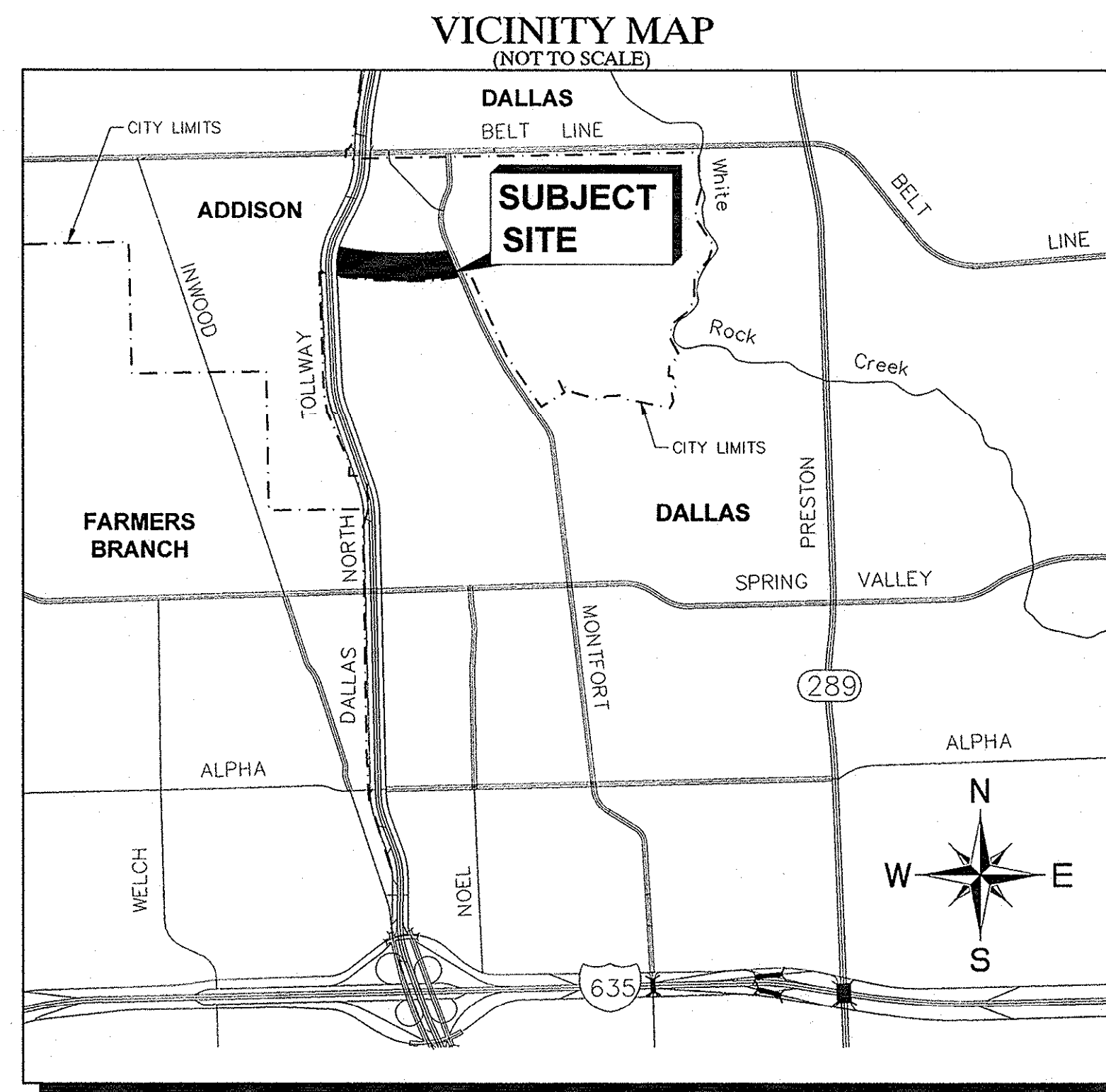


# CONSTRUCTION PLANS FOR VILLAGE ON THE PARKWAY PHASE II TOWN OF ADDISON, DALLAS COUNTY, TEXAS

CONSTRUCTION PLANS VILLAGE ON THE PARKWAY PHASE II  
TOWN OF ADDISON PROJECT NUMBER: 2013-01



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## BENCHMARKS

- BM 1 2" BRASS DISK STAMPED "TXDOT COA-5 GPS" 190" SOUTH FROM THE INTERSECTION OF EDWIN LEWIS DRIVE AND QUORUM DRIVE; 2" FROM WEST EDGE OF SIDEWALK, 50.5" FROM THE WEST SIDE OF MEDIAN IN QUORUM DRIVE & 76" FROM FIRE HYDRANT.  
ELEV=630.10
- BM 2 2" BRASS DISK STAMPED "TXDOT COA-10 GPS" AT THE NORTHWEST CORNER OF HAVENSHIRE PLACE AND BELLEBROOK DRIVE; 21.80" FROM SANITARY SEWER MANHOLE, 11.10" FROM STREET SIGN & 2.45" TO PC IN SIDEWALK.  
ELEV=693.89
- BM 3 CITY OF DALLAS WATER DEPT. BENCHMARK; □ ON CONCRETE BASE OF 6" BRICK WALL AT THE NORTHEAST CORNER OF CELESTIAL ROAD AND MONTFORT DRIVE.  
ELEV=626.61
- BM 4 STANDARD CITY OF DALLAS WATER DEPT. BENCHMARK ON CONCRETE CURB ON STORM SEWER INLET AT THE NORTHEAST CORNER OF HUGHES LANE AND PRESTON ROAD; 30" NORTH OF HUGHES LANE.  
ELEV=573.18
- BM 5 □ SET ON NORTHWEST CORNER OF CONCRETE PAD FOR SWB TELEPHONE BOX, 15" EAST OF NORTHBOUND DALLAS NORTH TOLLWAY ACCESS ROAD AND 1,450" SOUTH OF BELT LINE ROAD.  
ELEV=635.30
- BM 6 □ SET ON SOUTHEAST CORNER OF CURB INLET; 15" WEST OF MONTFORT DRIVE AND 750" SOUTH OF SAKOWITZ DRIVE.  
ELEV=603.67
- BM 7 □ SET ON SOUTHWEST CORNER OF CURB INLET ON SOUTH SIDE OF BELT LINE ROAD AND 200" EAST OF DALLAS NORTH TOLLWAY.  
ELEV=627.26
- BM 8 □ SET ON NORTHEAST CORNER OF "Y" INLET 20" SOUTH OF SAKOWITZ DRIVE AND 315" NORTHWEST OF MONTFORT DRIVE.  
ELEV=616.09
- BM 9 □ SET ON SOUTHEAST CORNER OF CURB INLET ON EAST SIDE OF DALLAS NORTH TOLLWAY ACCESS ROAD AND 300" SOUTH OF BELT LINE ROAD.  
ELEV=627.34

### OWNER / DEVELOPER

VOP, LP  
2000 MCKINNEY AVENUE  
SUITE 1000  
DALLAS, TX 75201  
CONTACT: JARROD YATES

### ARCHITECT

GOOD FULTON & FARRELL ARCHITECTS  
2808 FAIRMOUNT STREET  
SUITE 300  
DALLAS, TX 75201  
PH. (214) 303-1500  
CONTACT: MATTHEW BALSAMAN

### ENGINEER

 **Kimley-Horn  
and Associates, Inc.**

5750 GENESIS COURT  
FRISCO, TEXAS 75034  
PH. (972) 335-3580  
CONTACT: TREY BRASWELL, P.E.  
FIRM NO. F-928

### SURVEYOR

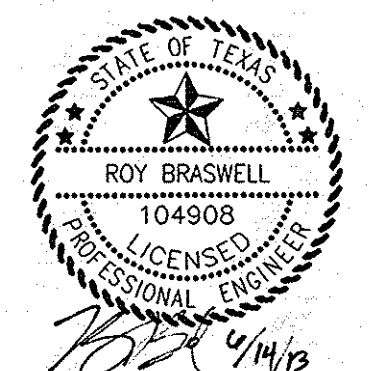
 **Kimley-Horn  
and Associates, Inc.**

12750 MERIT DRIVE  
DALLAS, TEXAS 75251  
SUITE 1000  
PH. (972) 770-1300  
CONTACT: DANA BROWN, R.P.L.S.  
FIRM NO. F-928

TOWN OF ADDISON PROJECT #: 2013-01  
JUNE 14, 2013

**STOP!**  
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(at least 72 hours prior to digging)

**RECORD DRAWINGS  
(October 2013)**  
Information Provided By:  
**BOB MOORE  
CONSTRUCTION**





**EROSION CONTROL SCHEDULE AND PHASING**

- THE PROJECT SHALL GENERALLY CONFORM TO THE FOLLOWING:
- PHASE 1 - GRADING**
- CONSTRUCT TEMPORARY CONSTRUCTION ENTRANCE AND SILT FENCE ACCORDING TO THE APPROXIMATE LOCATION AND SHOWN ON GRADING AND EROSION CONTROL PLAN NOTES AND DETAIL SHEET.
  - BEGIN CLEARING AND GRADING OF SITE.
  - SEED AND RE-VEGETATE SLOPES WHERE SHOWN.
- PHASE 2 - UTILITIES**
- KEEP ALL STORM WATER POLLUTION PREVENTION MEASURES IN PLACE.
  - INSTALL STORM DRAINS AS SPECIFIED ON PLAN SHEETS.
  - INSTALL INLET PROTECTION.
- PHASE 3 - PAVING**
- KEEP ALL STORM WATER POLLUTION PREVENTION MEASURES IN PLACE. REMOVE AS NEEDED TO PAVE.
  - STABILIZE SUBGRADE.
  - PAVE PARKING LOT AND SIDEWALKS AS SPECIFIED ON PLAN SHEETS.
  - REMOVE TEMPORARY CONSTRUCTION ENTRANCE.
- PHASE 4 - LANDSCAPING AND SOIL STABILIZATION**
- RE-VEGETATE LOT AND PARKWAYS.
  - LANDSCAPE CONTRACTOR SHALL REVEGETATE ALL AREAS RESERVED FOR LANDSCAPE VEGETATIVE COVERS.
  - REMOVE EROSION CONTROL DEVICES WHEN GROUND COVER ESTABLISHED.

**SITE MAP-SITE SPECIFIC NOTES**

- CONSTRUCTION ENTRANCE SHALL BE LOCATED SO AS TO PROVIDE THE LEAST AMOUNT OF DISTURBANCE TO THE FLOW OF TRAFFIC IN AND OUT OF THE SITE. ADDITIONALLY, THE CONSTRUCTION ENTRANCE SHALL BE LOCATED TO COINCIDE WITH THE PHASING OF THE PARKING LOT CONSTRUCTION.
- CONTRACTOR SHALL PROVIDE INLET PROTECTION FOR ANY AFFECTED INLETS DOWNSTREAM OF THE PROPOSED IMPROVEMENTS, IF NEEDED.
- THE NATURE OF THIS SITE'S CONSTRUCTION CONSISTS OF CLEARING & SITE PREPARATION, EARTHWORK, PAVING, AND LANDSCAPING.
- SEDIMENTATION BASIN: NEITHER A TEMPORARY NOR PERMANENT SEDIMENTATION BASIN HAS BEEN PROVIDED ON THIS SITE BECAUSE:
  - SITE IS LESS THAN 10 ACRES IN AREA.
- POST CONSTRUCTION STORM WATER POLLUTION CONTROL MEASURES INCLUDE STABILIZATION BY PERMANENT PAVING AND LANDSCAPING.
- VELOCITY DISSIPATION DEVICES ARE NOT REQUIRED AT THIS SITE.
- DISTURBED PORTIONS OF SITE MUST BE STABILIZED. STABILIZATION PRACTICES MUST BE INITIATED WITHIN 14 DAYS IN PORTIONS OF THE SITE WHERE CONSTRUCTION HAS BEEN EITHER TEMPORARILY OR PERMANENTLY CEASED, UNLESS EXCEPTED WITHIN THE TPDES PERMIT. CONTRACTOR SHALL REMOVE TEMPORARY EROSION CONTROL DEVICES UPON COMPLETION OF STABILIZATION OR PERMANENT DRAINAGE FACILITIES.
- CONTRACTOR IS RESPONSIBLE FOR MODIFYING THE SWPPP/SITE MAP TO INCLUDE BMP'S FOR ANY OFF-SITE MATERIAL WASTE, BORROW OR EQUIPMENT STORAGE AREAS.

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 (at least 72 hours prior to digging)

**DISTURBED AREA**

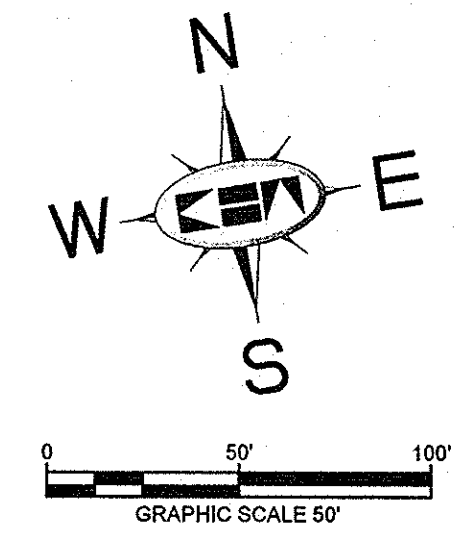
TOTAL AREA DISTURBED: 5.9 AC.

**FLOODPLAIN NOTE**

AS DETERMINED BY THE FLOOD INSURANCE RATE MAPS FOR DALLAS COUNTY, THE SUBJECT PROPERTY DOES NOT APPEAR TO LIE WITHIN A SPECIAL FLOOD HAZARD AREA (100-YEAR FLOOD), MAP DATE 08/20/01 COMMUNITY PANEL NO. 4811300180 J, SUBJECT LOT IS LOCATED IN ZONE 'X'. IF THIS SITE IS NOT WITHIN AN IDENTIFIED FLOOD HAZARD AREA, THIS FLOOD STATEMENT DOES NOT IMPLY THAT THE PROPERTY AND/OR STRUCTURES THEREON WILL BE FREE FROM FLOODING OR FLOOD DAMAGE. ON RARE OCCASIONS, GREATER FLOODS CAN AND WILL OCCUR AND FLOOD HEIGHTS MAY BE INCREASED BY MAN-MADE OR NATURAL CAUSES. THIS FLOOD STATEMENT SHALL NOT CREATE LIABILITY ON THE PART OF THE SURVEYOR OR ENGINEER.

**BENCHMARKS**

- BM 5 1/2" SET ON NORTHWEST CORNER OF CONCRETE PAD FOR SWB TELEPHONE BOX, 15' EAST OF NORTHBOUND DALLAS NORTH TOLLWAY ACCESS ROAD AND 1.450' SOUTH OF BELT LINE ROAD.  
 ELEV=635.30
- BM 6 1/2" SET ON SOUTHEAST CORNER OF CURB INLET, 15' WEST OF MONTFORT DRIVE AND 750' SOUTH OF SAKOVITZ DRIVE.  
 ELEV=603.67
- BM 9 1/2" SET ON SOUTHEAST CORNER OF CURB INLET ON EAST SIDE OF DALLAS NORTH TOLLWAY ACCESS ROAD AND 300' SOUTH OF BELT LINE ROAD.  
 ELEV=627.34



**NOTE**

REFER TO SHEET C-04 FOR EROSION CONTROL DETAILS AND ADDITIONAL GENERAL EROSION CONTROL NOTES.

**NOTE**

PRIOR TO CONSTRUCTION, CONTRACTOR TO NOTIFY ENGINEER IMMEDIATELY SHOULD THERE BE ANY DISCREPANCIES OR IF EXISTING FIELD CONDITIONS VARY FROM THOSE SHOWN ON THESE PLANS.

**RECORD DRAWINGS (October 2013)**  
 Information Provided By:  
**BOB MOORE CONSTRUCTION**



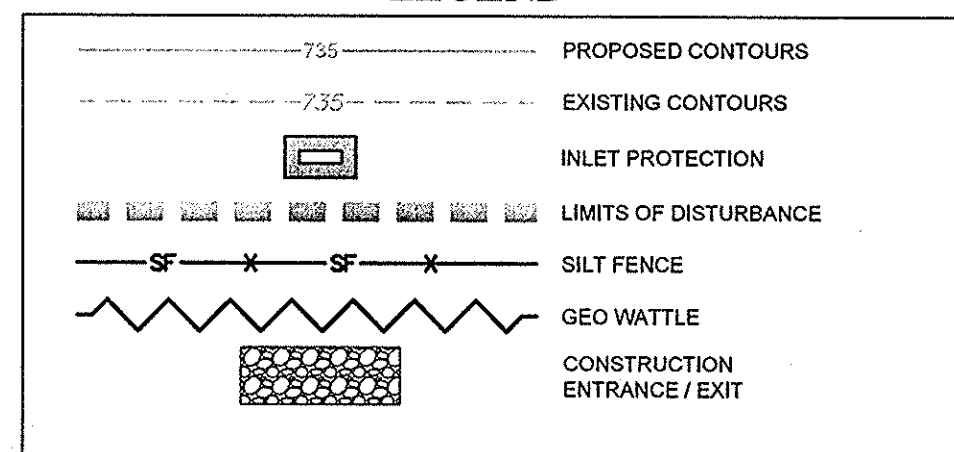
**SITE MAP-GENERAL NOTES**

- CONTRACTOR IS SOLELY RESPONSIBLE FOR SELECTION, IMPLEMENTATION, MAINTENANCE, AND EFFECTIVENESS OF ALL SWPPP CONTROLS - CONTROLS SHOWN ON THIS SITE MAP ARE SUGGESTED CONTROLS ONLY.
- CONTRACTOR SHALL RECORD INSTALLATION, MAINTENANCE OR MODIFICATION, AND REMOVAL DATES FOR EACH BMP EMPLOYED (WHETHER CALLED OUT ON ORIGINAL SWPPP OR NOT) DIRECTLY ON THE SITE MAP.
- DRAINAGE PATTERNS ARE SHOWN ON THIS PLAN BY PROPOSED AND EXISTING CONTOURS, FLOW ARROWS, AND SLOPES.
- TEMPORARY AND PERMANENT STABILIZATION PRACTICES AND BMP'S SHALL BE INSTALLED AT THE EARLIEST POSSIBLE TIME DURING THE CONSTRUCTION SEQUENCE. AS AN EXAMPLE, PERIMETER SILT FENCE SHALL BE INSTALLED BEFORE COMMENCEMENT OF ANY GRADING ACTIVITIES. OTHER BMP'S SHALL BE INSTALLED AS SOON AS PRACTICABLE AND SHALL BE MAINTAINED UNTIL FINAL SITE STABILIZATION IS ATTAINED. CONTRACTOR SHALL ALSO REFERENCE CIVIL PLANS SINCE PERMANENT STABILIZATION IS PROVIDED BY LANDSCAPING AND SITE PAVING.
- BMP'S HAVE BEEN LOCATED AS INDICATED ON THIS PLAN IN ACCORDANCE WITH GENERALLY ACCEPTED ENGINEERING PRACTICES IN ORDER TO MINIMIZE SEDIMENT TRANSFER. FOR EXAMPLE: SILT FENCES LOCATED AT TOE OF SLOPE AND INLET PROTECTION FOR INLETS RECEIVING SEDIMENT FROM SITE RUN-OFF.
- SANITARY SEWER EFFLUENT IS DISPOSED OF VIA AN ONSITE SEWER SYSTEM CONNECTED TO A MUNICIPAL SEWER SYSTEM.
- CONTRACTOR SHALL KEEP SEDIMENTATION OFF OF EXISTING PAVEMENT.

