

CIVIL CONSTRUCTION PLANS FOR OFF-SITE SANITARY SEWER

AMLI ADDISON 15250 QUORUM DRIVE TOWN OF ADDISON DALLAS COUNTY, TEXAS

TOWN PROJECT # 17-05

PLANS SUBMITTAL/REVIEW LOG

CIVIL ENGINEERING PACKAGE 05/01/2017
- NOT FOR CONSTRUCTION.

CIVIL ENGINEERING PACKAGE SUBMITTAL 2 06/02/2017
- NOT FOR CONSTRUCTION

CIVIL ENGINEERING PACKAGE SUBMITTAL 3 7/26/2017
- NOT FOR CONSTRUCTION

CIVIL ENGINEERING PACKAGE SUBMITTAL 4 8/14/2017
- FOR CONSTRUCTION

TOWN OF ADDISON CITY COUNCIL

TOWN HALL
5300 BELT LINE ROAD
ADDISON, TX 75254
(972) 450-7000

COUNCIL MEMBERS:

- JOE CHOW MAYOR
- IVAN HUGHES MAYOR PRO TEMPORE
- JIM DUFFY DEPUTY MAYOR PRO TEMPORE
- AL ANGELL COUNCIL MEMBER
- TOM BRAUN COUNCIL MEMBER
- PAUL WALDEN COUNCIL MEMBER
- LORI WARD COUNCIL MEMBER

ENGINEER

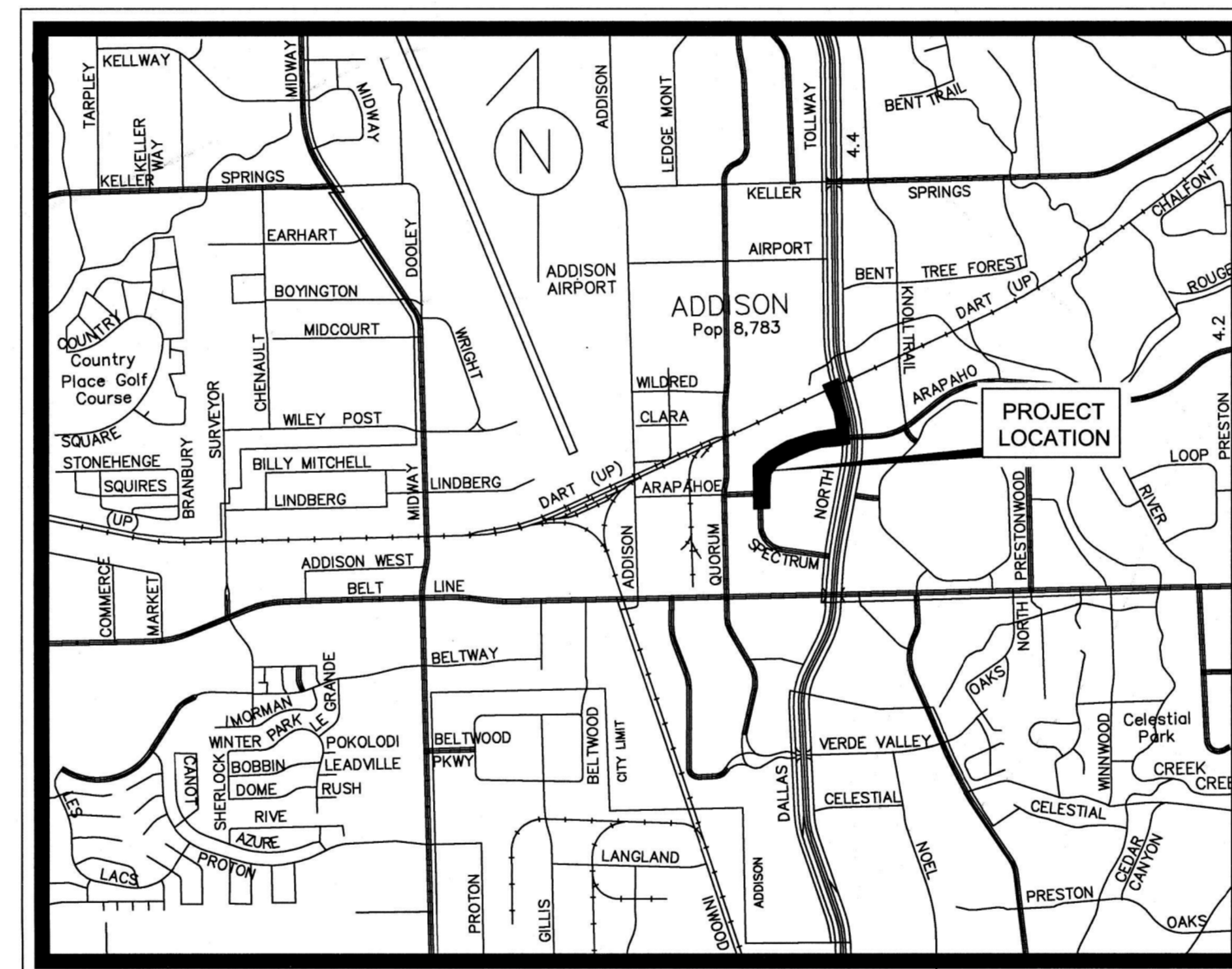
Kimley»Horn

5750 GENESIS COURT SUITE 200
FRISCO, TEXAS 75034
PH. (972) 335-3580
CONTACT: MICHAEL WESTFALL, P.E.
MICHAEL.WESTFALL@KIMLEY-HORN.COM

STATE OF TEXAS
REGISTRATION NO. F-928

OWNER/DEVELOPER

AMLI RESIDENTIAL
5057 KELLER SPRINGS ROAD
SUITE 250
ADDISON, TX 75001
CONTACT: TAYLOR BOWEN



VICINITY MAP

SCALE: 1" = 2,000'

NOTES:

1. THE PROJECT SHALL MEET OR EXCEED THE STANDARD SPECIFICATIONS FOR PUBLIC WORKS CONSTRUCTION FOR THE NCTCOG - NORTH CENTRAL TEXAS COUNCIL OF GOVERNMENTS (LATEST EDITION), THE TOWN OF ADDISON STANDARDS, AND ANY SPECIAL PROVISIONS ADOPTED BY THE TOWN OF ADDISON. TOWN OF ADDISON STANDARDS GOVERN OVER NCTCOG IN THE EVENT OF A CONFLICT.
2. ALL NECESSARY INSPECTIONS AND/OR CERTIFICATIONS REQUIRED BY CODES AND/OR UTILITY SERVICE COMPANIES SHALL BE PERFORMED PRIOR TO CONTRACT COMPLETION AND THE FINAL CONNECTION OF SERVICES.
3. IF REPRODUCED, THE SCALES SHOWN ON THESE PLANS ARE BASED ON A 22" X 34" SHEET.



APPROVED FOR CONSTRUCTION

INFRASTRUCTURE & DEVELOPMENT SERVICES

BY: *Michael Westfall*
DATE: 8/15/17

IN APPROVING THESE PLANS, THE TOWN OF ADDISON ASSUMES NO RESPONSIBILITY FOR THEIR ADEQUACY, WHICH REMAINS WITH THE ENGINEER OF RECORD.

16801 WESTGROVE DRIVE, ADDISON, TX 75001 - (972) 450-2871



Know what's below.
Call before you dig.



INDEX OF SHEETS

SHEET NO.	DESCRIPTION	DATE	REVISION
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TCP-6	TRAFFIC CONTROL PLAN	08/14/2017	

AUGUST 2017

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GENERAL NOTES

- 1. ALL CONSTRUCTION SHALL BE IN GENERAL ACCORDANCE WITH THESE PLANS, TOWN OF ADDISON, TX STANDARD SPECIFICATIONS, THE FINAL GEOTECHNICAL REPORT, AND COMMONLY ACCEPTED CONSTRUCTION STANDARDS.
2. THE CONTRACTOR IS RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND APPROVALS.
3. THE CONTRACTOR SHALL BE RESPONSIBLE FOR REMOVAL AND DISPOSAL OFFSITE OF ANY EXISTING PAVING AND STRUCTURES REMOVED.
...
31. THE CONTRACTOR WILL BE SOLELY RESPONSIBLE FOR IMPLEMENTING THE TRAFFIC CONTROL PLAN.

STORM DRAINAGE

- 1. THE CONTRACTOR SHALL BE SOLELY RESPONSIBLE FOR SITE TRENCH SAFETY DURING ALL PHASES OF CONSTRUCTION. THE CONTRACTOR SHALL SUBMIT A TRENCH EXCAVATION PROTECTION PLAN, SEALED BY AN ENGINEER REGISTERED IN THE STATE OF TEXAS, FOR ALL TRENCHES DEEPER THAN FIVE (5) FEET.
2. THE CONTRACTOR SHALL VERIFY AND COORDINATE ALL DIMENSIONS SHOWN, INCLUDING THE HORIZONTAL AND VERTICAL LOCATION OF CURB INLETS AND GRATE INLETS AND ALL UTILITIES CROSSING THE STORM SEWER.
...
10. REFER TO TNRCC/CEC DESIGN GUIDELINES (CHAPTER 290) FOR ALL UTILITY CROSSINGS.

GRADING

- 1. UNLESS OTHERWISE NOTED, PROPOSED CONTOURS AND SPOT ELEVATIONS SHOWN IN PAVED AREAS REFLECT TOP OF PAVEMENT SURFACE. ADD 50' TO PAVING GRADE FOR TOP OF CURB GRADE. THE LIMITS OF EARTHWORK IN PAVED AREAS IS THE BOTTOM OF PAVEMENT.
2. THE CONTRACTOR SHALL FIELD VERIFY THE HORIZONTAL AND VERTICAL LOCATIONS OF ALL EXISTING UTILITIES PRIOR TO START OF CONSTRUCTION AND SHALL NOTIFY THE CONSTRUCTION MANAGER AND ENGINEER OF ANY CONFLICTS DISCOVERED.
...
15. AFTER PLACEMENT OF SUBGRADE AND PRIOR TO PLACEMENT OF PAVEMENT, CONTRACTOR SHALL TEST AND OBSERVE PAVEMENT AREAS FOR EVIDENCE OF PONDING. ALL AREAS SHALL ADEQUATELY DRAIN TOWARDS THE INTENDED STRUCTURE TO CONVEY STORM RUNOFF. CONTRACTOR SHALL IMMEDIATELY NOTIFY OWNER AND ENGINEER IF ANY DISCREPANCIES ARE DISCOVERED.

PAVING

- 1. BARRIER FREE RAMPS SHALL BE CONSTRUCTED PER PROWAG (2011) STANDARDS.
2. ALL SIGNS, PAVEMENT MARKINGS, AND OTHER TRAFFIC CONTROL DEVICES SHALL CONFORM TO THE MOST RECENT ADDITION OF THE "TEXAS MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES".
3. CONTRACTOR SHALL FURNISH AND INSTALL ALL PAVEMENT MARKINGS FOR FIRE LANES, PARKING STALLS, HANDICAPPED PARKING SYMBOLS, AND MISCELLANEOUS STRIPING WITHIN PARKING LOT AND AROUND BUILDING AS SHOWN ON THE PLANS.
...
17. REFER TO GEOTECHNICAL REPORT FOR SOIL COMPACTION SPECIFICATION.

WATER AND SANITARY

- 1. IN THE EVENT AN ITEM IS NOT COVERED BY THIS MANUAL, THEN THE N.C.T.C.O.G. GENERAL SPECIFICATIONS COVERING SUCH ITEMS SHALL APPLY.
2. WATER MAINS SHALL BE PVC C-900 DR 18, EXCEPT WHEN OTHERWISE NOTED.
3. SEWER PIPE SHALL BE MINIMUM SDR 35 PVC, ULTRA RIB PVC SDR 26, OR DR 21 HDPE.
4. WATER MAINS SHALL HAVE THE FOLLOWING MINIMUM COVER BELOW STREET GRADES:
...
17. THE CONTRACTOR SHALL PROVIDE CONSTRUCTION STAKING FOR ALL WATER AND SANITARY SEWER LINES AND OTHER UTILITIES.

FLOW DIVERSION MANAGEMENT NOTES

- 1. THE CONTRACTOR SHALL PROVIDE AND MAINTAIN ADEQUATE BYPASS EQUIPMENT, FORCE MAINS, AND OTHER NECESSARY APPURTENANCES IN ORDER TO MAINTAIN RELIABLE SANITARY SEWER SERVICE IN ALL SANITARY SEWER LINES AS REQUIRED FOR CONSTRUCTION.
2. THE CONTRACTOR SHALL HAVE BACKUP PUMP(S), FORCE MAIN(S), AND APPURTENANCES READY TO DEPLOY IMMEDIATELY. APPURTENANCES AND DISCHARGE POINTS SHALL BE APPROVED BY THE INSPECTOR.
3. ANY SPILLAGE, BACKUPS AND/OR OVERFLOWS, ETC. AS THE RESULT OF INADEQUATE EQUIPMENT ARE THE SOLE RESPONSIBILITY OF THE CONTRACTOR.
...
18. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBSERVING WEATHER FORECASTS AND NOTIFYING INSPECTOR OF ANY SEVERE WEATHER POSSIBILITIES DURING CONSTRUCTION. IN THE EVENT OF INCLEMENT WEATHER, THE CONTRACTOR SHALL BE RESPONSIBLE FOR SUSPENDING ALL BYPASS ACTIVITIES, INCLUDING THE COST OF ANY MOBILIZATION, DE-MOBILIZATION, AND RE-MOBILIZATION.



APPROVED FOR CONSTRUCTION

INFRASTRUCTURE & DEVELOPMENT SERVICES

BY [Signature] DATE: 8/15/17

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REFER TO TNRCC/CEC DESIGN GUIDELINES (CHAPTER 290) FOR ALL UTILITY CROSSINGS.

GEOTECHNICAL REPORT

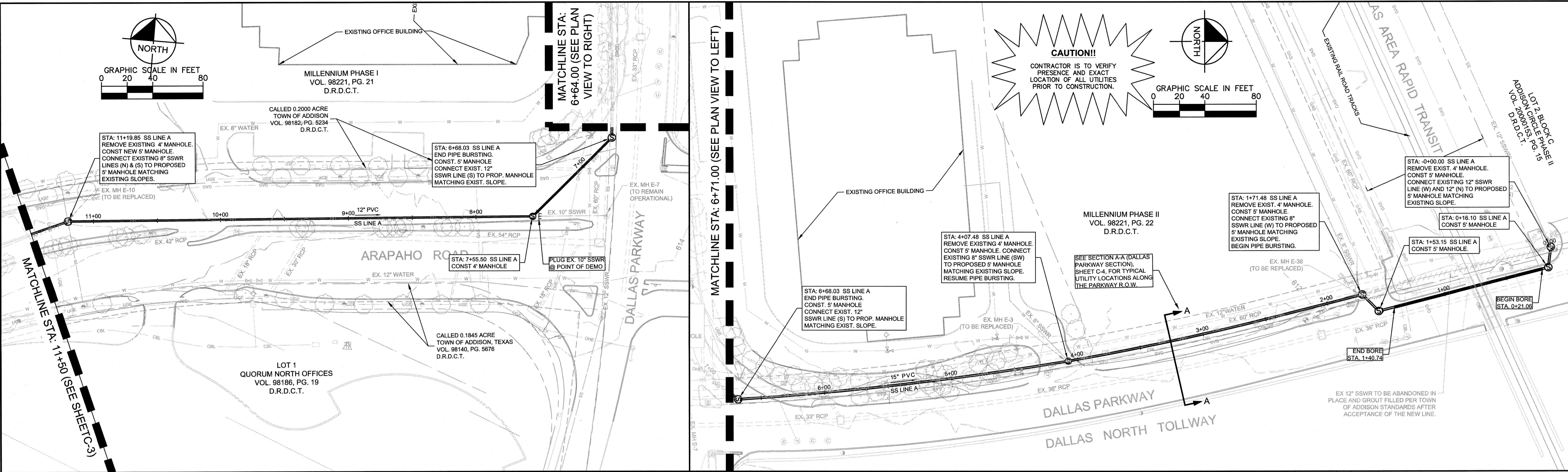
REPORT NO. G140072
ALPHA TESTING, INC.
DALLAS, TEXAS
MARCH 15, 2014
CONTACT: DAVID SCHLEDORF, P.E.
(972) 620-8911

