# **CIVIL CONSTRUCTION PLANS FOR OFF-SITE SANITARY SEWER**

# **PLANS SUBMITTAL/REVIEW LOG**

CIVIL ENGINEERING PACKAGE - NOT FOR CONSTRUCTION.	05/01/2017	
CIVIL ENGINEERING PACKAGE SUBMITTAL 2 -NOT FOR CONSTRUCTION	06/02/2017	
CIVIL ENGINEERING PACKAGE SUBMITTAL 3 -NOT FOR CONSTRUCTION	7/26/2017	
CIVIL ENGINEERING PACKAGE SUBMITTAL 4 -FOR CONSTRUCTION	8/14/2017	

# TOWN OF ADDISON CITY COUNCIL

## TOWN HALL

5300 BELT LINE ROAD ADDISON, TX 75254 (972) 450-7000

### COUNCIL MEMBERS:

- JOE CHOW
- IVAN HUGHES
- JIM DUFFY
- AL ANGELL
- TOM BRAUN
- PAUL WALDEN LORI WARD

MAYOR PRO TEMPORE DEPUTY MAYOR PRO TEMPORE COUNCIL MEMBER COUNCIL MEMBER COUNCIL MEMBER COUNCIL MEMBER

# ENGINEER **Kimley**»Horn

MAYOR

5750 GENESIS COURT STATE OF TEXAS **REGISTRATION NO. F-928** SUITE 200 FRISCO, TEXAS 75034 PH. (972) 335-3580 CONTACT: MICHAEL WESTFALL, P.E. MICHAEL.WESTFALL@KIMLEY-HORN.COM

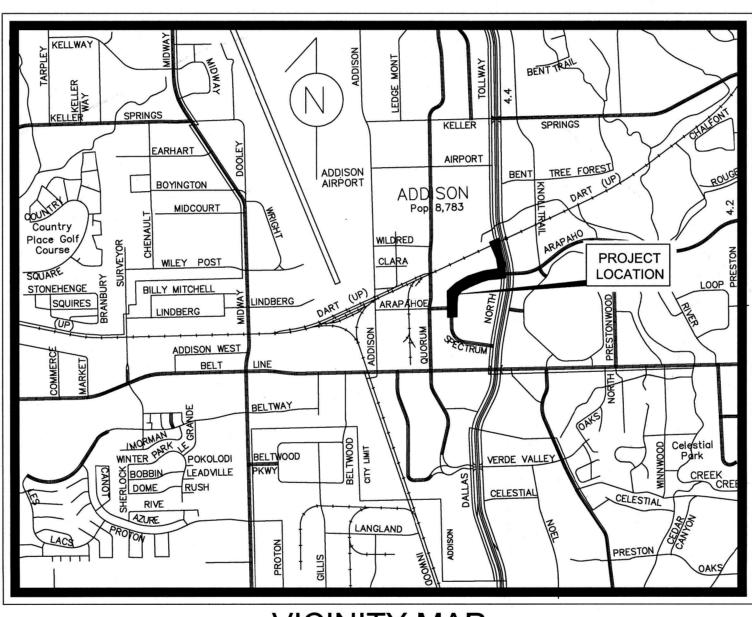
# **OWNER/DEVELOPER**

AMLI RESIDENTIAL 5057 KELLER SPRINGS ROAD SUITE 250 ADDISON, TX 75001 CONTACT: TAYLOR BOWEN

# **AMLI ADDISON 15250 QUORUM DRIVE**

# **TOWN OF ADDISON** DALLAS COUNTY, TEXAS

**TOWN PROJECT # 17-05** 



## VICINITY MAP SCALE: 1" = 2,000'

- NOTES 1. THE PROJECT SHALL MEET OR EXCEED THE STANDARD SPECIFICATIONS FOR PUBLIC WORKS CONSTRUCTION FOR THE NCTCOG - NORTH CENTRAL TEXAS COUNCIL OF GOVERNMENTS (LATEST EDITION), THE TOWN OF ADDISON STANDARDS, AND ANY SPECIAL PROVISIONS ADOPTED BY THE TOWN OF ADDISON. TOWN OF ADDISON STANDARDS GOVERN OVER NCTCOG IN THE EVENT OF A CONFLICT.
- 2. ALL NECESSARY INSPECTIONS AND/OR CERTIFICATIONS REQUIRED BY CODES AND/OR UTILITY SERVICE COMPANIES SHALL BE PERFORMED PRIOR TO CONTRACT COMPLETION AND THE FINAL CONNECTION OF SERVICES.
- 3. IF REPRODUCED, THE SCALES SHOWN ON THESE PLANS ARE BASED ON A 22" X 34" SHEET.

# **AUGUST 2017**



# **INDEX OF SHEETS**

SHEET NO.	DESCRIPTION	DATE	REVISION
C-0	COVER SHEET	08/14/2017	
C-1	GENERAL NOTES	08/14/2017	
C-2	SEWER LINE A PLAN AND PROFILE	08/14/2017	
C-3	SEWER LINE A PLAN AND PROFILE	08/14/2017	
C-4	CONSTRUCTION DETAILS	08/14/2017	
C-5	CONSTRUCTION DETAILS	08/14/2017	
TCP-1	TRAFFIC CONTROL PLAN	08/14/2017	
TCP-2	TRAFFIC CONTROL PLAN	08/14/2017	
TCP-3	TRAFFIC CONTROL PLAN	08/14/2017	
TCP-4	TRAFFIC CONTROL PLAN	08/14/2017	
TCP-5	TRAFFIC CONTROL PLAN	08/14/2017	
TCP-6	TRAFFIC CONTROL PLAN	08/14/2017	



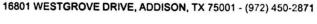
APPROVED FOR CONSTRUCTION **INFRASTRUCTURE & DEVELOPMENT SERVICES** 

15/17

OOS SUL



IN APPROVING THESE PLANS, THE TOWN OF ADDISON ASSUMES NO RESPONSIBILITY FOR THEIR ADEQUACY, WHICH REMAINS WITH THE ENGINEER OF RECORD.





Know what's **below**. Call before you dig.





AMLI OFFSFE 55

### GENERAL NOTES

### GRADING

ALL CONSTRUCTION SHALL BE IN GENERAL ACCORDANCE WITH THESE PLANS, TOWN OF ADDISON, TX STANDARD SPECIFICATIONS, THE FINAL GEOTECHNICAL REPORT, AND COMMONLY ACCEPTED CONSTRUCTION STANDARDS.

THE CONTRACTOR IS RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND APPROVALS.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR REMOVAL AND DISPOSAL OFFSITE OF ANY EXISTING PAVING AND STRUCTURES REMOVED.

BEFORE STARTING CONSTRUCTION, CONTRACTOR SHALL FIELD VERIFY THE HORIZONTAL AND VERTICAL LOCATION OF ALL EXISTING UTILITIES WHERE PROPOSED UTILITIES ARE BEING CONNECTED. THE LOCATION OF ALL UTILITIES SHOWN ON THESE PLANS WAS TAKEN FROM EXISTING PUBLIC RECORDS. THE EXACT LOCATION AND ELEVATION OF ALL PUBLIC UTILITIES MUST BE DETERMINED BY CONTRACTOR. IT SHALL BE THE DUTY AND RESPONSIBILITY OF THE CONTRACTOR TO ASCERTAIN WHETHER ANY ADDITIONAL FACILITIES OTHER THAN THOSE SHOWN ON THE PLANS MAY BE PRESENT. CONTRACTOR SHALL NOTIFY THE ENGINEER IMMEDIATELY IF A DISCREPANCY AND/OR CONFLICT IS DISCOVERED. CONTRACTOR WILL BE RESPONSIBLE FOR ANY DAMAGE CAUSED TO EXISTING UTILITIES DURING CONSTRUCTION.

5. IT IS THE CONTRACTOR'S RESPONSIBILITY TO MAINTAIN NEAT AND ACCURATE CONSTRUCTION RECORD PLANS.

CONTRACTOR SHALL BE RESPONSIBLE FOR ALL CONSTRUCTION SURVEYING.

WATER AND SEWER SERVICES SHALL BE CONSTRUCTED IN ACCORDANCE WITH THE TOWN OF ADDISON, TX STANDARDS AND SPECIFICATIONS.

- 8. ALL EXISTING TRAFFIC AND STREET SIGNS DISTURBED SHALL BE REINSTALLED WHERE APPLICABLE BY THE CONTRACTOR.
- CONTRACTOR SHALL BE RESPONSIBLE FOR LOCATING ALL EXISTING STRUCTURES, UTILITIES, AND SERVICES PRIOR TO EXCAVATION AND CONSTRUCTION
- 10. CONTRACTOR SHALL BE RESPONSIBLE FOR COORDINATING WITH UTILITY COMPANIES FOR THE RELOCATION OF ANY EXISTING UTILITIES.
- 11. CONTRACTOR SHALL USE ALL NECESSARY PRECAUTIONS TO AVOID CONTACT WITH OVERHEAD AND UNDERGROUND POWER LINES.
- 12. THE CONTRACTOR SHALL REVIEW AND VERIFY ALL DIMENSIONS SHOWN ON THE PLANS AND REVIEW ALL FIELD CONDITIONS, INCLUDING EXISTING GRADES AND UTILITY FLOW LINES, AND SHOULD DISCREPANCIES OCCUR, THE CONTRACTOR SHALL NOTIFY THE
- ENGINEER TO OBTAIN THE ENGINEER'S CLARIFICATION BEFORE COMMENCING WITH CONSTRUCTION. 13. THE CONTRACTOR SHALL PROTECT ALL EXISTING STRUCTURES, UTILITIES, AND OTHER FACILITIES TO REMAIN AND SHALL REPAIR ANY DAMAGES DUE TO HIS CONSTRUCTION ACTIVITIES AT NO COST TO THE OWNER.
- 14. ALL EXISTING SHRUBS, TREES, PLANTING, AND OTHER VEGETATION, OUTSIDE OF PROPERTY LIMITS DISTURBED DURING CONSTRUCTION SHALL BE REPLACED WITH EQUIVALENT MATERIAL BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE OWNER.
- 15. THE CONTRACTOR SHALL CONSTRUCT SILT SCREENS OR OTHER APPROVED DEVICES PRIOR TO CONSTRUCTION TO PREVENT ADVERSE OFF SITE IMPACT OF STORM WATER QUALITY, AS REQUIRED BY THE TOWN OF ADDISON, TX. CONTRACTOR IS RESPONSIBLE FOR PROPER MAINTENANCE OF THE REQUIRED EROSION CONTROL DEVICES THROUGHOUT THE ENTIRE CONSTRUCTION PROCESS. THE EROSION CONTROL DEVICES SHOULD REMAIN IN PLACE, WHERE PRACTICAL, UPON COMPLETION OF CONSTRUCTION.
- 16. CONTRACTOR IS RESPONSIBLE FOR REMOVING ALL SILT AND DEBRIS OFFSITE FROM THE EXISTING ROADWAYS AND PROJECT SITE THAT ARE A RESULT OF THE PROPOSED CONSTRUCTION AS REQUESTED BY THE TOWN OF ADDISON, TX. AS A MINIMUM, THIS TASK SHOULD OCCUR ONCE A WEEK
- 17. CONNECTIONS TO EXISTING FACILITIES SHALL BE ACCOMPLISHED IN A NEAT AND PROFESSIONAL MANNER. WHEN FIELD CONDITIONS INDICATE ANY VARIANCE FROM DETAILED METHODS, THE CONTRACTOR SHALL PROVIDE COMPREHENSIVE AND DETAILED DRAWINGS (FOR APPROVAL) OF METHODS PROPOSED.
- 18. WATER SHALL NOT BE PERMITTED IN OPEN TRENCHES DURING CONSTRUCTION.

OTHERWISE SPECIFIED ON THE PLANS.