**NOTE**

IN ORDER TO COMPLY WITH THE UNITED STATES ENVIRONMENTAL PROTECTION AGENCY AND ALL OTHER AGENCIES HAVING JURISDICTION, THE CONTRACTOR SHALL PROVIDE ANY ADDITIONAL EROSION OR POLLUTION DEVICES, AS REQUIRED, DURING CONSTRUCTION. FILING OF N.O.I. AND N.O.T. (PER EPA REQUIREMENTS) SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR AND THE OWNER.

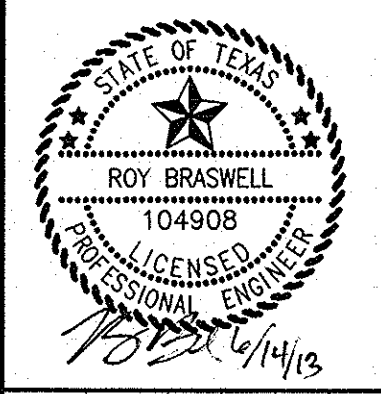
**LEGEND**



**EROSION CONTROL CONSTRUCTION PLAN**

	INSTALLATION	MAINTENANCE	REMOVAL
(CE) CONSTRUCTION ENTRANCE	PRIOR TO STARTING CONSTRUCTION	INSPECTIONS SHALL BE MADE WEEKLY AND AFTER RAIN STORM EVENTS TO ENSURE THAT THE FACILITY IS FUNCTIONING PROPERLY. AGGREGATE PAD SHALL BE WASHED DOWN OR REPLACED WHEN SEDIMENT OR MUD HAS CLOGGED THE VOID SPACES BETWEEN THE STONES OR MUD IS BEING TRACKED ONTO THE PUBLIC ROADWAY. RUNOFF FROM WASHDOWN OPERATION SHALL BE FILTERED THROUGH ANOTHER B.M.P. PRIOR TO DRAINING OFF-SITE.	IMMEDIATELY AFTER PAVEMENT PLACEMENT
(SF) SILT FENCE	PRIOR TO STARTING CONSTRUCTION	INSPECTIONS SHALL BE MADE WEEKLY AND AFTER RAIN STORM EVENTS. SEDIMENT SHALL BE REMOVED FROM BEHIND THE FENCE WHEN THE DEPTH OF SEDIMENT HAS BUILT UP TO ONE-THIRD THE HEIGHT OF THE FENCE ABOVE GRADE.	TO BE REMOVED AFTER PAVING IS COMPLETE AND GRASS COVER IS ESTABLISHED
(IP) INLET PROTECTION	UPON INLET COMPLETION	INSPECTIONS SHALL BE MADE WEEKLY AND AFTER RAIN STORM EVENTS. SEDIMENT SHALL BE REMOVED FROM BEHIND THE FILTER FABRIC WHEN THE DEPTH OF SEDIMENT HAS BUILT UP TO ONE-THIRD THE HEIGHT OF THE INLET ABOVE GRADE. INSPECT FOR A 2 INCH GAP BETWEEN TOP OF FABRIC AND TOP OF INLET OPENING. INSPECT FOR GAPS BETWEEN THE PAVEMENT AND FILTER FABRIC.	TO BE REMOVED AFTER SITE GRADING AROUND EXIST INLET AND FINAL ACCEPTANCE.

**Kimley-Horn and Associates, Inc.**  
 State of Texas Registration No. F-928  
 5750 GENESIS COURT, SUITE 200, FRISCO, TX 75034  
 PHONE: 972-335-3580 FAX: 972-335-3779  
 WWW.KIMLEY-HORN.COM



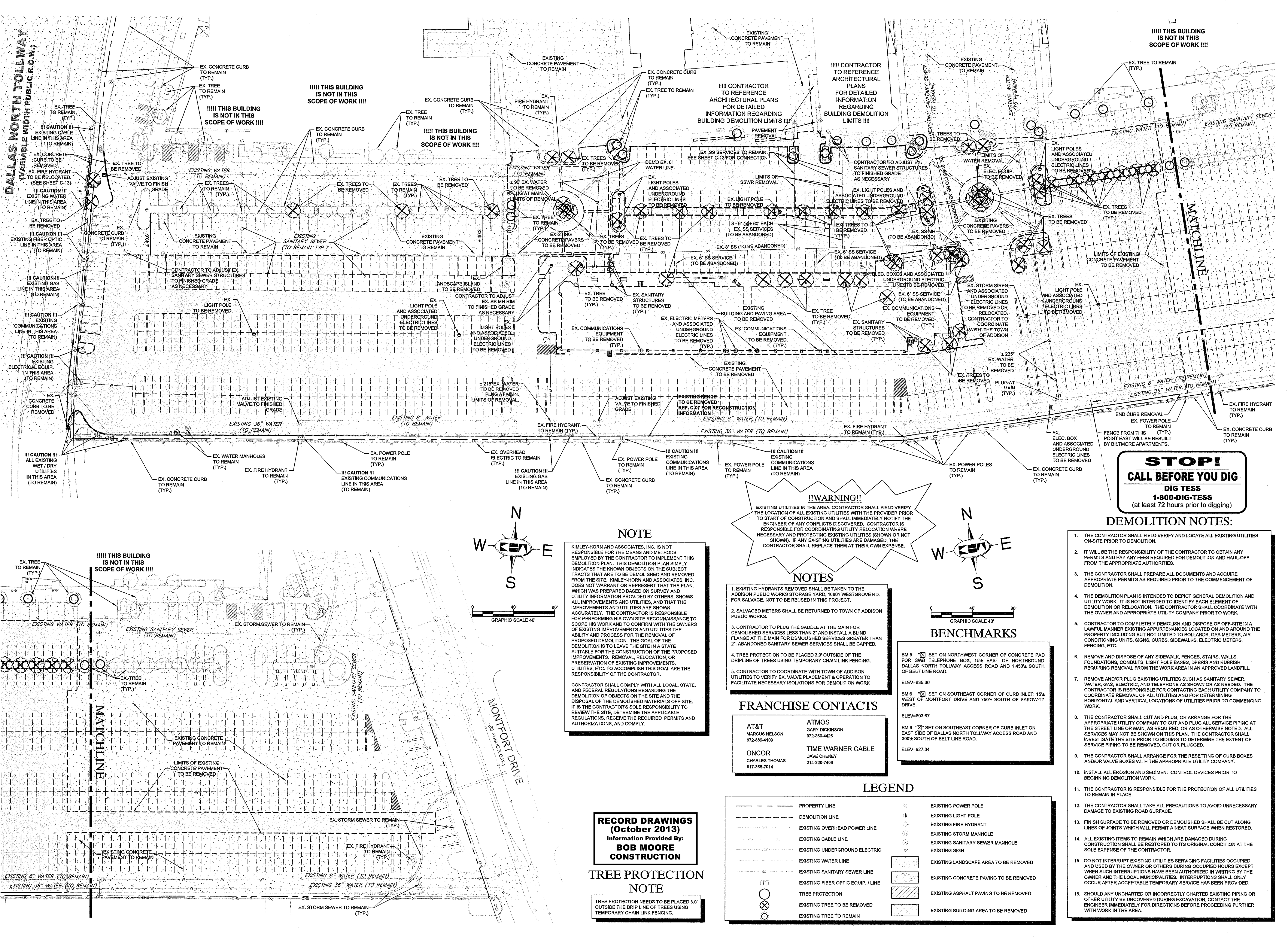
KHA PROJECT	063319038
DATE	06/14/2013
SCALE	AS SHOWN
DESIGNED BY	TBB
DRAWN BY	AAE
CHECKED BY	TBB

**VILLAGE ON THE PARKWAY PHASE II ADDISON, TEXAS**

**EROSION CONTROL PLAN**



KIMLEY-HORN AND ASSOCIATES, INC. SHALL BE WITHOUT LIABILITY TO KIMLEY-HORN AND ASSOCIATES, INC. FOR ANY AND ALL DAMAGES, LOSSES, AND EXPENSES, INCLUDING REASONABLE ATTORNEY'S FEES, INCURRED BY ANY PARTY AS A RESULT OF THE USE OF THIS DOCUMENT. THIS DOCUMENT IS THE PROPERTY OF KIMLEY-HORN AND ASSOCIATES, INC. AND IS NOT TO BE REPRODUCED, COPIED, OR TRANSMITTED IN ANY FORM OR BY ANY MEANS, ELECTRONIC OR MECHANICAL, INCLUDING PHOTOCOPYING, RECORDING, OR BY ANY INFORMATION STORAGE AND RETRIEVAL SYSTEM, WITHOUT THE WRITTEN PERMISSION OF KIMLEY-HORN AND ASSOCIATES, INC.



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 (at least 72 hours prior to digging)

**!!WARNING!!**  
 EXISTING UTILITIES IN THE AREA. CONTRACTOR SHALL FIELD VERIFY THE LOCATION OF ALL EXISTING UTILITIES WITH THE PROVIDER PRIOR TO START OF CONSTRUCTION AND SHALL IMMEDIATELY NOTIFY THE ENGINEER OF ANY CONFLICTS DISCOVERED. CONTRACTOR IS RESPONSIBLE FOR COORDINATING UTILITY RELOCATION WHERE NECESSARY AND PROTECTING EXISTING UTILITIES (SHOWN OR NOT SHOWN). IF ANY EXISTING UTILITIES ARE DAMAGED, THE CONTRACTOR SHALL REPLACE THEM AT THEIR OWN EXPENSE.

**NOTE**

KIMLEY-HORN AND ASSOCIATES, INC. IS NOT RESPONSIBLE FOR THE MEANS AND METHODS EMPLOYED BY THE CONTRACTOR TO IMPLEMENT THIS DEMOLITION PLAN. THIS DEMOLITION PLAN SIMPLY INDICATES THE KNOWN OBJECTS ON THE SUBJECT TRACTS THAT ARE TO BE DEMOLISHED AND REMOVED FROM THE SITE. KIMLEY-HORN AND ASSOCIATES, INC. DOES NOT WARRANT OR REPRESENT THAT THE PLAN, WHICH WAS PREPARED BASED ON SURVEY AND UTILITY INFORMATION PROVIDED BY OTHERS, SHOWS ALL IMPROVEMENTS AND UTILITIES, AND THAT THE IMPROVEMENTS AND UTILITIES ARE SHOWN ACCURATELY. THE CONTRACTOR IS RESPONSIBLE FOR PERFORMING HIS OWN SITE RECONNAISSANCE TO SCOPE HIS WORK AND TO CONFIRM WITH THE OWNERS OF EXISTING IMPROVEMENTS AND UTILITIES THE ABILITY AND PROCESS FOR THE REMOVAL OF PROPOSED DEMOLITION. THE GOAL OF THE DEMOLITION IS TO LEAVE THE SITE IN A STATE SUITABLE FOR THE CONSTRUCTION OF THE PROPOSED IMPROVEMENTS. REMOVAL, RELOCATION, OR PRESERVATION OF EXISTING IMPROVEMENTS, UTILITIES, ETC. TO ACCOMPLISH THIS GOAL ARE THE RESPONSIBILITY OF THE CONTRACTOR.

CONTRACTOR SHALL COMPLY WITH ALL LOCAL, STATE, AND FEDERAL REGULATIONS REGARDING THE DEMOLITION OF OBJECTS ON THE SITE AND THE DISPOSAL OF THE DEMOLISHED MATERIALS OFF-SITE. IT IS THE CONTRACTOR'S SOLE RESPONSIBILITY TO REVIEW THE SITE, DETERMINE THE APPLICABLE REGULATIONS, RECEIVE THE REQUIRED PERMITS AND AUTHORIZATIONS, AND COMPLY.

- NOTES**
- EXISTING HYDRANTS REMOVED SHALL BE TAKEN TO THE ADDISON PUBLIC WORKS STORAGE YARD, 1680 WESTGROVE RD. FOR SALVAGE. NOT TO BE REUSED IN THIS PROJECT.
  - SALVAGED METERS SHALL BE RETURNED TO TOWN OF ADDISON PUBLIC WORKS.
  - CONTRACTOR TO PLUG THE SADDLE AT THE MAIN FOR DEMOLISHED SERVICES LESS THAN 2" AND INSTALL A BLIND FLANGE AT THE MAIN FOR DEMOLISHED SERVING GREATER THAN 2". ABANDONED SANITARY SEWER SERVICES SHALL BE CAPPED.
  - TREE PROTECTION TO BE PLACED 3.0' OUTSIDE OF THE DRIPLINE OF TREES USING TEMPORARY CHAIN LINK FENCING.
  - CONTRACTOR TO COORDINATE WITH TOWN OF ADDISON UTILITIES TO VERIFY EX VALVE PLACEMENT & OPERATION TO FACILITATE NECESSARY ISOLATIONS FOR DEMOLITION WORK.

**FRANCHISE CONTACTS**

<b>AT&amp;T</b> MARCUS NELSON 972-889-4109	<b>ATMOS</b> GARY DICKINSON 972-380-4428
<b>ONCOR</b> CHARLES THOMAS 817-355-7014	<b>TIME WARNER CABLE</b> DAVE CHENEY 214-320-7406

**BENCHMARKS**

BM 5 (X) SET ON NORTHWEST CORNER OF CONCRETE PAD FOR SWR TELEPHONE BOX 15' EAST OF NORTHBOND DALLAS NORTH TOLLWAY ACCESS ROAD AND 1,450± SOUTH OF BELT LINE ROAD.  
 ELEV=635.30

BM 6 (X) SET ON SOUTHEAST CORNER OF CURB INLET, 15± WEST OF MONTFORT DRIVE AND 750± SOUTH OF SAKOWITZ DRIVE.  
 ELEV=603.67

BM 9 (X) SET ON SOUTHEAST CORNER OF CURB INLET ON EAST SIDE OF DALLAS NORTH TOLLWAY ACCESS ROAD AND 300± SOUTH OF BELT LINE ROAD.  
 ELEV=627.34

**LEGEND**

	PROPERTY LINE		EXISTING POWER POLE
	DEMOLITION LINE		EXISTING LIGHT POLE
	EXISTING OVERHEAD POWER LINE		EXISTING FIRE HYDRANT
	EXISTING CABLE LINE		EXISTING STORM MANHOLE
	EXISTING UNDERGROUND ELECTRIC		EXISTING SANITARY SEWER MANHOLE
	EXISTING WATER LINE		EXISTING SIGN
	EXISTING SANITARY SEWER LINE		EXISTING LANDSCAPE AREA TO BE REMOVED
	EXISTING FIBER OPTIC EQUIP. LINE		EXISTING CONCRETE PAVING TO BE REMOVED
	TREE PROTECTION		EXISTING ASPHALT PAVING TO BE REMOVED
	EXISTING TREE TO BE REMOVED		EXISTING BUILDING AREA TO BE REMOVED
	EXISTING TREE TO REMAIN		

**RECORD DRAWINGS**  
**(October 2013)**  
 Information Provided By:  
**BOB MOORE**  
**CONSTRUCTION**

**TREE PROTECTION NOTE**

TREE PROTECTION NEEDS TO BE PLACED 3.0' OUTSIDE THE DRIP LINE OF TREES USING TEMPORARY CHAIN LINK FENCING.