BENCHMARKS

- BM-1 Town of Addison GPS Control Station "TXDOT-COA-5", located on the corner of a curb inlet, 190' south of the intersection of Edwin Lewis Drive and Quorum Drive, 2' from the west edge of sidewalk, 50.5 feet from the west side of median and 76' from a fire hydrant. Elevation=630.10'
BM-2 Town of Addison GPS Control Station "TXDOT-COA-14", located 630' west of intersection of Addison Road & Westgrove Drive, 15.7' south of back of curb of concrete drive, 5' from street sign and 8' north of back of curb of Westgrove Road. Elevation=642.09'
TBM-1 Square cut with "X" set on N.E. corner of drop inlet 545'+- south of south R.O.W of Edwin Lewis Dr. and 8'+- west of Spectrum Drive back-of-curb. Elevation=630.89'
TBM-2 Square cut with "X" set on concrete headwall at the southeast corner of Quorum Drive and Edwin Lewis Drive. Elevation=631.28'

Vertical sidebar containing: Kimley-Horn logo, MICHAEL WESTFALL LICENSED PROFESSIONAL ENGINEER, SCALE, DATE, REVISIONS, GENERAL NOTES, AMLI AT ADDISON OFF-SITE SEWER CITY OF ADDISON DALLAS COUNTY, TEXAS, PROJECT NO. 63257052, SHEET NUMBER C-1

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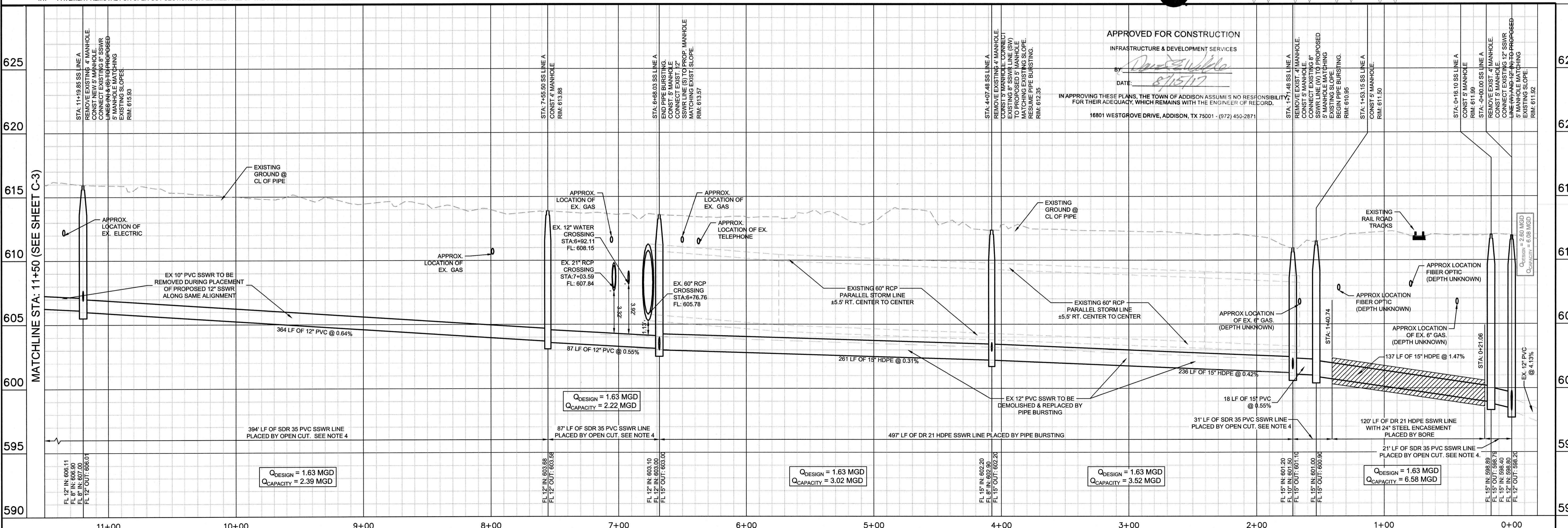


NOTES:

- GENERAL TRAFFIC CONTROL PLANS ARE PROVIDED FOR THE OVERALL WASTEWATER PROJECT. REFERENCE SHEETS TOP 1 TO TOP 16 FOR DETAILS.
- CONTRACTOR SHALL SUBMIT ADDITIONAL TRAFFIC CONTROL PLANS AS NECESSARY FOR BYPASS PUMPING THAT IMPACTS PUBLIC ROADWAYS OR DRIVEWAY CONNECTIONS.
- CONTRACTOR SHALL SUBMIT BYPASS PUMPING PLANS TO THE TOWN OF ADDISON FOR ACCEPTANCE PRIOR TO BEGINNING WORK.
- OPEN CUT PAVEMENT REMOVAL AND REPLACEMENT REQUIRED FOR THE INSTALLATION OF THE PROPOSED SANITARY SEWER LINE SHALL MEET THE FOLLOWING CRITERIA:
 - PAVEMENT REMOVAL FOR OPEN CUT SECTIONS SHALL MEET ALL OF THE TOWN OF ADDISON DETAILS AND REQUIREMENTS AND MUST ADHERE TO NCTCOG PUBLIC WORKS CONSTRUCTION STANDARDS DIVISION 3000 FOR THE NECESSARY UTILITY TRENCH CUT SECTION. NO PROPOSED PAVEMENT JOINTS SHALL BE ACCEPTED WITHIN THREE (3) FEET OF AN EXISTING PAVEMENT JOINT.
 - PROPOSED PAVEMENT JOINTS MUST BE LOCATED OUTSIDE OF THE WHEEL PATH WITHIN A LANE OF TRAVEL. PAVEMENT REPLACEMENT LIMITS MUST BE ACCEPTED BY THE TOWN OF ADDISON PRIOR TO INSTALLATION OF NEW PAVEMENT SECTIONS. CONTRACTOR SHALL COORDINATE WITH, AND OBTAIN CONCURRENCE FROM, THE TOWN OF ADDISON INSPECTORS PRIOR TO FINALIZING PAVEMENT REMOVAL AND REPLACEMENT WORK.

PROFILE SCALE
1" = 40' HORIZONTAL
1" = 4' VERTICAL

NOTE: IF ANY SANITARY SERVICES ARE ENCOUNTERED THAT ARE UNKNOWN/UN-MAPPED, THE CONTRACTOR SHALL NOTIFY THE INSPECTOR IMMEDIATELY TO DETERMINE IF THE SERVICE IS LIVE AND NEEDS TO BE RE-CONNECTED OR IF IT IS ABANDONED.



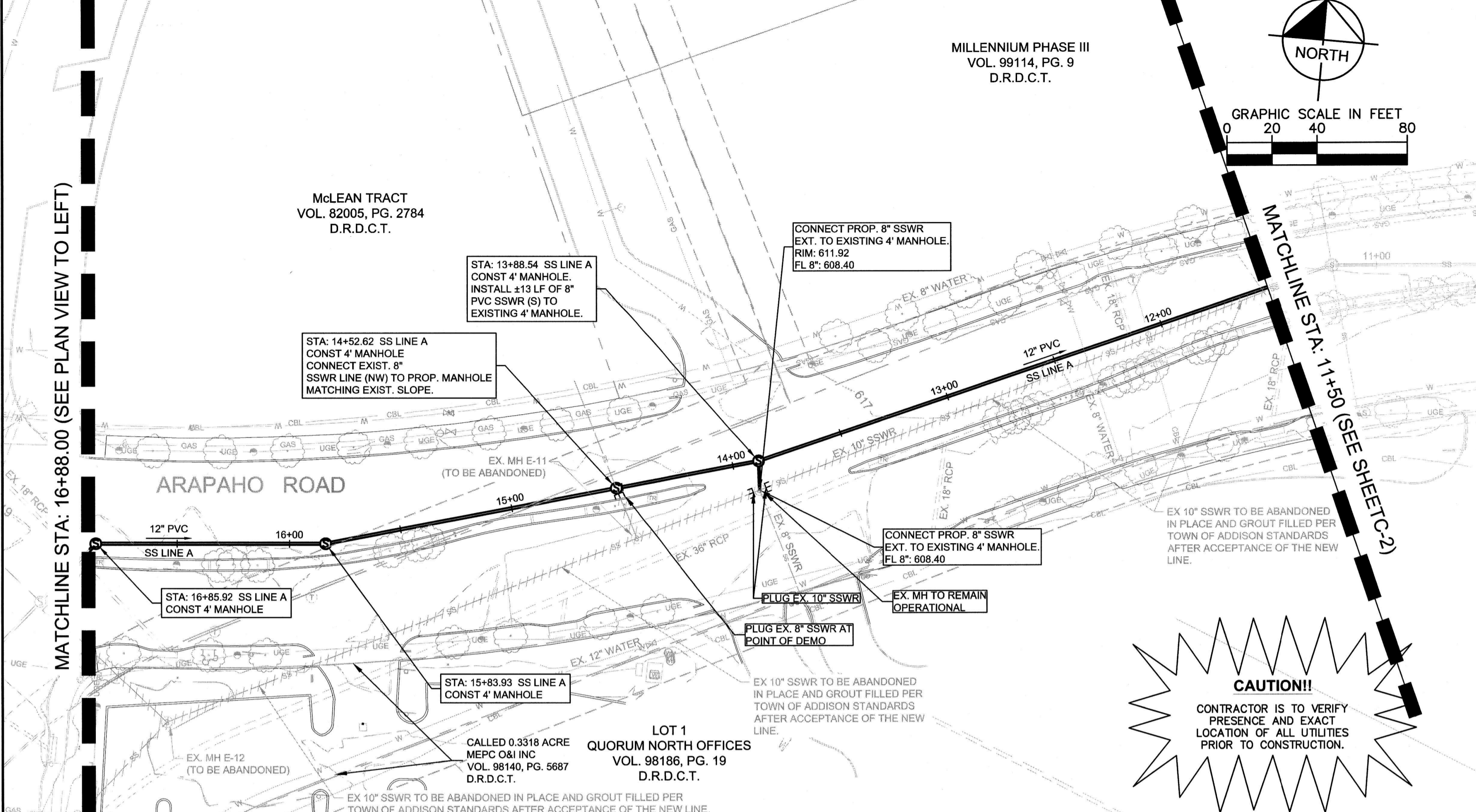
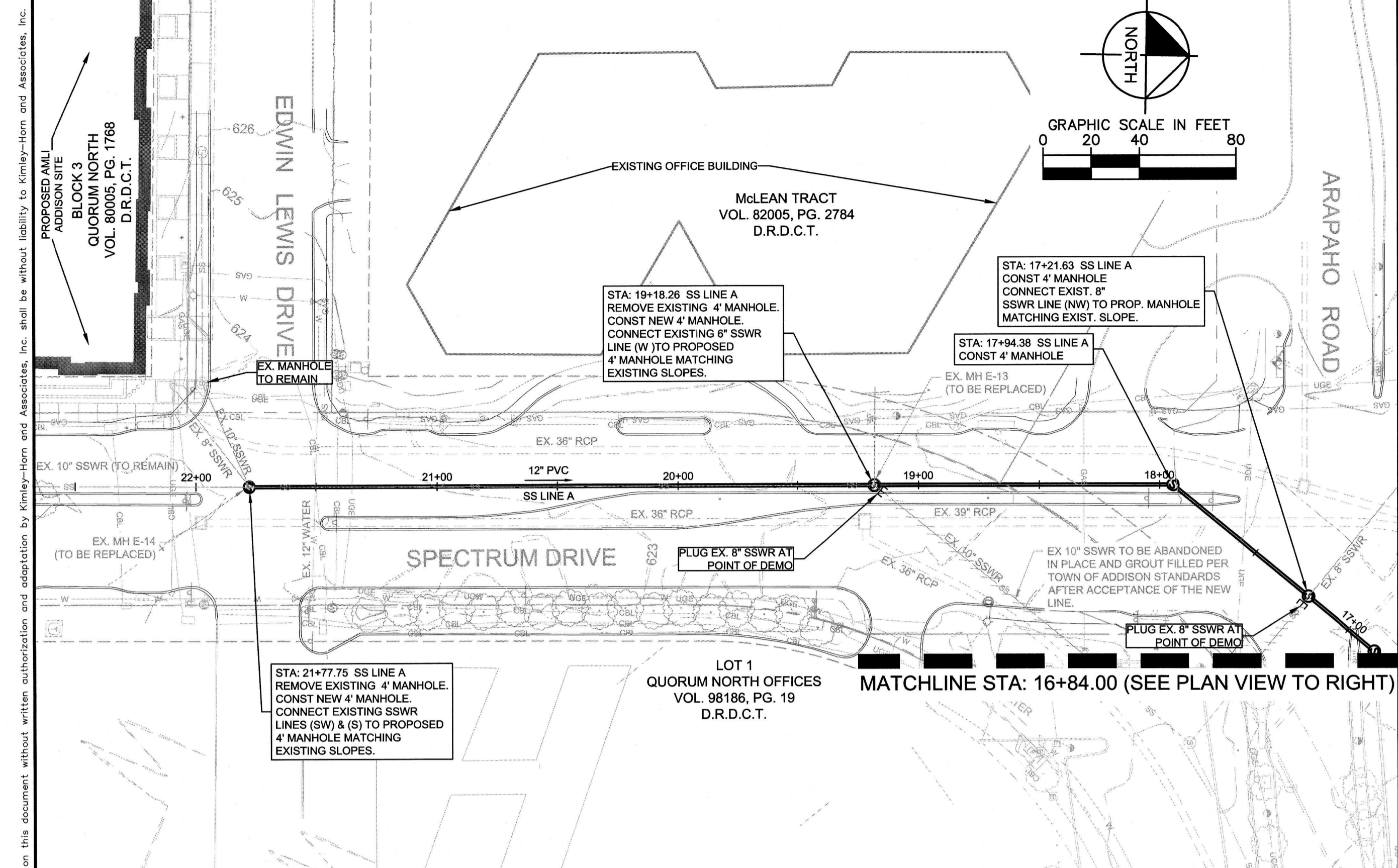
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WWW.KIMLEY-HORN.COM
TEXAS REGISTERED ENGINEERING FIRM F-928