- 19. CONTRACTOR SHALL CONTACT THE TOWN INFRASTRUCTURE DEPARTMENT'S INSPECTOR ASSIGNED TO THIS PROJECT AT LEAST 48 HOURS PRIOR TO STARTING CONSTRUCTION.
- 20. CONTRACTOR IS RESPONSIBLE FOR GRASSING DISTURBED AREAS FROM BACK OF CURB TO THE RIGHT-OF-WAY AND AREAS
- 21. ALL STORM DRAIN PIPE SHALL BE CLASS III RCP WITH A MIN. ALLOWABLE STRENGTH OF 4,200 PSI.
- 22. ALL PRIVATE LANDSCAPE AREA DRAINS SHALL BE OF MATERIAL APPROVED BY BOTH ENGINEER AND TOWN OF ADDISON, TX.
- 23. CONTRACTOR IS TO CONSTRUCT A STABILIZED CONSTRUCTION EXIT AT ALL PRIMARY POINTS OF ACCESS. THIS STABILIZED EXIT SHALL BE CONSTRUCTED PER TOWN STANDARDS.
- 24. ANY WATER OR SANITARY SEWER SERVICE LOCATED OUTSIDE OF A STREET, ALLEY, OR EASEMENT SHALL BE INSTALLED BY A PLUMBER AND BE INSPECTED BY CODE ENFORCEMENT.
- 25. THE CONTRACTOR SHALL BE RESPONSIBLE FOR SUBMITTING A TRENCH SAFETY PLAN TO THE TOWN OF ADDISON, TX INFRASTRUCTURE DEPARTMENT AT THE TIME OF THE PRECONSTRUCTION MEETING, OR PRIOR TO BEGINNING CONSTRUCTION OF THESE IMPROVEMENTS. CONTRACTOR SHALL BE RESPONSIBLE FOR MAINTAINING TRENCH SAFETY REQUIREMENTS IN ACCORDANCE WITH TOWN STANDARDS, TEXAS STATE LAW, AND O.S.H.A. STANDARDS FOR ALL EXCAVATION IN EXCESS OF FIVE FEET IN DEPTH. NO OPEN TRENCHES WILL BE ALLOWED OVERNIGHT WITHOUT THE PRIOR SPECIFIC WRITTEN APPROVAL OF THE TOWN OF ADDISON, TX INFRASTRUCTURE DEPARTMENT. ONSITE SAFETY IS THE SOLE RESPONSIBILITY OF THE CONTRACTOR.
- 26. DURING CONSTRUCTION, ALL MATERIAL TESTING SHALL BE COORDINATED WITH THE TOWN OF ADDISON, TX'S CONSTRUCTION INSPECTOR
- 27. CONTRACTOR SHALL CONTACT THE TOWN ENGINEER, OR HIS DESIGNER, TO LEARN OF ANY UNUSUAL CONSTRUCTION SEQUENCING REQUIREMENTS THE TOWN MAY REQUIRE.
- 28. THE CONTRACTOR WILL BE RESPONSIBLE FOR COMPLYING WITH TOWN SPECIFICATIONS FOR PAVING CONSTRUCTION, COMPACTION REQUIREMENTS, AND SUBGRADE PREPARATION.
- 29. CONTRACTOR TO REVIEW DESIGN INTENT OF THESE PLANS AND SUBMIT REQUESTS-FOR-INFORMATION IN A TIMELY MANNER PRIOR TO COMMENCING THAT WORK.
- 30. ALL APPURTENANCES INSTALLED IN PAVEMENT AREAS SHALL BE ADJUSTED AS REQUIRED TO BE FLUSH WITH FINISHED PAVEMENT.
- 31. THE CONTRACTOR WILL BE SOLELY RESPONSIBLE FOR IMPLEMENTING THE TRAFFIC CONTROL PLAN.

### STORM DRAINAGE

OTHER UTILITIES.

- THE CONTRACTOR SHALL BE SOLELY RESPONSIBLE FOR SITE TRENCH SAFETY DURING ALL PHASES OF CONSTRUCTION. THE CONTRACTOR SHALL SUBMIT A TRENCH EXCAVATION PROTECTION PLAN, SEALED BY AN ENGINEER REGISTERED IN THE STATE OF TEXAS, FOR ALL TRENCHES DEEPER THAN FIVE (5) FEET.
- THE CONTRACTOR SHALL VERIFY AND COORDINATE ALL DIMENSIONS SHOWN, INCLUDING THE HORIZONTAL AND VERTICAL LOCATION OF CURB INLETS AND GRATE INLETS AND ALL UTILITIES CROSSING THE STORM SEWER.
- 3. THE SITE UTILITY CONTRACTOR SHALL PROVIDE ALL MATERIALS AND APPURTENANCES NECESSARY FOR COMPLETE INSTALLATION OF THE STORM SEWER.
- 4. THE INSPECTOR SHALL INSPECT ALL "PUBLIC" CONSTRUCTION. THE CONTRACTOR'S BID PRICE SHALL INCLUDE ALL INSPECTION FEES.
- 5. CONTRACTOR IS RESPONSIBLE FOR OBTAINING ALL REQUIRED PERMITS AND APPROVALS PRIOR TO COMMENCING CONSTRUCTION.
- 6. ALL PVC TO RCP CONNECTIONS SHALL BE CONSTRUCTED WITH CONCRETE COLLARS. ALL MATERIALS AND WORKMANSHIP SHALL CONFORM TO THE STANDARD SPECIFICATIONS FOR PUBLIC WORKS CONSTRUCTION FOR NORTH CENTRAL TEXAS, LATEST EDITION, AND ANY SPECIAL
- PROVISION AS APPROVED BY THE TOWN OF ADDISON, TX. THE CONTRACTOR SHALL PROVIDE CONSTRUCTION STAKING FOR ALL STORM SEWER LINES AND
- 9. EMBEDMENT FOR ALL ONSITE SEWER LINES, PUBLIC OR PRIVATE, SHALL BE PER TOWN OF ADDISON, TX STANDARD DETAILS.
- 10. REFER TO TNRCC/TCEQ DESIGN GUIDELINES (CHAPTER 290) FOR ALL UTILITY CROSSINGS

EXPENSE.

THE CONTRACTOR SHALL MAINTAIN ADEQUATE SITE DRAINAGE DURING ALL PHASES OF CONSTRUCTION. THE CONTRACTOR SHALL USE SILT FENCES (OR OTHER METHODS APPROVED BY THE ENGINEER AND TOWN) AS REQUIRED TO PREVENT SILT AND CONSTRUCTION DEBRIS FROM FLOWING ONTO ADJACENT PROPERTIES. CONTRACTOR SHALL COMPLY WITH ALL APPLICABLE FEDERAL, STATE, OR LOCAL EROSION, CONSERVATION, AND SILTATION REQUIREMENTS. CONTRACTOR SHALL REMOVE ALL TEMPORARY EROSION CONTROL DEVICES UPON COMPLETION OF PERMANENT DRAINAGE FACILITIES AND THE ESTABLISHMENT OF A STAND OF GRASS OR OTHER GROWTH TO PREVENT EROSION. CONTRACTOR IS RESPONSIBLE FOR FILING N.O.I. AND N.O.T. WITH THE TCEQ. CONTRACTOR SOLELY RESPONSIBLE FOR ALL MANDATED SWPPP RECORD KEEPING AND

ALL EXCAVATING IS UNCLASSIFIED AND SHALL INCLUDE ALL MATERIALS ENCOUNTERED. UNUSABLE EXCAVATED MATERIAL AND ALL WASTE RESULTING FROM SITE CLEARING AND GRUBBING SHALL BE DISPOSED OF OFF SITE BY THE GRADING CONTRACTOR AT HIS EXPENSE.

BEFORE ANY EARTHWORK IS PERFORMED, THE CONTRACTOR SHALL STAKE OUT AND MARK THE LIMITS OF PAVEMENT AND OTHER ITEMS ESTABLISHED BY THE PLANS. THE CONTRACTOR SHALL PROVIDE ALL NECESSARY ENGINEERING AND SURVEYING FOR LINE AND GRADE CONTROL POINTS RELATED TO EARTHWORK

6. THE CONTRACTOR SHALL SALVAGE AND PROTECT ALL EXISTING POWER POLES, SIGNS, MANHOLES, TELEPHONE RISERS, WATER VALVES, ETC. THAT ARE TO REMAIN OR BE RELOCATED DURING ALL CONSTRUCTION PHASES

GRADING CONTRACTOR SHALL COORDINATE WITH THE UTILITY COMPANIES FOR ANY REQUIRED UTILITY ADJUSTMENTS AND/OR RELOCATIONS.

TESTING OF MATERIALS REQUIRED FOR THE CONSTRUCTION OF THE PAVING IMPROVEMENTS SHALL BE PERFORMED BY AN APPROVED AGENCY FOR TESTING MATERIALS. THE NOMINATION OF THE TESTING LABORATORY AND THE PAYMENTS FOR SUCH TESTING SERVICES SHALL BE MADE BY THE CONTRACTOR. THE OWNER SHALL APPROVE THE LABORATORY NOMINATED TO DO THE TESTING OF MATERIALS. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO SHOW BY STANDARD TESTING PROCEDURES THAT THE WORK CONSTRUCTED DOES MEET THE REQUIREMENTS OF THE TOWN'S SPECIFICATIONS AND THESE PLANS.

10. PROPOSED CONTOURS ARE APPROXIMATE. PROPOSED SPOT ELEVATIONS AND DESIGNATED GRADIENT ARE TO BE USED IN CASE OF DISCREPANCY.

11. REFER TO SITE PLAN AND FINAL PLAT FOR HORIZONTAL DIMENSIONS.

CONSTRUCTION

## PAVING

3. CONTRACTOR SHALL FURNISH AND INSTALL ALL PAVEMENT MARKINGS FOR FIRE LANES, PARKING STALLS, HANDICAPPED PARKING SYMBOLS, AND MISCELLANEOUS STRIPING WITHIN PARKING LOT AND AROUND BUILDING AS SHOWN ON THE PLANS. ALL PAINT FOR PAVEMENT MARKINGS SHALL ADHERE TO TOWN OF ADDISON STANDARDS.

4. REFER TO GEOTECHNICAL REPORT FOR PAVING JOINT LAYOUT INFORMATION.

5. REFER TO GEOTECHNICAL REPORT FOR REINFORCEMENT STEEL.

6. FIRE LANES SHALL BE DESIGNATED BY CONTINUOUS PAINTED LINES SIX (6) INCHES IN WIDTH ON EACH SIDE OF THE FIRE LANE STARTING AT THE ENTRANCE FROM THE STREET AND TO BE CONTINUED TO THE EXIT. SUCH LINES SHALL BE BRIGHT RED IN COLOR.

7. FIRE LANES ADJACENT TO CURBS SHALL BE OUTLINED BY A SIX (6) INCH WIDE RED STRIP WITH FOUR (4) INCH LETTERS PAINTED BRIGHT WHITE IN COLOR ALONG THE CURB'S GUTTER LINE.

OTHER PAVING DETAILS.

AND FOR FLATWORK.

12. SIDEWALKS ADJACENT TO CURB SHALL BE CONNECTED TO BACK OF CURB USING LONGITUDINAL BUTT JOINT PER TOWN OF ADDISON STANDARD DETAIL.

13. UNLESS THE PLANS SPECIFICALLY DICTATE TO THE CONTRARY, ON-SITE AND OTHER DIRECTIONAL SIGNS SHALL BE LOCATED OUT OF THE PEDESTRIAN AND AUTOMOBILE ROUTES AND SHALL BE LOCATED BETWEEN THREE TO FIVE FEET BEHIND THE NEAREST BACK OF CURB. SIGN HEIGHT, LOCATION, AND STRUCTURE SHALL BE SUCH THAT THE SIGNS POSE NO THREAT TO PUBLIC SAFETY.

COMMENCES.

UNLESS OTHERWISE NOTED, PROPOSED CONTOURS AND SPOT ELEVATIONS SHOWN IN PAVED AREAS REFLECT TOP OF PAVEMENT SURFACE. ADD .50' TO PAVING GRADE FOR TOP OF CURB GRADE. THE LIMITS OF EARTHWORK IN PAVED AREAS IS THE BOTTOM OF PAVEMENT.

THE CONTRACTOR SHALL FIELD VERIFY THE HORIZONTAL AND VERTICAL LOCATIONS OF ALL EXISTING UTILITIES PRIOR TO START OF CONSTRUCTION AND SHALL NOTIFY THE CONSTRUCTION MANAGER AND ENGINEER OF ANY CONFLICTS DISCOVERED. CONTRACTOR IS RESPONSIBLE FOR PROTECTING EXISTING UTILITIES (SHOWN OR NOT SHOWN) WITHIN SCOPE OF CONSTRUCTION. IF ANY EXISTING UTILITIES ARE DAMAGED, THE CONTRACTOR SHALL REPLACE THEM AT HIS OWN

9. CONTRACTOR SHALL CALL 1-800-DIG-TESS AT LEAST 72 HOURS PRIOR TO COMMENCING CONSTRUCTION FOR FIELD LOCATIONS OF UTILITIES IN THE VICINITY OF THE SITE.

12. CONTRACTOR IS RESPONSIBLE FOR OBTAINING ALL PERMITS AND APPROVALS PRIOR TO

13. NO TREE SHALL BE REMOVED OR DAMAGED WITHOUT PRIOR AUTHORIZATION OF THE OWNER OR OWNER'S REPRESENTATIVE. EXISTING TREES SHALL BE PRESERVED WHENEVER POSSIBLE.

14. CONTRACTOR SHALL VERIFY HORIZONTAL AND VERTICAL CONTROL PRIOR TO COMMENCING CONSTRUCTION AND SHALL NOTIFY ENGINEER OF ANY DISCREPANCIES BEFORE CONSTRUCTION

AFTER PLACEMENT OF SUBGRADE AND PRIOR TO PLACEMENT OF PAVEMENT, CONTRACTOR SHALL TEST AND OBSERVE PAVEMENT AREAS FOR EVIDENCE OF PONDING. ALL AREAS SHALL ADEQUATELY DRAIN TOWARDS THE INTENDED STRUCTURE TO CONVEY STORM RUNOFF. CONTRACTOR SHALL IMMEDIATELY NOTIFY OWNER AND ENGINEER IF ANY DISCREPANCIES ARE DISCOVERED.