- DEMOLITION NOTES:**
- THE CONTRACTOR SHALL FIELD VERIFY AND LOCATE ALL EXISTING UTILITIES ON-SITE PRIOR TO DEMOLITION.
  - IT WILL BE THE RESPONSIBILITY OF THE CONTRACTOR TO OBTAIN ANY PERMITS AND PAY ANY FEES REQUIRED FOR DEMOLITION AND HAUL-OFF FROM THE APPROPRIATE AUTHORITIES.
  - THE CONTRACTOR SHALL PREPARE ALL DOCUMENTS AND ACQUIRE APPROPRIATE PERMITS AS REQUIRED PRIOR TO THE COMMENCEMENT OF DEMOLITION.
  - THE DEMOLITION PLAN IS INTENDED TO DEPICT GENERAL DEMOLITION AND UTILITY WORK. IT IS NOT INTENDED TO IDENTIFY EACH ELEMENT OF DEMOLITION OR RELOCATION. THE CONTRACTOR SHALL COORDINATE WITH THE OWNER AND APPROPRIATE UTILITY COMPANY PRIOR TO WORK.
  - CONTRACTOR TO COMPLETELY DEMOLISH AND DISPOSE OF OFF-SITE IN A LAWFUL MANNER EXISTING APPURTENANCES LOCATED ON AND AROUND THE PROPERTY INCLUDING BUT NOT LIMITED TO BOLLARDS, GAS METERS, AIR CONDITIONING UNITS, SIGNS, CURBS, SIDEWALKS, ELECTRIC METERS, FENCING, ETC.
  - REMOVE AND DISPOSE OF ANY SIDEWALK, FENCES, STAIRS, WALLS, FOUNDATIONS, CONDUIITS, LIGHT POLE BASES, DEBRIS AND RUBBISH REQUIRING REMOVAL FROM THE WORK AREA IN AN APPROVED LANDFILL.
  - REMOVE AND/OR PLUG EXISTING UTILITIES SUCH AS SANITARY SEWER, WATER, GAS, ELECTRIC, AND TELEPHONE AS SHOWN OR AS NEEDED. THE CONTRACTOR IS RESPONSIBLE FOR CONTACTING EACH UTILITY COMPANY TO COORDINATE REMOVAL OF ALL UTILITIES AND FOR DETERMINING HORIZONTAL AND VERTICAL LOCATIONS OF UTILITIES PRIOR TO COMMENCING WORK.
  - THE CONTRACTOR SHALL CUT AND PLUG, OR ARRANGE FOR THE APPROPRIATE UTILITY COMPANY TO CUT AND PLUG ALL SERVICE PIPING AT THE STREET LINE OR MAIN, AS REQUIRED, OR AS OTHERWISE NOTED. ALL SERVICES MAY NOT BE SHOWN ON THIS PLAN. THE CONTRACTOR SHALL INVESTIGATE THE SITE PRIOR TO BIDDING TO DETERMINE THE EXTENT OF SERVICE PIPING TO BE REMOVED, CUT OR PLUGGED.
  - THE CONTRACTOR SHALL ARRANGE FOR THE RESETTling OF CURB BOXES AND/OR VALVE BOXES WITH THE APPROPRIATE UTILITY COMPANY.
  - INSTALL ALL EROSION AND SEDIMENT CONTROL DEVICES PRIOR TO BEGINNING DEMOLITION WORK.
  - THE CONTRACTOR IS RESPONSIBLE FOR THE PROTECTION OF ALL UTILITIES TO REMAIN IN PLACE.
  - THE CONTRACTOR SHALL TAKE ALL PRECAUTIONS TO AVOID UNNECESSARY DAMAGE TO EXISTING ROAD SURFACE.
  - FINISH SURFACE TO BE REMOVED OR DEMOLISHED SHALL BE CUT ALONG LINES OF JOINTS WHICH WILL PERMIT A NEAT SURFACE WHEN RESTORED.
  - ALL EXISTING ITEMS TO REMAIN WHICH ARE DAMAGED DURING CONSTRUCTION SHALL BE RESTORED TO ITS ORIGINAL CONDITION AT THE SOLE EXPENSE OF THE CONTRACTOR.
  - DO NOT INTERRUPT EXISTING UTILITIES SERVICING FACILITIES OCCUPIED AND USED BY THE OWNER OR OTHERS DURING OCCUPIED HOURS EXCEPT WHEN SUCH INTERRUPTIONS HAVE BEEN AUTHORIZED IN WRITING BY THE OWNER AND THE LOCAL MUNICIPALITIES. INTERRUPTIONS SHALL ONLY OCCUR AFTER ACCEPTABLE TEMPORARY SERVICE HAS BEEN PROVIDED.
  - SHOULD ANY UNCHARTED OR INCORRECTLY CHARTED EXISTING PIPING OR OTHER UTILITY BE UNCOVERED DURING EXCAVATION, CONTACT THE ENGINEER IMMEDIATELY FOR DIRECTIONS BEFORE PROCEEDING FURTHER WITH WORK IN THE AREA.

KIMLEY-HORN AND ASSOCIATES, INC.

State of Texas Registration No. F-928

5750 GENESS COURT, SUITE 200, FRISCO, TX 75034

PHONE: 972-339-3580 FAX: 972-339-3779

WWW.KIMLEY-HORN.COM

DATE

NO.

REVISIONS

**VILLAGE ON THE PARKWAY PHASE II**  
**DEMOLITION PLAN**  
 ADDISON, TEXAS

KHA PROJECT 063319038

DATE 06/14/2013

SCALE AS SHOWN

DESIGNED BY TBS

DRAWN BY AAE

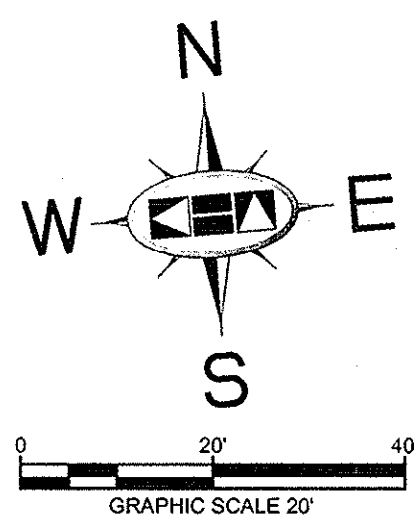
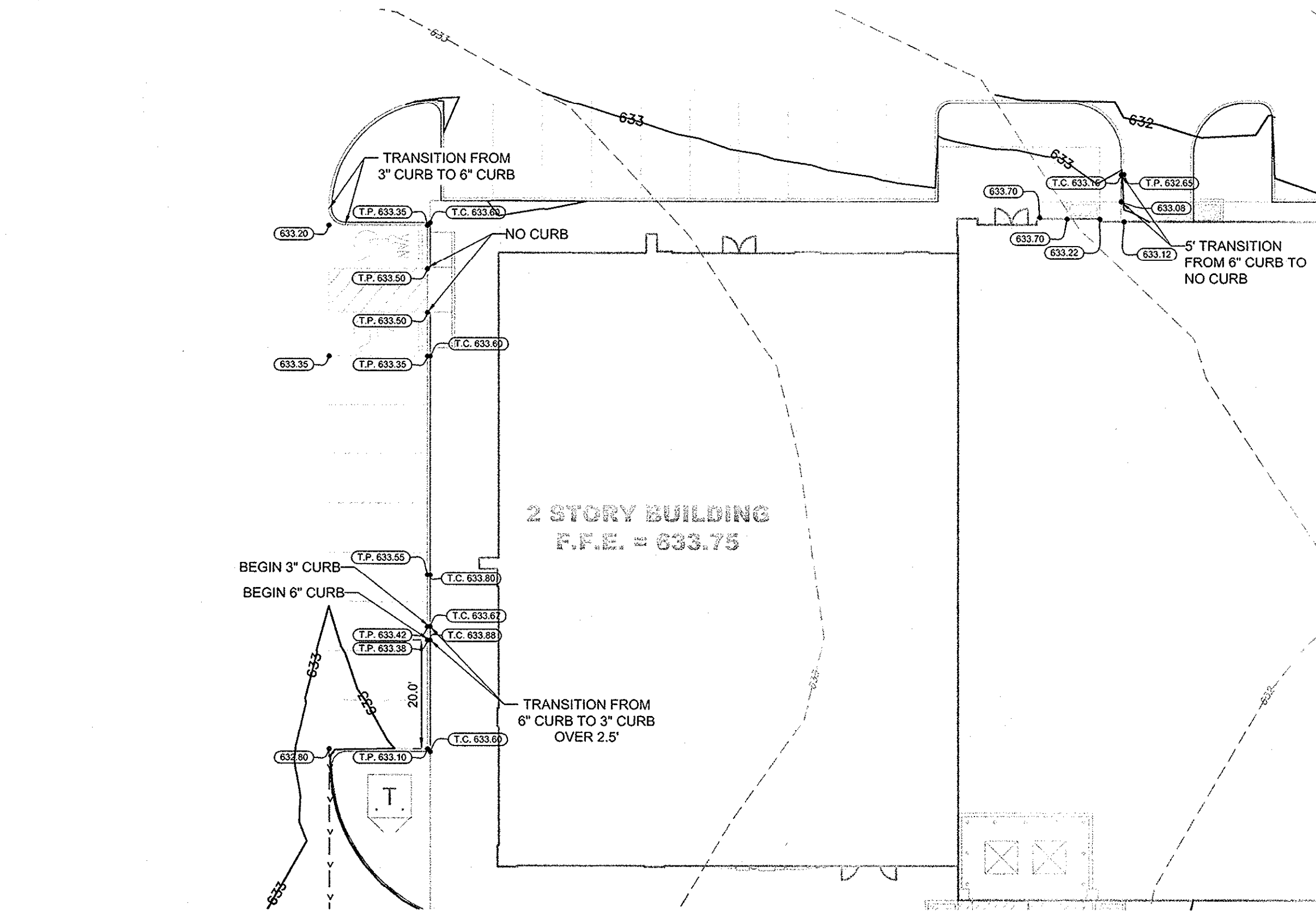
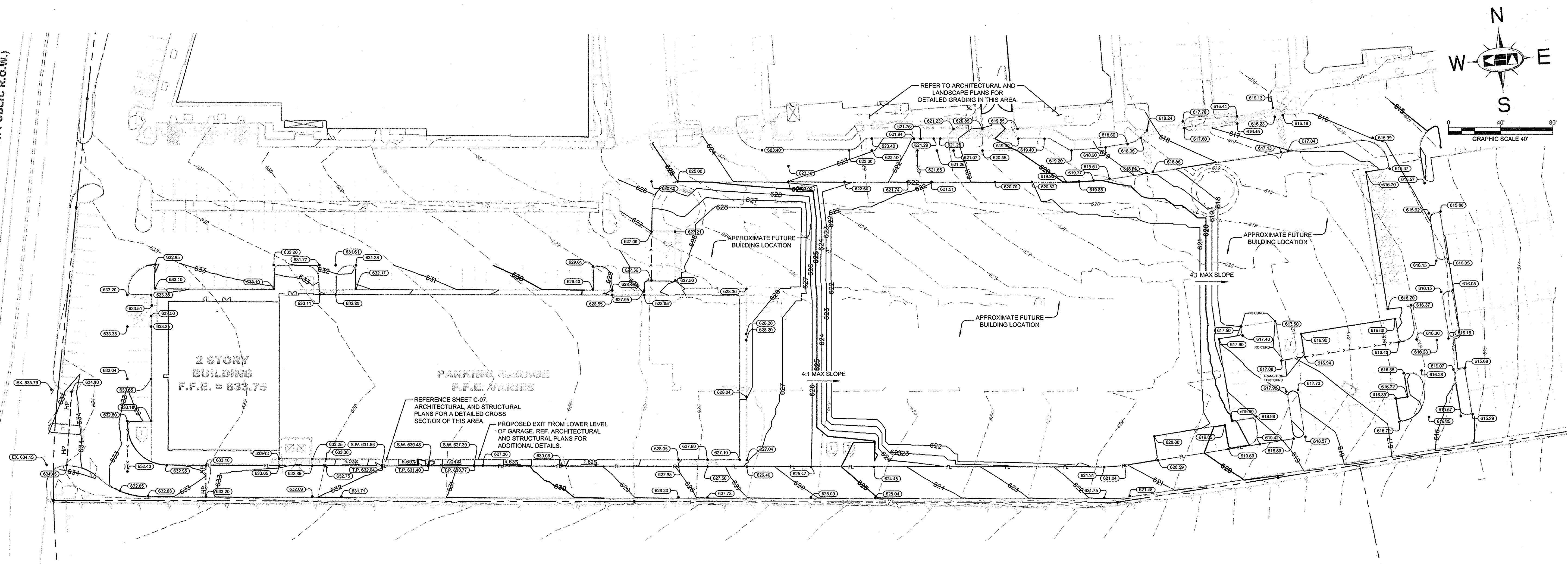
CHECKED BY TBS

SHEET NUMBER

C-05

DRAWN BY: LINDA BARNARD, CIVIL ENGINEER  
 DWG NO: 063319038  
 LAST SAVED: 6/14/2013 11:21 AM  
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**DALLAS NORTH TOLLWAY**  
 (VARIABLE WIDTH PUBLIC R.O.W.)



**RECORD DRAWINGS**  
 (October 2013)  
 Information Provided by:  
**BOB MOORE**  
**CONSTRUCTION**

**NOTES**

- THE CONTRACTOR IS SPECIFICALLY CAUTIONED THAT THE LOCATION AND/OR ELEVATION OF EXISTING UTILITIES AS SHOWN ON THESE PLANS IS BASED ON RECORDS OF THE VARIOUS UTILITY COMPANIES, AND WHERE POSSIBLE, MEASUREMENTS TAKEN IN THE FIELD. THE INFORMATION IS NOT TO BE RELIED ON AS BEING EXACT OR COMPLETE. THE CONTRACTOR MUST CALL THE APPROPRIATE UTILITY COMPANIES AT LEAST 72 HOURS BEFORE ANY EXCAVATION TO REQUEST EXACT FIELD LOCATION OF UTILITIES. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO RELOCATE ALL EXISTING UTILITIES WHICH CONFLICT WITH THE PROPOSED IMPROVEMENTS SHOWN ON THE PLANS.
- ALL CUT OR FILL SLOPES SHALL BE 4:1 OR FLATTER UNLESS OTHERWISE NOTED.
- EXISTING GRADE CONTOUR INTERVALS SHOWN AT 1 FOOT.
- PROPOSED GRADE CONTOUR INTERVALS SHOWN AT 1 FOOT INTERVALS.
- IF ANY EXISTING STRUCTURES TO REMAIN ARE DAMAGED DURING CONSTRUCTION IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO REPAIR AND/OR REPLACE THE EXISTING STRUCTURE AS NECESSARY TO RETURN IT TO EXISTING CONDITIONS OR BETTER.
- THE CONTRACTOR SHALL ADHERE TO ALL TERMS & CONDITIONS AS OUTLINED IN THE EPA OR APPLICABLE STATE GENERAL N.P.D.E.S. PERMIT FOR STORM WATER DISCHARGE ASSOCIATED WITH CONSTRUCTION ACTIVITIES.
- CONTRACTOR SHALL ADJUST AND/OR CUT EXISTING PAVEMENT AS NECESSARY TO ASSURE A SMOOTH FIT AND CONTINUOUS GRADE.
- CONTRACTOR SHALL ASSURE POSITIVE DRAINAGE AWAY FROM BUILDINGS FOR ALL NATURAL AND PAVED AREAS.
- TOPOGRAPHIC INFORMATION IS TAKEN FROM A TOPOGRAPHIC SURVEY BY LAND SURVEYORS. IF THE CONTRACTOR DOES NOT ACCEPT EXISTING TOPOGRAPHY AS SHOWN ON THE PLANS, WITHOUT EXCEPTION, THEN THE CONTRACTOR SHALL SUPPLY, AT THEIR EXPENSE, A TOPOGRAPHIC SURVEY BY A REGISTERED LAND SURVEYOR TO THE OWNER FOR REVIEW.
- ALL UNSURFACED AREAS DISTURBED BY GRADING OPERATION SHALL RECEIVE 4 INCHES OF TOPSOIL. CONTRACTOR SHALL APPLY STABILIZATION FABRIC TO ALL SLOPES 4:1 OR STEEPER. CONTRACTOR SHALL STABILIZE DISTURBED AREAS IN ACCORDANCE WITH GOVERNING SPECIFICATIONS UNTIL A HEALTHY STAND OF VEGETATION IS OBTAINED.
- CONSTRUCTION SHALL COMPLY WITH ALL APPLICABLE GOVERNING CODES AND BE CONSTRUCTED TO SAME.
- CONTRACTOR SHALL REFER TO ARCHITECTURAL PLANS FOR EXACT BUILDING FOOTPRINT DIMENSIONS.
- CONTRACTOR SHALL REFER TO ARCHITECTURAL PLANS AND FINAL GEOTECH REPORT FOR BUILDING SUBGRADE PREPARATION REQUIREMENTS.
- CONTRACTOR SHALL ADJUST EXISTING VALVES, MANHOLE RIMS, ETC. AS NECESSARY TO MATCH EXISTING GRADE.
- ALL ELEVATIONS ARE TOP OF PAVEMENT UNLESS NOTED OTHERWISE. TO GET TOP OF CURB ELEVATIONS ADD 6" TO THE ELEVATION SHOWN.
- GRADING FOR ALL SIDEWALKS AND ACCESSIBLE ROUTES INCLUDING CROSSING DRIVEWAYS SHALL CONFORM TO ADA STANDARDS. SLOPES SHALL NOT EXCEED 5% LONGITUDINAL SLOPE OR 2% CROSS SLOPE. SIDEWALK ACCESS TO EXTERNAL BUILDING DOORS SHALL BE ADA COMPLIANT. CONTRACTOR SHALL NOTIFY ENGINEER IMMEDIATELY IF ADA CRITERIA CANNOT BE MET AT ANY LOCATION.