SCALE
SCALE SHOWN
DESIGNED BY: KKB
DRAWN BY: KKB
CHECKED BY: MKW

AMLI AT ADDISON OFF-SITE SEWER PLAN AND PROFILE
CITY OF ADDISON
DALLAS COUNTY, TEXAS

DATE: 07/26/2017
PROJECT NO: 63257052
SHEET NUMBER: C-2

NO.	REVISIONS	DATE

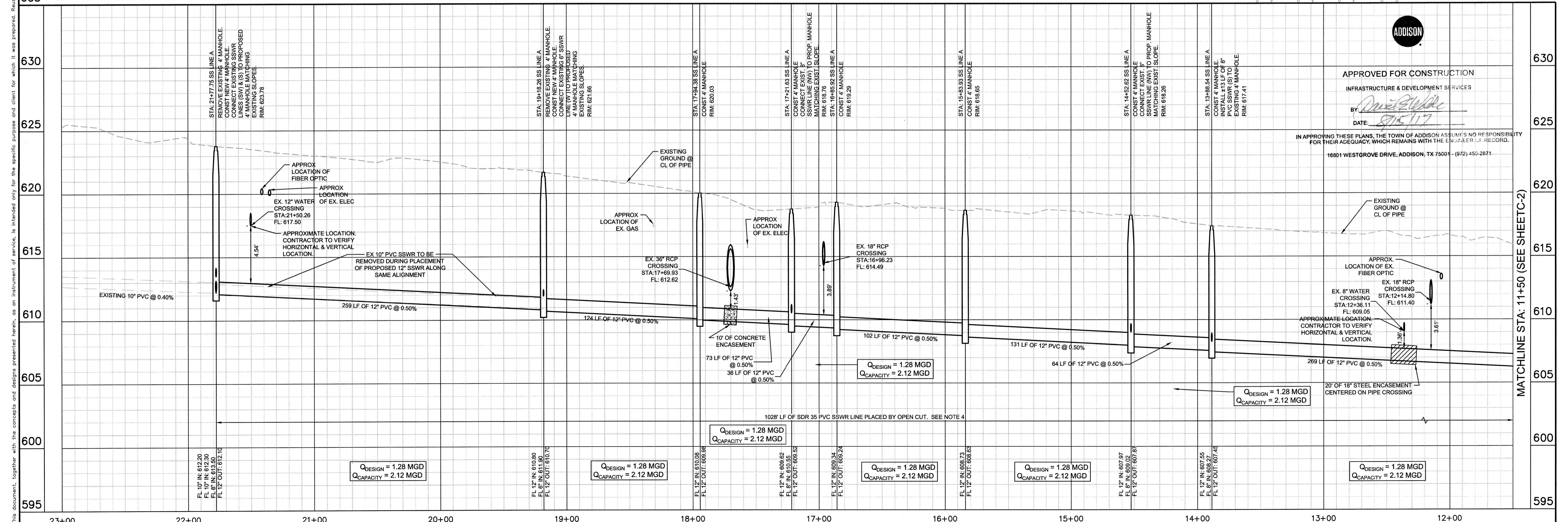


- NOTES:**
- GENERAL TRAFFIC CONTROL PLANS ARE PROVIDED FOR THE OVERALL WASTEWATER PROJECT. REFERENCE SHEETS TCR-1 TO TCR-6 FOR DETAILS.
 - CONTRACTOR SHALL SUBMIT ADDITIONAL TRAFFIC CONTROL PLANS AS NECESSARY FOR BYPASS PUMPING THAT IMPACTS PUBLIC ROADWAYS OR DRIVEWAY CONNECTIONS.
 - CONTRACTOR SHALL SUBMIT BYPASS PUMPING PLANS TO THE TOWN OF ADDISON FOR ACCEPTANCE PRIOR TO BEGINNING WORK.
 - OPEN CUT PAVEMENT REMOVAL AND REPLACEMENT REQUIRED FOR THE INSTALLATION OF THE PROPOSED SANITARY SEWER LINE SHALL MEET THE FOLLOWING CRITERIA:
 - PAVEMENT REMOVAL FOR OPEN CUT SECTIONS SHALL MEET ALL OF THE TOWN OF ADDISON DETAILS AND REQUIREMENTS AND MUST ADHERE TO NCTCOG PUBLIC WORKS CONSTRUCTION STANDARDS DIVISION 3000 FOR THE NECESSARY UTILITY TRENCH CUT SECTION.
 - NO PROPOSED PAVEMENT JOINTS SHALL BE ACCEPTED WITHIN THREE (3) FEET OF AN EXISTING PAVEMENT JOINT.
 - PROPOSED PAVEMENT JOINTS MUST BE LOCATED OUTSIDE OF THE WHEEL PATH WITHIN A LANE OF TRAVEL.
 - PAVEMENT REPLACEMENT LIMITS MUST BE ACCEPTED BY THE TOWN OF ADDISON PRIOR TO INSTALLATION OF NEW PAVEMENT SECTIONS. CONTRACTOR SHALL COORDINATE WITH AND OBTAIN CONCURRENCE FROM THE TOWN OF ADDISON INSPECTORS PRIOR TO FINALIZING PAVEMENT REMOVAL AND REPLACEMENT WORK.

CAUTION!!
CONTRACTOR IS TO VERIFY PRESENCE AND EXACT LOCATION OF ALL UTILITIES PRIOR TO CONSTRUCTION.

PROFILE SCALE
1" = 40' HORIZONTAL
1" = 4' VERTICAL

NOTE: IF ANY SANITARY SERVICES ARE ENCOUNTERED THAT ARE UNKNOWN/UN-MAPPED, THE CONTRACTOR SHALL NOTIFY THE INSPECTOR IMMEDIATELY TO DETERMINE IF THE SERVICE IS LIVE AND NEEDS TO BE RE-CONNECTED OR IF IT IS ABANDONED.



APPROVED FOR CONSTRUCTION
INFRASTRUCTURE & DEVELOPMENT SERVICES
BY: *[Signature]*
DATE: 07/26/17

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WWW.KIMLEY-HORN.COM
TEXAS REGISTERED ENGINEERING FIRM F-928

AMLI AT ADDISON
OFF-SITE SEWER
CITY OF ADDISON
DALLAS COUNTY, TEXAS

SEWER LINE A
PLAN AND PROFILE

DATE: 07/26/2017
PROJECT NO: 63257052
SHEET NUMBER: C-3

NO.	REVISIONS	DATE

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PAVING - GENERAL NOTES

- GENERAL: PAVEMENT THICKNESS IS AS SHOWN IN ITEM 7. SUBGRADE DESIGN SHALL CONFORM TO TOWN OF ADDISON PUBLIC WORKS REQUIREMENTS IN ITEM 3, AND SHALL EXTEND 12" MIN. BEHIND THE BACK OF CURB.
- REINFORCED CONCRETE PAVEMENT:
 - CONCRETE STRENGTH SHALL BE AS SHOWN IN ITEM 7 (NCTCOG LATEST EDITION).
 - ALL CURBS SHALL BE INTEGRAL WITH PAVEMENT AND SHALL BE OF THE SAME STRENGTH AS CONCRETE PAVEMENT.
 - DETAIL AND ARRANGEMENT OF PAVEMENT JOINTS, ALL TYPES, SHALL BE AS SHOWN ON THE TOWN STANDARD CONSTRUCTION DETAILS.
 - BAR LAPS SHALL BE THIRTY DIAMETERS.
 - REINFORCING STEEL SHALL BE #3 REBAR (3/8") ON 18" CENTERS FOR 8" OR LESS. #4 FOR 10" OR ABOVE.
- SUBGRADE: SUBGRADE UNDER ALL PAVEMENT SHALL BE 6" THICK AND SHALL BE STABILIZED WITH AT LEAST 30 LBS. PER SQ. YD. HYDRATED LIME, COMPACTED TO A DENSITY NOT LESS THAN 95 PERCENT. LABORATORY TESTS MUST BE SUBMITTED TO THE PUBLIC WORKS DEPARTMENT FOR APPROVAL TO DETERMINE AMOUNT OF LIME REQUIRED. LABORATORY TEST MAY BE WAIVED PROVIDED AT LEAST 36 LBS. OF LIME PER SQ. YD. IS USED. SEE NCTCOG ITEM 301.2 "LIME TREATMENT". FLEXIBLE BASE (CRUSHED STONE/CONCRETE) PER NCTCOG ITEM 301.5 MAY BE SUBSTITUTED FOR LIME TREATMENT WITH THE APPROVAL OF THE TOWN ENGINEER.
- REBAR SHALL BE SUPPORTED BY BAR CHAIRS OR OTHER DEVICES APPROVED BY TOWN ENGINEER.
- NO TRAFFIC ON FINISHED SUBGRADE SHALL BE PERMITTED AFTER REINFORCING STEEL IS INSTALLED ABOVE SUBGRADE. NO TRAFFIC SHALL BE PERMITTED BEFORE OR DURING THE PLACING OF CONCRETE.
- CROSS SLOPE OF STRAIGHT CROWN STREETS SHALL BE 1/4" PER FOOT UNLESS APPROVED BY THE TOWN ENGINEER.
- PAVEMENT THICKNESS AND STRENGTHS SHALL BE AS FOLLOWS:

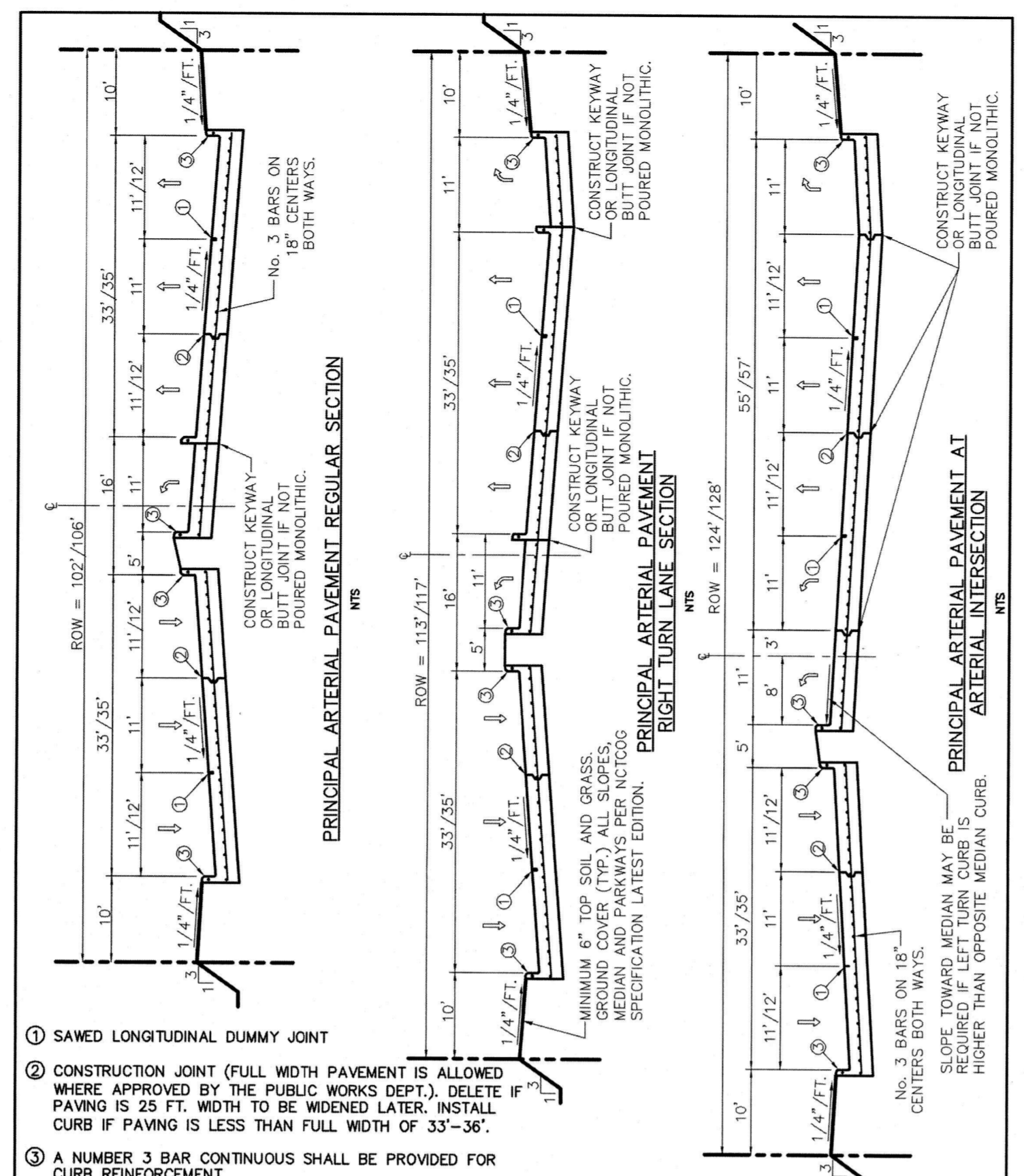
MAJOR ARTERIAL	- 10" CLASS "P1" OR "P2."
MINOR ARTERIAL	- 8" CLASS "P1" OR "P2."
COMMERCIAL/INDUSTRIAL COLLECTOR	- 8" CLASS "P1" OR "P2."
RESIDENTIAL COLLECTOR	- 8" CLASS "P1" OR "P2."
RESIDENTIAL LOCAL	- 8" CLASS "P1" OR "P2."
SIDEWALK AND BFR-S-4-CLASS "A"	
DRIVE APPROACH-8-CLASS "P2"	
ALLEY-6" CLASS "P1" OR "P2."	
- CONCRETE MIX DESIGN SHALL BE AS DEFINED BY NCTCOG 303.3.
- ALL MEDIANS AND PARKWAYS SHALL BE PROVIDED WITH BERMUDA GROUND COVER.
- ONCE A CURB ABUTTING A THOROUGHFARE HAS BEEN SAWCUT AND REMOVED, THE CONTRACTOR MUST REPLACE THE CONCRETE WITH A NEW POUR (I.E. DRIVEWAY) WITHIN 14 CALENDAR DAYS. LIQUIDATED DAMAGES WILL BE ASSESSED AT \$500 PER DAY FOR EACH CALENDAR DAY IN EXCESS OF 14 CALENDAR DAYS. PAYMENT SHALL BE MADE PRIOR TO ACCEPTANCE OR ISSUANCE OF A CERTIFICATE OF OCCUPANCY.
- ALL SIDEWALKS AND ACCESSIBLE ROUTES SHALL HAVE A MAXIMUM LONGITUDINAL SLOPE OF 5% AND A MAXIMUM CROSS SLOPE OF 2%.
 - CONCRETE FOR ALLEY RETURNS AND DRIVEWAYS SHALL HAVE A MINIMUM COMPRESSIVE STRENGTH AT 28 DAYS IDENTICAL TO THAT SPECIFIED FOR THE STREET PAVEMENT OR BASE WHEN BUILT AS COMPONENTS OF A CONCRETE PAVING PROJECT. WHEN BUILT SEPARATELY, THE STRENGTH SHALL BE AS SPECIFIED ON THE CONSTRUCTION PLAN.
 - SPACING AND CONSTRUCTION OF JOINTS SHALL CONFORM TO PARABOLIC STREET PAVEMENT.

Addison!
PUBLIC WORKS DEPARTMENT

PAVING
GENERAL NOTES

STANDARD CONSTRUCTION DETAILS
PAVING

DATE: AUGUST, 2010 REV DATE: SHEET: SD-P01



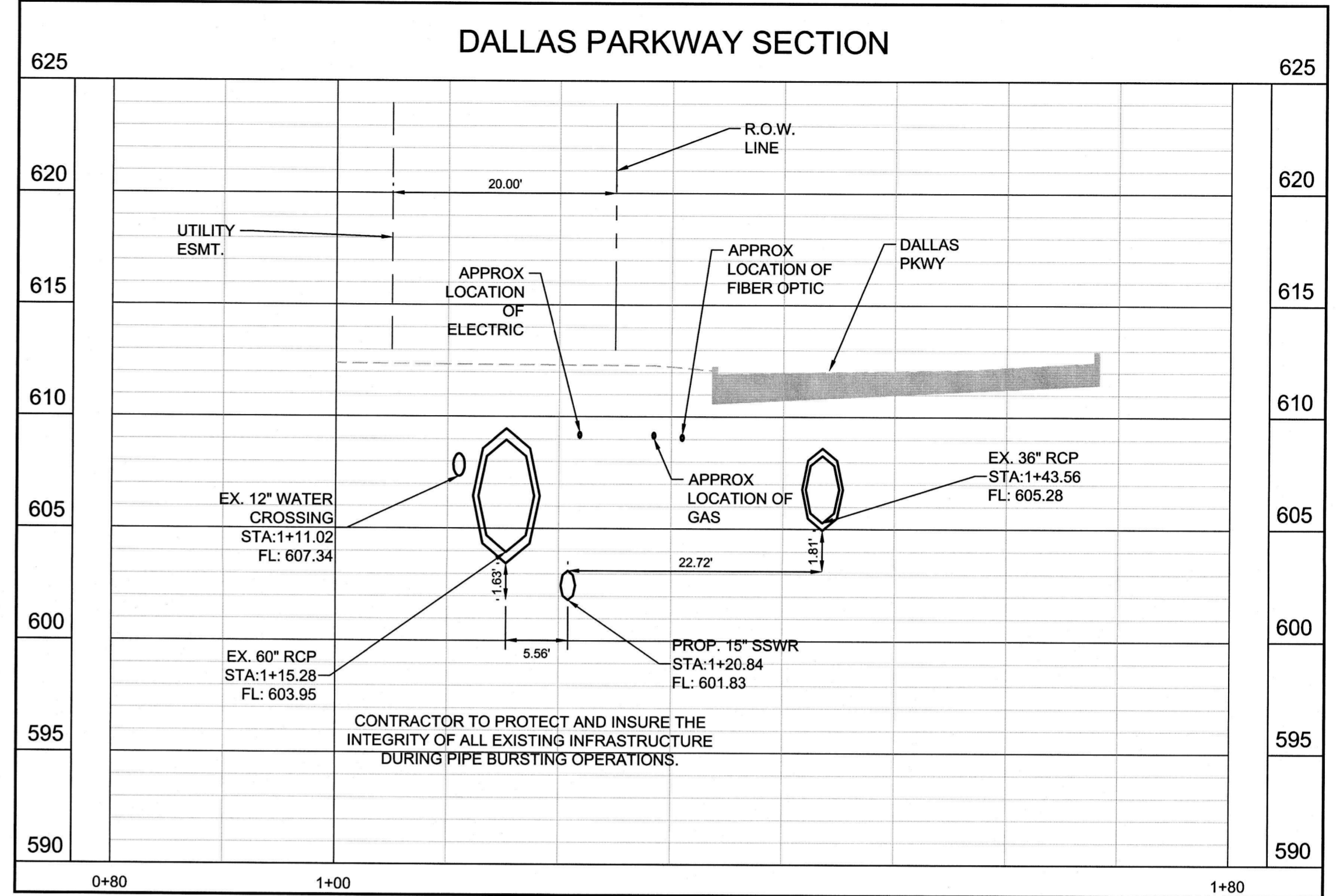
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PUBLIC WORKS DEPARTMENT