1. BARRIER FREE RAMPS SHALL BE CONSTRUCTED PER PROWAG (2011) STANDARDS

2. ALL SIGNS, PAVEMENT MARKINGS, AND OTHER TRAFFIC CONTROL DEVICES SHALL CONFORM TO THE MOST RECENT ADDITION OF THE "TEXAS MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES".

8. THE WORDS "FIRE LANE" AND "NO PARKING" SHALL BE STENCILED IN FOUR (4) INCH HIGH WHITE LETTERS ALTERNATELY EVERY FIFTEEN (15) FEET ALONG THE RED FIRE LANE STRIPES.

ALL HANDICAP RAMPING, STRIPING, AND PAVEMENT MARKINGS SHALL CONFORM TO THE AMERICANS WITH DISABILITIES ACT OF 1990 AND PROWAG (2011).

REFERENCE PROWAG OR TXDOT STANDARD CONSTRUCTION DETAILS FOR HANDICAP RAMP AND

11. REFERENCE LANDSCAPE PLANS FOR LOCATION AND TYPE OF HANDICAP RAMPS TO BE PROVIDED

UNLESS THE PLANS SPECIFICALLY DICTATE TO THE CONTRARY, ON-SITE AND OTHER DIRECTIONAL SIGNS SHALL BE ORIENTED SO THEY ARE READILY VISIBLE TO THE ONCOMING TRAFFIC FOR WHICH THEY ARE INTENDED, FIELD ADJUSTMENTS OF LOCATION AND ORIENTATION OF THE SIGNS ARE TO BE MADE TO ACCOMPLISH THIS.

15. CONTRACTOR RESPONSIBLE FOR INSTALLING NECESSARY CONDUIT FOR LIGHTING, IRRIGATION, ETC. PRIOR TO PLACEMENT OF PAVEMENT. ALL CONSTRUCTION DOCUMENTS (CIVIL, MEP, LANDSCAPE, AND ARCHITECT) SHALL BE CONSULTED.

16. BEFORE PLACING PAVEMENT, CONTRACTOR SHALL VERIFY THAT SUITABLE HANDICAPPED ROUTES (PER A.D.A. & T.A.S) EXIST TO AND FROM EVERY DOOR. IN NO CASE SHALL HANDICAP RAMP SLOPES EXCEED 1 VERTICAL TO 12 HORIZONTAL. IN NO CASE SHALL SIDEWALK CROSS SLOPES EXCEED 2.0 PERCENT. IN NO CASE SHALL LONGITUDINAL SIDEWALK SLOPES EXCEED 5.0 PERCENT. CONTRACTOR SHALL CONTACT ENGINEER PRIOR TO PAVING IF ANY EXCESSIVE SLOPES ARE ENCOUNTERED: NO CONTRACTOR CHANGE ORDERS WILL BE ACCEPTED FOR A.D.A. AND T.A.S. COMPLIANCE ISSUES.

17. REFER TO GEOTECHNICAL REPORT FOR SOIL COMPACTION SPECIFICATION.

### WATER AND SANITARY

- 1. IN THE EVENT AN ITEM IS NOT COVERED BY THIS MANUAL, THEN THE N.C.T.C.O.G. GENERAL SPECIFICATIONS COVERING SUCH ITEMS SHALL APPLY.
- WATER MAINS SHALL BE PVC C-900 DR 18, EXCEPT WHEN OTHERWISE NOTED.
- 3. SEWER PIPE SHALL BE MINIMUM SDR 35 PVC, ULTRA RIB PVC SDR 26, OR DR 21 HDPE.
- 4. WATER MAINS SHALL HAVE THE FOLLOWING MINIMUM COVER BELOW STREET GRADES: 8" AND SMALLER

8" TO 16" 5.0' **GREATER THAN 16**" 6.0'

5. PLASTIC TAPE FOR UTILITY SERVICES SHALL BE ATTACHED TO THE ENDS OF ALL WATER AND SEWER SERVICE LINES AND EXTEND ABOVE GROUND LEVEL. EXCEPT IN PIPE BURST SECTIONS, THE TAPE SHALL MEET THE FOLLOWING SPECIFICATION:

A. "ALLEN MARKING TAPE" OR APPROVED EQUAL B. ROLL MARKED CONTINUOUSLY, "CAUTION WATER LINE" OR "CAUTION SEWER LINE". C. SIX (6) INCHES IN WIDTH.

D. GREEN TAPE FOR SEWER SERVICES. E. BLUE TAPE FOR WATER SERVICES.

- VIDEO TAPE OF SEWER LINE TO BE PROVIDED TO TOWN OF ADDISON DIRECTOR OF INFRASTRUCTURE, BY THE CONTRACTOR.
- 7. FIRE HYDRANTS SHALL BE LOCATED BETWEEN 3' AND 6' FROM THE BACK OF CURB.
- 8. IT IS THE CONTRACTORS RESPONSIBILITY TO FIELD VERIFY EXACT LOCATIONS OF EXISTING PUBLIC AND PRIVATE UTILITIES AND SERVICES PRIOR TO COMMENCING CONSTRUCTION. THE CONTRACTOR SHALL CALL 1-800-DIG-TESS FOR FIELD LOCATION OF EXISTING UTILITIES. CALL AT LEAST 48 HOURS BEFORE LOCATIONS ARE NEEDED. NOTE THAT THE DIG TESS SERVICE DOES NOT LOCATE ALL UTILITIES, ONLY THOSE REGISTERED WITH THE SERVICE.
- 9. REFER TO OFFSITE SEWER PLANS FOR DETERMINING PROPOSED MANHOLE RIM ELEVATIONS.
- 10. LOCATIONS AND SIZES OF EXISTING PUBLIC AND PRIVATE UTILITIES SHOWN ON THESE PLANS ARE FROM TOWN AND UTILITY COMPANY RECORDS ONLY. THE CONTRACTOR IS SOLELY RESPONSIBLE FOR LOCATING ALL UTILITIES AND FOR DAMAGES RESULTING FROM FAILURE TO DO SO.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROVIDING "RECORD" PLANS TO THE ENGINEER AND TO THE TOWN OF ADDISON SHOWING THE LOCATION OF WATER AND SEWER SERVICES AND ANY DEVIATIONS FROM PLANS MADE DURING CONSTRUCTION.
- THE CONTRACTOR SHALL VERIFY ALL DIMENSIONS SHOWN, COORDINATING THE HORIZONTAL AND 12. VERTICAL LOCATION OF ALL UTILITY SERVICES ENTERING THE BUILDING AND/OR CROSSING OTHER UTILITIES.
- 13. ALL WATER AND SANITARY SEWER SERVICES SHALL TERMINATE FIVE (5) FEET OUTSIDE THE BUILDING, UNLESS NOTED OTHERWISE.
- THE SITE UTILITY CONTRACTOR SHALL PROVIDE ALL MATERIALS AND APPURTENANCES NECESSARY FOR COMPLETE INSTALLATION OF THE UTILITIES. ALL PUBLIC PIPE, STRUCTURES, AND FITTINGS SHALL BE INSPECTED BY THE TOWN INSPECTOR PRIOR TO BEING COVERED. THE INSPECTOR MUST ALSO BE PRESENT DURING DISINFECTION AND PRESSURE TESTING OF ALL MAINS. THE CONTRACTOR'S BID PRICE SHALL INCLUDE ALL INSPECTION FEES.
- 15. THE CONTRACTOR SHALL BE SOLELY RESPONSIBLE FOR SITE TRENCH SAFETY DURING ALL PHASES OF CONSTRUCTION. THE CONTRACTOR SHALL SUBMIT A TRENCH EXCAVATION PROTECTION PLAN, SEALED BY A GEOTECHNICAL ENGINEER REGISTERED IN THE STATE OF TEXAS, FOR ALL TRENCHES DEEPER THAN FIVE (5) FEET
- 16. ALL MATERIALS AND WORKMANSHIP SHALL CONFORM TO THE STANDARD SPECIFICATIONS FOR PUBLIC WORKS CONSTRUCTION FOR NORTH CENTRAL TEXAS, LATEST EDITION, AND ANY SPECIAL PROVISION AS APPROVED BY THE TOWN OF ADDISON, TX.
- 17. THE CONTRACTOR SHALL PROVIDE CONSTRUCTION STAKING FOR ALL WATER AND SANITARY SEWER LINES AND OTHER UTILITIES.

## FLOW DIVERSION MANAGEMENT NOTES

- 1. THE CONTRACTOR SHALL PROVIDE AND MAINTAIN ADEQUATE BYPASS EQUIPMENT, FORCE MAINS, AND OTHER NECESSARY APPURTENANCES IN ORDER TO MAINTAIN RELIABLE SANITARY SEWER SERVICE IN ALL SANITARY SEWER LINES AS REQUIRED FOR CONSTRUCTION.
- 2. THE CONTRACTOR SHALL HAVE BACKUP PUMP(S), FORCE MAIN(S), AND APPURTENANCES READY TO DEPLOY IMMEDIATELY. APPURTENANCES AND DISCHARGE POINTS SHALL BE APPROVED BY THE
- 3. ANY SPILLAGE, BACKUPS AND/OR OVERFLOWS, ETC. AS THE RESULT OF INADEQUATE EQUIPMENT ARE THE SOLE RESPONSIBILITY OF THE CONTRACTOR.
- 4. THE CONTRACTOR SHALL DEMONSTRATE THAT THE PUMPING SYSTEM IS IN GOOD WORKING ORDER AND IS SUFFICIENTLY SIZED TO SUCCESSFULLY HANDLE FLOWS BY PERFORMING A TEST RUN FOR A PERIOD OF 24 HOURS PRIOR TO BEGINNING THE WORK. TESTING IS NOT TO BE SCHEDULED DURING WEEKEND HOURS.
- 5. THE CONTRACTOR SHALL BE REQUIRED TO HAVE ALL MATERIALS, EQUIPMENT AND LABOR NECESSARY TO COMPLETE THE REPAIR OR REPLACEMENT ON THE JOB SITE PRIOR TO ISOLATING THE SEWER MANHOLE OR LINE SEGMENT AND BEGINNING FLOW DIVERSION OPERATIONS.
- 6. THE CONTRACTOR WILL BE REQUIRED TO PROVIDE IN WRITING A SEQUENCE OF FLOW DIVERSION FOR REVIEW AND APPROVAL BY THE INSPECTOR. THE CONTRACTOR SHALL ALSO PROVIDE THE INSPECTOR A SKETCH SHOWING THE LOCATION OF FLOW DIVERSION EQUIPMENT FOR EACH LINE SEGMENT(S) AROUND WHICH FLOWS ARE DIVERTED.
- 7. ONCE NEW SEWER COLLECTION SYSTEM HAS BEEN COMPLETED. TESTED AND ACCEPTED. THE CONTRACTOR SHALL CEASE FLOW DIVERSION OPERATIONS AND RETURN FLOWS TO THE NEW AND/OR EXISTING SEWER WHEN DIRECTED BY THE INSPECTOR.
- 8. ALL PIPING(S), JOINTS AND ACCESSORIES OF THE FLOW DIVERSION SYSTEM SHALL BE DESIGNED TO WITHSTAND AT LEAST TWICE THE MAXIMUM SYSTEM PRESSURE, OR A MINIMUM OF 50 PSI WHICHEVER IS GREATER.
- 9. DURING FLOW DIVERSION, NO SEWAGE SHALL BE LEAKED, DUMPED, OR SPILLED IN OR UNTO, ANY AREA OUTSIDE OF THE EXISTING SANITARY SEWER SYSTEM AND TEMPORARY BY-PASS SYSTEM. WHEN FLOW DIVERSION OPERATIONS ARE COMPLETE, ALL FLOW DIVERSION SHALL BE DRAINED INTO THE SANITARY SEWER PRIOR TO DISASSEMBLY.
- 10. THE CONTRACTOR SHALL PLUG OFF AND PUMP DOWN THE SEWER MANHOLE OR LINE SEGMENT IN THE IMMEDIATE WORK AREA AND SHALL MAINTAIN THE SANITARY SEWER SYSTEM SO THAT SURCHARGING DOES NOT OCCUR. WHERE WORK REQUIRES THE LINE TO BE BLOCKED BEYOND WORKING HOURS, CONTRACTOR SHALL OPERATE FLOW DIVERSION AND MAN THE SYSTEM 24 HOURS
- 11. THE CONTRACTOR SHALL ENSURE THAT NO DAMAGE WILL BE CAUSED TO PRIVATE PROPERTY AS A RESULT OF FLOW DIVERSION OPERATIONS. INGRESS AND EGRESS TO ADJACENT PROPERTIES SHALL BE MAINTAINED AT ALL TIMES. RAMPS, STEEL PLATES OR OTHER METHODS SHALL BE DEPLOYED BY THE CONTRACTOR TO FACILITATE TRAFFIC OVER SURFACE PIPING. HIGH TRAFFIC COMMERCIAL PROPERTIES MAY REQUIRE ALTERNATE METHODS.
- 12. THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE INSPECTOR SHOULD A SURCHARGE OCCUR THAT RESULTS IN AN OVERFLOW OF SEWAGE. IF THE CONTRACTOR IS UNABLE TO REMEDY THE SITUATION, THEN HE SHOULD SUSPEND OR TERMINATE THE WORK UNTIL SUCH TIME AS THE OVERFLOWS HAVE BEEN CONTROLLED. SHOULD SUCH SURCHARGE DAMAGE THE MATERIALS AND/OR EQUIPMENT THAT ARE USED ON THE JOB AND/OR ADJACENT PROPERTY, IT SHALL BE CORRECTED AT NO ADDITIONAL COST TO THE OWNER.
- 13. IN THE EVENT THAT SEWAGE ACCIDENTALLY DRAINS INTO THE DRAINAGE SYSTEM OR STREET, THE CONTRACTOR SHALL IMMEDIATELY STOP THE OVERFLOW, NOTIFY THE TOWN, AND TAKE THE NECESSARY ACTION TO CLEAN UP AND DISINFECT THE SPILLAGE TO THE SATISFACTION OF THE TOWN/TCEQ. IF SEWAGE IS SPILLED ONTO PUBLIC OR PRIVATE PROPERTY, THE CONTRACTOR SHALL WASH DOWN, CLEAN UP AND DISINFECT THE SPILLAGE TO THE SATISFACTION OF THE INSPECTOR.
- 14. IN THE EVENT OF A BYPASS PIPING BREAK, THE CONTRACTOR IS RESPONSIBLE FOR CLEANING THE FULL EXTENT OF THE SPILL, INCLUDING ANY STORM DRAIN TO THE END OF THE STORM WAYS RECEIVING THE SPILL.
- 15. THE CONTRACTOR SHALL NOT INTENTIONALLY DAMAGE OR REMOVE PORTIONS SANITARY SEWER STRUCTURES FOR THE PURPOSE OF INSTALLING THE FLOW DIVERSION SYSTEM WITHOUT SPECIFIC APPROVAL FROM THE INSPECTOR. IF A STRUCTURE IS DAMAGED, IT SHALL BE RECONSTRUCTED OR REPLACED TO THE SATISFACTION OF THE INSPECTOR AT NO ADDITIONAL COST TO THE OWNER.
- 16. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ANY AND ALL DAMAGE THAT RESULTS DIRECTLY OR INDIRECTLY FROM THE INTERFERENCE OF STORM WATER RUNOFF TO FLOW DIVERSION EQUIPMENT, PIPING AND/OR APPURTENANCES. THE CONTRACTOR SHALL ESTABLISH ADEQUATE FLOW DIVERSION AS REQUIRED REGARDLESS OF THE FLOW CONDITIONS.
- 17. CONTRACTOR WILL BE REQUIRED TO PROVIDE TEMPORARY VEHICULAR ACCESS ACROSS ANY 3YPASS PIPING CROSSING PUBLIC R.O.W. (NO SEPARATE PAY ITEM)
- 18. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBSERVING WEATHER FORECASTS AND NOTIFYING INSPECTOR OF ANY SEVERE WEATHER POSSIBILITIES DURING CONSTRUCTION. IN THE EVENT OF INCLEMENT WEATHER, THE CONTRACTOR SHALL BE RESPONSIBLE FOR SUSPENDING ALL BYPASS ACTIVITIES, INCLUDING THE COST OF ANY MOBILIZATION, DE-MOBILIZATION, AND RE-MOBILIZATION.