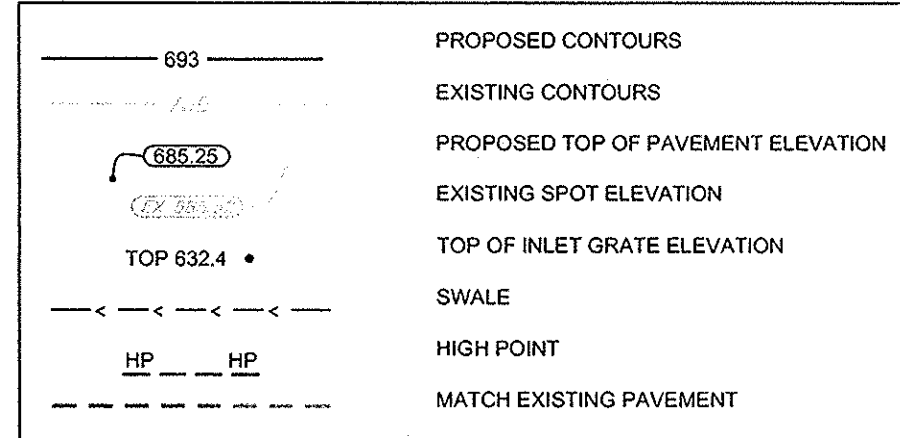
**BENCHMARKS**

BM 5 (5) SET ON NORTHWEST CORNER OF CONCRETE PAD FOR SWB TELEPHONE BOX, 15' EAST OF NORTHBOUND DALLAS NORTH TOLLWAY ACCESS ROAD AND 1,450' SOUTH OF BELT LINE ROAD.  
 ELEV=835.30  
  
 BM 6 (5) SET ON SOUTHEAST CORNER OF CURB INLET, 15' WEST OF MONTFORT DRIVE AND 750' SOUTH OF SAKOWITZ DRIVE.  
 ELEV=803.67  
  
 BM 9 (5) SET ON SOUTHWEST CORNER OF CURB INLET ON EAST SIDE OF DALLAS NORTH TOLLWAY ACCESS ROAD AND 300' SOUTH OF BELT LINE ROAD.  
 ELEV=827.34

**FLOODPLAIN NOTE**

AS DETERMINED BY THE FLOOD INSURANCE RATE MAPS FOR DALLAS COUNTY, THE SUBJECT PROPERTY DOES NOT APPEAR TO LIE WITHIN A SPECIAL FLOOD HAZARD AREA (100 YEAR FLOOD), MAP DATE 08/23/01 COMMUNITY PANEL NO. 4813C0180 J. SUBJECT LOT IS LOCATED IN ZONE "X". IF THIS SITE IS NOT WITHIN AN IDENTIFIED FLOOD HAZARD AREA, THIS FLOOD STATEMENT DOES NOT IMPLY THAT THE PROPERTY AND/OR STRUCTURES THEREON WILL BE FREE FROM FLOODING OR FLOOD DAMAGE. ON RARE OCCASIONS, GREATER FLOODS CAN AND WILL OCCUR AND FLOOD HEIGHTS MAY BE INCREASED BY MAN-MADE OR NATURAL CAUSES. THIS FLOOD STATEMENT SHALL NOT CREATE LIABILITY ON THE PART OF THE SURVEYOR OR ENGINEER.

**LEGEND**

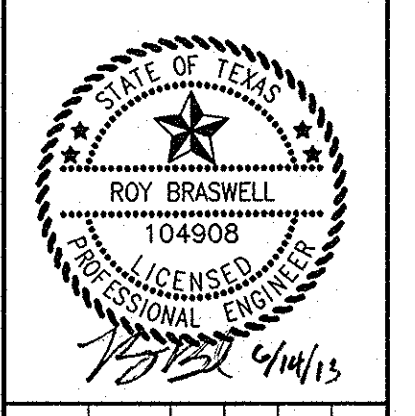


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**DIG TESS**  
 1-800-DIG-TESS  
 (at least 72 hours prior to digging)

**!!WARNING!!**  
 EXISTING UTILITIES IN THE AREA. CONTRACTOR SHALL FIELD VERIFY THE LOCATION OF ALL EXISTING UTILITIES WITH THE PROVIDER PRIOR TO START OF CONSTRUCTION AND SHALL IMMEDIATELY NOTIFY THE ENGINEER OF ANY CONFLICTS DISCOVERED. CONTRACTOR IS RESPONSIBLE FOR COORDINATING UTILITY RELOCATION WHERE NECESSARY AND PROTECTING EXISTING UTILITIES (SHOWN OR NOT SHOWN). IF ANY EXISTING UTILITIES ARE DAMAGED, THE CONTRACTOR SHALL REPLACE THEM AT THEIR OWN EXPENSE.

NO.	REVISIONS	DATE	BY

**Kimley-Horn and Associates, Inc.**  
 State of Texas Registration No. F-928  
 5750 GENESEE COURT, SUITE 200, FRISCO, TX 75034  
 PHONE: 972-335-3580 FAX: 972-335-3779  
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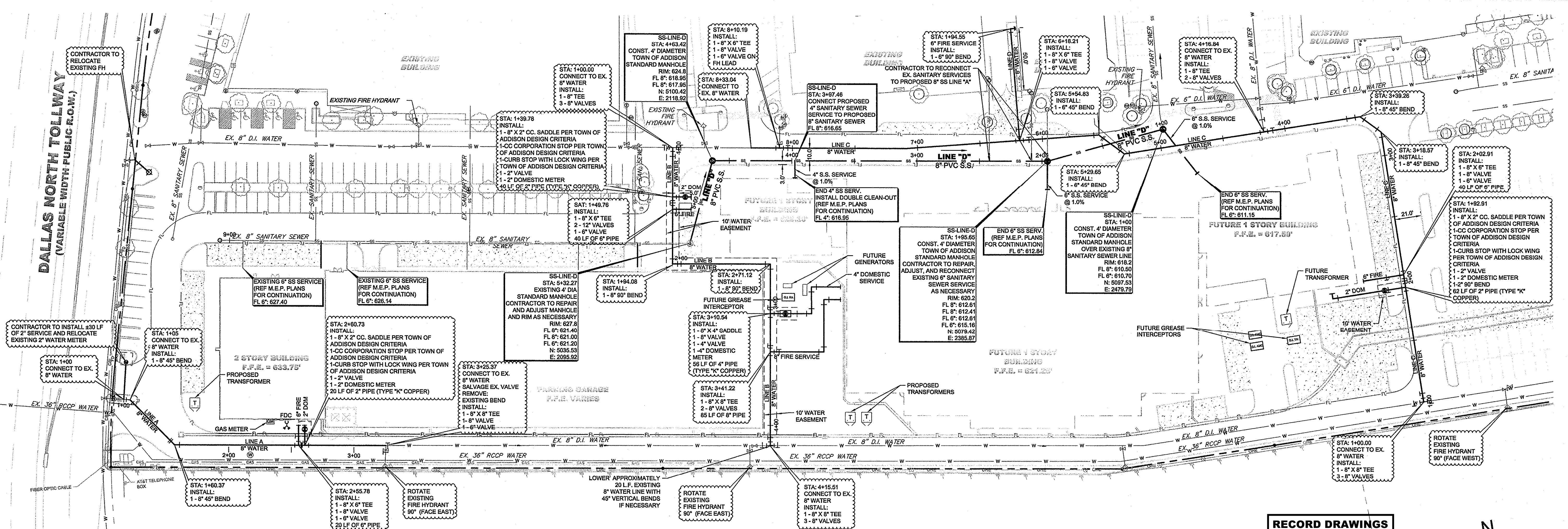
KHA PROJECT	063319038
DATE	06/14/2013
SCALE	AS SHOWN
DESIGNED BY	TBB
DRAWN BY	AJE
CHECKED BY	TBB

**VILLAGE ON THE PARKWAY PHASE II**  
 ADDISON, TEXAS

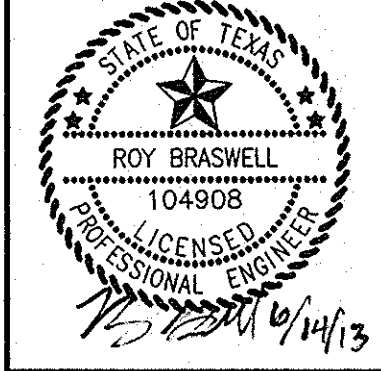
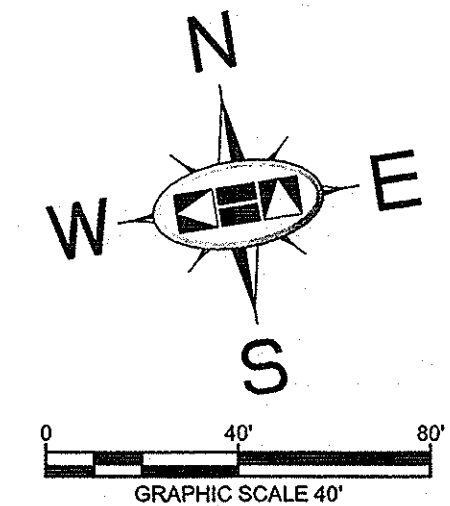
**GRADING PLAN**  
 SHEET NUMBER  
**C-06**



DRAWN BY: LUCAS BAUMER, CIVIL ENGINEER, L.P.E. (0400011874)  
 DWG NO: 063319038  
 DATE: 06/14/2013  
 PROJECT: VILLAGE ON THE PARKWAY PHASE II UTILITY PLAN & SANITARY SEWER PROFILE  
 LAST SAVED: 04/02/13 10:00 AM  
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**RECORD DRAWINGS**  
 (October 2013)  
 Information Provided By:  
**BOB MOORE**  
**CONSTRUCTION**



LEGEND	
	PROPOSED WATER
	PROPOSED SANITARY SEWER
	PROPOSED STORM SEWER
	EXISTING OVERHEAD POWER
	EXISTING CABLE
	EXISTING GAS
	EXISTING UNDERGROUND TELEPHONE
	EXISTING FIBER OPTIC CABLE
	EXISTING UNDERGROUND ELECTRIC
	EXISTING WATER
	EXISTING SANITARY SEWER
	PROPOSED SANITARY SEWER MANHOLE
	PROPOSED STORM SEWER JUNCTION BOX
	PROPOSED FIRE HYDRANT
	PROPOSED METER
	EXISTING METER
	EXISTING IRRIGATION METER
	EXISTING FIRE HYDRANT
	EXISTING CLEAN OUT
	EXISTING MANHOLE

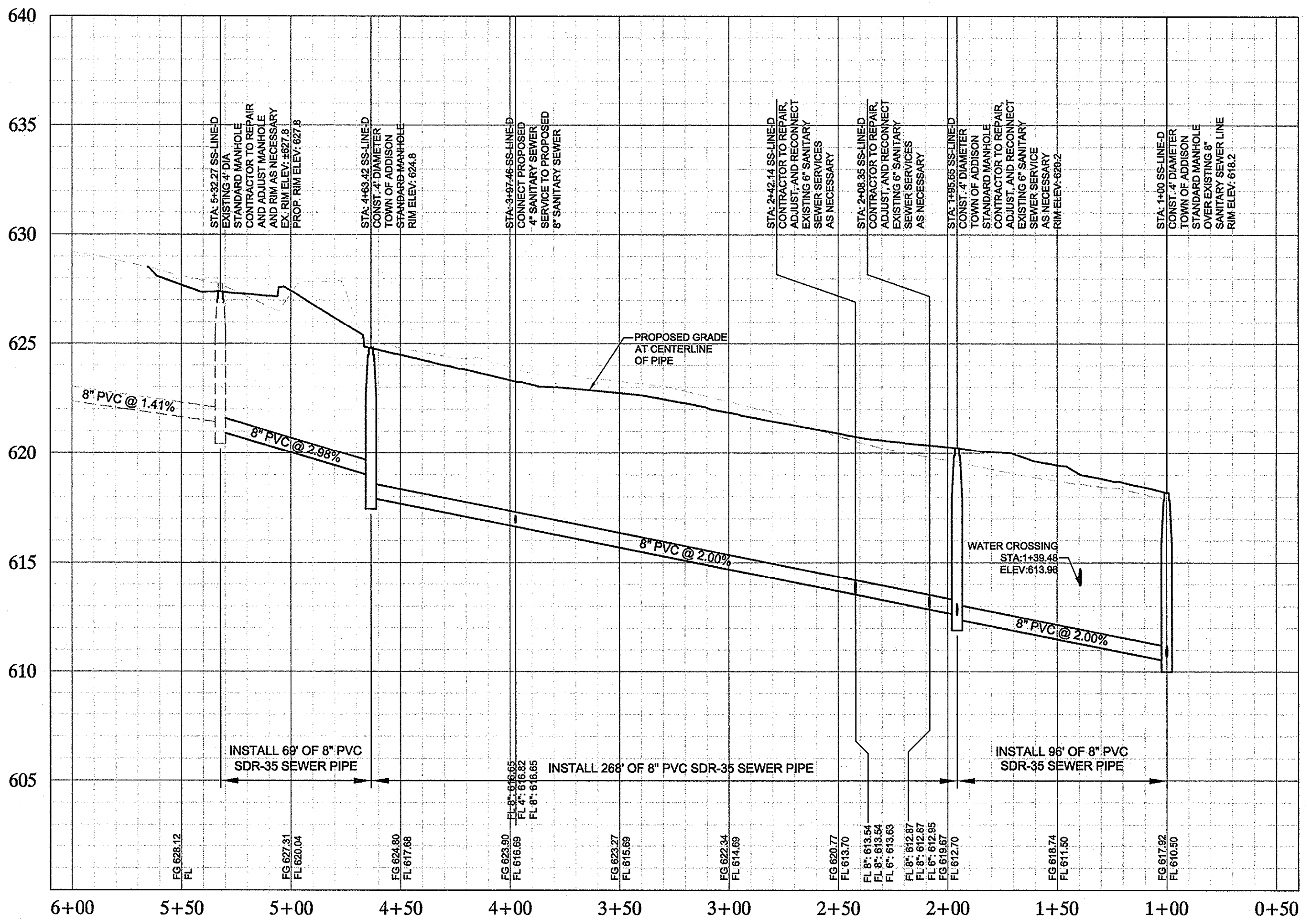
**!!WARNING!!**  
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 (at least 72 hours prior to digging)

**FRANCHISE CONTACTS**  
**AT&T**  
 MARCUS NELSON  
 972-899-4109  
**ONCOR**  
 KAREN EASTMAN  
 817-355-7050  
**ATMOS**  
 GARY DICKINSON  
 972-360-4428  
**TIME WARNER CABLE**  
 DAVE CHENEY  
 214-320-7406

**NOTE**  
 PRIOR TO CONSTRUCTION, CONTRACTOR TO NOTIFY ENGINEER IMMEDIATELY SHOULD THERE BE ANY DISCREPANCIES OR IF EXISTING FIELD CONDITIONS VARY FROM THOSE SHOWN ON THESE PLANS.

**FIRE SPRINKLER NOTE**  
 FIRE SPRINKLER LINE SHALL BE SIZED AND INSTALLED BY A STATE LICENSED FIRE SPRINKLER CONTRACTOR



- UTILITY NOTES**
- SEE MEP PLANS FOR ALL UTILITY CONNECTIONS INTO BUILDING.
  - WATER AND SEWER LINES SHALL STUB 5 FEET FROM BUILDING FOR SERVICE CONNECTIONS UNLESS OTHERWISE NOTED.
  - A FIVE FOOT CLEARANCE SHALL BE MAINTAINED AROUND ALL FIRE HYDRANTS. FIRE HYDRANTS SHALL BE LOCATED 3'-6" FROM BACK OF CURB PER TOWN OF ADDISON STANDARD DETAIL.
  - REFER TO TOWN OF ADDISON DETAILS FOR WATER METER, METER BOX, FIRE HYDRANTS, VALVES, VALVE BOXES AND SERVICE INSTALLATION.
  - REFER TO DETAILS FOR SANITARY SEWER MANHOLE REQUIREMENTS.
  - REFER TO DETAILS FOR TRENCHING, BEDDING, BACKFILL, AND TRENCH COMPACTION REQUIREMENTS.
  - WATER AND SANITARY SEWER PIPE SHALL BE CONSTRUCTED OF SLIP-ON OR MECHANICAL JOINT CAST OR DUCTILE IRON PIPE, OR PVC PIPE.
  - REFER TO ARCHITECTURE PLANS FOR LOCATION AND SIZING OF SLEEVES FOR FRANCHISE UTILITIES, IRRIGATION, ETC.
  - ALL PIPE DEFLECTIONS SHALL BE PER MANUFACTURER'S SPECIFICATIONS. ANY VARIATIONS SHALL BE APPROVED IN WRITING BY THE ENGINEER BEFORE INSTALLATION.
  - IT IS THE CONTRACTOR'S RESPONSIBILITY TO DEFLECT ELECTRIC, GAS, CABLE, AND TELEPHONE CONDUIT AND PIPING AS REQUIRED TO AVOID UTILITY CONFLICTS.
  - CONTRACTOR TO VERIFY IRRIGATION METER SIZE WITH LICENSED IRRIGATOR PRIOR TO INSTALLATION.
  - EXISTING HYDRANTS SHALL BE TAKEN TO TOWN OF ADDISON PUBLIC WORKS FOR REFURBISHMENT PRIOR TO BEING PUT BACK IN SERVICE/RELOCATED.
  - SALVAGED METERS SHALL BE RETURNED TO TOWN OF ADDISON PUBLIC WORKS.

