2.55	\$200,000	\$0	N/A	\$0	UPGRADE TO QUAD GATES, INSTALL SIDEWALK SURFACE AND ITS CAMERA
9	SIX FLAGS DRIVE AT UPRR	163	SIX FLAGS DR	UP	Arlington	0	30	15	0	43	\$183,500	2.43	\$140,400	\$0	N/A	\$0	INSTALL MEDIAN, LED WARNING LIGHTS, AND CONSTANT WARNING TIME DEVICE, RAISE TRACK TO IMPROVE SURFACE
10	BASSWOOD BOULEVARD AT UPRR	185	BASSWOOD BLVD	UP	Fort Worth	20	30	7	5	62	\$280,000	2.38	\$208,000	\$0	N/A	\$0	UPGRADE TO QUAD GATES, INSTALL SIDEWALK SURFACE
11	HUDSON CEMETARY ROAD AT UPRR	122	HUDSON CEMETARY RD	UP	Tarrant County	5	15	18	0	35	\$175,000	2.00	\$140,000	\$0	N/A	\$0	INSTALL GATES AND LIGHTS
12	TIERNY ROAD AT UPRR	180	TIERNY RD	UP	Fort Worth	10	30	7	5	52	\$280,000	2.00	\$208,000	\$0	N/A	\$0	UPGRADE TO QUAD GATES, INSTALL SIDEWALK SURFACE
13	AYERS AVENUE AT UPRR	187	AYERS	UP	Fort Worth	5	30	7	5	47	\$290,000	1.81	\$208,000	\$0	N/A	\$0	UPGRADE TO QUAD GATES, INSTALL SIDEWALK SURFACE
14	WINNIE STREET AT UPRR	191	WINNIE	UP	Fort Worth	5	30	7	5	47	\$290,000	1.81	\$208,000	\$0	N/A	\$0	UPGRADE TO QUAD GATES, INSTALL SIDEWALK SURFACE
15	TARRANT PARKWAY NORTH AT UPRR	186	TARRANT PKWY (NORTH)	UP	Fort Worth	5	30	7	5	47	\$280,000	1.81	\$208,000	\$0	N/A	\$0	UPGRADE TO QUAD GATES, INSTALL SIDEWALK SURFACE
16	KATY PARKWAY / TIMBERLAND DRIVE AT UPRR	123	KATY RD / TIMBERLAND DR	UP	Tarrant County	5	30	0	0	35	\$225,000	1.56	\$160,000	\$0	N/A	\$0	INSTALL GATES AND LIGHTS
17	AVENUE H AT UPRR	186	AVE H	UP	Arlington	0	15	7	0	22	\$180,000	1.38	\$128,000	\$0	N/A	\$0	INSTALL CONSTANT WARNING TIME DEVICE AND WAYSIDE HORN
18	23RD STREET SW AT UPRR	154	23RD ST (SW)	UP	Grand Prairie	10	30	7	5	52	\$380,000	1.37	\$304,000	\$0	N/A	\$0	INSTALL QUAD GATES, MEDIAN, AND WAYSIDE HORN
19	ERIC STREET AT UPRR	240	ERIC	UP	Fort Worth	10	30	7	5	52	\$445,000	1.17	\$356,000	\$0	N/A	\$0	UPGRADE TO QUAD GATES, INSTALL SIDEWALK SURFACE
20	HANDLEY DRIVE AT UPRR	238	HANDLEY DR	UP	Fort Worth	10	30	7	5	52	\$450,000	1.16	\$390,000	\$0	N/A	\$0	UPGRADE TO QUAD GATES, INSTALL SIDEWALK SURFACE AND ITS CAMERA

* All projects require a minimum non-federal share of 20%. Right-of-way acquisition costs are not eligible for funding under this call for projects. Federal funds will not be eligible to fund cost overruns.