APPROVED FOR CONSTRUCTION

INFRASTRUCTURE & DEVELOPMENT SERVICES

IN APPROVING THESE PLANS, THE TOWN OF ADDISON ASSUMES NO RESPONSIBILITY FOR THEIR ADEQUACY, WHICH REMAINS WITH THE ENGINEER OF RECORD. 16801 WESTGROVE DRIVE, ADDISON, TX 75001 - (972) 450-2871

**REFER TO TNRCC/TCEQ DESIGN** GUIDELINES (CHAPTER 290) FOR ALL UTILITY CROSSINGS.

> GEOTECHNICAL REPORT REPORT NO. G140072 ALPHA TESTING, INC. DALLAS, TEXAS MARCH 18, 2014 CONTACT: DAVID SCHLEDORN, P.E. (972) 620-8911

## BENCHMARKS

BM-1 Town of Addison GPS Control Station "TXDOT-COA-5 located on the corner of a curb inlet, 190' south of the intersection of Edwin Lewis Drive and Quorum Drive, 2' from the west edge of sidewalk, 50.5 feet from the west side of median and 76' from a fire hvdrant. Elevation=630.10

BM-2 Town of Addison GPS Control Station "TXDOT-COA-14", located 630' west of intersection of Addison Road & Westgrove Drive, 15.7' south of back of curb of concrete drive, 5' from street sign and 8' north of back of curb of Westgrove Road. Elevation=642.09

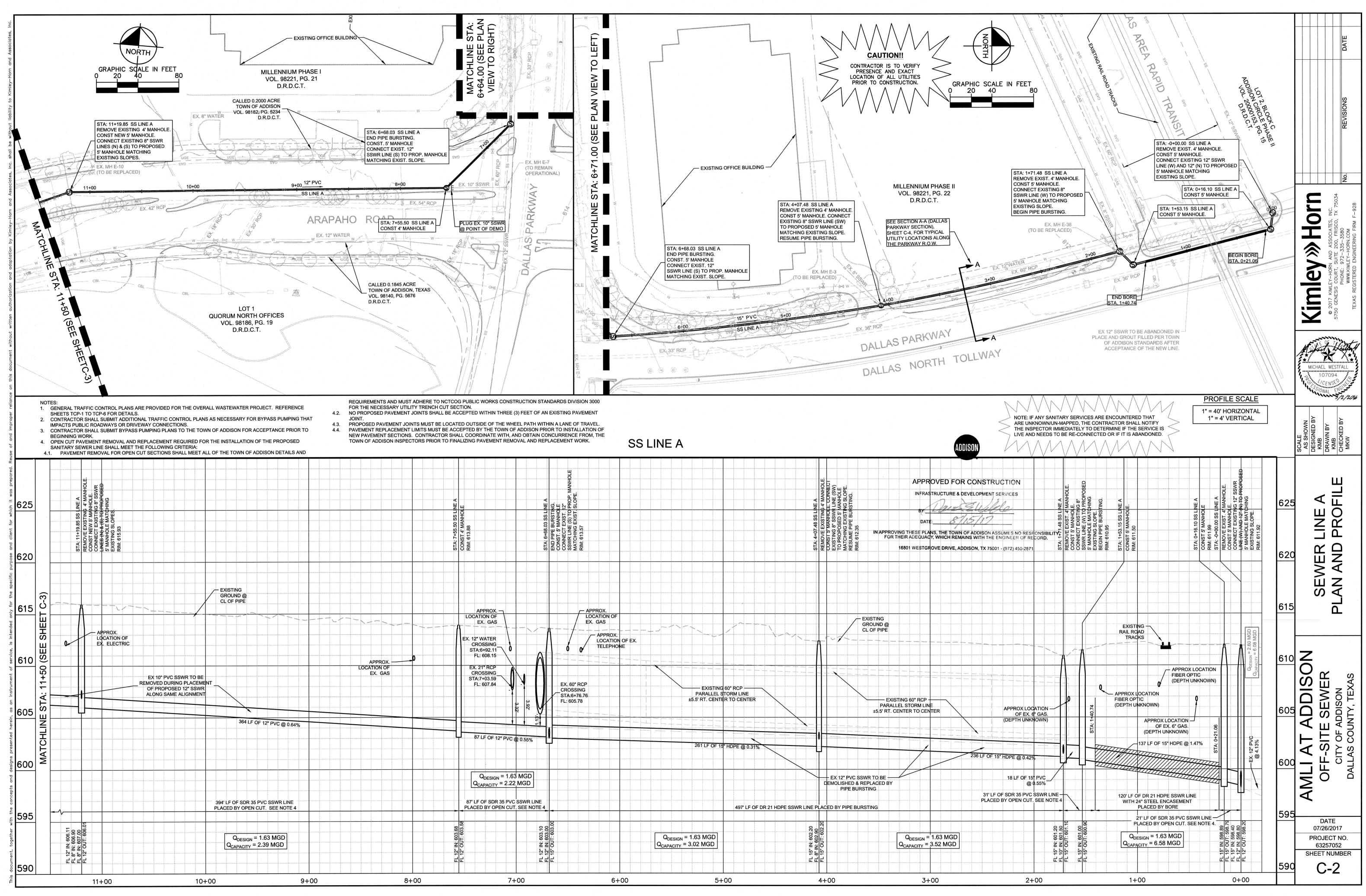
TBM-1 Square cut with "X" set on N.E. corner of drop inlet 545'+/south of south R.O.W of Edwin Lewis Dr. and 8'+/- west of Spectrum Drive back-of-curb. Elevation=630.89

TBM-2 Square cut with "X" set on concrete headwall at trhe southeast corner of Quorum Drive and Edwin Lewis Drive.