PRINCIPAL ARTERIAL
PAVEMENT

STANDARD CONSTRUCTION DETAILS
PAVING

DATE: AUGUST, 2010 REV DATE: SHEET: SD-P02

SECTION A-A
SCALE: 1" = 40' HORIZONTAL, 1" = 4" VERTICAL

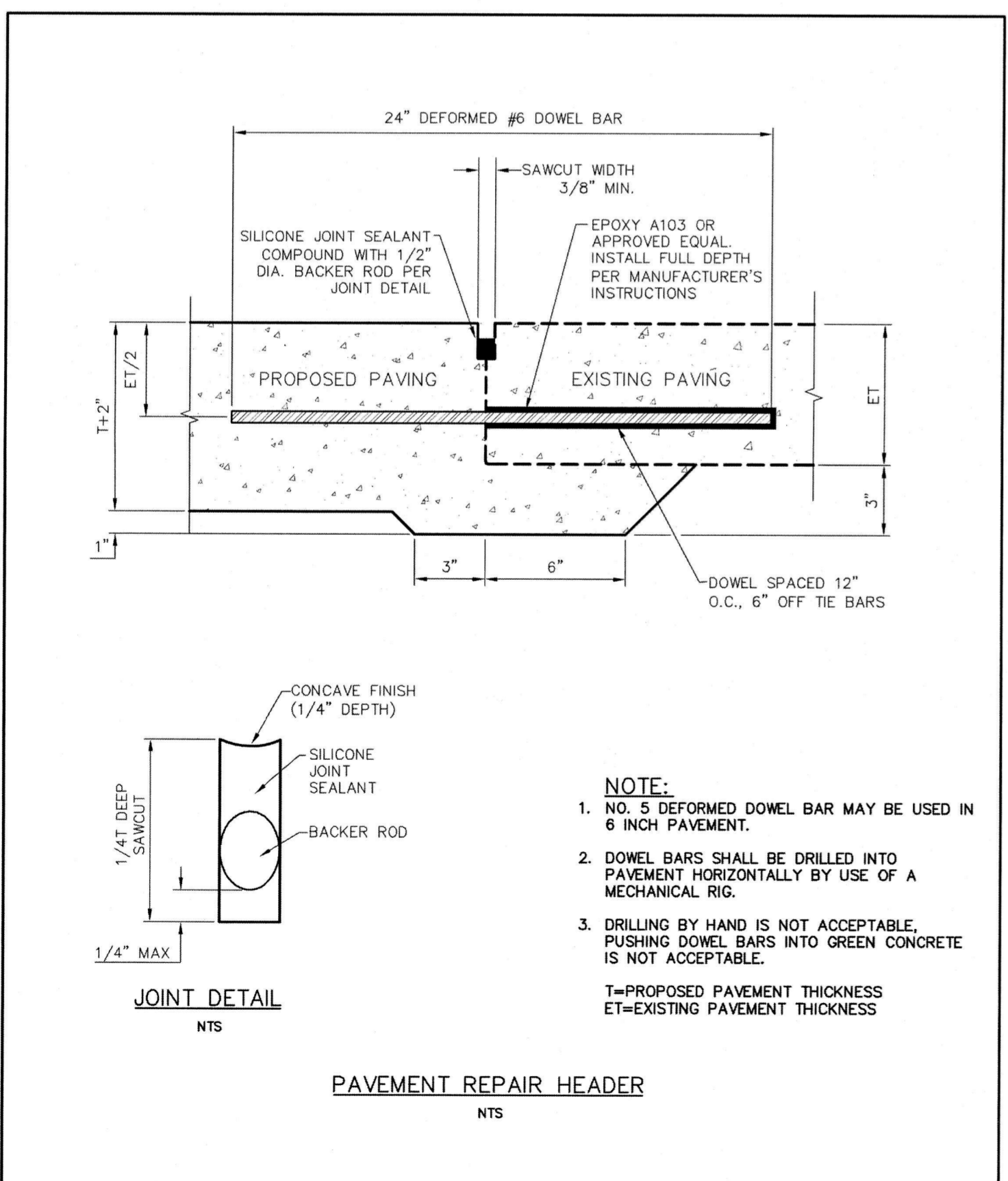


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MINOR ARTERIAL PAVEMENT

STANDARD CONSTRUCTION DETAILS
PAVING

DATE: AUGUST, 2010 REV DATE: SHEET: SD-P03

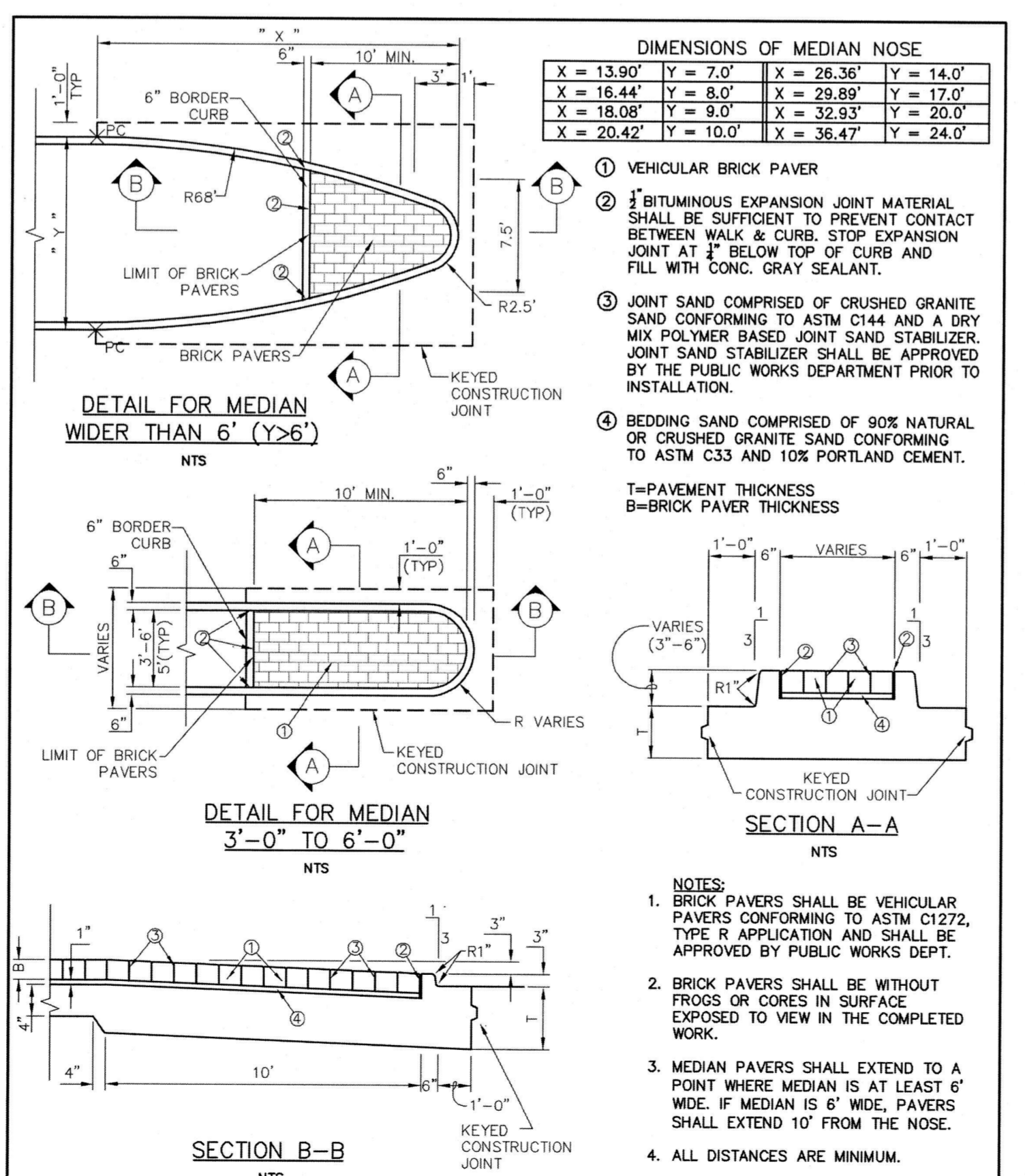


Addison!
PUBLIC WORKS DEPARTMENT

PAVEMENT REPAIR HEADER

STANDARD CONSTRUCTION DETAILS
PAVING

DATE: AUGUST, 2010 REV DATE: SHEET: SD-P04

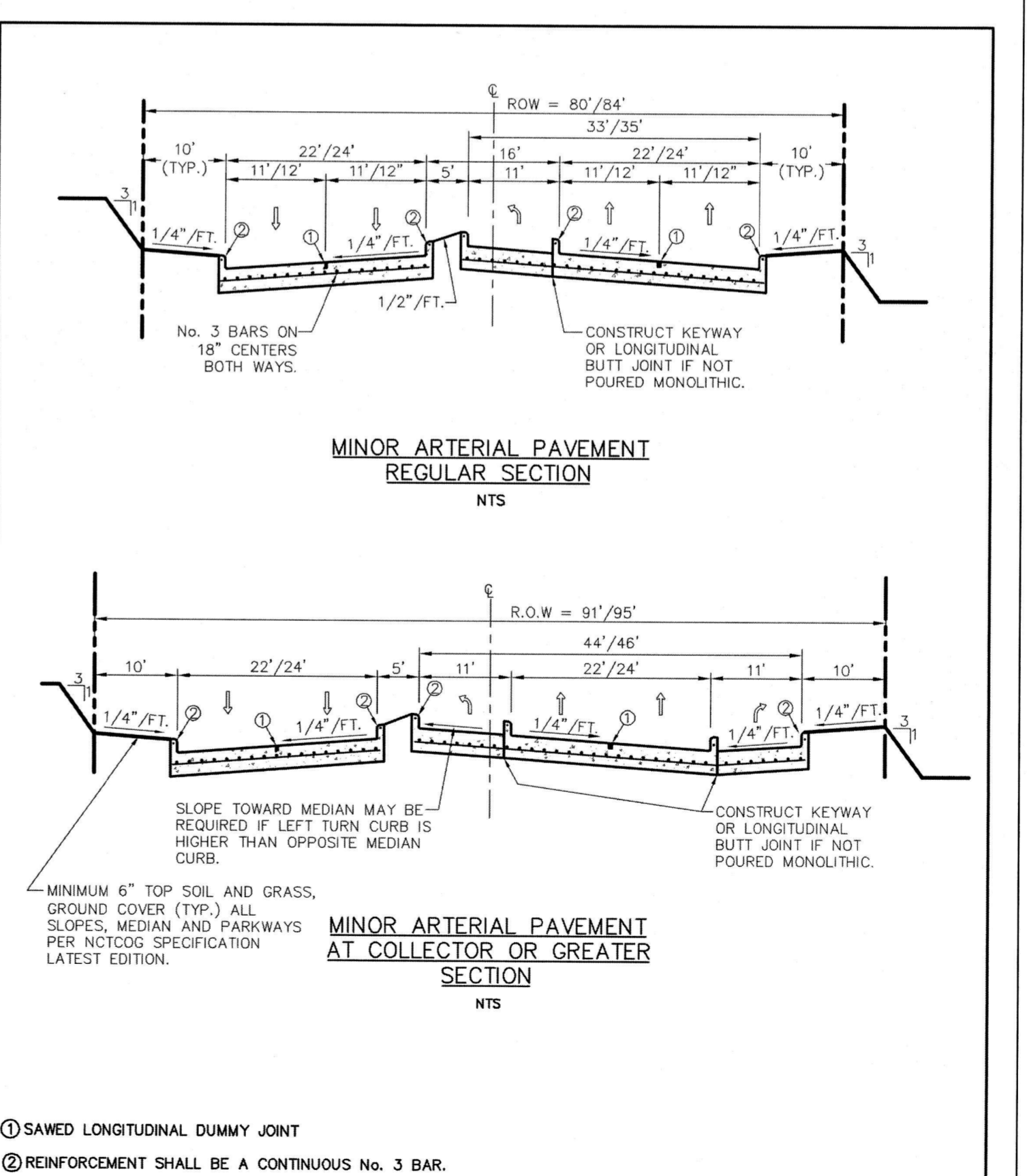


Addison!
PUBLIC WORKS DEPARTMENT

MEDIAN DETAILS

STANDARD CONSTRUCTION DETAILS
PAVING

DATE: AUGUST, 2010 REV DATE: SHEET: SD-P14



Addison!
PUBLIC WORKS DEPARTMENT

MINOR ARTERIAL PAVEMENT

STANDARD CONSTRUCTION DETAILS
PAVING

DATE: AUGUST, 2010 REV DATE: SHEET: SD-P04

APPROVED FOR CONSTRUCTION
INFRASTRUCTURE & DEVELOPMENT SERVICES
BY: *[Signature]*
DATE: 8/15/17
IN APPROVING THESE PLANS, THE TOWN OF ADDISON ASSUMES NO RESPONSIBILITY FOR THEIR ADEQUACY, WHICH REMAINS WITH THE ENGINEER OF RECORD.
18601 WESTGROVE DRIVE, ADDISON, TX 75001 - (972) 450-2871

BENCHMARKS

BM-1 Town of Addison GPS Control Station "TXDOT-COA-5", located on the corner of a curb inlet, 190' south of the intersection of Edwin Lewis Drive and Quorum Drive, 2' from the west edge of sidewalk, 50.5 feet from the west side of median and 76' from a fire hydrant. Elevation=630.10'

BM-2 Town of Addison GPS Control Station "TXDOT-COA-14", located 630' west of intersection of Addison Road & Westgrove Drive, 15.7' south of back of curb of concrete drive, 5' from street sign and 8' north of back of curb of Westgrove Road. Elevation=642.09'

BM-1 Square cut with "X" set on N.E. corner of drop inlet 545'-7" south of south R.O.W. of Edwin Lewis Dr. and 8'-4" west of Spectrum Drive back-of-curb. Elevation=630.89'

BM-2 Square cut with "X" set on concrete headwall at 12' southeast corner of Quorum Drive and Edwin Lewis Drive. Elevation=631.28'