**BENCHMARKS**  
 BM 5 SET ON NORTHWEST CORNER OF CONCRETE PAD FOR SWB TELEPHONE BOX, 15' EAST OF NORTHBOND DALLAS NORTH TOLLWAY ACCESS ROAD AND 1450' SOUTH OF BELT LINE ROAD.  
 ELEV=636.30  
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 ELEV=603.67  
 BM 9 SET ON SOUTHEAST CORNER OF CURB INLET ON EAST SIDE OF DALLAS NORTH TOLLWAY ACCESS ROAD AND 300' SOUTH OF BELT LINE ROAD.  
 ELEV=627.34

**Kimley-Horn and Associates, Inc.**  
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**VILLAGE ON THE PARKWAY PHASE II SANITARY SEWER PROFILE**

KHA PROJECT: 063319038  
 DATE: 06/14/2013  
 SCALE: AS SHOWN  
 DESIGNED BY: TBB  
 DRAWN BY: AAE  
 CHECKED BY: TBB

SHEET NUMBER  
**C-13**



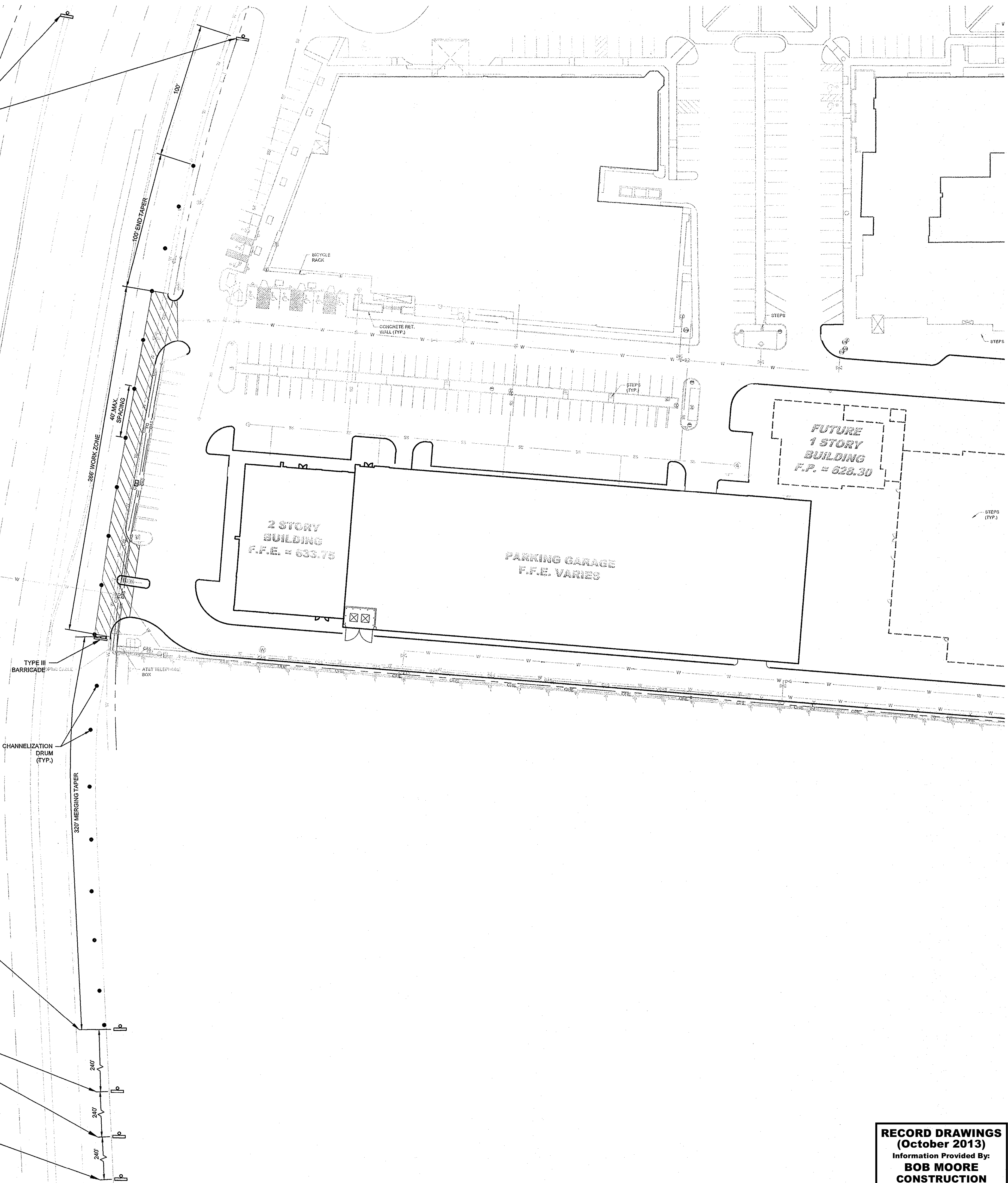
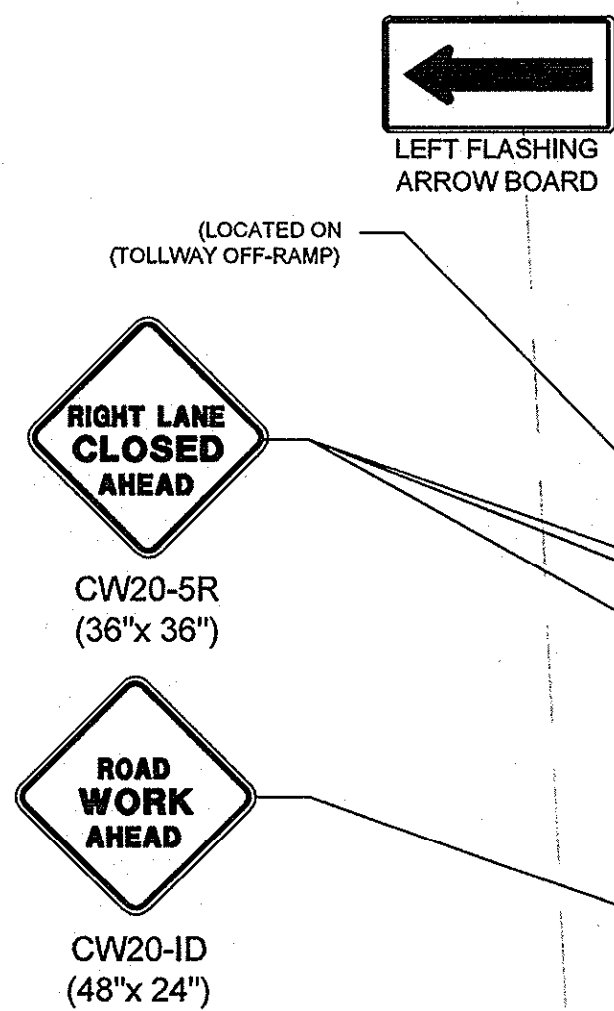
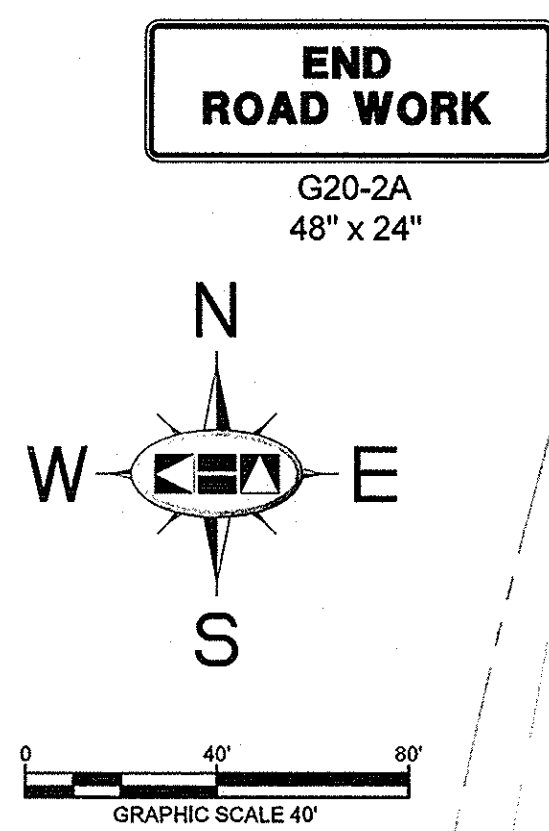
PLANNED BY: LUISA BARRON, CHAIRMAN, LRPB [CITY TRAFFIC CONTROL PLAN DALLAS PARKWAY I]  
 DRAWN BY: KERRI CIVIL, DESIGNER, LRPB [CITY TRAFFIC CONTROL PLAN DALLAS PARKWAY I]  
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### TOWN OF ADDISON GENERAL CONSTRUCTION NOTES

1. ALL MATERIALS AND WORKMANSHIP (WITHIN DALLAS PARKWAY RIGHT-OF-WAY) SHALL CONFORM WITH THE STANDARD SPECIFICATIONS FOR PUBLIC WORKS CONSTRUCTION FOR NORTH CENTRAL TEXAS, LATEST EDITION, AND THE CITY OF DALLAS DEPARTMENT OF PUBLIC WORKS AND TRANSPORTATION ADDENDUM.
2. DURING THE CONSTRUCTION OF THESE IMPROVEMENTS (WITHIN DALLAS PARKWAY RIGHT-OF-WAY), ANY INTERPRETATION OF THE STANDARD SPECIFICATIONS FOR PUBLIC WORKS CONSTRUCTION FOR NORTH CENTRAL TEXAS, AND ANY MATTER WHICH REQUIRES THE APPROVAL OF THE OWNER, MUST BE APPROVED BY THE DIRECTOR OF PUBLIC WORKS AND TRANSPORTATION OR HIS DESIGNEE BEFORE ANY CONSTRUCTION INVOLVING THAT DECISION COMMENCES. ASSUMPTIONS ABOUT WHAT THESE DECISIONS MIGHT BE WHICH ARE MADE DURING THE BIDDING PHASE WILL HAVE NO BEARING ON THE DECISION.
3. FOR ADJUSTMENTS OF DALLAS WATER UTILITIES APPURTENANCES OR TO VERIFY LOCATIONS OF EXISTING WATER AND WASTEWATER MAINS IN AREA, CALL (214) 670-1770 AT LEAST (3) THREE WORKING DAYS PRIOR TO CONSTRUCTION. FOR SERVICES NEEDED WITHIN THE TOWN OF ADDISON, CALL (972) 450-2871 AT LEAST (3) THREE WORKING DAYS PRIOR TO CONSTRUCTION.
4. STREETS, ALLEYS, SIDEWALKS, DRIVEWAYS AND STORM DRAINAGE FACILITIES WITHIN RIGHT-OF-WAY, SHALL BE CONSTRUCTED IN CONFORMANCE WITH THE CITY OF DALLAS STANDARD CONSTRUCTION DETAILS, FILE 251D-1, LATEST EDITION.
5. ALL CONCRETE FOR PAVEMENT WITHIN DALLAS PARKWAY RIGHT-OF-WAY SHALL BE 4,000 PSI FOR MACHINE FINISH AND 4,500 PSI IF IT IS NECESSARY FOR HAND FINISH.

### TRAFFIC CONTROL NOTES

1. ALL TRAFFIC CONTROL SHALL CONFORM TO THE LATEST VERSION OF THE TEXAS MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MUTCD), PART VI. FIELD MODIFICATIONS MAY BE MADE TO ADDRESS LOCAL CONDITIONS WITH THE APPROVAL OF THE ENGINEER.
2. DESIGN SPEED OF DALLAS PARKWAY IS 40 MPH. MINIMUM SIGN SPACING IS 240'. MERGING TAPER FOR A 12' LANE IS 320' MINIMUM. CHANNELIZATION DEVICE SPACING IS AS SHOWN ON PLANS.
3. CONTRACTOR IS RESPONSIBLE FOR INSTALLATION, MAINTENANCE, AND REMOVAL OF TRAFFIC CONTROL DEVICES. TRAFFIC CONTROL DEVICES SHOULD BE INSPECTED DAILY AND REPAIRED OR REPLACED AS NECESSARY. AFTER REMOVAL, CONTRACTOR IS RESPONSIBLE FOR RESTORATION OF MODIFICATIONS TO ROADWAY AND SIDEWALK SURFACES, ROADWAY MARKINGS, AND SIGNAGE DUE TO TRAFFIC CONTROL DEVICES OR CONSTRUCTION ACTIVITY.
4. CHANNELIZATION DRUMS ARE THE MINIMUM LEVEL OF CHANNELIZATION DEVICE WHICH SHALL BE USED.
5. IF THE TCP IS ACTIVE DURING THE HOURS OF DARKNESS, ALL CHANNELIZATION DEVICES SHALL HAVE A TYPE "C" STEADY-BURN WARNING LIGHT OR EQUIVALENT REFLECTOR, AND ALL WARNING SIGNS SHALL HAVE A TYPE "A" LOW-INTENSITY FLASHING WARNING LIGHT.
6. TWO-WAY VEHICULAR TRAFFIC FLOW AND ACCESS TO ALL OCCUPIED PROPERTIES SHALL BE MAINTAINED AT ALL TIMES UNLESS NOTED. PEDESTRIAN PATHWAYS SHALL BE PROVIDED ACROSS OR AROUND THE WORK AREA IN ACCORDANCE WITH THE MUTCD.
7. WHEN THE TCP IS NOT IN EFFECT, ALL CHANNELIZING DEVICES SHALL BE REMOVED FROM THE TRAVEL LANES AND ALL SIGNS SHALL BE COVERED OR TURNED AWAY FROM THE DIRECTION OF TRAFFIC. THE TRAVEL LANE SURFACES SHALL BE RESTORED WITH STEEL PLATES OR TEMPORARY PAVEMENT. WHERE A SAW CUT OR PAVEMENT REMOVAL RESULTS IN MORE THAN A 2" DROP-OFF ADJACENT TO THE TRAVEL LANE, THE EDGE SHALL BE MARKED WITH VERTICAL PANELS OR CHANNELIZATION DRUMS AT 25' SPACING, AND WARNING SIGN CW 8-9a "SHOULDER DROP-OFF" SHALL BE POSTED 160' IN ADVANCE OF THE DROP-OFF CONDITION.



**RECORD DRAWINGS**  
**(October 2013)**  
 Information Provided By:  
**BOB MOORE**  
**CONSTRUCTION**

<b>VILLAGE ON THE PARKWAY ON THE PARKWAY PHASE II ADDISON, TEXAS</b>	<b>TRAFFIC CONTROL PLAN DALLAS PARKWAY</b>						
KHA PROJECT: 06331903B DATE: 06/14/2013 SCALE: AS SHOWN DESIGNED BY: TBB DRAWN BY: AAE CHECKED BY: TBB	Kimley-Horn and Associates, Inc. State of Texas Registration No. F-528 5750 GENESIS COURT, SUITE 200, FRISCO, TX 75034 PHONE: 972-335-3580 FAX: 972-335-3779 WWW.KIMLEY-HORN.COM						
SHEET NUMBER <b>C-14</b>	REVISIONS <table border="1" style="width: 100%; border-collapse: collapse;"> <thead> <tr> <th>No.</th> <th>DATE</th> <th>BY</th> </tr> </thead> <tbody> <tr> <td> </td> <td> </td> <td> </td> </tr> </tbody> </table>	No.	DATE	BY			
No.	DATE	BY					

PLOTTED BY: LUCIO BARRERO (4/20/13) 1:07 PM  
DRAWN NAME: KAPRI CLEVELAND (4/20/13) 1:07 PM  
LAST SAVED: 4/20/13 1:07 PM  
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### PAVING - GENERAL NOTES

- GENERAL: PAVEMENT THICKNESS IS AS SHOWN IN ITEM 7. SUBGRADE DESIGN SHALL CONFORM TO TOWN OF ADDISON PUBLIC WORKS REQUIREMENTS IN ITEM 3, AND SHALL EXTEND 12" MIN. BEHIND THE BACK OF CURB.
- REINFORCED CONCRETE PAVEMENT:
  - CONCRETE STRENGTH SHALL BE AS SHOWN IN ITEM 7 (NOTICE LATEST EDITION).
  - ALL CURBS SHALL BE INTEGRAL WITH PAVEMENT AND SHALL BE OF THE SAME STRENGTH AS CONCRETE PAVEMENT.
  - DETAILS AND ARRANGEMENT OF PAVEMENT JOINTS, ALL TYPES, SHALL BE AS SHOWN ON THE TOWN STANDARD CONSTRUCTION DETAILS.
  - BAR LAPS SHALL BE THIRTY DIAMETERS.
  - REINFORCING STEEL SHALL BE #3 BARS (2/3") ON 18" CENTERS FOR 8" OR LESS, #4 FOR 10" OR ABOVE.
- SUBGRADE: SUBGRADE UNDER ALL PAVEMENT SHALL BE 4" THICK AND SHALL BE STABILIZED WITH AT LEAST 30 LBS PER CU YD OF HYDRATED LIME, COMPACTED TO A DENSITY NOT LESS THAN 85 PERCENT TAMPED DENSITY. TESTS MUST BE SUBMITTED TO THE PUBLIC WORKS DEPARTMENT FOR APPROVAL TO DETERMINE AMOUNT OF LIME REQUIRED. LIME TREATMENT SHALL BE APPROVED BY THE TOWN ENGINEER. IF LIME IS USED, SEE NOTICE ITEM 301.2 "LIME TREATMENT" FLEXIBLE BASE (CRUSHED STONE/CONCRETE) PER NOTICE ITEM 301.5 MAY BE SUBSTITUTED FOR LIME TREATMENT WITH THE APPROVAL OF THE TOWN ENGINEER.
- REBAR SHALL BE SUPPORTED BY BAR CHAIRS OR OTHER DEVICES APPROVED BY TOWN ENGINEER. NO TRAFFIC ON FINISHED SUBGRADE SHALL BE PERMITTED AFTER REINFORCING STEEL IS INSTALLED ABOVE SUBGRADE. NO TRAFFIC SHALL BE PERMITTED BEFORE OR DURING THE PLACING OF CONCRETE.
- CROSS SLOPE OF STRAIGHT CROWN STREETS SHALL BE 1/4" PER FOOT UNLESS APPROVED BY THE TOWN ENGINEER.
- PAVEMENT THICKNESS AND STRENGTHS SHALL BE AS FOLLOWS:

MAJOR ARTERIAL	10" CLASS "P1" OR "P2"
COMMERCIAL/INDUSTRIAL COLLECTOR	8" CLASS "P1" OR "P2"
RESIDENTIAL LOCAL	6" CLASS "P1" OR "P2"
SIDEWALK AND BR-1	4" CLASS "P1" OR "P2"
DRIVE APPROACH	3" CLASS "P1" OR "P2"
ALLEY	4" CLASS "P1" OR "P2"
- CONCRETE MIX DESIGN SHALL BE AS DEFINED BY NOTICE 303.3.
- ALL MEDIAN AND PARKWAYS SHALL BE PROVIDED WITH BERMDA GROUND COVER.
  - ONCE A CURB HAVING A BERMDA GROUND COVER HAS BEEN SHOWN AND REQUIRED, THE CONTRACTOR MUST REPLACE THE CONCRETE WITH A NEW POUR (I.E. DRIVEWAY) WITHIN 14 CALENDAR DAYS, LOCATED DIMENSIONS WILL BE ASSESSED 45 DAYS PER DAY PER EACH CALENDAR DAY IN EXCESS OF 14 CALENDAR DAYS. PAVEMENT SHALL BE MADE PRIOR TO ACCEPTANCE OR DELAY OF A CERTIFICATE OF OCCUPANCY.
  - ALL SIDEWALKS AND ACCESSIBLE ROUTES SHALL HAVE A MAXIMUM LONGITUDINAL SLOPE OF 5% AND A MAXIMUM CROSS SLOPE OF 3%.
- ALLEYS AND DRIVEWAYS:
  - CONCRETE FOR ALLEY RETURNS AND DRIVEWAYS SHALL HAVE A MINIMUM COMPRESSIVE STRENGTH AT 28 DAYS IDENTICAL TO THAT SPECIFIED FOR THE STREET PAVEMENT OR BASE WHEN BUILT AS COMPONENTS OF A CONCRETE PAVING PROJECT. WHEN BUILT SEPARATELY, THE STRENGTH SHALL BE AS SPECIFIED ON THE CONSTRUCTION PLAN.
  - SPACING AND CONSTRUCTION OF JOINTS SHALL CONFORM TO PARALLEL STREET PAVEMENT.