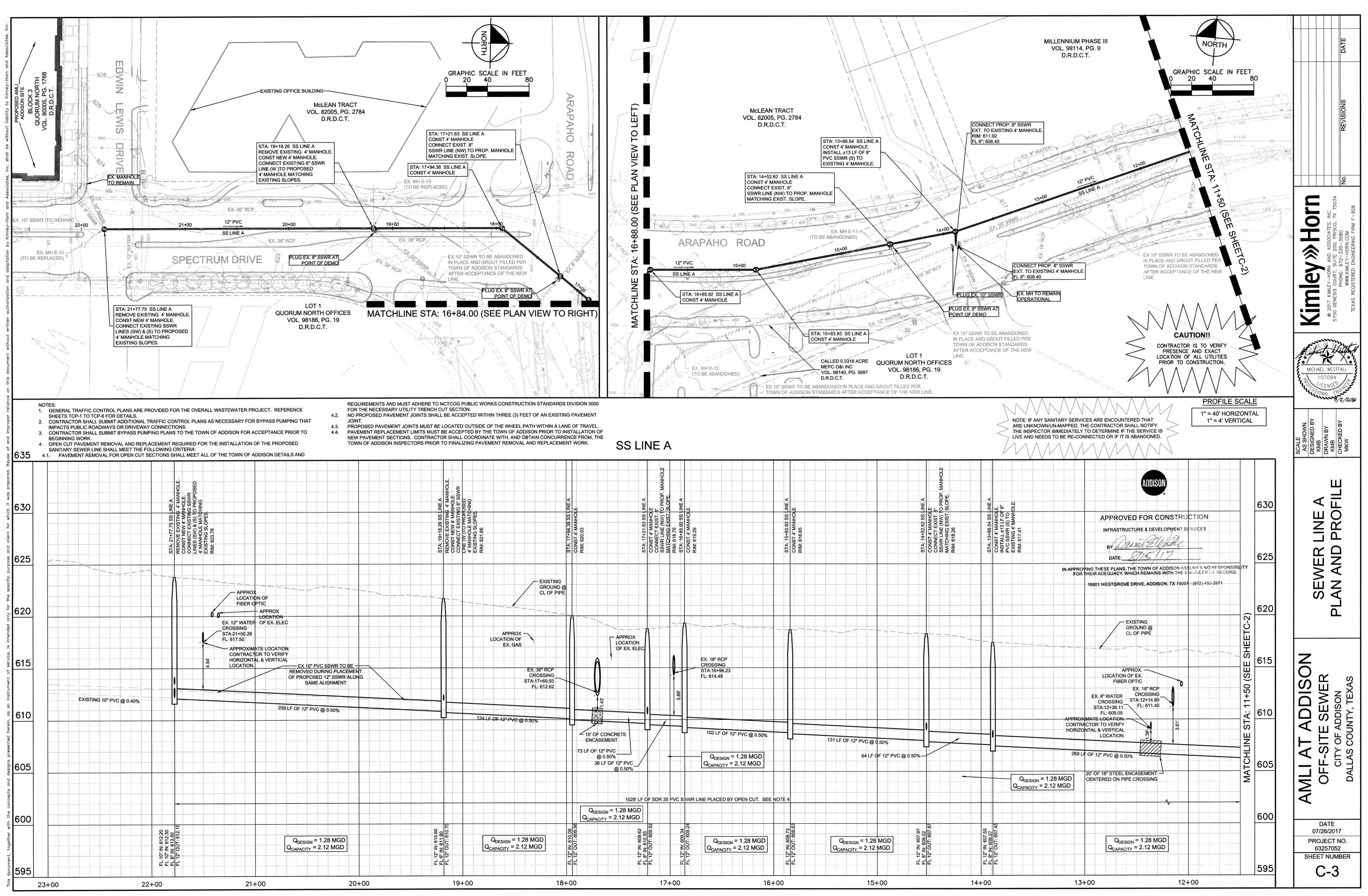
Elevation=631.2

Kimley » Horn	© 2017 KIMLEY-HORN AND ASSOCIATES, INC. 5750 GENESIS COURT, SUITE 200, FRISCO, TX 75034	PHONE: 972–335–3580 No. REVISIONS DATE www.kimleY-Horn.com Texas registered engineering Firm F-928	
PORLI	O7094 CENSE WAL E	CHECKED BY	
	<b>GENERAL NOTES</b>		
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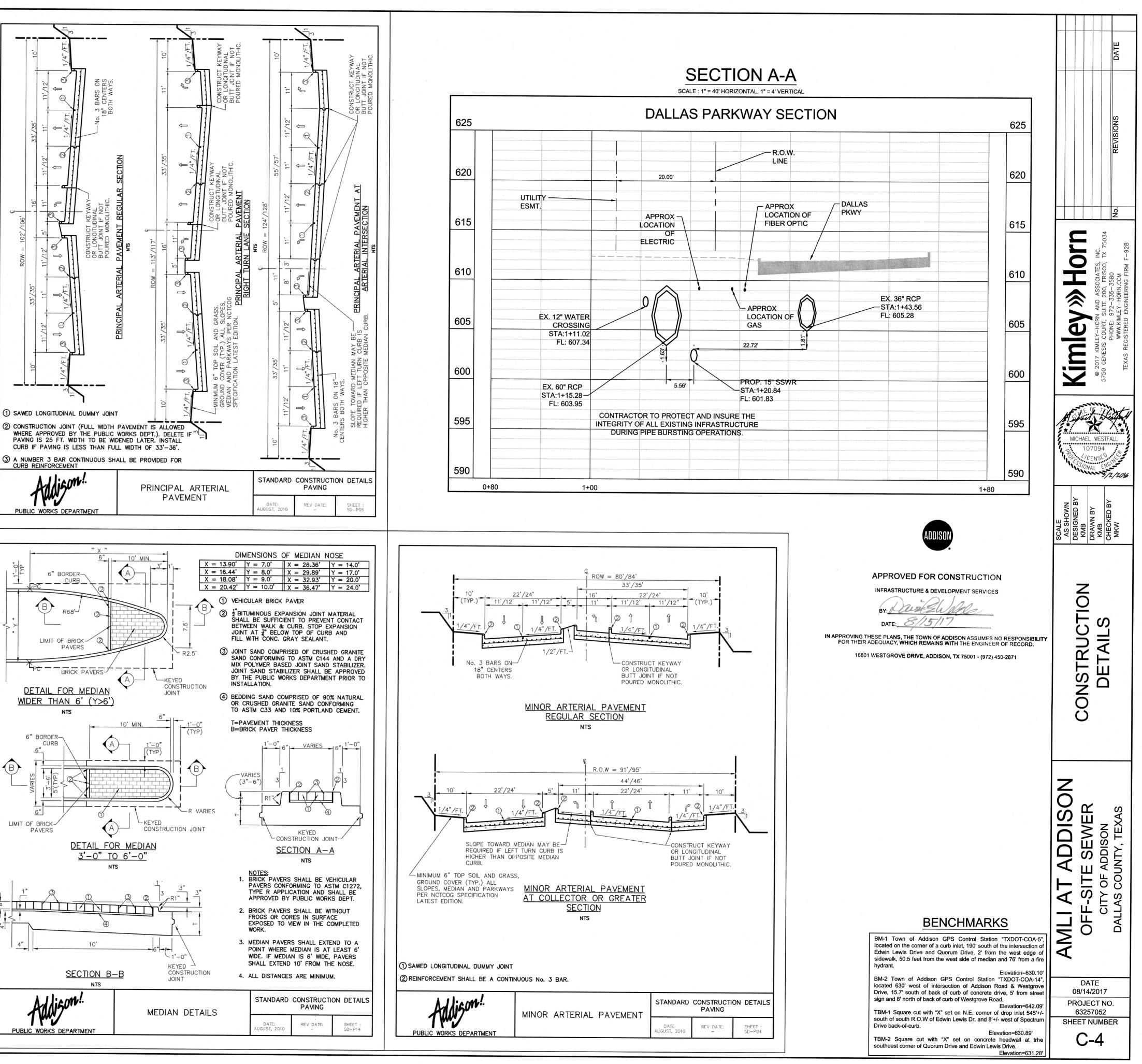


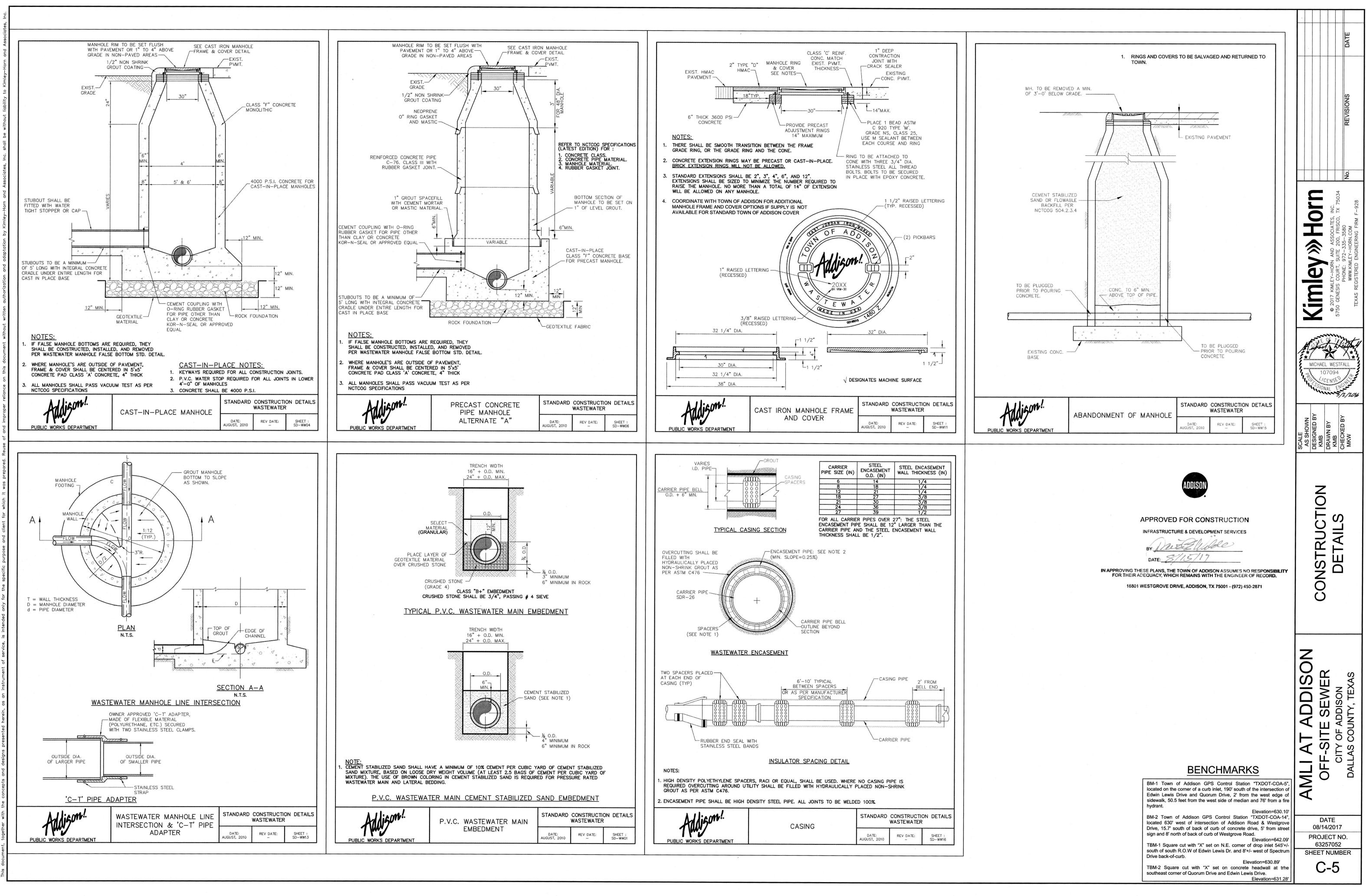
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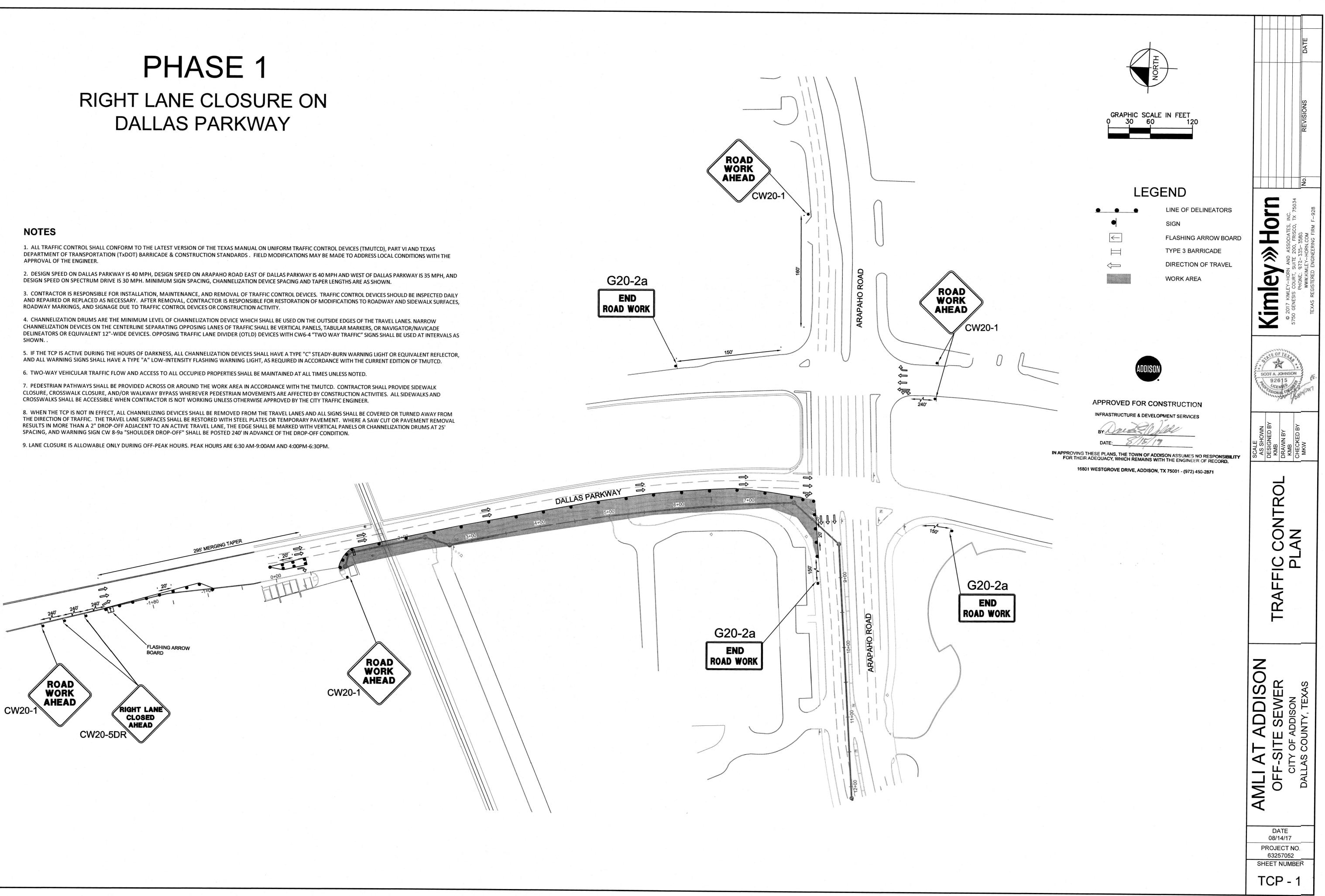
PAVING - GENERAL NOTES GENERAL PAVEMENT THICKNESS IS AS SHOWN IN ITEM 7. SUBGRADE DESIGN SHALL CONFORM TO TOWN OF ADDISON PUBLIC WORKS REQUIREMENTS IN ITEM 3, AND SHALL EXTEND 12" MIN. BEHIND THE BACK OF 2. REINFORCED CONCRETE PAVEMENT: A. CONCRETE STRENGTH SHALL BE AS SHOWN IN ITEM 7 (NCTCOG LATEST EDITION). B. ALL CURBS SHALL BE INTEGRAL WITH PAVEMENT AND SHALL BE OF THE SAME STRENGTH AS CONCRETE Q PAVEMENT. C. DETAIL AND ARRANGEMENT OF PAVEMENT JOINTS, ALL TYPES, SHALL BE AS SHOWN ON THE TOWN STANDARD CONSTRUCTION DETAILS. D. BAR LAPS SHALL BE THIRTY DIAMETERS. E. REINFORCING STEEL SHALL BE #3 REBAR (3/8") ON 18" CENTERS FOR 8" OR LESS. #4 FOR 10" OR ABOVE 3. SUBGRADE: SUBGRADE UNDER ALL PAVEMENT SHALL BE 6" THICK AND SHALL BE STABILIZED WITH AT LEAST 30 LBS. PER SQ. YD. HYDRATED LIME, COMPACTED TO A DENSITY NOT LESS THAN 95 PERCENT. LABORATORY TESTS MUST BE SUBMITTED TO THE PUBLIC WORKS DEPARTMENT FOR APPROVAL TO DETERMINE AMOUNT OF LIME REQUIRED. LABORATORY TEST MAY BE WAIVED PROVIDED AT LEAST 36 LBS. OF LIME PER SQ. YD. IS USED. SEE NCTCOG ITEM 301.2 "LIME TREATMENT". FLEXIBLE BASE (CRUSHED STONE/CONCRETE) PER NCTCOG ITEM 301.5 MAY BE SUBSTITUTED FOR LIME TREATMENT WITH THE APPROVAL OF THE TOWN ENGINEER. 4. REBAR SHALL BE SUPPORTED BY BAR CHAIRS OR OTHER DEVICES APPROVED BY TOWN ENGINEER. 5. NO TRAFFIC ON FINISHED SUBGRADE SHALL BE PERMITTED AFTER REINFORCING STEEL IS INSTALLED ABOVE SUBGRAGE. NO TRAFFIC SHALL BE PERMITTED BEFORE OR DURING THE PLACING OF CONCRETE. 6. CROSS SLOPE OF STRAIGHT CROWN STREETS SHALL BE 1/4" PER FOOT UNLESS APPROVED BY THE TOWN ENGINEER. 7. PAVEMENT THICKNESS AND STRENGTHS SHALL BE AS FOLLOWS:  $\Rightarrow$ MAJOR ARTERIAL - 10" CLASS "P1" OR "P2." 0 MINOR ARTERIAL - 8" CLASS "P1" OR "P2." COMMERCIAL/INDUSTRIAL COLLECTOR - 8" CLASS "P1" OR "P2." RESIDENTIAL COLLECTOR - 8" CLASS "P1" OR "P2." RESIDENTIAL LOCAL - 8" CLASS "P1" OR "P2." SIDEWALK AND BFR's-4"-CLASS "A" DRIVE APPROACH-8"-CLASS "P2" ALLEY-6" CLASS "P1" OR "P2." 8. CONCRETE MIX DESIGN SHALL BE AS DEFINED BY NCTCOG 303.3.  $\Rightarrow$ 9. ALL MEDIANS AND PARKWAYS SHALL BE PROVIDED WITH BERMUDA GROUND COVER. 3 10. ONCE A CURB ABUTTING A THOROUGHFARE HAS BEEN SAWCUT AND REMOVED, THE CONTRACTOR MUST REPLACE THE CONCRETE WITH A NEW POUR (I.e. DRIVEWAY) WITHIN 14 CALENDAR DAYS. LIQUIDATED DAMAGES WILL BE ASSESSED AT \$500 PER DAY FOR EACH CALENDAR DAY IN EXCESS OF 14 CALENDAR DAYS, PAYMENT SHALL BE MADE PRIOR TO ACCEPTANCE OR ISSUANCE OF A CERTIFICATE OF OCCUPANCY. 11. ALL SIDEWALKS AND ACCESSIBLE ROUTES SHALL HAVE A MAXIMUM LONGITUDINAL SLOPE OF 5% AND A \_\_\_\_ MAXIMUM CROSS SLOPE OF 2%. 12. ALLEYS AND DRIVEWAYS A. CONCRETE FOR ALLEY RETURNS AND DRIVEWAYS SHALL HAVE A MINIMUM COPRESSIVE STRENGTH AT 28 DAYS IDENTICAL TO THAT SPECIFIED FOR THE STREET PAVEMENT OR BASE WHEN BUILT AS COMPONENTS () SAWED LONGITUDINAL DUMMY JOINT OF A CONCRETE PAVING PROJECT. WHEN BUILT SEPARATELY, THE STRENGTH SHALL BE AS SPECIFIED ON THE CONSTRUCTION PLAN. B. SPACING AND CONSTRUCTION OF JOINTS SHALL CONFORM TO PARABOLIC STREET PAVEMENT. CURB REINFORCEMENT STANDARD CONSTRUCTION DETAILS Aison ! Alicon. PAVING PAVING GENERAL NOTES 1.1 REV DATE: SHEET : SD-P01 PUBLIC WORKS DEPARTMENT PUBLIC WORKS DEPARTMENT 6" BORDER-24" DEFORMED #6 DOWEL BAR <u>X' ×</u> B -----SAWCUT WIDTH 3/8" MIN. R68'--EPOXY A103 OR SILICONE JOINT SEALANT-APPROVED EQUAL. COMPOUND WITH 1/2" INSTALL FULL DEPTH DIA. BACKER ROD PER LIMIT OF BRICK-PER MANUFACTURER'S JOINT DETAIL PAVERS INSTRUCTIONS 4 A 100 s21 A PROPOSED PAVING EXISTING PAVING DETAIL FOR MEDIAN WIDER THAN 6' (Y>6') NTS 4 4 4 4 4 4 A 4 4 4 4 4 6" BORDER-CURB -DOWEL SPACED 12" O.C., 6" OFF TIE BARS **(**B**)** -CONCAVE FINISH (1/4" DEPTH) 6" - SILICONE LIMIT OF BRICK-JOINT PAVERS SEALANT 1. NO. 5 DEFORMED DOWEL BAR MAY BE USED IN 6 INCH PAVEMENT. -BACKER ROD 2. DOWEL BARS SHALL BE DRILLED INTO PAVEMENT HORIZONTALLY BY USE OF A MECHANICAL RIG. 3. DRILLING BY HAND IS NOT ACCEPTABLE, PUSHING DOWEL BARS INTO GREEN CONCRETE 1/4" MAX IS NOT ACCEPTABLE. T=PROPOSED PAVEMENT THICKNESS JOINT DETAIL ET=EXISTING PAVEMENT THICKNESS NTS PAVEMENT REPAIR HEADER NTS STANDARD CONSTRUCTION DETAILS Adjson Allison! PAVING PAVEMENT REPAIR HEADER SHEET : SD-P16 REV DATE: PUBLIC WORKS DEPARTMENT PUBLIC WORKS DEPARTMENT