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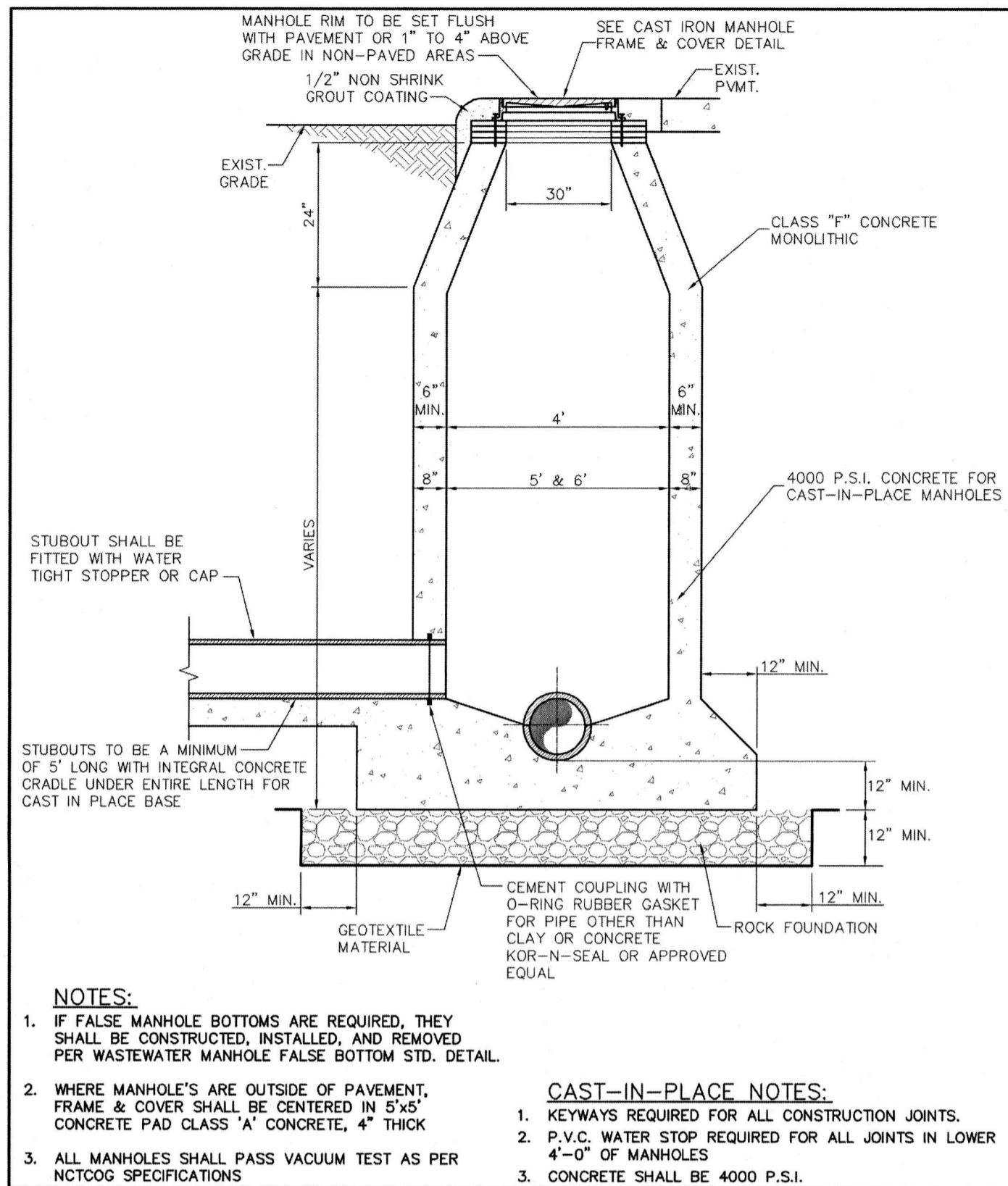
SCALE: AS SHOWN
DESIGNED BY: KMB
DRAWN BY: KMB
CHECKED BY: MKW

CONSTRUCTION DETAILS

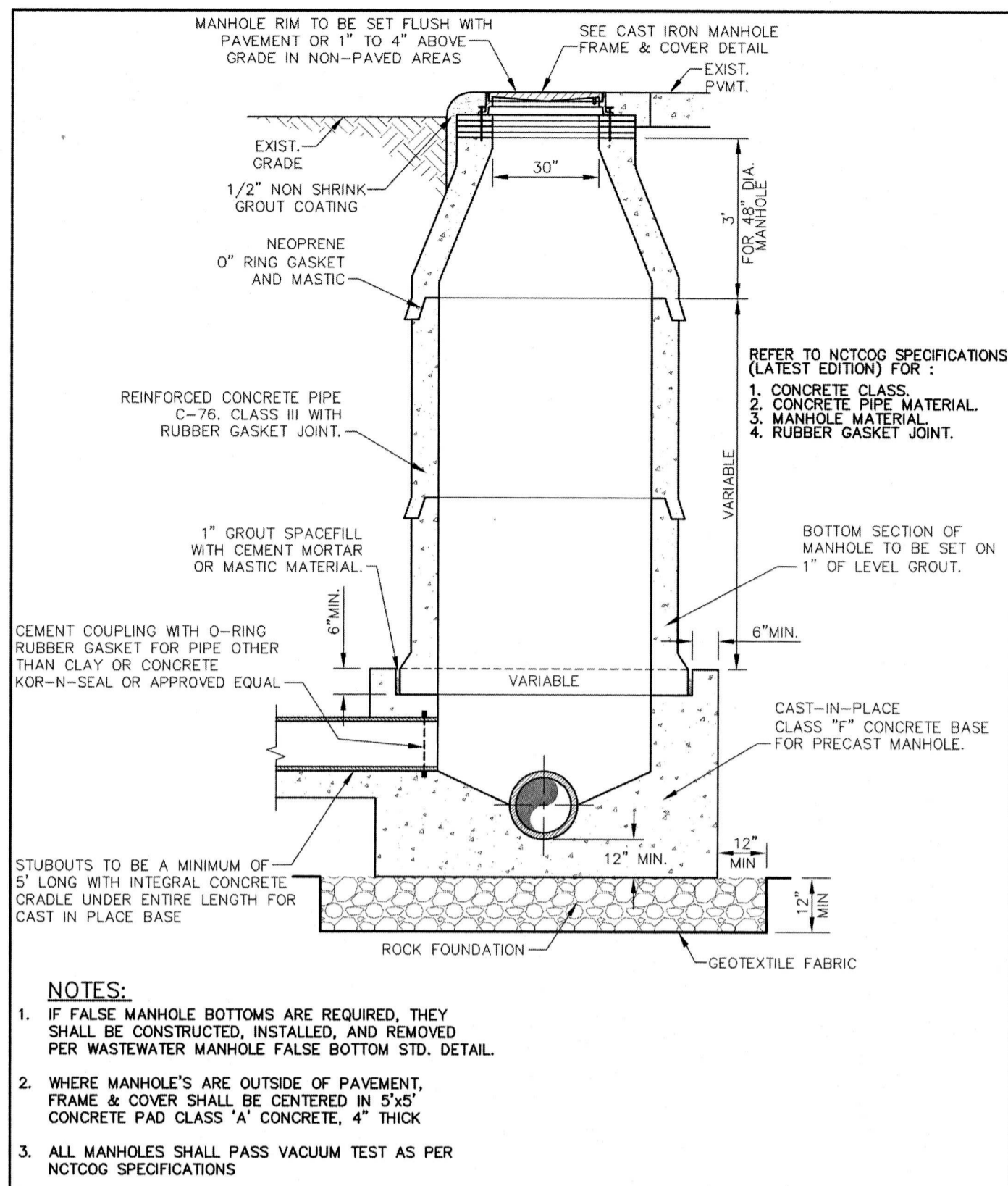
AMLI AT ADDISON OFF-SITE SEWER
CITY OF ADDISON
DALLAS COUNTY, TEXAS

DATE: 08/14/2017
PROJECT NO: 63257052
SHEET NUMBER: C-4

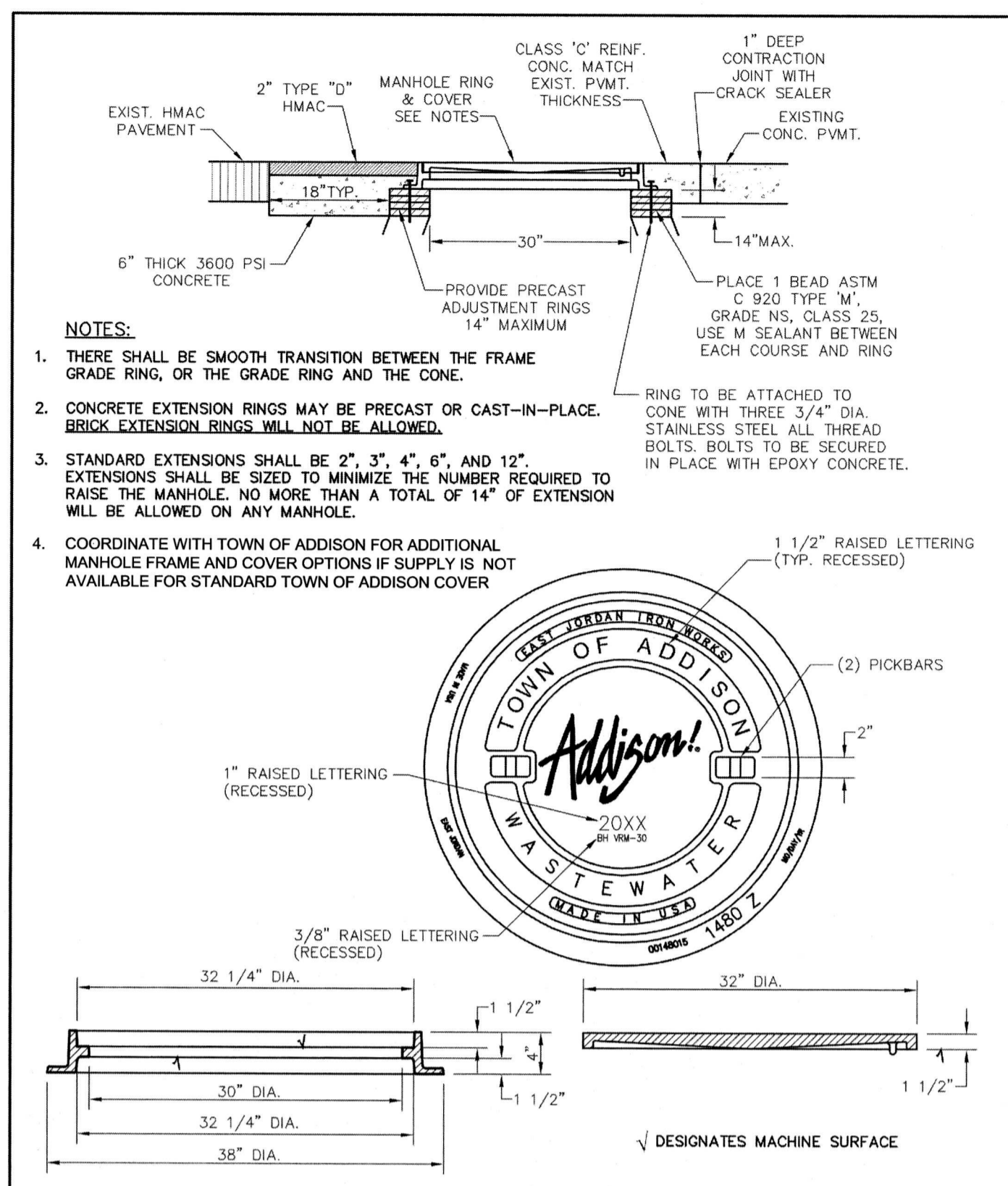
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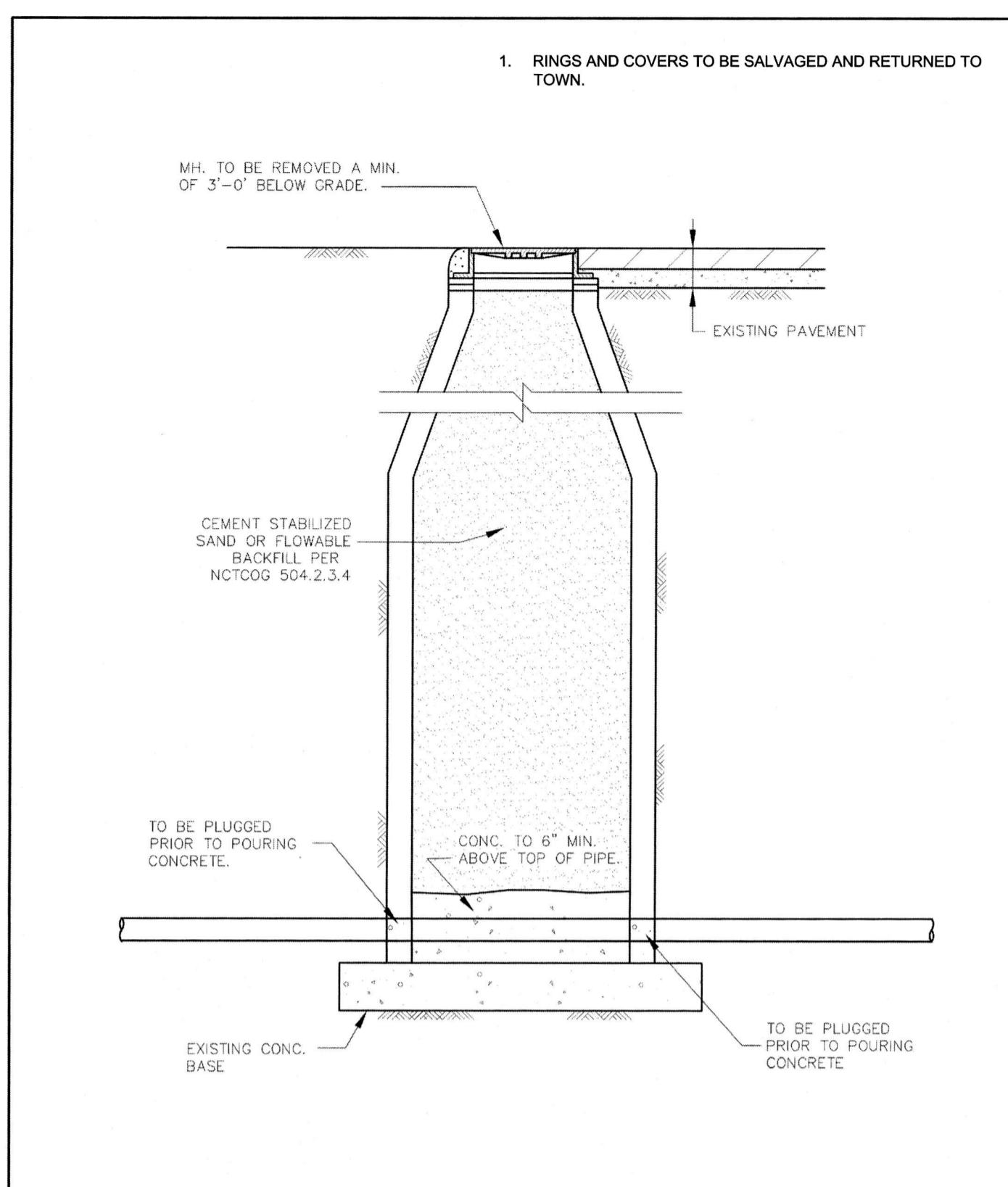
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		DATE: AUGUST, 2010	REV DATE: -	SHEET: SD-WW4