DATE: AUGUST 2010	REV DATE:	SHEET: 30-100
DESIGNED BY:	CHECKED BY:	DATE: 08-11-10

### ASPHALT PAVING CROSS SECTION

NOTE: CUT OF 6" INTO EXISTING PAVEMENT IS REQUIRED FOR CONSTRUCTION JOINT WITH NEW POURS TO GET A SMOOTH FINISH.

DATE: AUGUST 2010	REV DATE:	SHEET: 30-100
DESIGNED BY:	CHECKED BY:	DATE: 08-11-10

### CONSTRUCTION JOINTS FOR PAVEMENT

#### TRANSVERSE AND LONGITUDINAL JOINTS

NOTE: CONTRACTOR SHALL PROTECT KEYWAY PRIOR TO SECOND POUR. IF LONGITUDINAL KEYWAY IS DAMAGED, CONTRACTOR SHALL REPAIR WITH THE USE OF LONGITUDINAL BUTT JOINT, DRILL AND GROUT DOWELS INTO FIRST POUR.

DATE: AUGUST 2010	REV DATE:	SHEET: 30-102
DESIGNED BY:	CHECKED BY:	DATE: 08-11-10

### LONGITUDINAL BUTT JOINT

NOTE: NO 3 REINFORCED BAR MAY BE USED IN 6 INCH PAVEMENT.  
1. LONGITUDINAL BUTT CONSTRUCTION MAY BE UTILIZED IN PLACES OF LONGITUDINAL HINGED (TRANSVERSE) JOINT AT CONTRACTOR'S OPTION.  
2. DOWEL BARS SHALL BE DRILLED INTO PAVEMENT HORIZONTALLY BY USE OF A REINFORCING PUNCH DOWEL BARS INTO GREEN CONCRETE IS NOT ACCEPTABLE.

DATE: AUGUST 2010	REV DATE:	SHEET: 30-101
DESIGNED BY:	CHECKED BY:	DATE: 08-11-10

### TRANSVERSE EXPANSION JOINT

NOTE:  
1. DOWELS AND REINFORCING BARS SHALL BE SUPPORTED BY AN APPROVED DEVICE AND MUST BE BUILT ON CHAIRS OR BRGS.  
2. DOWELS MUST BE PERPENDICULAR TO FACE OF CONCRETE.  
3. NO 3 SMOOTH DOWEL BARS MAY BE USED IN 6" PAVEMENT.  
4. TRANSVERSE EXPANSION JOINTS SHALL HAVE A MAXIMUM SPACING OF 500 FT.  
5. TRANSVERSE EXPANSION JOINTS SHALL BE LOCATED AT INTERSECTIONS.

DATE: AUGUST 2010	REV DATE:	SHEET: 30-100
DESIGNED BY:	CHECKED BY:	DATE: 08-11-10

### SPACING DIAGRAM FOR TRANSVERSE JOINTS

NOTE: JOINTS TO BE SPACED TRANSVERSE AT 120'.

DATE: AUGUST 2010	REV DATE:	SHEET: 30-102
DESIGNED BY:	CHECKED BY:	DATE: 08-11-10

### SIDEWALK WIDENING REQUIRED AROUND OBSTACLE IN SIDEWALK

NOTE: 1/4\"/>

DATE: AUGUST 2010	REV DATE:	SHEET: 30-103
DESIGNED BY:	CHECKED BY:	DATE: 08-11-10

### CONCRETE SIDEWALK

NOTE: 1/2\"/>

DATE: AUGUST 2010	REV DATE:	SHEET: 30-104
DESIGNED BY:	CHECKED BY:	DATE: 08-11-10

### PEDESTRIAN FACILITIES GENERAL NOTES

- GENERAL NOTES FOR PEDESTRIAN FACILITIES:
  - ALL BARBS AND HOOKS ADJACENT TO THE LEAD PROPOSED SLOPE THAT WILL STILL DRAIN PROPERLY SHOULD BE USED. ADJUST CURB RAMP LENGTH ON GRADE OF APPROACH TO MATCH AS DIRECTED.
  - LANDINGS SHALL BE 5'0\"/>
- GENERAL NOTES FOR DETECTABLE WARNINGS:
  - CURB RAMPS MUST CONTAIN A DETECTABLE WARNING SURFACE THAT CONSIST OF RAISED TRUNCATED DOME CURBS COUPLED WITH SECTION 4.29 OF THE TEXAS ACCESSIBILITY STANDARDS (TAS). THE SURFACE MUST CONTRAST VISUALLY WITH THE ADJACENT SURFACES INCLUDING UNPAVED CONCRETE AND CLEAN COLORED DETECTABLE WARNING SURFACE ADJACENT TO DARK, RED COLORED CURB. DETECTABLE WARNING SURFACE ADJACENT TO DARK, RED COLORED CURB MUST BE 1/4\"/>

DATE: AUGUST 2010	REV DATE:	SHEET: 30-105
DESIGNED BY:	CHECKED BY:	DATE: 08-11-10

### PEDESTRIAN RAMPS

NOTE: 6\"/>

DATE: AUGUST 2010	REV DATE:	SHEET: 30-106
DESIGNED BY:	CHECKED BY:	DATE: 08-11-10

### TYPICAL PATTERN FOR PAVEMENT SIDEWALK AND CURB RAMP

NOTE: ALL PAVEMENT COLORS TO BE APPROVED BY TOWN OF ADDISON.

DATE: AUGUST 2010	REV DATE:	SHEET: 30-107
DESIGNED BY:	CHECKED BY:	DATE: 08-11-10

### DETECTABLE WARNING PAVEMENT

NOTE: 4\"/>

DATE: AUGUST 2010	REV DATE:	SHEET: 30-108
DESIGNED BY:	CHECKED BY:	DATE: 08-11-10

### SIDEWALK FLUME DETAIL

NOTE: BASED ON 100 P.S.F. LIVE LOAD AND MAX. DEAD & LIVE LOAD DEFLECTION OF 1/240 BOLT PLATE DOWN WITH BRASS BOLTS. BOLTS SHALL BE 1/2\"/>

DATE: AUGUST 2010	REV DATE:	SHEET: 30-109
DESIGNED BY:	CHECKED BY:	DATE: 08-11-10

### SIDEWALK FLUME DETAIL CONTINUED

NOTE: 1/2\"/>

DATE: AUGUST 2010	REV DATE:	SHEET: 30-110
DESIGNED BY:	CHECKED BY:	DATE: 08-11-10

### UTILITY INSTALLATION BENEATH CONCRETE ROAD SURFACE

NOTE:  
1. REPAIRS SHALL EXTEND TO 1' BEYOND EACH SIDE OF TRENCH (8').  
2. REINFORCING CHAIRS OR APPROVED DEVICE SHALL BE USED.  
3. REPAIRS SHALL MATCH EXISTING GRADE.

DATE: AUGUST 2010	REV DATE:	SHEET: 30-111
DESIGNED BY:	CHECKED BY:	DATE: 08-11-10

NO.	REVISIONS	DATE	BY

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Information Provided By: BOB MOORE CONSTRUCTION