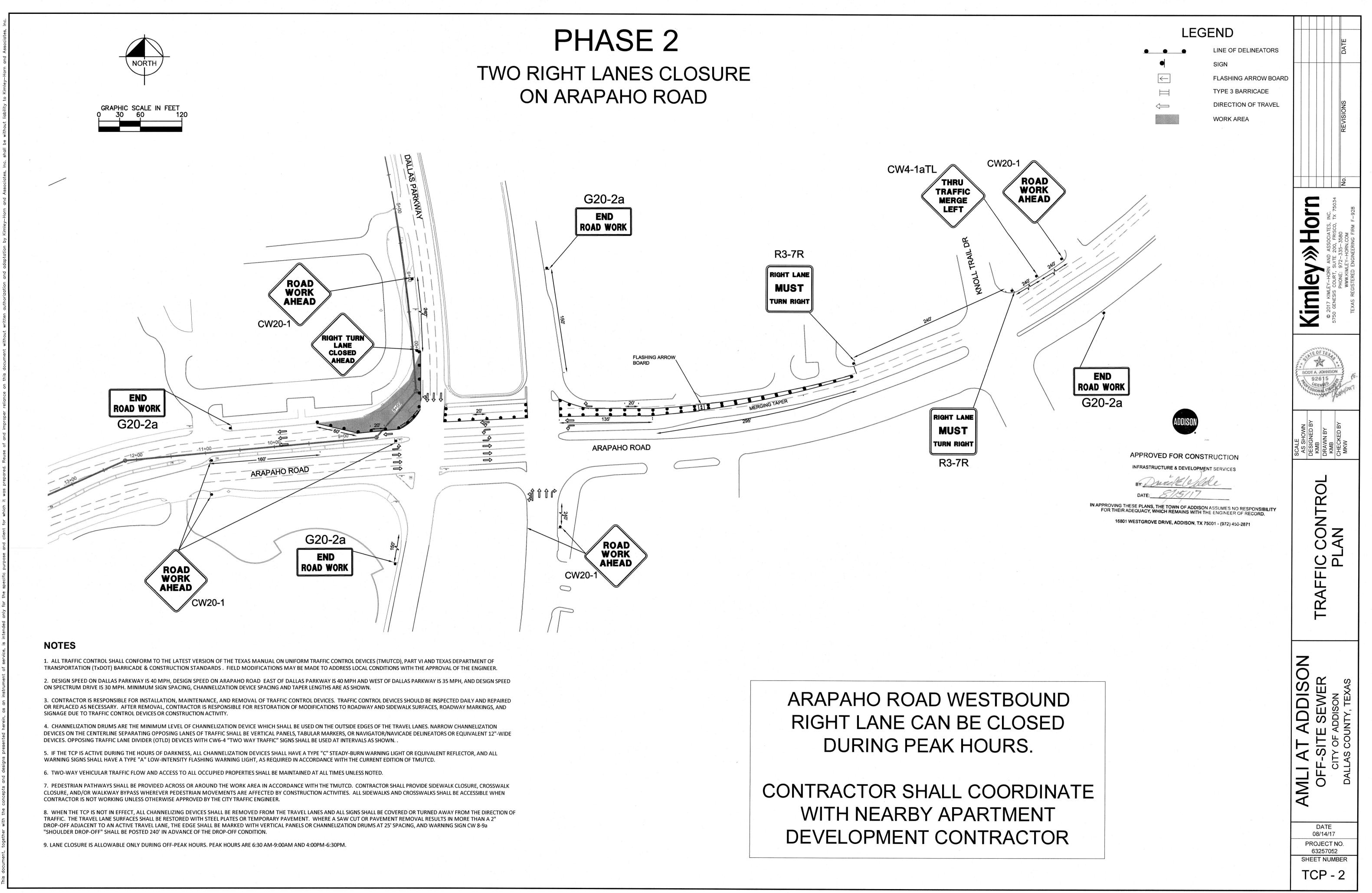
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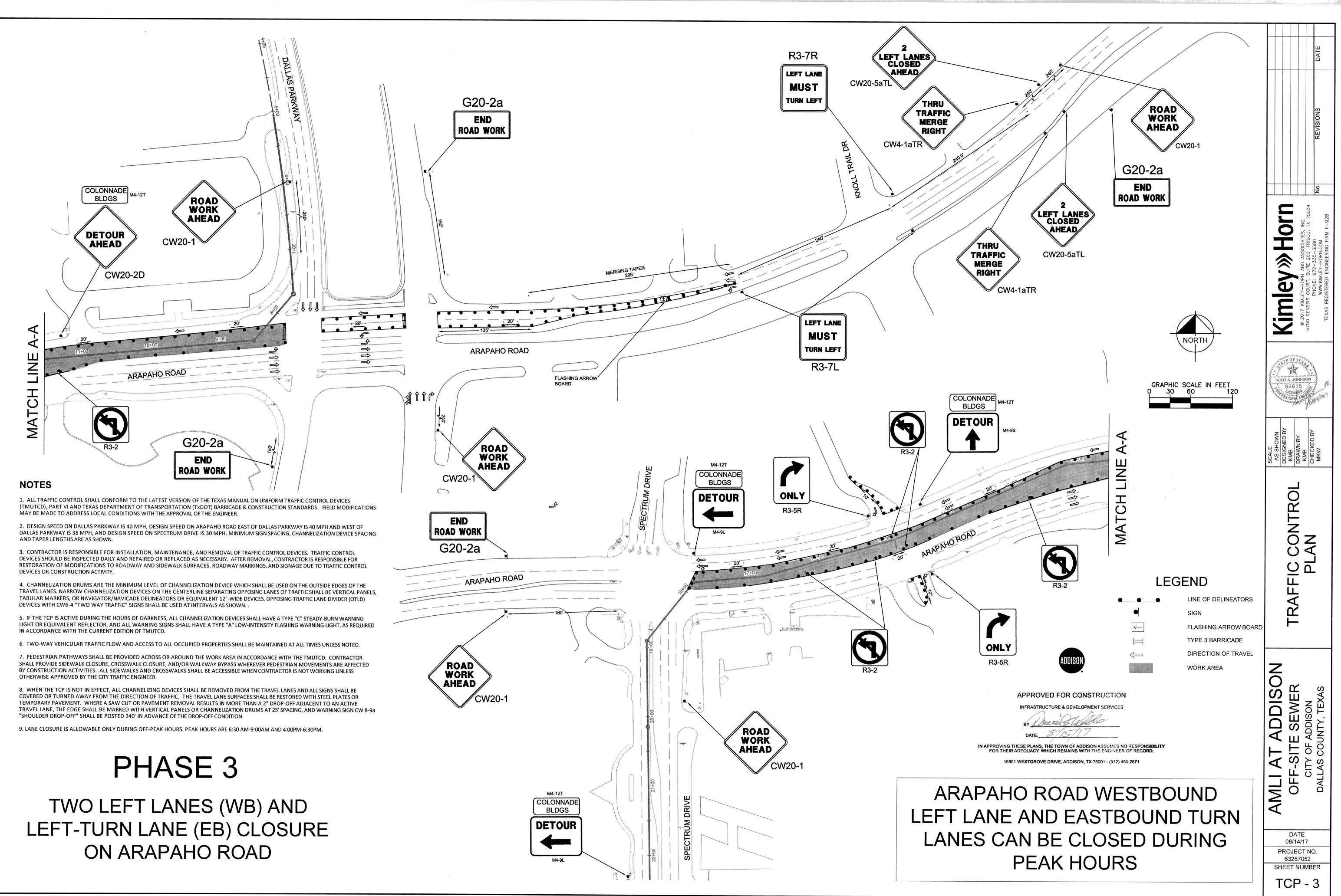