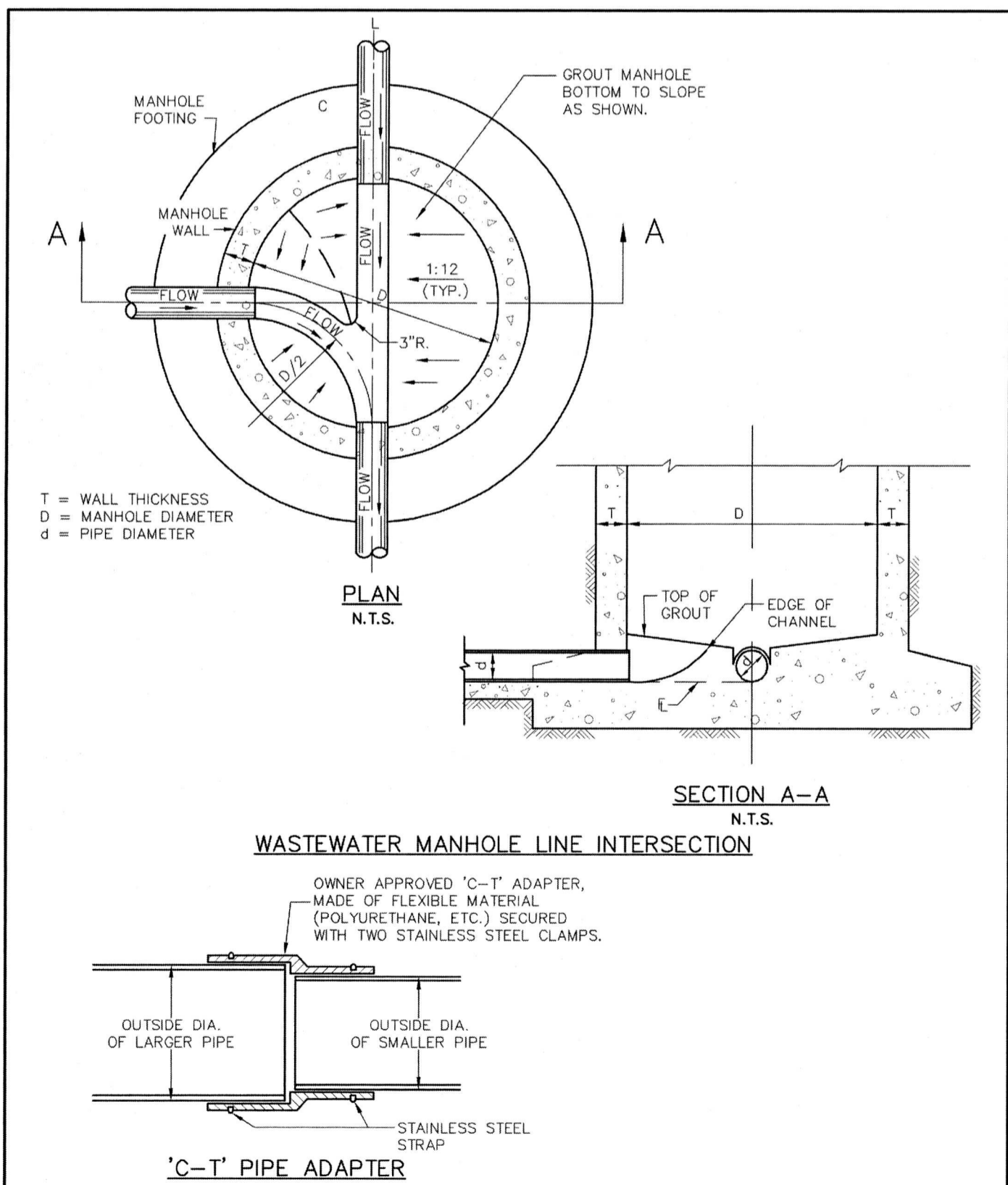
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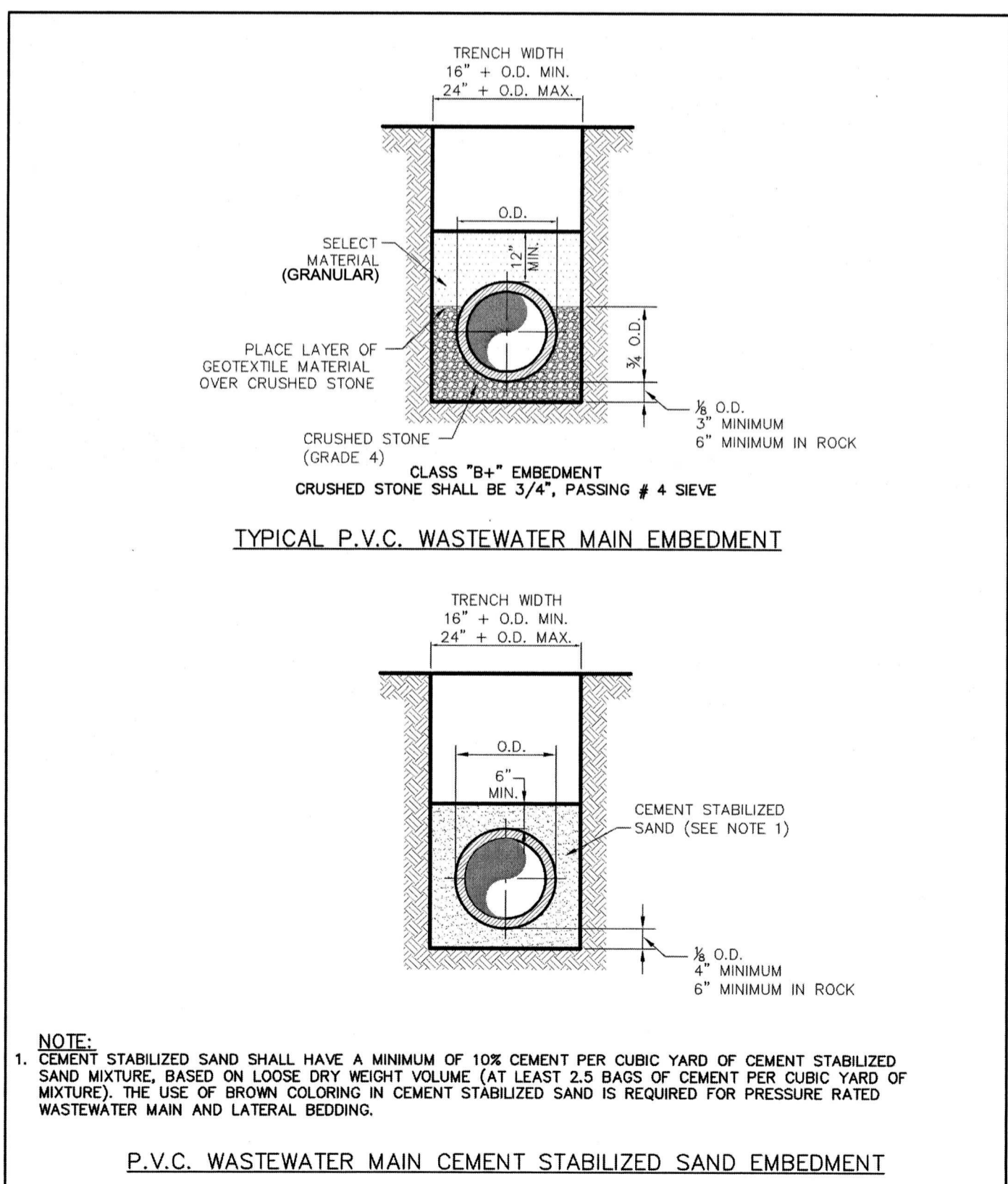
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		DATE: AUGUST, 2010	REV DATE: -	SHEET: SD-WW1



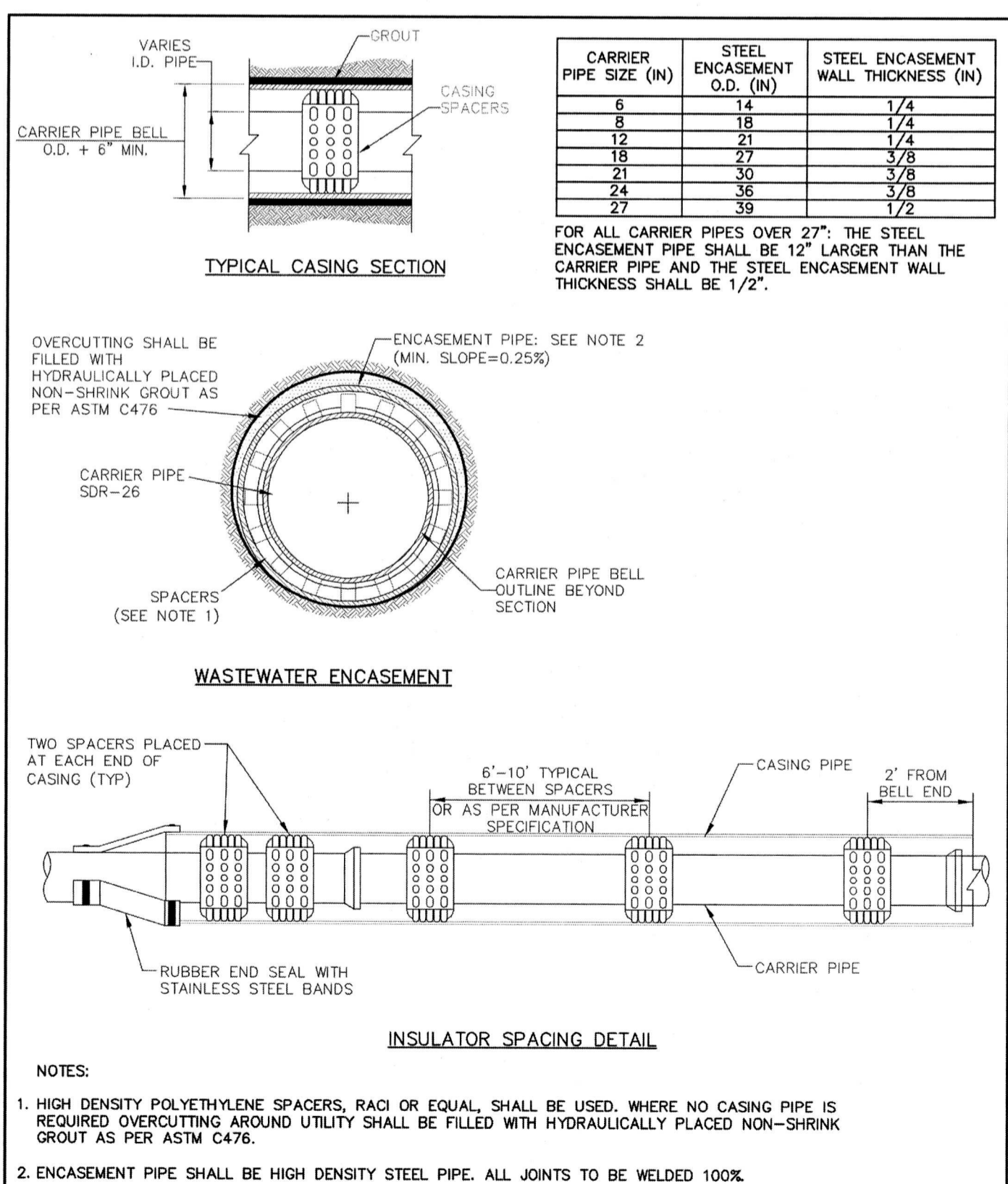
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		DATE: AUGUST, 2010	REV DATE: -	SHEET: SD-WW5



	WASTEWATER MANHOLE LINE INTERSECTION & "C-T" PIPE ADAPTER	STANDARD CONSTRUCTION DETAILS WASTEWATER		
		DATE: AUGUST, 2010	REV DATE: -	SHEET: SD-WW3



	P.V.C. WASTEWATER MAIN EMBEDMENT	STANDARD CONSTRUCTION DETAILS WASTEWATER		
		DATE: AUGUST, 2010	REV DATE: -	SHEET: SD-WW1



	CASING	STANDARD CONSTRUCTION DETAILS WASTEWATER		
		DATE: AUGUST, 2010	REV DATE: -	SHEET: SD-WW6

APPROVED FOR CONSTRUCTION
INFRASTRUCTURE & DEVELOPMENT SERVICES
BY: *[Signature]*
DATE: 8/15/10

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16801 WESTGROVE DRIVE, ADDISON, TX 75001 - (972) 450-2871

BENCHMARKS

BM-1 Town of Addison GPS Control Station "TXDOT-COA-5", located on the corner of a curb inlet, 190' south of the intersection of Edwin Lewis Drive and Quorum Drive, 2' from the west edge of sidewalk, 50.5 feet from the west side of median and 76' from a fire hydrant. Elevation=630.10'

BM-2 Town of Addison GPS Control Station "TXDOT-COA-14", located 630' west of intersection of Addison Road & Westgrove Drive, 15.7' south of back of curb of concrete drive, 5' from street sign and 8' north of back of curb of Westgrove Road. Elevation=642.09'

BM-1 Square cut with "X" set on N.E. corner of drop inlet 545'-4" south of south R.O.W of Edwin Lewis Dr. and 8'-4" west of Spectrum Drive back-of-curb. Elevation=630.89'

BM-2 Square cut with "X" set on concrete headwall at the southeast corner of Quorum Drive and Edwin Lewis Drive. Elevation=631.28'

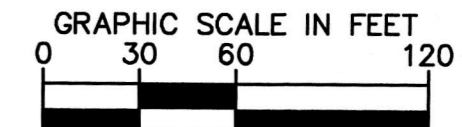
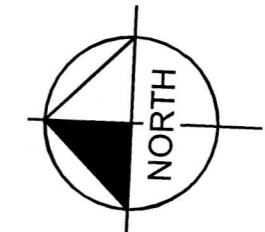
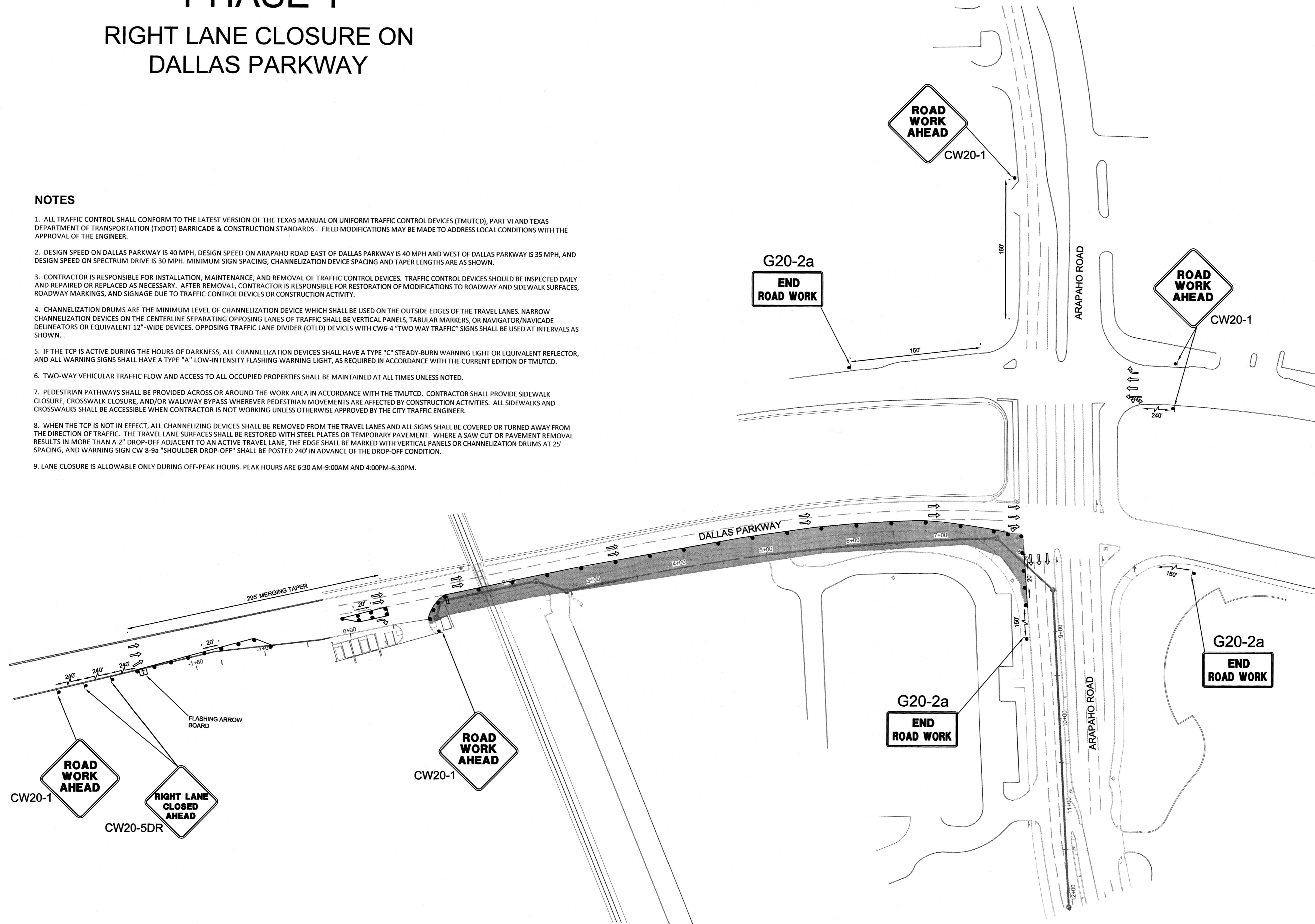
<p>© 2017 KIMLEY-HORN AND ASSOCIATES, INC. 5750 GENESIS COURT, SUITE 200, FRISCO, TX 75034 WWW.KIMLEY-HORN.COM TEXAS REGISTERED ENGINEERING FIRM F-528</p>	<p>MICHAEL WESTFALL LICENSED PROFESSIONAL ENGINEER 107094 8/1/2016</p>				
<p>SCALE: AS SHOWN DESIGNED BY: KMB DRAWN BY: KMB CHECKED BY: MKW</p>	<p>CONSTRUCTION DETAILS</p> <p>AMLI AT ADDISON OFF-SITE SEWER CITY OF ADDISON DALLAS COUNTY, TEXAS</p>				
<p>DATE: 08/14/2017 PROJECT NO: 63257052 SHEET NUMBER: C-5</p>	<p>REVISIONS</p> <table border="1"> <tr> <th>No.</th> <th>DATE</th> </tr> <tr> <td> </td> <td> </td> </tr> </table>	No.	DATE		
No.	DATE				

PHASE 1

RIGHT LANE CLOSURE ON DALLAS PARKWAY

NOTES

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9. LANE CLOSURE IS ALLOWABLE ONLY DURING OFF-PEAK HOURS. PEAK HOURS ARE 6:30 AM-9:00AM AND 4:00PM-6:30PM.