ROY BRASWELL  
LICENSURED PROFESSIONAL ENGINEER  
10/28/13

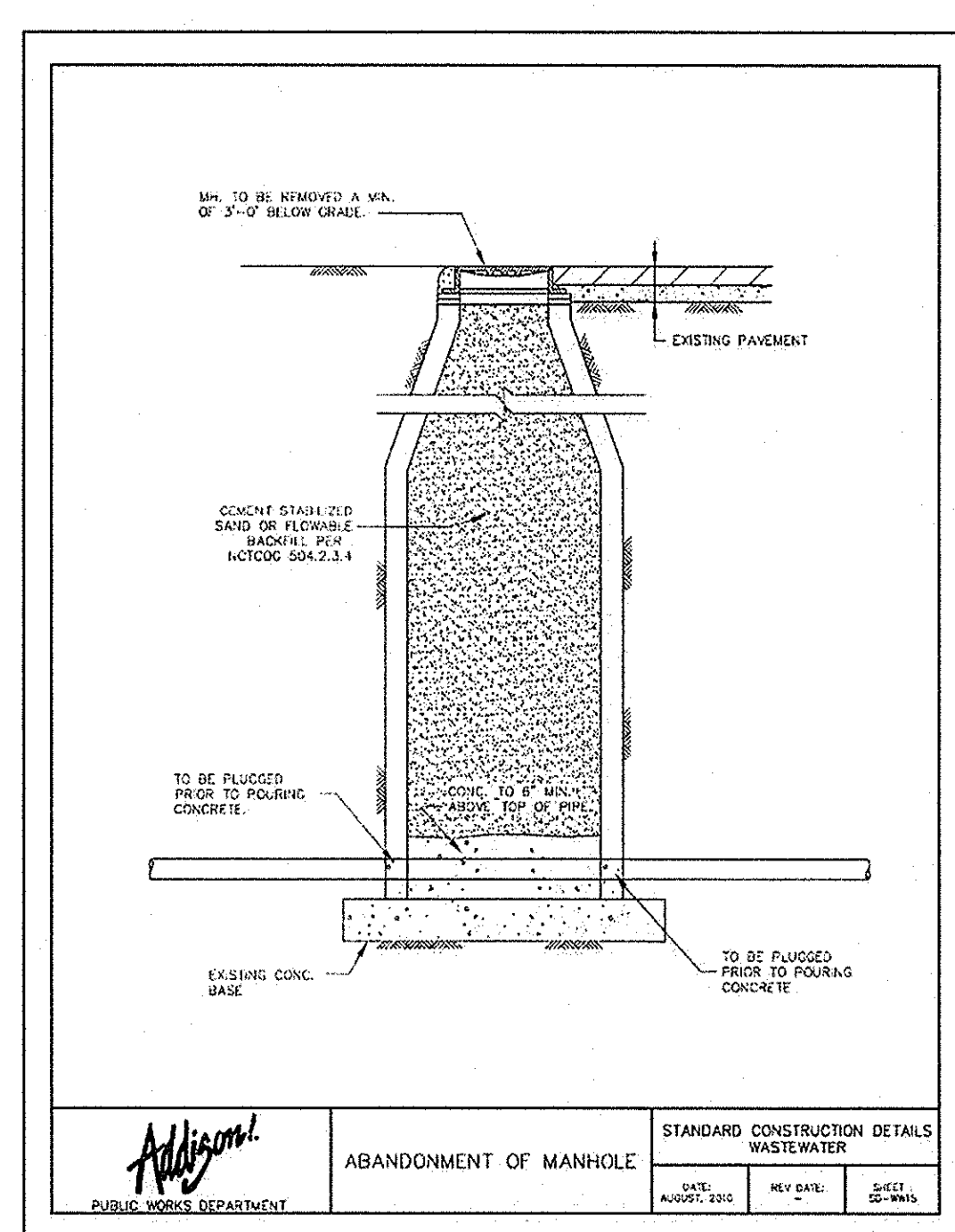
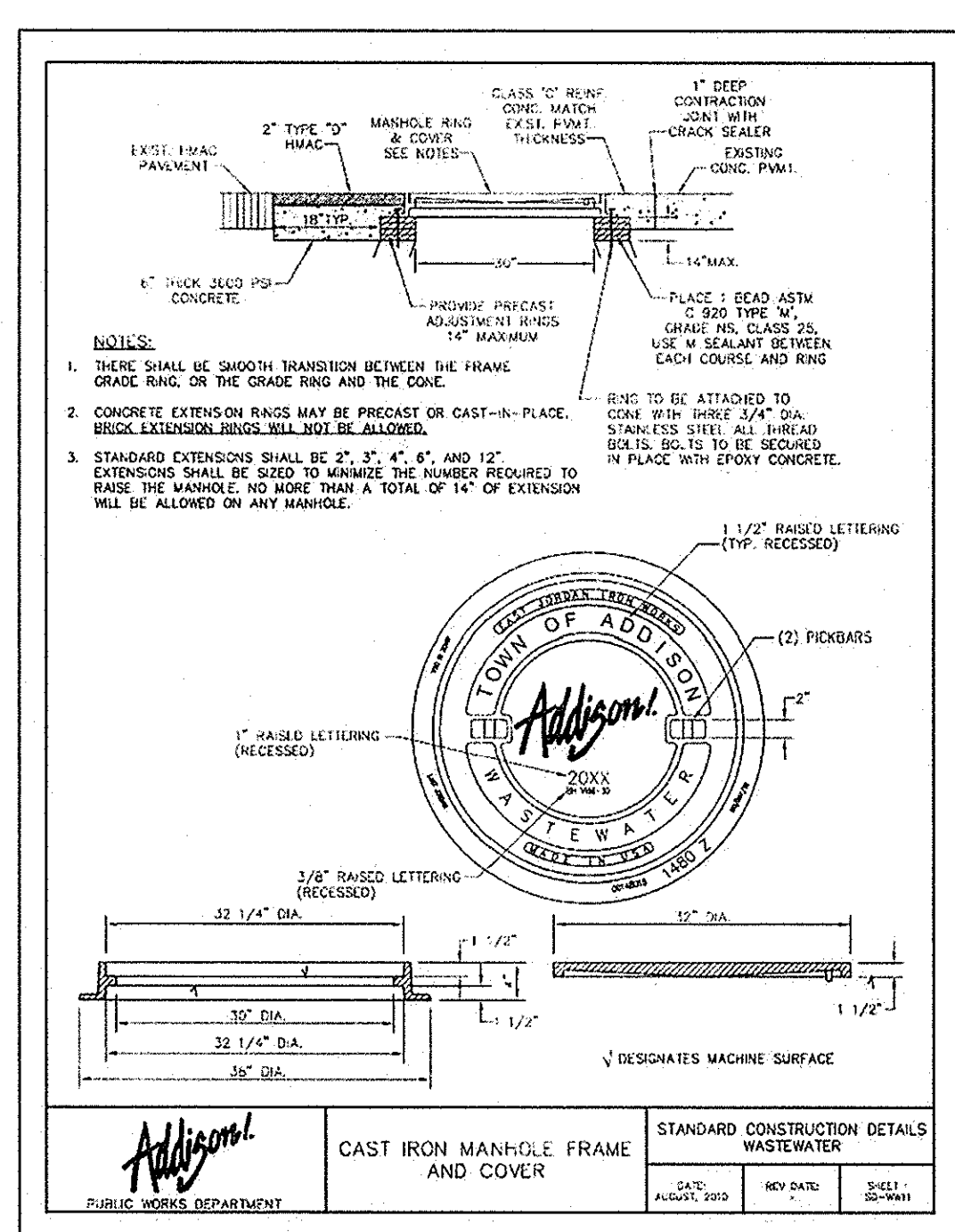
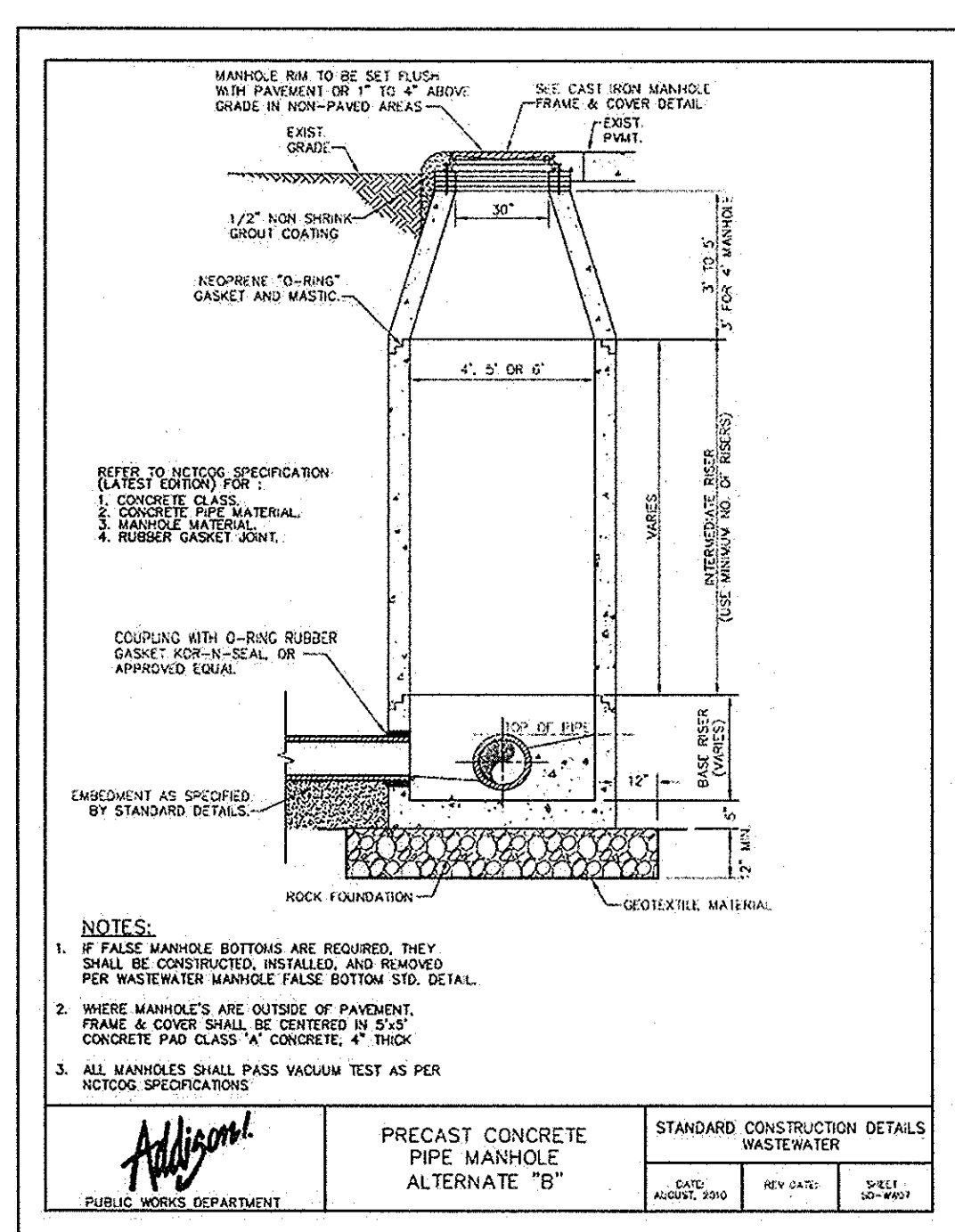
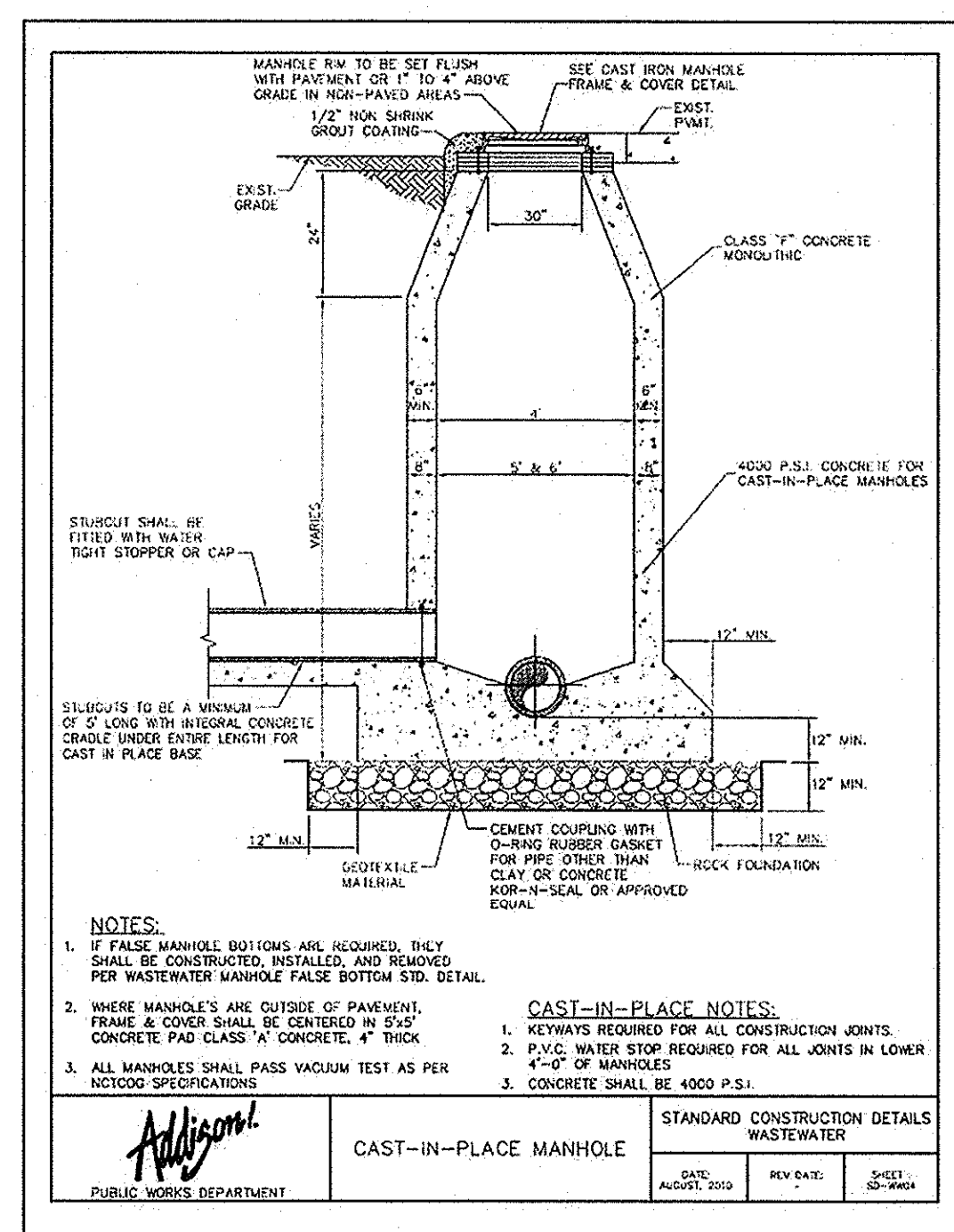
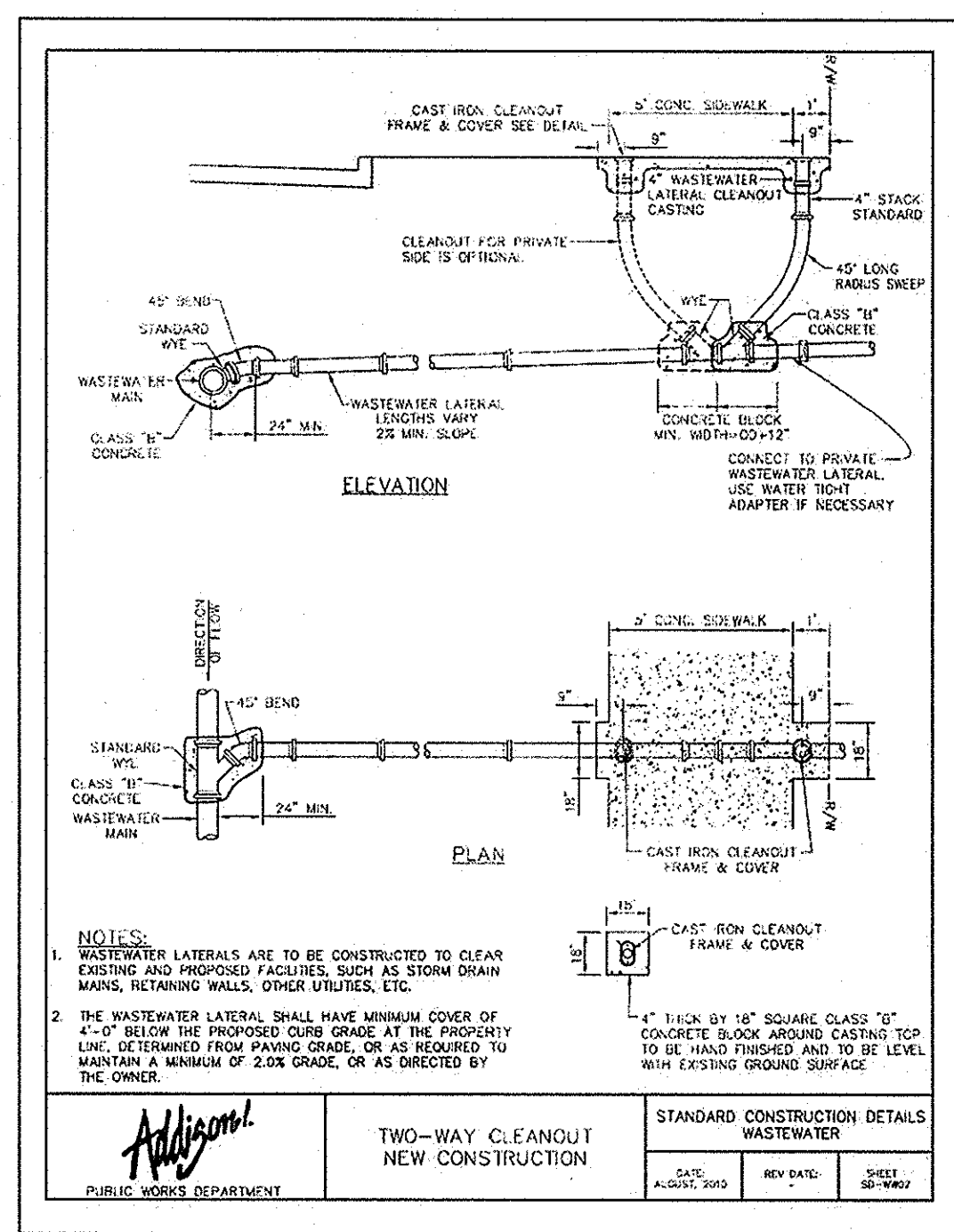
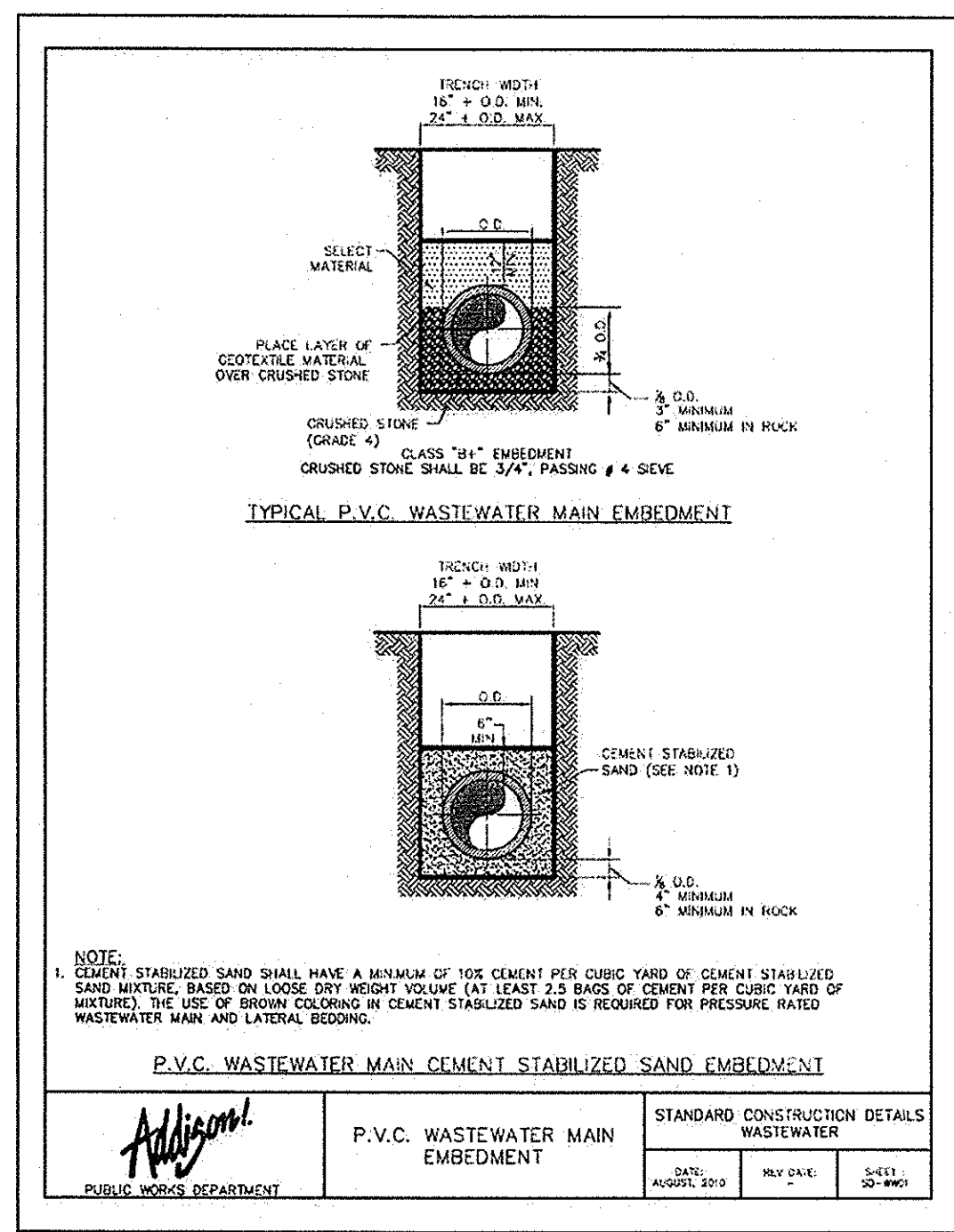
KHA PROJECT	063319038
DATE	06/14/2013
SCALE	AS SHOWN
DESIGNED BY	TBB
DRAWN BY	ALE
CHECKED BY	TBB

VILLAGE ON THE PARKWAY PHASE II ADDISON, TEXAS

TOWN OF ADDISON PAVING DETAILS

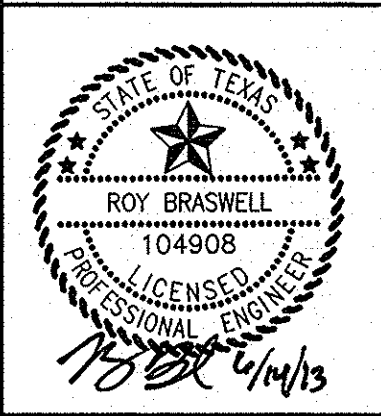
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KHA PROJECT	063319038
DATE	06/14/2013
SCALE	AS SHOWN
DESIGNED BY	TBB
DRAWN BY	AJE
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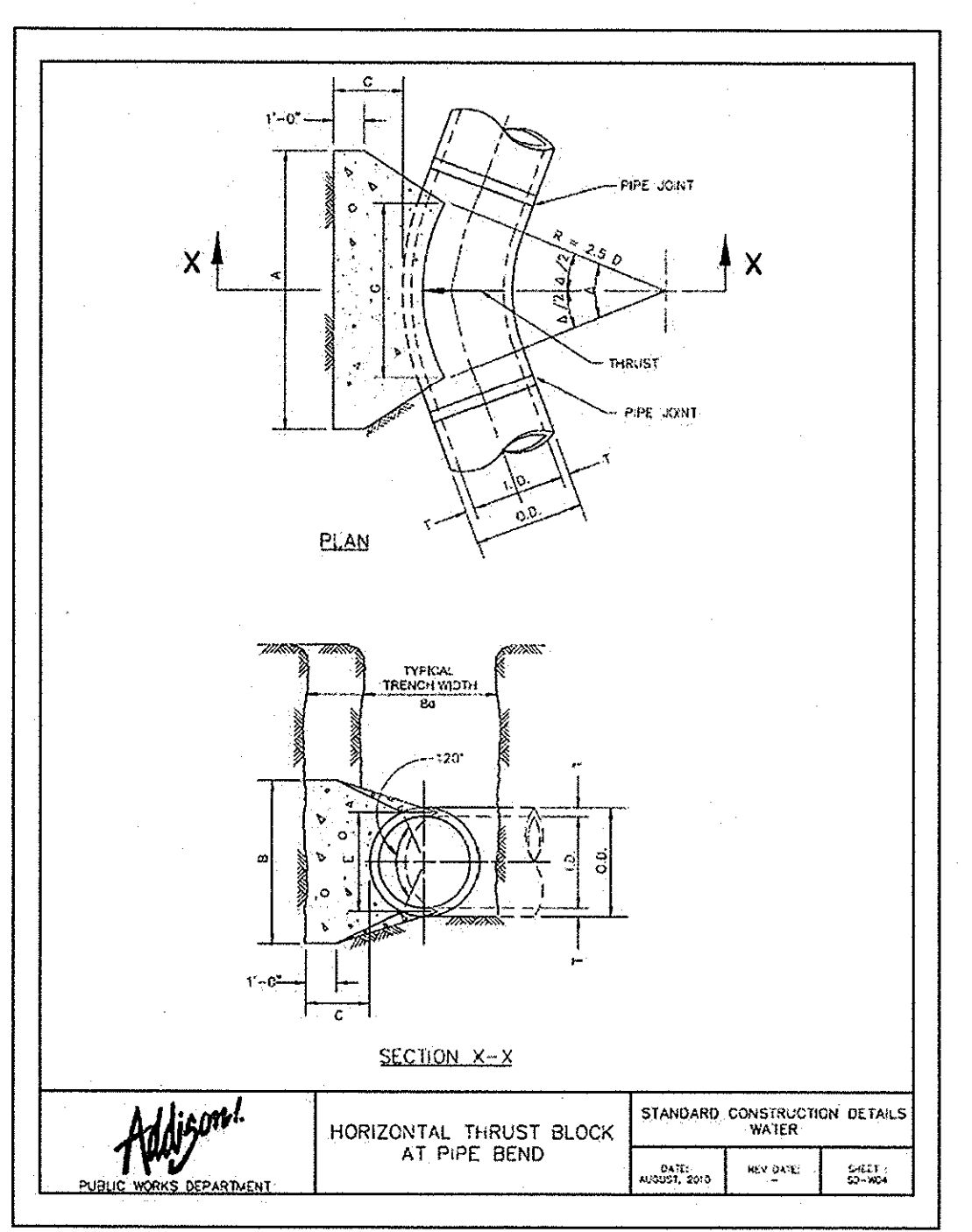
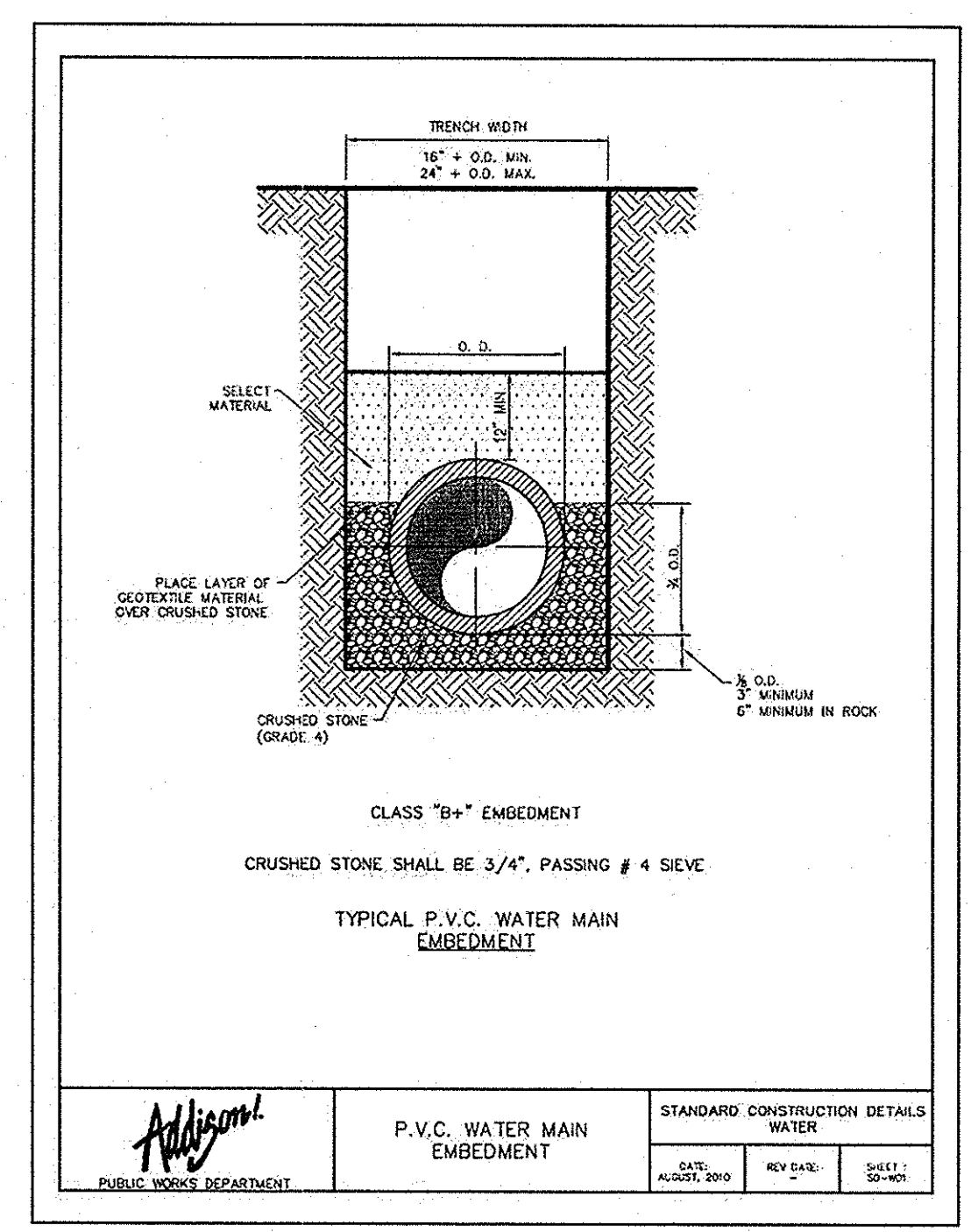
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**ADDITION, TEXAS**