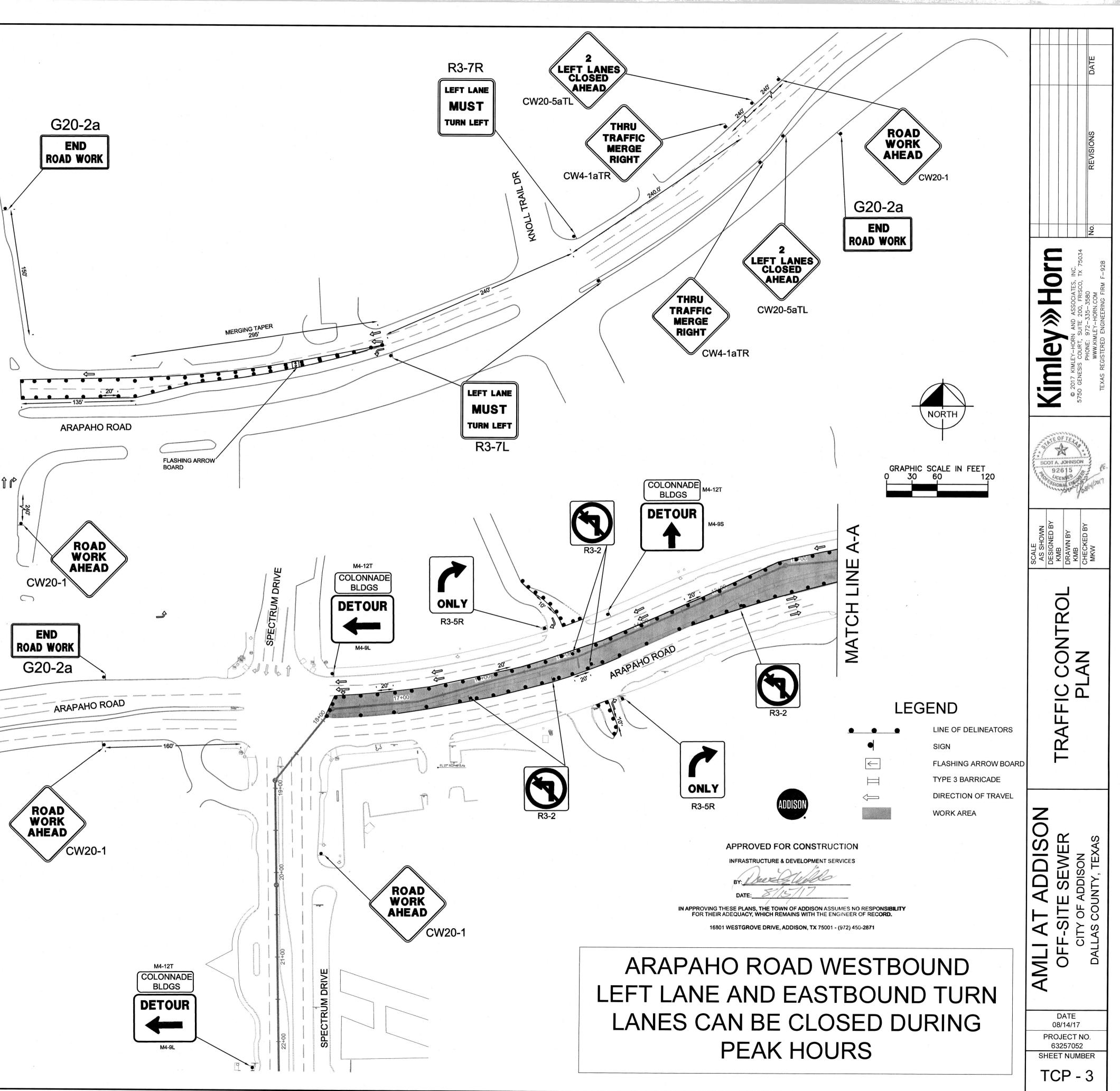


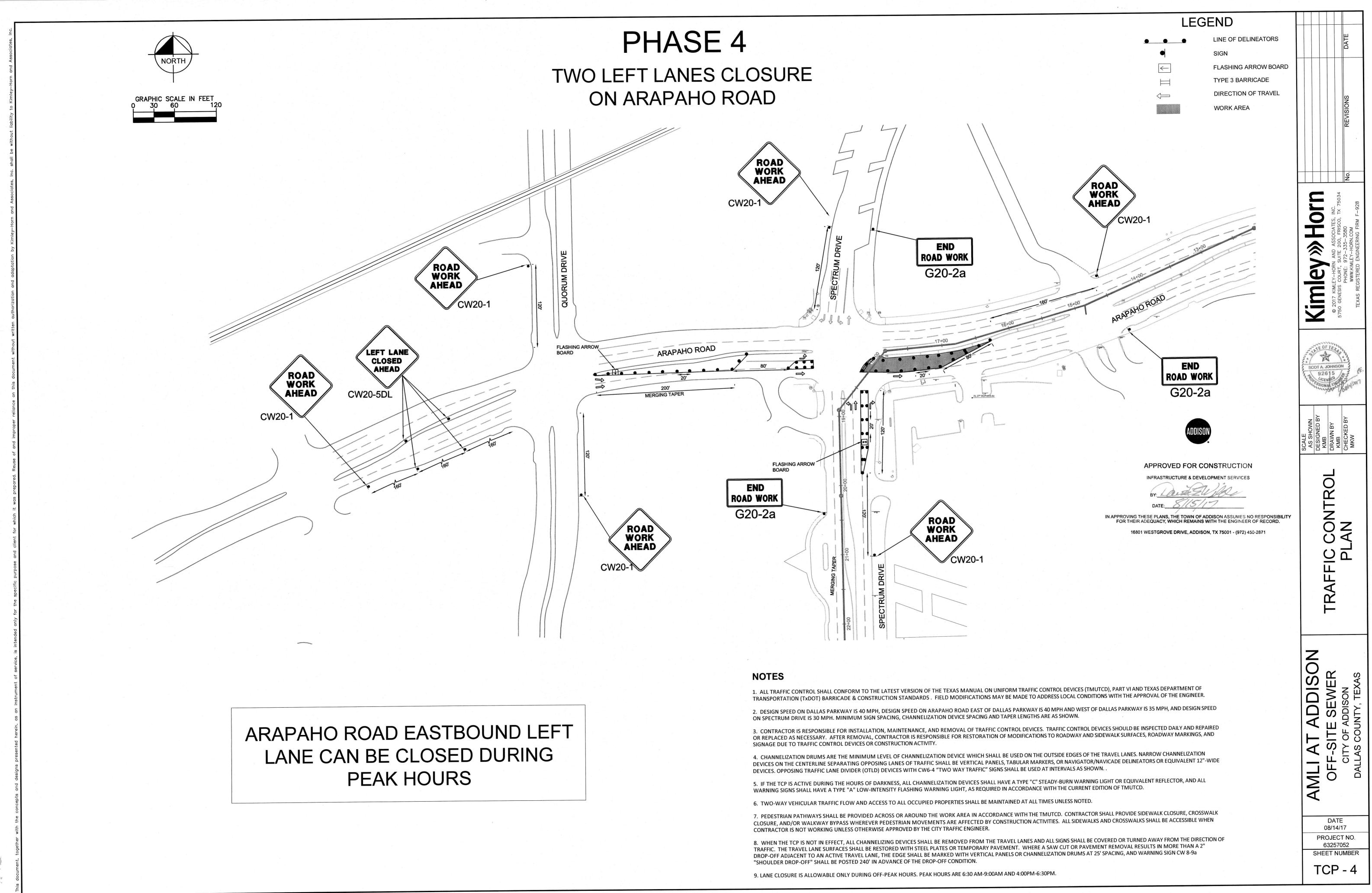
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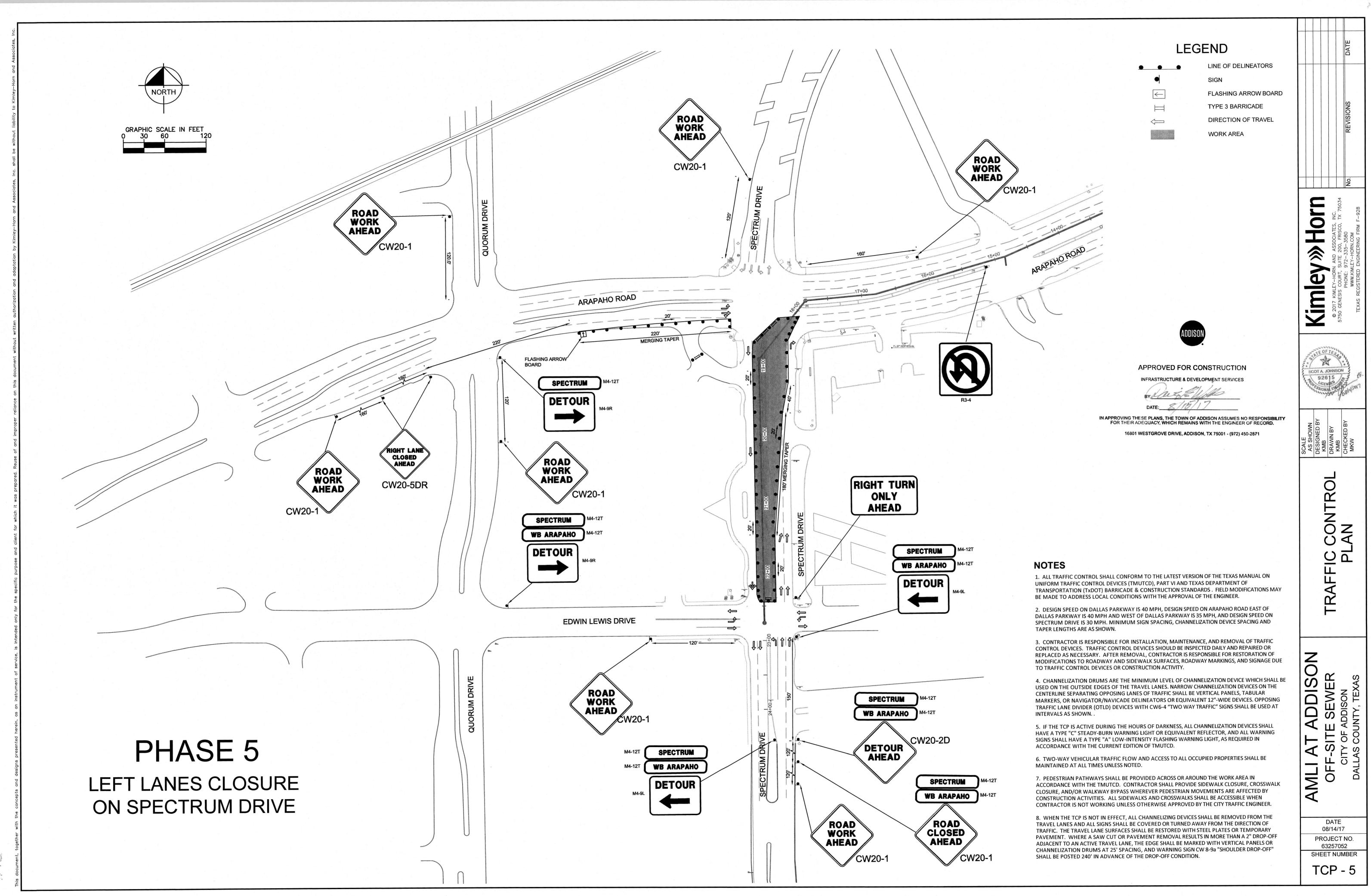