** Federal funding received in May 2004 from TxDOT's Section 130 Program.

RAILROAD CROSSING RELIABILITY PARTNERSHIP PROGRAM

NCTCOG Staff Recommendation

Trinity Railway Express Railroad Crossings To Be Considered Under Partnership Program Two (Shown in order from West to East)

Judkins	Greenbelt	
Galvez	Motocross	
Riverside	Mosier Valley	
Beach	Calloway Cemetary	
Haltom	Tarrant Main	
Elliot Reeder	Valley View	
Carson	Irving Yard Way	
Minnis	Gilbert	
Handley-Ederville	Irby	
Precinct Line	Rogers	
Norwood	MacArthur	
Bell Spur		
Total Project Cost:		\$7,462,427
Federal Funds Requested:		\$5,969,942
Local Matching Funds Required:		\$1,492,485

RAILROAD CROSSING RELIABILITY PARTNERSHIP PROGRAM

Requested Action

- 1. Approve staff recommended projects**
- 2. Place projects in the TIP**

RAILROAD CROSSING RELIABILITY PARTNERSHIP PROGRAM

For more information:

Mike Sims

(817) 695-9226

msims@nctcog.org

Rachel Harshman

(817) 608-2395

rhharshman@nctcog.org

Town of Addison
Project Application

85300
per family - include
w/ 85300 d.

Project Name (Maximum 30 characters)

RAILROAD CROSSING QUIET ZONES

Project Description

DESIGN & CONSTRUCTION OF NEW RAILROAD CROSSINGS AT VARIOUS LOCATIONS DART CAP. FUNDS WILL BE USED, IN THE AMOUNT OF \$500K.

Project Type: Operating/Unique Operating/Recurring Capital Special Event

Time to Complete (In months)

Located in Planning Sector: 1 2 3 4 5 6 N/A

Project Time Line

Phase (engineering, ROW, acquisition, construction, etc.)

Cost

Phase	1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	16	17	18	19	20	21	22	23	24	Cost
1 TX DOT AGREEMENT																									\$ -0-
2 ENGINEERING																									\$ 50,000
3 BIDDING																									\$ -0-
4 CONSTRUCTION																									\$ 580,000
5																									\$
6																									\$
7																									\$
8																									\$
	11/04	3	6	9	12	15	18	21	24														\$ 630,000		
	Start Date	Months																			Total Budget				

Finance Department Use

Project # Year Sector Type Free Seq.

15302 85300

Railroad Crossing Quiet Zones Project Number:

Fund 41

85300

Town of Addison Project Application

Project Name (Maximum 30 characters)	RAILROAD CROSSING QUIET ZONES						
Project Description	DESIGN & CONSTRUCTION OF NEW RAILROAD CROSSINGS AT VARIOUS LOCATIONS DART CAP FUNDS WILL BE USED, IN THE AMOUNT OF \$50K						
Project Type:	<input type="checkbox"/> Operating/Unique	<input type="checkbox"/> Operating/Recurring	<input type="checkbox"/> Capital	<input type="checkbox"/> Special Event			
Time to Complete (In months)							
Planned in Planning Sector :	1	2	3	4	5	6	N/A

Project Time Line

Phase (engineering, ROW, acquisition, construction, etc.)	11/04	3	6	9	12	15	18	21	24	Cost
TX DOT AGREEMENT										\$ -0-
ENGINEERING										\$ 50,000
BIDDING										\$ -0-
CONSTRUCTION										\$ 580,000
										\$
										\$
										\$
										\$
										\$
										\$
Start Date	Months									\$ 630,000
Total Budget										

Finance Department Use

Project # _____

Year	Sector	Type	Free	Seq.
------	--------	------	------	------

RR Xing Reliability Program

9-27-04

Rec'd a call from Bita Ramkar of TX DOT.
She will be our contact. They are meeting
this morning to find out about the program
and get organized. She will call back when
ready to start work on the agreement.
her # is 214-320-4424

BRAMKAR@DOT.STATE.TX.GS

~~XXXXXXXXXX~~

Steve Chutchian

To: bramkar@dot.state.tx.us
Subject: Railroad Reliability Program

Bit:

My email address is as follows: schutchian@ci.addison.tx.us. I look forward to working with you on this endeavor in the future. Thank you.