LEGEND

- LINE OF DELINEATORS
- SIGN
- FLASHING ARROW BOARD
- TYPE 3 BARRICADE
- DIRECTION OF TRAVEL
- WORK AREA



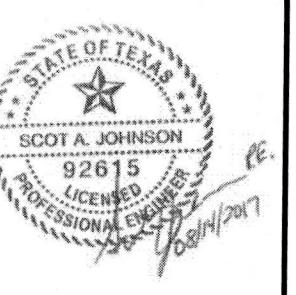
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INFRASTRUCTURE & DEVELOPMENT SERVICES

BY: *[Signature]*
DATE: 8/15/17

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NO.	REVISIONS	DATE

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WWW.KIMLEY-HORN.COM
TEXAS REGISTERED ENGINEERING FIRM F-928



SCALE AS SHOWN	DESIGNED BY KMB	DRAWN BY KMB	CHECKED BY MKW
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TRAFFIC CONTROL PLAN

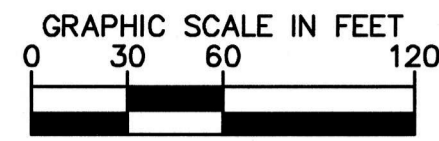
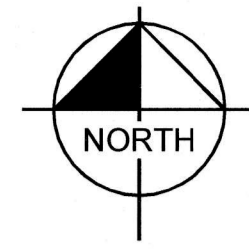
AMLI AT ADDISON
OFF-SITE SEWER
CITY OF ADDISON
DALLAS COUNTY, TEXAS

DATE	08/14/17
PROJECT NO.	63257052
SHEET NUMBER	TCP - 1

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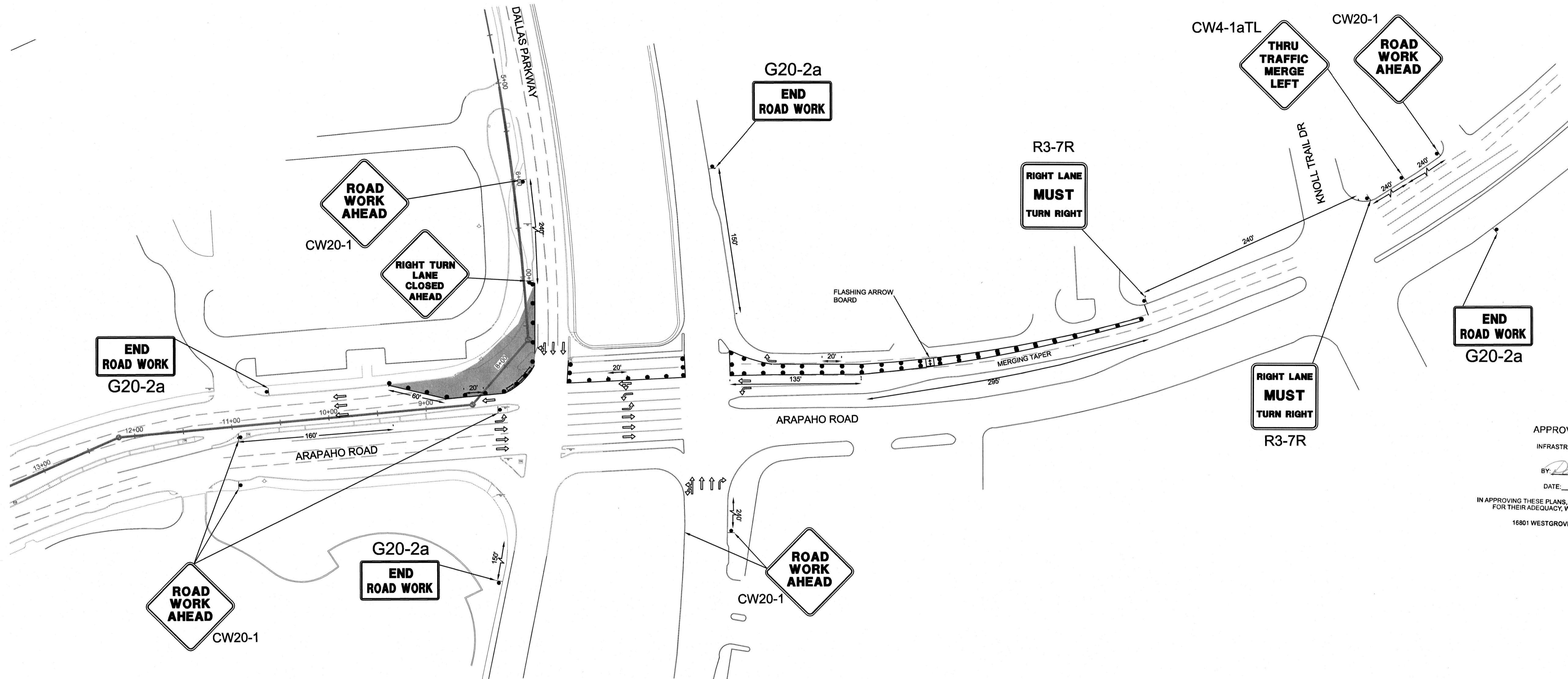
PHASE 2

TWO RIGHT LANES CLOSURE ON ARAPAHO ROAD

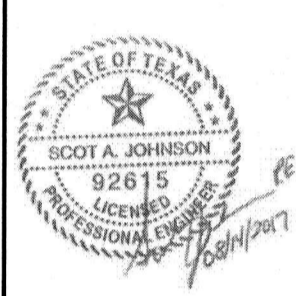


LEGEND

- LINE OF DELINEATORS
- SIGN
- FLASHING ARROW BOARD
- TYPE 3 BARRICADE
- DIRECTION OF TRAVEL
- WORK AREA



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INFRASTRUCTURE & DEVELOPMENT SERVICES

BY: *Daniel White*
 DATE: 8/15/17

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SCALE AS SHOWN	DESIGNED BY KIM	DRAWN BY KIM	CHECKED BY MKW
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TRAFFIC CONTROL PLAN

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**ARAPAHO ROAD WESTBOUND
 RIGHT LANE CAN BE CLOSED
 DURING PEAK HOURS.**

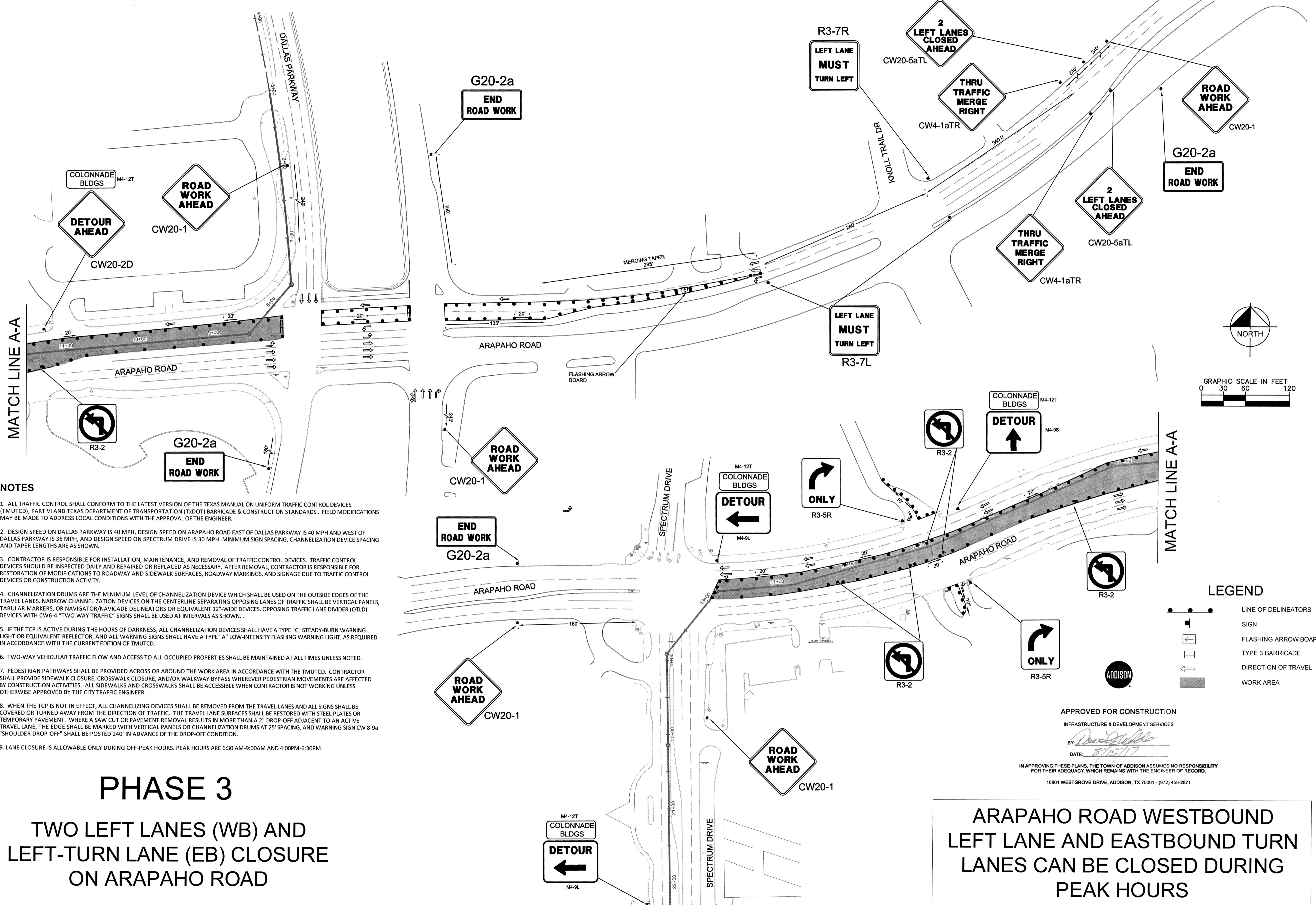
**CONTRACTOR SHALL COORDINATE
 WITH NEARBY APARTMENT
 DEVELOPMENT CONTRACTOR**

**AMLI AT ADDISON
 OFF-SITE SEWER
 CITY OF ADDISON
 DALLAS COUNTY, TEXAS**

DATE	08/14/17
PROJECT NO.	63257052
SHEET NUMBER	TCP - 2

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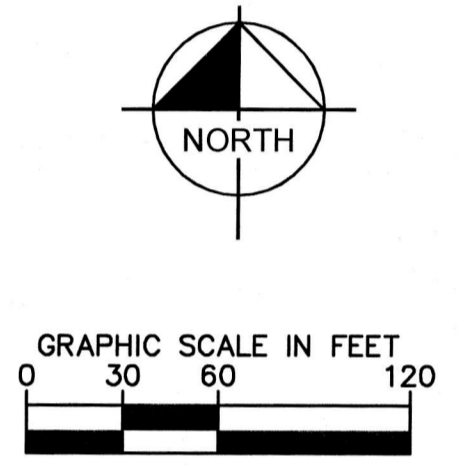
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PHASE 3

TWO LEFT LANES (WB) AND LEFT-TURN LANE (EB) CLOSURE ON ARAPAHO ROAD



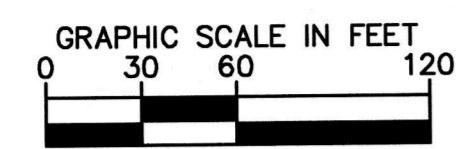
- LEGEND**
- LINE OF DELINEATORS
 - SIGN
 - FLASHING ARROW BOARD
 - TYPE 3 BARRICADE
 - DIRECTION OF TRAVEL
 - WORK AREA

APPROVED FOR CONSTRUCTION
 INFRASTRUCTURE & DEVELOPMENT SERVICES
 BY: *[Signature]*
 DATE: 8/15/17
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ARAPAHO ROAD WESTBOUND LEFT LANE AND EASTBOUND TURN LANES CAN BE CLOSED DURING PEAK HOURS

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<p>TRAFFIC CONTROL PLAN</p>	<p>SCALE: AS SHOWN DESIGNED BY: KMB DRAWN BY: KMB CHECKED BY: MKW</p>
<p>AMLI AT ADDISON OFF-SITE SEWER CITY OF ADDISON DALLAS COUNTY, TEXAS</p>	
<p>DATE: 08/14/17 PROJECT NO.: 63257052 SHEET NUMBER: TCP - 3</p>	

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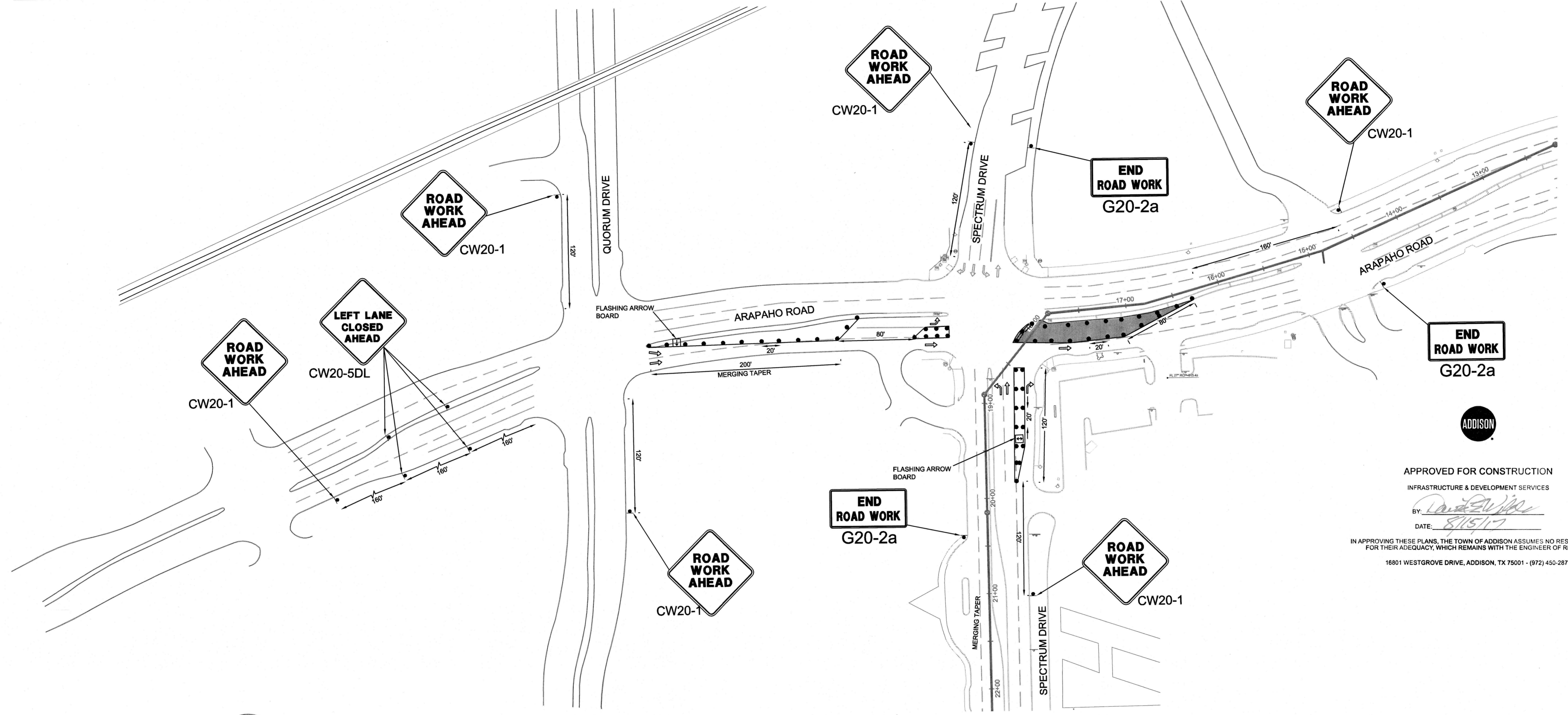


PHASE 4

TWO LEFT LANES CLOSURE ON ARAPAHO ROAD

LEGEND

- LINE OF DELINEATORS
- SIGN
- FLASHING ARROW BOARD
- TYPE 3 BARRICADE
- DIRECTION OF TRAVEL
- WORK AREA



**ARAPAHO ROAD EASTBOUND LEFT
LANE CAN BE CLOSED DURING
PEAK HOURS**

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8. WHEN THE TCP IS NOT IN EFFECT, ALL CHANNELIZING DEVICES SHALL BE REMOVED FROM THE TRAVEL LANES AND ALL SIGNS SHALL BE COVERED OR TURNED AWAY FROM THE DIRECTION OF TRAFFIC. THE TRAVEL LANE SURFACES SHALL BE RESTORED WITH STEEL PLATES OR TEMPORARY PAVEMENT. WHERE A SAW CUT OR PAVEMENT REMOVAL RESULTS IN MORE THAN A 2" DROP-OFF ADJACENT TO AN ACTIVE TRAVEL LANE, THE EDGE SHALL BE MARKED WITH VERTICAL PANELS OR CHANNELIZATION DRUMS AT 25' SPACING, AND WARNING SIGN CW 8-9a "SHOULDER DROP-OFF" SHALL BE POSTED 240' IN ADVANCE OF THE DROP-OFF CONDITION.
9. LANE CLOSURE IS ALLOWABLE ONLY DURING OFF-PEAK HOURS. PEAK HOURS ARE 6:30 AM-9:00AM AND 4:00PM-6:30PM.