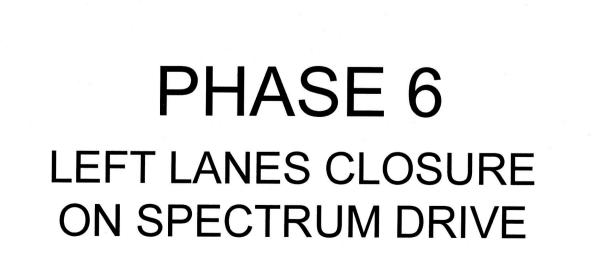












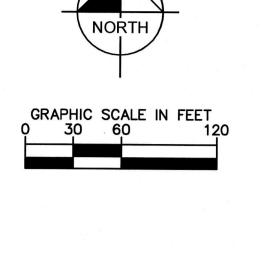
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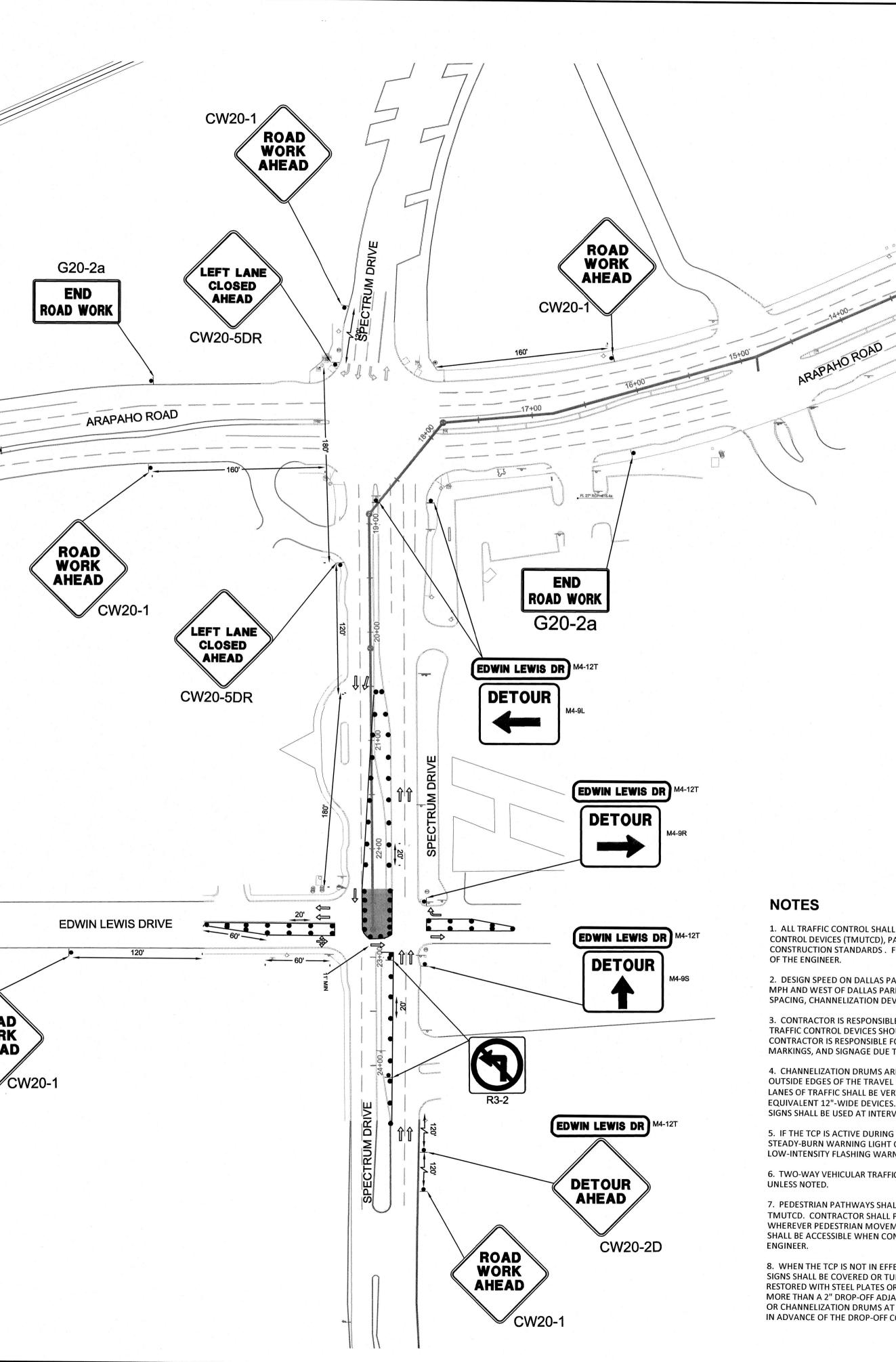
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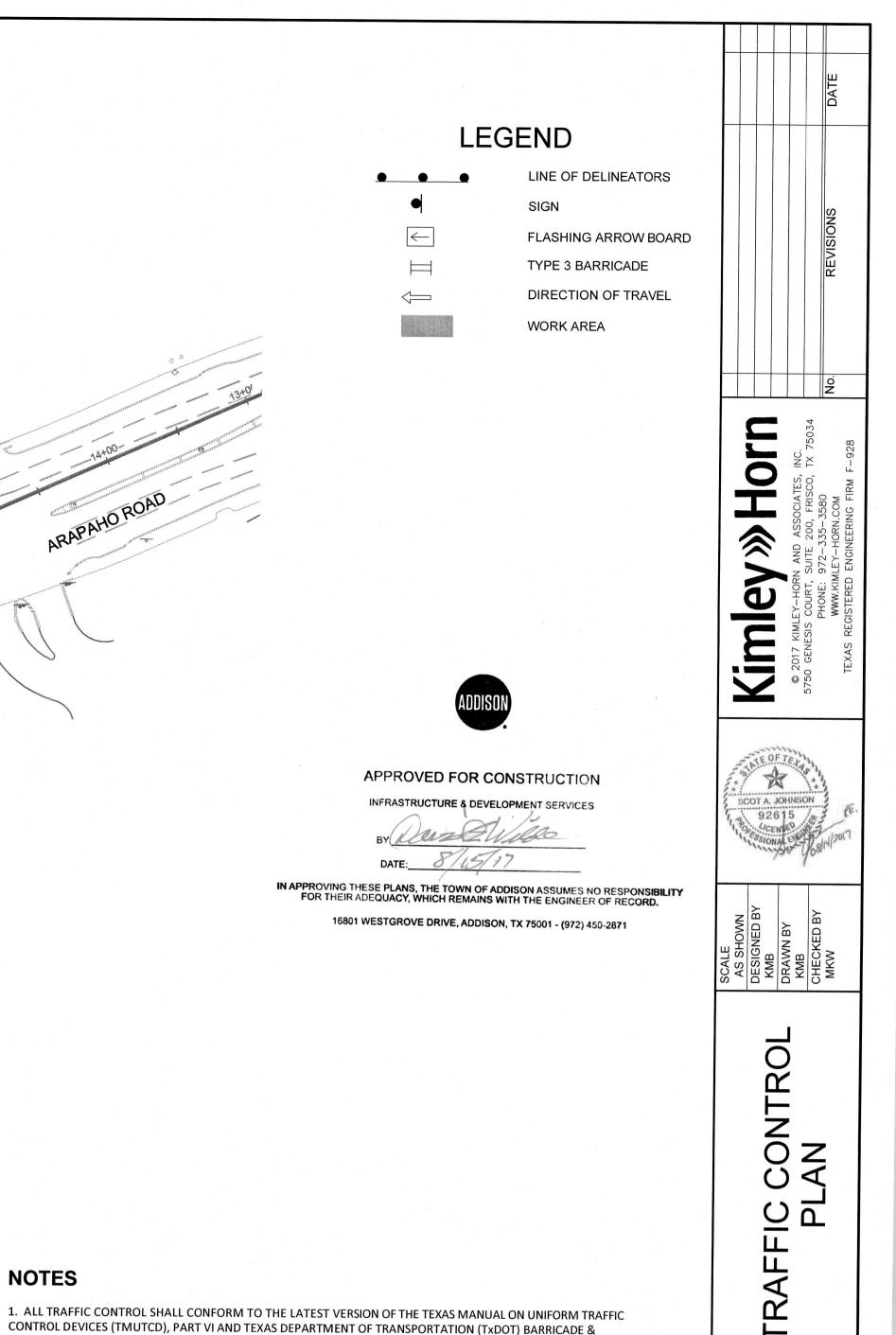
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CONTROL DEVICES (TMUTCD), PART VI AND TEXAS DEPARTMENT OF TRANSPORTATION (TXDOT) BARRICADE & CONSTRUCTION STANDARDS. FIELD MODIFICATIONS MAY BE MADE TO ADDRESS LOCAL CONDITIONS WITH THE APPROVAL

2. DESIGN SPEED ON DALLAS PARKWAY IS 40 MPH, DESIGN SPEED ON ARAPAHO ROAD EAST OF DALLAS PARKWAY IS 40 MPH AND WEST OF DALLAS PARKWAY IS 35 MPH, AND DESIGN SPEED ON SPECTRUM DRIVE IS 30 MPH. MINIMUM SIGN SPACING, CHANNELIZATION DEVICE SPACING AND TAPER LENGTHS ARE AS SHOWN.

3. CONTRACTOR IS RESPONSIBLE FOR INSTALLATION, MAINTENANCE, AND REMOVAL OF TRAFFIC CONTROL DEVICES. TRAFFIC CONTROL DEVICES SHOULD BE INSPECTED DAILY AND REPAIRED OR REPLACED AS NECESSARY. AFTER REMOVAL, CONTRACTOR IS RESPONSIBLE FOR RESTORATION OF MODIFICATIONS TO ROADWAY AND SIDEWALK SURFACES, ROADWAY MARKINGS, AND SIGNAGE DUE TO TRAFFIC CONTROL DEVICES OR CONSTRUCTION ACTIVITY.

4. CHANNELIZATION DRUMS ARE THE MINIMUM LEVEL OF CHANNELIZATION DEVICE WHICH SHALL BE USED ON THE OUTSIDE EDGES OF THE TRAVEL LANES. NARROW CHANNELIZATION DEVICES ON THE CENTERLINE SEPARATING OPPOSING LANES OF TRAFFIC SHALL BE VERTICAL PANELS, TABULAR MARKERS, OR NAVIGATOR/NAVICADE DELINEATORS OR EQUIVALENT 12"-WIDE DEVICES. OPPOSING TRAFFIC LANE DIVIDER (OTLD) DEVICES WITH CW6-4 "TWO WAY TRAFFIC" SIGNS SHALL BE USED AT INTERVALS AS SHOWN. .

5. IF THE TCP IS ACTIVE DURING THE HOURS OF DARKNESS, ALL CHANNELIZATION DEVICES SHALL HAVE A TYPE "C" STEADY-BURN WARNING LIGHT OR EQUIVALENT REFLECTOR, AND ALL WARNING SIGNS SHALL HAVE A TYPE "A" LOW-INTENSITY FLASHING WARNING LIGHT, AS REQUIRED IN ACCORDANCE WITH THE CURRENT EDITION OF TMUTCD.

6. TWO-WAY VEHICULAR TRAFFIC FLOW AND ACCESS TO ALL OCCUPIED PROPERTIES SHALL BE MAINTAINED AT ALL TIMES

7. PEDESTRIAN PATHWAYS SHALL BE PROVIDED ACROSS OR AROUND THE WORK AREA IN ACCORDANCE WITH THE TMUTCD. CONTRACTOR SHALL PROVIDE SIDEWALK CLOSURE, CROSSWALK CLOSURE, AND/OR WALKWAY BYPASS WHEREVER PEDESTRIAN MOVEMENTS ARE AFFECTED BY CONSTRUCTION ACTIVITIES. ALL SIDEWALKS AND CROSSWALKS SHALL BE ACCESSIBLE WHEN CONTRACTOR IS NOT WORKING UNLESS OTHERWISE APPROVED BY THE CITY TRAFFIC

8. WHEN THE TCP IS NOT IN EFFECT, ALL CHANNELIZING DEVICES SHALL BE REMOVED FROM THE TRAVEL LANES AND ALL SIGNS SHALL BE COVERED OR TURNED AWAY FROM THE DIRECTION OF TRAFFIC. THE TRAVEL LANE SURFACES SHALL BE RESTORED WITH STEEL PLATES OR TEMPORARY PAVEMENT. WHERE A SAW CUT OR PAVEMENT REMOVAL RESULTS IN MORE THAN A 2" DROP-OFF ADJACENT TO AN ACTIVE TRAVEL LANE, THE EDGE SHALL BE MARKED WITH VERTICAL PANELS OR CHANNELIZATION DRUMS AT 25' SPACING, AND WARNING SIGN CW 8-9a "SHOULDER DROP-OFF" SHALL BE POSTED 240' IN ADVANCE OF THE DROP-OFF CONDITION.

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