Steve Chutchian

Steve Chutchian

From: Jim Pierce
Sent: Friday, August 13, 2004 2:44 PM
To: Mike Murphy
Cc: Chris Terry; Ron Whitehead; Steve Chutchian; Robin Jones
Subject: Railroad Crossing Reliability Partnership Program

Mike: Some good news: The NCTCOG Regional Transportation Council has approved staff's recommendations for projects to be included in the above referenced program. The 2 projects that we applied for were to install quad gates at the Addison Road and Surveyor Blvd. railroad crossings effectively qualifying them as a quiet zone crossings. The total project cost was estimated to be \$630,000 and we are eligible for a grant of \$504,000 toward the project. We have DART LAP/CMS funds programmed to cover our local share.

Jim Pierce, P.E.
Assistant Public Works Director
P.O. Box 9010
Addison, TX 75001-9010
972-450-2879

Steve: *Were in the money so far...*

Jim **DRAFT**
Railroad Crossing Reliability Partnership Program

RANK	NAME OF CORRIDOR	APP #	STREET	RR OWNER	APPLICANT	PROJECT EXPOSURE SCORE	PROJECT PROTECTION SCORE	PROJECT BENEFITS SCORE	PROJECT CORRIDOR SCORE	TOTAL PROJECT POINTS	TOTAL PROJECT COST	PROJECT BENEFIT COST INDEX	FEDERAL FUNDS	FEDERAL FUNDS REMAINING	PROJECT DESCRIPTION
1	SOUTH GARLAND KCS	265	KINGSLEY RD	KCS	Garland	15	75	67	30	187	\$145,000	12.90	\$116,000	\$6,156,000	INSTALL ITS / TRAIN DETECTION
		264	LEON RD	KCS	Garland										
		233	SHILOH RD	KCS	Garland										
2	N/A	203	COIT (S OF PLANO PKWY)	KCS	Plano	40	15	7	0	62	\$125,000	4.96	\$100,000	\$6,056,000	RE-SURFACE
3	N/A	131	US 75 FRONTAGE (NB)	DART	Plano	40	15	7	0	62	\$125,000	4.96	\$100,000	\$5,956,000	RE-SURFACE
4	N/A	111	KEALY (NORTH)	KCS	Lewisville	10	30	15	0	55	\$190,000	2.89	\$152,000	\$5,804,000	INSTALL DUAL GATES AND ADD PAVEMENT MARKINGS
5	N/A	207	PLANO PKWY	DART	Plano	10	15	7	0	32	\$125,000	2.56	\$100,000	\$5,704,000	RE-SURFACE
6	N/A	133	19TH ST (SW)	UP	Grand Prairie	40	30	15	5	90	\$380,000	2.37	\$304,000	\$5,400,000	INSTALL QUAD GATES, MEDIAN, AND WAYSIDE HORN
7	SOUTHERN DALLAS	262	CLEVELAND RD	BNSF	Dallas	0	30	14	10	54	\$250,000	2.16	\$200,000	\$5,200,000	RE-SURFACE
		262	WITT RD	BNSF	Dallas										
8	N/A	132	US 75 FRONTAGE (SB)	DART	Plano	5	15	7	0	27	\$125,000	2.16	\$100,000	\$5,100,000	RE-SURFACE
9	ADDISON	165	ADDISON RD	DART	Addison	25	60	37	10	132	\$630,000	2.10	\$504,000	\$4,596,000	INSTALL QUAD GATES (ALSO ADD MEDIAN AND RE-SURFACE AT ADDISON RD)
		196	SURVEYOR BLVD	DART	Addison										
10	N/A	275	MACON	UP	Dallas	10	30	7	0	47	\$225,000	2.09	\$180,000	\$4,416,000	INSTALL MEDIANS
11	EAST DALLAS	255	PEAVY RD	KCS	Dallas	35	240	79	40	394	\$2,260,000	1.74	\$1,808,000	\$2,608,000	INSTALL MEDIANS (ALSO RE-SURFACE AT ST. FRANCIS AND LAKELAND)