ADDISON

APPROVED FOR CONSTRUCTION
INFRASTRUCTURE & DEVELOPMENT SERVICES

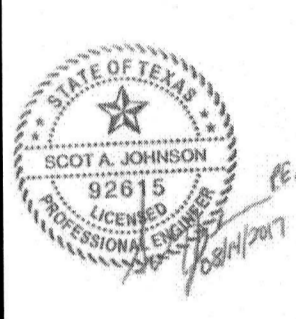
BY: *[Signature]*
DATE: 8/15/17

IN APPROVING THESE PLANS, THE TOWN OF ADDISON ASSUMES NO RESPONSIBILITY FOR THEIR ADEQUACY, WHICH REMAINS WITH THE ENGINEER OF RECORD.

16801 WESTGROVE DRIVE, ADDISON, TX 75001 - (972) 450-2871

NO.	REVISIONS	DATE

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 PHONE: 972-335-3580
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 TEXAS REGISTERED ENGINEERING FIRM F-928



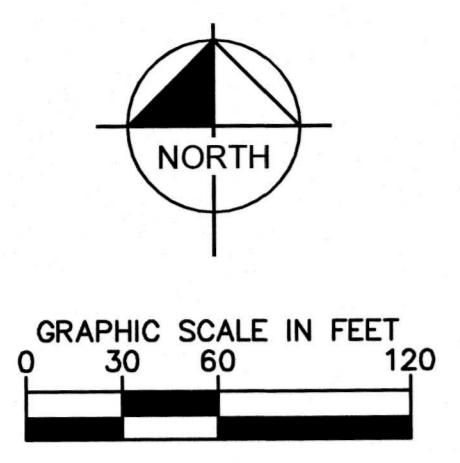
SCALE SHOWN
DESIGNED BY
KMB
DRAWN BY
KMB
CHECKED BY
MKW

TRAFFIC CONTROL
PLAN

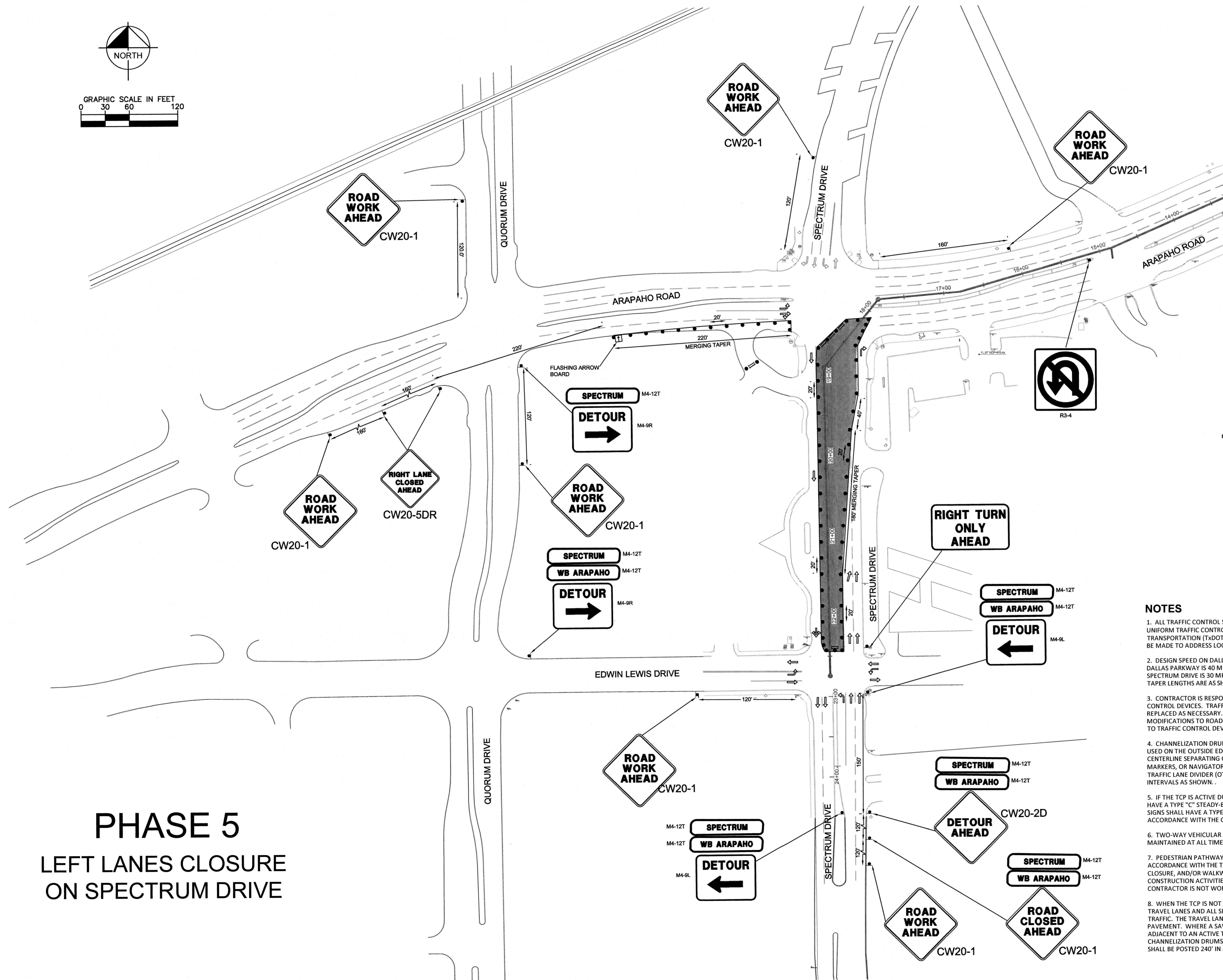
AMLI AT ADDISON
OFF-SITE SEWER
CITY OF ADDISON
DALLAS COUNTY, TEXAS

DATE 08/14/17	PROJECT NO. 63257052
SHEET NUMBER TCP - 4	

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LEGEND	
	LINE OF DELINEATORS
	SIGN
	FLASHING ARROW BOARD
	TYPE 3 BARRICADE
	DIRECTION OF TRAVEL
	WORK AREA



PHASE 5 LEFT LANES CLOSURE ON SPECTRUM DRIVE

ADDISON

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DATE: 8/14/17

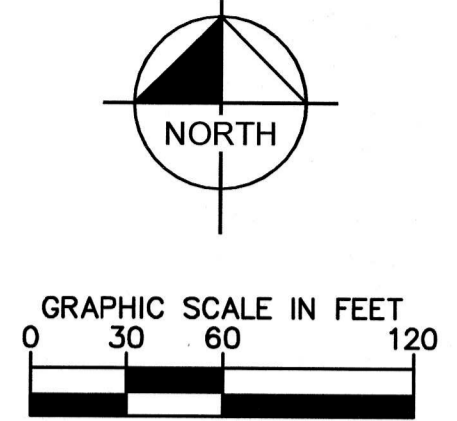
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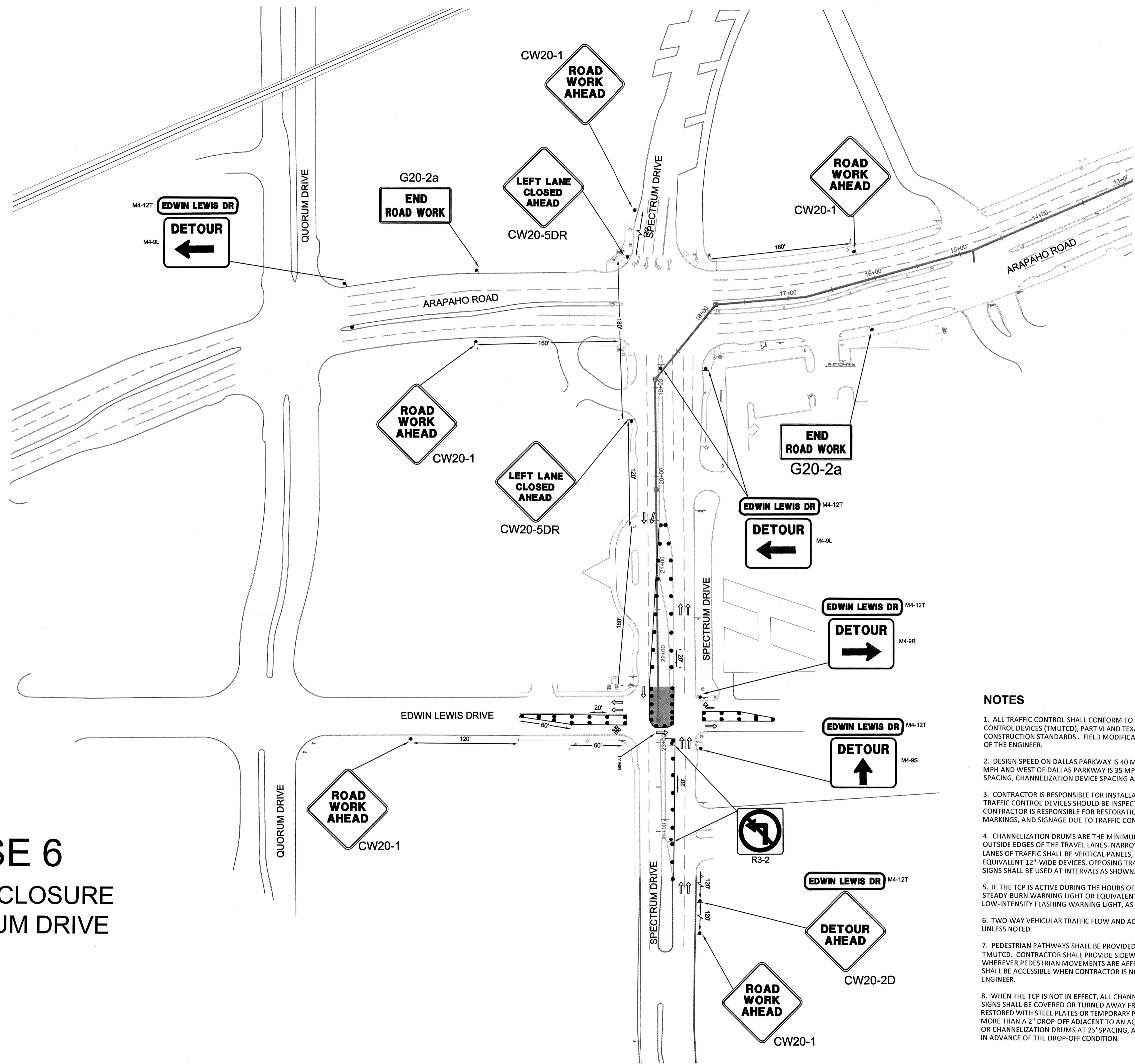
- NOTES**
1. ALL TRAFFIC CONTROL SHALL CONFORM TO THE LATEST VERSION OF THE TEXAS MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (TMUTCD), PART VI AND TEXAS DEPARTMENT OF TRANSPORTATION (TXDOT) BARRICADE & CONSTRUCTION STANDARDS. FIELD MODIFICATIONS MAY BE MADE TO ADDRESS LOCAL CONDITIONS WITH THE APPROVAL OF THE ENGINEER.
 2. DESIGN SPEED ON DALLAS PARKWAY IS 40 MPH, DESIGN SPEED ON ARAPAHO ROAD EAST OF DALLAS PARKWAY IS 40 MPH AND WEST OF DALLAS PARKWAY IS 35 MPH, AND DESIGN SPEED ON SPECTRUM DRIVE IS 30 MPH. MINIMUM SIGN SPACING, CHANNELIZATION DEVICE SPACING AND TAPER LENGTHS ARE AS SHOWN.
 3. CONTRACTOR IS RESPONSIBLE FOR INSTALLATION, MAINTENANCE, AND REMOVAL OF TRAFFIC CONTROL DEVICES. TRAFFIC CONTROL DEVICES SHOULD BE INSPECTED DAILY AND REPAIRED OR REPLACED AS NECESSARY. AFTER REMOVAL, CONTRACTOR IS RESPONSIBLE FOR RESTORATION OF MODIFICATIONS TO ROADWAY AND SIDEWALK SURFACES, ROADWAY MARKINGS, AND SIGNAGE DUE TO TRAFFIC CONTROL DEVICES OR CONSTRUCTION ACTIVITY.
 4. CHANNELIZATION DRUMS ARE THE MINIMUM LEVEL OF CHANNELIZATION DEVICE WHICH SHALL BE USED ON THE OUTSIDE EDGES OF THE TRAVEL LANES. NARROW CHANNELIZATION DEVICES ON THE CENTERLINE SEPARATING OPPOSING LANES OF TRAFFIC SHALL BE VERTICAL PANELS, TABULAR MARKERS, OR NAVIGATOR/NAVICADE DELINEATORS OR EQUIVALENT 12"-WIDE DEVICES. OPPOSING TRAFFIC LANE DIVIDER (OTLD) DEVICES WITH CW6-4 "TWO WAY TRAFFIC" SIGNS SHALL BE USED AT INTERVALS AS SHOWN.
 5. IF THE TCP IS ACTIVE DURING THE HOURS OF DARKNESS, ALL CHANNELIZATION DEVICES SHALL HAVE A TYPE "C" STEADY-BURN WARNING LIGHT OR EQUIVALENT REFLECTOR, AND ALL WARNING SIGNS SHALL HAVE A TYPE "A" LOW-INTENSITY FLASHING WARNING LIGHT, AS REQUIRED IN ACCORDANCE WITH THE CURRENT EDITION OF TMUTCD.
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	APPROVED FOR CONSTRUCTION INFRASTRUCTURE & DEVELOPMENT SERVICES BY: <i>[Signature]</i> DATE: 8/14/17
SCALE AS SHOWN DESIGNED BY KMB DRAWN BY KMB CHECKED BY MAV	REVISIONS NO. _____ DATE _____
TRAFFIC CONTROL PLAN	
AMLI AT ADDISON OFF-SITE SEWER CITY OF ADDISON DALLAS COUNTY, TEXAS	
DATE 08/14/17 PROJECT NO. 63257052 SHEET NUMBER TCP - 5	

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PHASE 6 LEFT LANES CLOSURE ON SPECTRUM DRIVE



LEGEND

	LINE OF DELINEATORS
	SIGN
	FLASHING ARROW BOARD
	TYPE 3 BARRICADE
	DIRECTION OF TRAVEL
	WORK AREA



APPROVED FOR CONSTRUCTION
 INFRASTRUCTURE & DEVELOPMENT SERVICES
 BY: *[Signature]*
 DATE: 8/15/17
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<p>TRAFFIC CONTROL PLAN</p>	<p>AMLI AT ADDISON OFF-SITE SEWER CITY OF ADDISON DALLAS COUNTY, TEXAS</p>
<p>SCALE AS SHOWN</p> <p>DESIGNED BY KMB</p> <p>DRAWN BY KMB</p> <p>CHECKED BY MKW</p>	<p>DATE 08/14/17</p> <p>PROJECT NO. 63257052</p> <p>SHEET NUMBER TCP - 6</p>