		254	GUS THOMASSON RD	KCS	Dallas										
		251	BARNES BRIDGE RD	KCS	Dallas										
		253	CENTERVILLE RD	KCS	Dallas										
		256	LAKELAND DR	KCS	Dallas										
		258	HIGHLAND DR	KCS	Dallas										
		250	SANTA ANNA AVE	KCS	Dallas										
257	ST FRANCIS AVE	KCS	Dallas												
12	N/A	149	BAGDAD RD	UP	Grand Prairie	20	30	7	5	62	\$380,000	1.63	\$304,000	\$2,304,000	INSTALL QUAD GATES, MEDIAN, AND WAYSIDE HORN
13	N/A	140	CR 605	KCS	Collin County	20	30	7	0	57	\$352,000	1.62	\$281,600	\$2,022,400	INSTALL QUAD GATES, IMPROVE DRAINAGE
14	N/A	108	14TH ST (SW)	UP	Grand Prairie	10	30	15	5	60	\$380,000	1.58	\$304,000	\$1,718,400	INSTALL QUAD GATES, MEDIAN, AND WAYSIDE HORN
15	NORTH DALLAS	236	MCCALLUM	DART	Dallas	50	285	70	50	455	\$2,900,000	1.57	\$2,320,000	-\$601,600	INSTALL GATES, MEDIANS, AND PEDESTRIAN CROSSING SURFACES. (ALSO RE-SURFACE AT DICKERSON)
		243	CAMPBELL RD	DART	Dallas										
		271	DAVENPORT (EAST-WEST PORTION)	DART	Dallas										
		237	HILLCREST	DART	Dallas										
		204	COIT	DART	Dallas										
		273	KNOLL TRAIL DR	DART	Dallas										
		235	MEANDERING WAY	DART	Dallas										
		272	DAVENPORT (NORTH-SOUTH PORTION)	DART	Dallas										
		234	DICKERSON	DART	Dallas										
274	DALLAS PKWY	DART	Dallas												
16	N/A	112	VALLEY VIEW LN	DART	Farmers Branch	10	30	7	0	47	\$320,000	1.47	\$256,000	-\$857,600	RE-SURFACE, UPGRADE GATES
17	N/A	135	2ND ST (SW)	UP	Grand Prairie	5	30	7	5	47	\$325,000	1.45	\$260,000	-\$1,117,600	INSTALL QUAD GATES AND MEDIAN
18	N/A	145	5TH (SE)	UP	Grand Prairie	10	30	7	5	52	\$380,000	1.37	\$304,000	-\$1,421,600	INSTALL QUAD GATES, MEDIAN, AND WAYSIDE HORN
19	N/A	146	5TH (SW)	UP	Grand Prairie	10	30	7	5	52	\$380,000	1.37	\$304,000	-\$1,725,600	INSTALL QUAD GATES, MEDIAN, AND WAYSIDE HORN
20	N/A	147	CENTER ST	UP	Grand Prairie	5	30	7	5	47	\$380,000	1.24	\$304,000	-\$2,029,600	INSTALL QUAD GATES, MEDIAN, AND WAYSIDE HORN
21	N/A	266	JOSEPH HARDIN DR	BNSF	Dallas	0	0	7	5	12	\$125,000	0.96	\$100,000	-\$2,129,600	RE-SURFACE
22	N/A	267	JOSEPH HARDIN RD	BNSF	Dallas	0	0	7	5	12	\$125,000	0.96	\$100,000	-\$2,229,600	RE-SURFACE
23	N/A	268	MINT	BNSF	Dallas	0	0	7	0	7	\$125,000	0.56	\$100,000	-\$2,329,600	RE-SURFACE
24	N/A	269	COCKRELL HILL RD	BNSF	Dallas	5	0	7	0	12	\$250,000	0.48	\$200,000	-\$2,529,600	RE-SURFACE

APR-11-2003 11:12

RAILROAD CONTROLS

817 820 6340 P.01/02

**FAX MEMORANDUM**

To: Mr. Jim Pierce
Phone:
Fax: (972) 450-2837
From: Robert Albritton
Voice Phone: (817) 820-6347
Fax Phone: (817) 820-6340
Date: 04/11/03
Pages incl. Cover: 2
RE: House Bill 896 "Creation of Quiet Zones"

cc Ron Whitehead
Chris Terry
Mike Murphy
Steve Chutchan

for appropriate
political action...

Jim

RAILROAD CONTROLS LLC
500 South Freeway
Fort Worth, TX 76104

Phone (817) 820-6300

Fax (817) 820-6340

APR-11-2003 11:12

RAILROAD CONTROLS

B17 820 6340 P.02/02

The Good News:

The Federal Railroad Administration is making some rule changes, which allow communities to create "quiet zones".....areas in which trains do not sound their horns.

The Bad News:

There is a conflict, because, Texas has some old laws on the books which say that they must sound their horns at the railroad crossings.

More Good News:

While the legislature is in session, we can "fix" that little problem. House Bill 896, amends our old Texas law, and allows communities to create quiet zones as long as the FRA rules are followed. (Emergency situations are excluded.) Making this change to the law will greatly enhance the value and quality of life in many neighborhoods throughout the state.

More Bad News:

If we don't get the law changed before the legislature adjourns in May, we can't fix the conflict for another two years.

Note: You can look at the law by going to www.capitol.state.tx.us, & entering HB896.

HB896 is now in committee. We need to make sure the bill gets out of committee. We need to make sure the bill gets out of committee, and through the legislative process in an expedient manner. Voice your support for passage of HB896 to the parties listed below.....

Governor Rick Perry
State Capitol
P.O. Box 12428
Austin, Texas 78711
(512)463-2000
fax 463-0039

and

Lt. Gov. David Dewhurst
Capitol Station
P.O. Box 12068
Austin, Texas 78711
(512)463-0001
fax 938-6700

Members of the Transportation Committee, e-mail, phone and fax numbers (512 a/c)

Mike Krusee, Chair	Mike.Krusee@house.state.tx.us	463-0670	463-1469
Larry Phillips, Vice Chair	Larry.Phillips@house.state.tx.us	463-0297	463-1561
Al Edwards	Al.Edwards@house.state.tx.us	463-0518	463-7968
Peggy Hamric	Peggy.Hamric@house.state.tx.us	463-0496	463-2077
Fred Hill	Fred.Hill@house.state.tx.us	463-0486	480-0512
Timoteo "Timo" Garza	Timoteo.Garza@house.state.tx.us	463-0194	463-0852
Linda Harper-Brown	Linda.Harper.Brown@house.state.tx.us	463-0641	499-8287
James "Pete" Laney	Pete.Laney@house.state.tx.us	463-0604	463-1071
Ken Mercer	Ken.Mercer@house.state.tx.us	463-0269	463-1608

Steve Chutchian

From: Jim Pierce
Sent: Wednesday, October 31, 2001 3:45 PM
To: Steve Chutchian
Cc: Michael Murphy
Subject: RE: Railroad Crossing Quiet Zones

Steve: Next step is to set up a meeting with David Eyerman, General Manager, DGNO, 972-487-8180. Mike, you and I should be there. Jim.

-----Original Message-----

From: Steve Chutchian
Sent: Wednesday, October 31, 2001 11:14 AM
To: Jim Pierce
Subject: Railroad Crossing Quiet Zones

Jim - I have included the Railroad Crossing Quiet Zones on our Project List. If I am understanding the process correctly on this project, the next step is to obtain conceptual input from Ron Whitehead. One issue to be resolved consists of our desire to design the same double cross bar crossing at all locations. Please let me know what I can do at this point to move ahead on the project. Thanks.

Steve C.

PROJECT on
HOLD!
FILED.
11/20/01

Town of Addison Project Application

Project Name (Maximum 30 characters) **RR CROSSINGS QUIET ZONES**

Project Description

This project will establish quiet zones along the Cotton Belt Railroad by installing facilities that increase the safety of the crossings. The following facilities are anticipated:
 Knoll Trail - Wayside Horns
 Addison Rd - 4 Quadrant Gates
 Surveyor Rd - 4 Quadrant Gates
 Project Funded from DART LAP/CMS PROGRAM

Project Type: Operati

Time to Complete (In months)

Located in Planning Sector

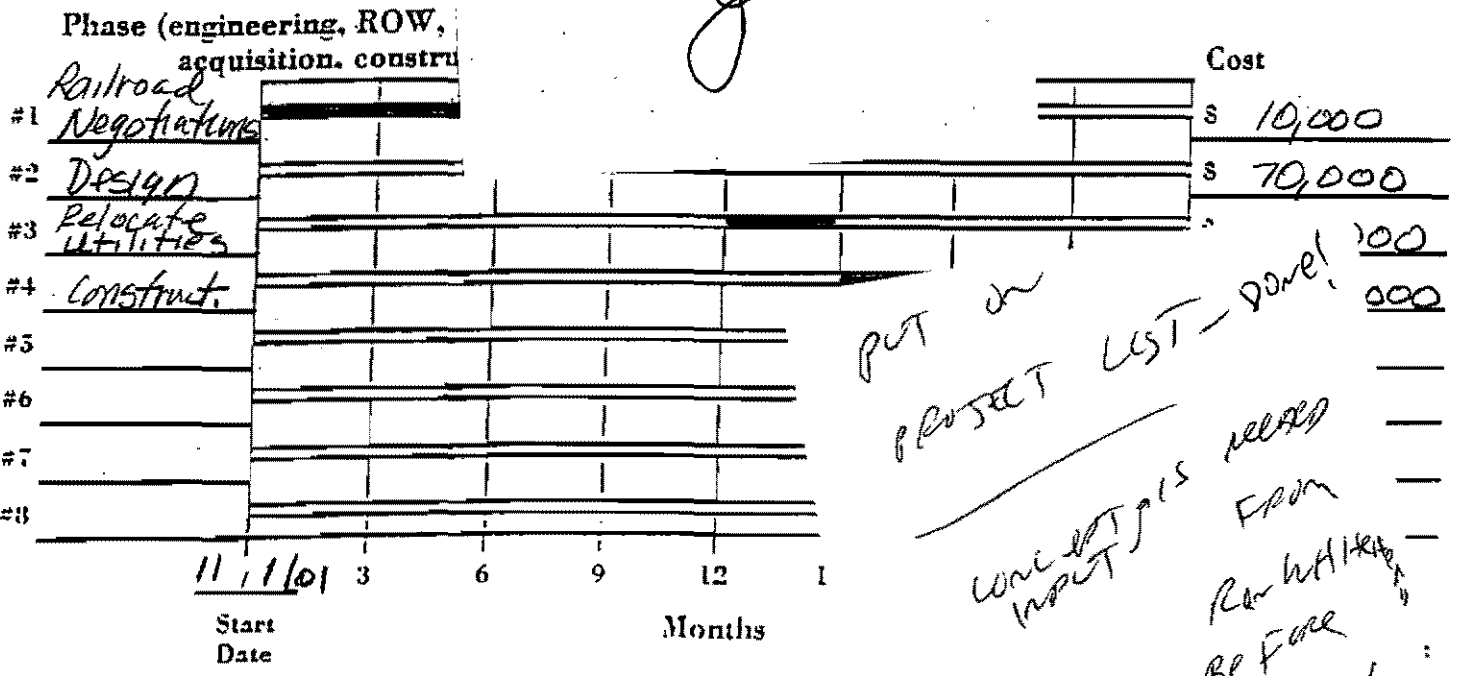
Capital Special Event

N/A

*Steve -
Let's kick this off*

Jim

times Each



PUT ON PROJECT LIST - DONE!
CONCEPTS NEEDED FROM BE FORE PROCEED WITH ONLY SECTOR PROCEEDS

Finance Department Use

Project # _____
 Year _____ Sector _____ Type _____ Free _____ Seq. _____

Steve Chutchian

From: Steve Chutchian
Sent: Wednesday, October 31, 2001 11:14 AM
To: Jim Pierce
Subject: Railroad Crossing Quiet Zones

Jim - I have included the Railroad Crossing Quiet Zones on our Project List. If I am understanding the process correctly on this project, the next step is to obtain conceptual input from Ron Whitehead. One issue to be resolved consists of our desire to design the same double cross bar crossing at **all** locations. Please let me know what I can do at this point to move ahead on the project. Thanks.

Steve C.