**TOWN OF ADDISON SANITARY SEWER DETAILS**

SHEET NUMBER  
**C-17**

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 State of Texas Registration No. F-928  
 ROY BRASWELL  
 LICENSED PROFESSIONAL ENGINEER  
 19874 11/13  
 KHA PROJECT 063319038  
 DATE 06/14/2013  
 SCALE AS SHOWN  
 DESIGNED BY TBB  
 DRAWN BY ALE  
 CHECKED BY TBB  
 VILLAGE ON THE PARKWAY PHASE II WATER DETAILS  
 TOWN OF ADDISON ON THE PARKWAY PHASE II WATER DETAILS  
 ADDISON, TEXAS  
 RECORD DRAWINGS (October 2013)  
 Information Provided by:  
 BOB MOORE CONSTRUCTION  
 SHEET NUMBER C-18



**TABLES OF DIMENSIONS AND QUANTITIES**

**HORIZONTAL THRUST BLOCK DIMENSIONS & QUANTITIES**

STANDARD CONSTRUCTION DETAILS WATER

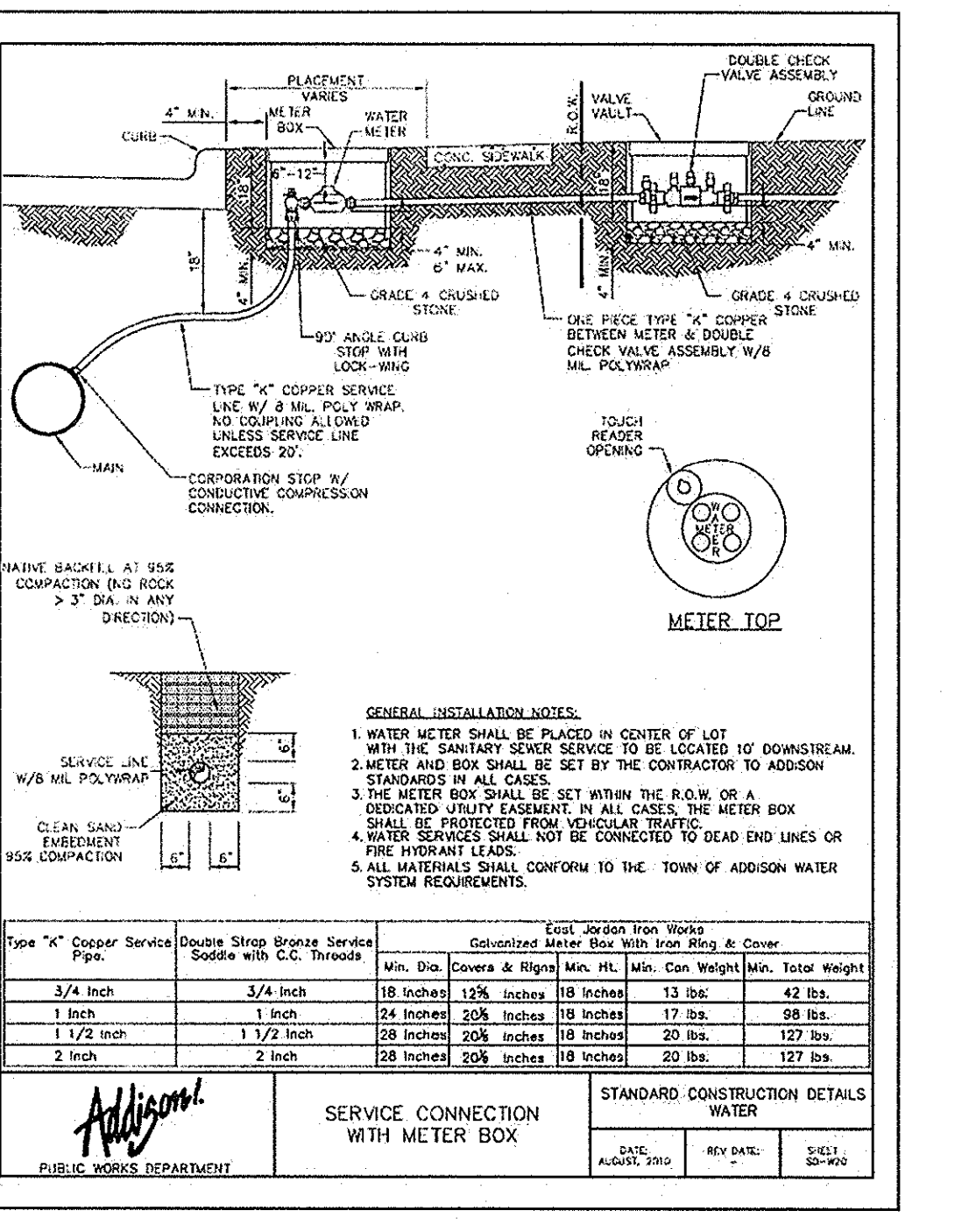
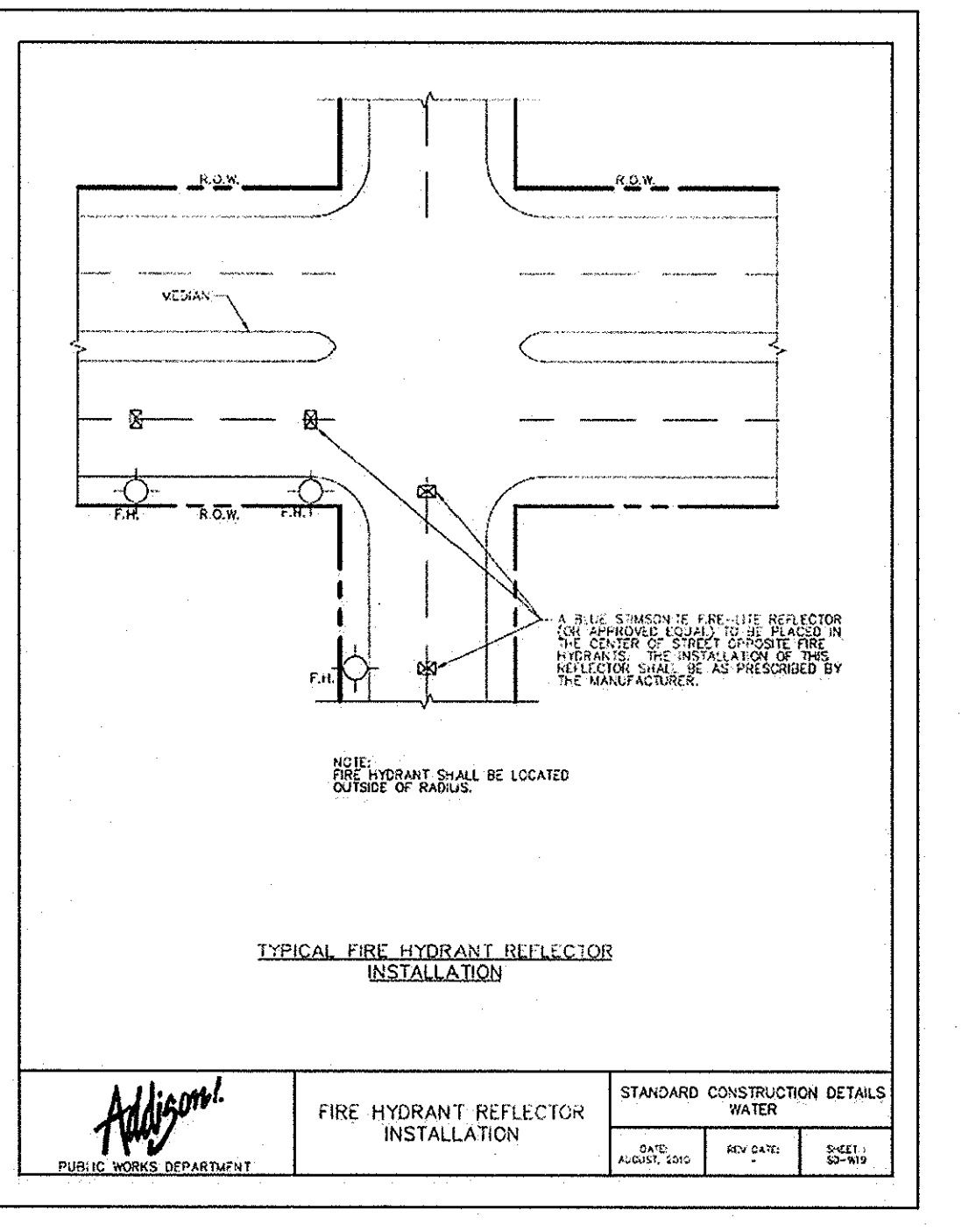
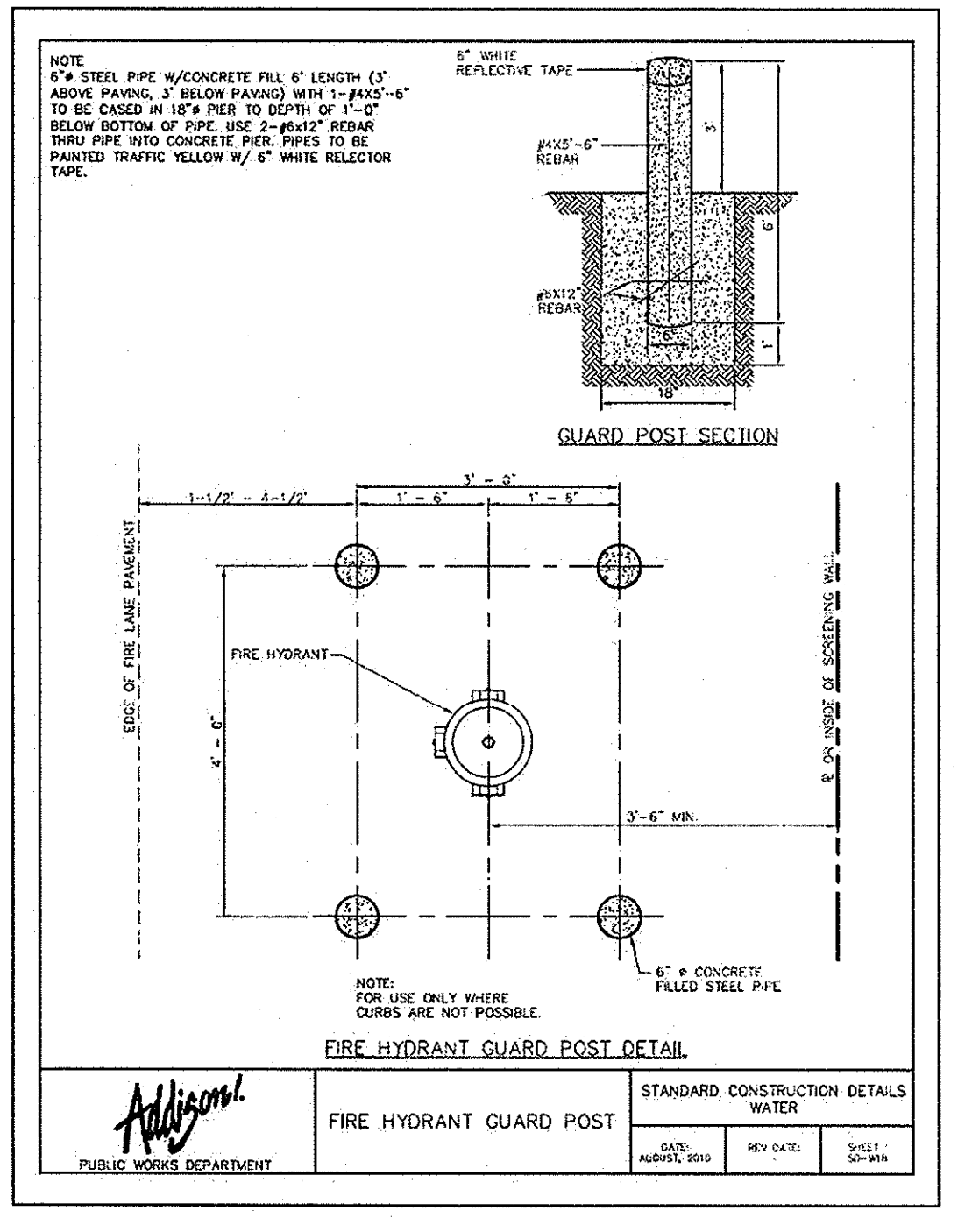
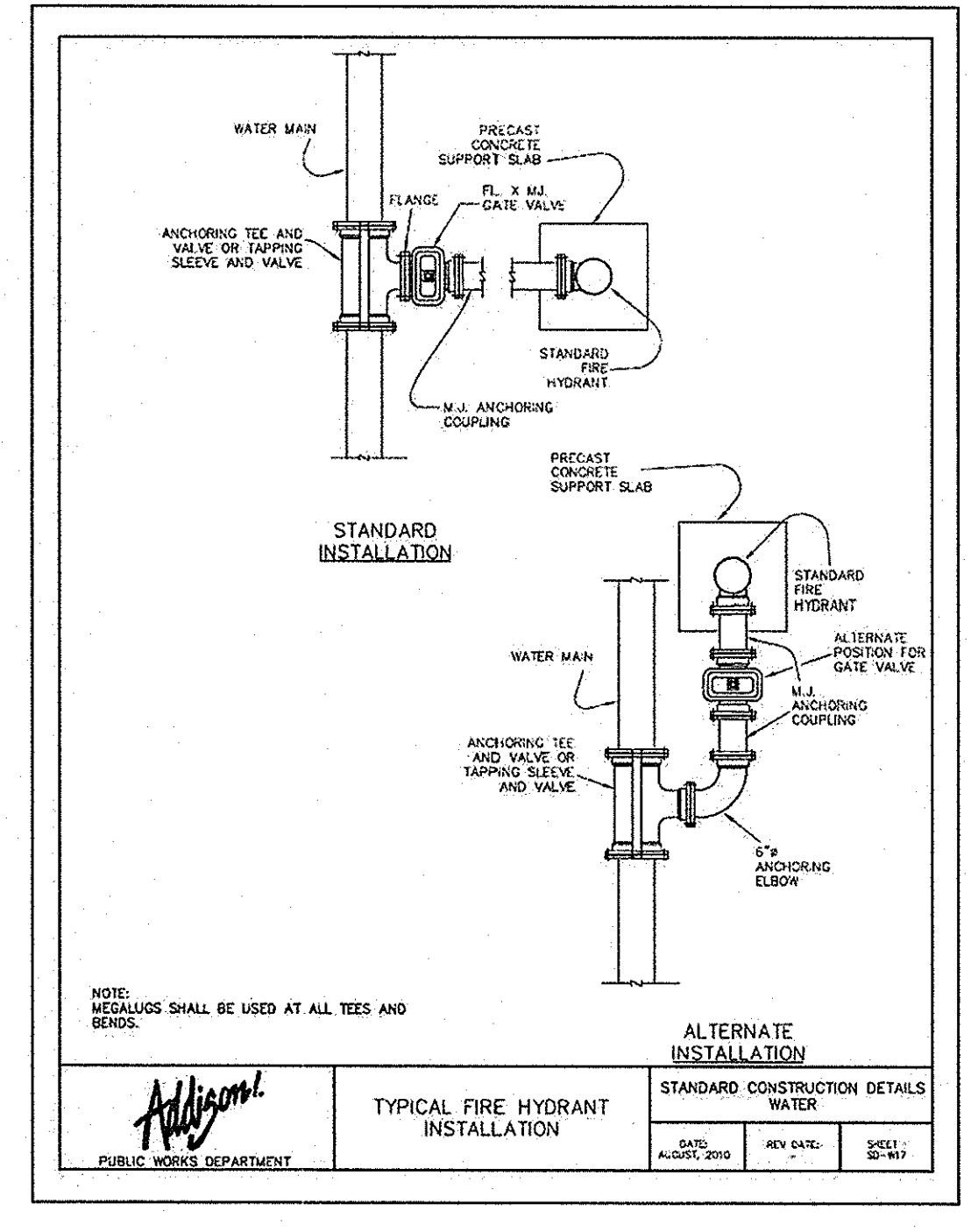
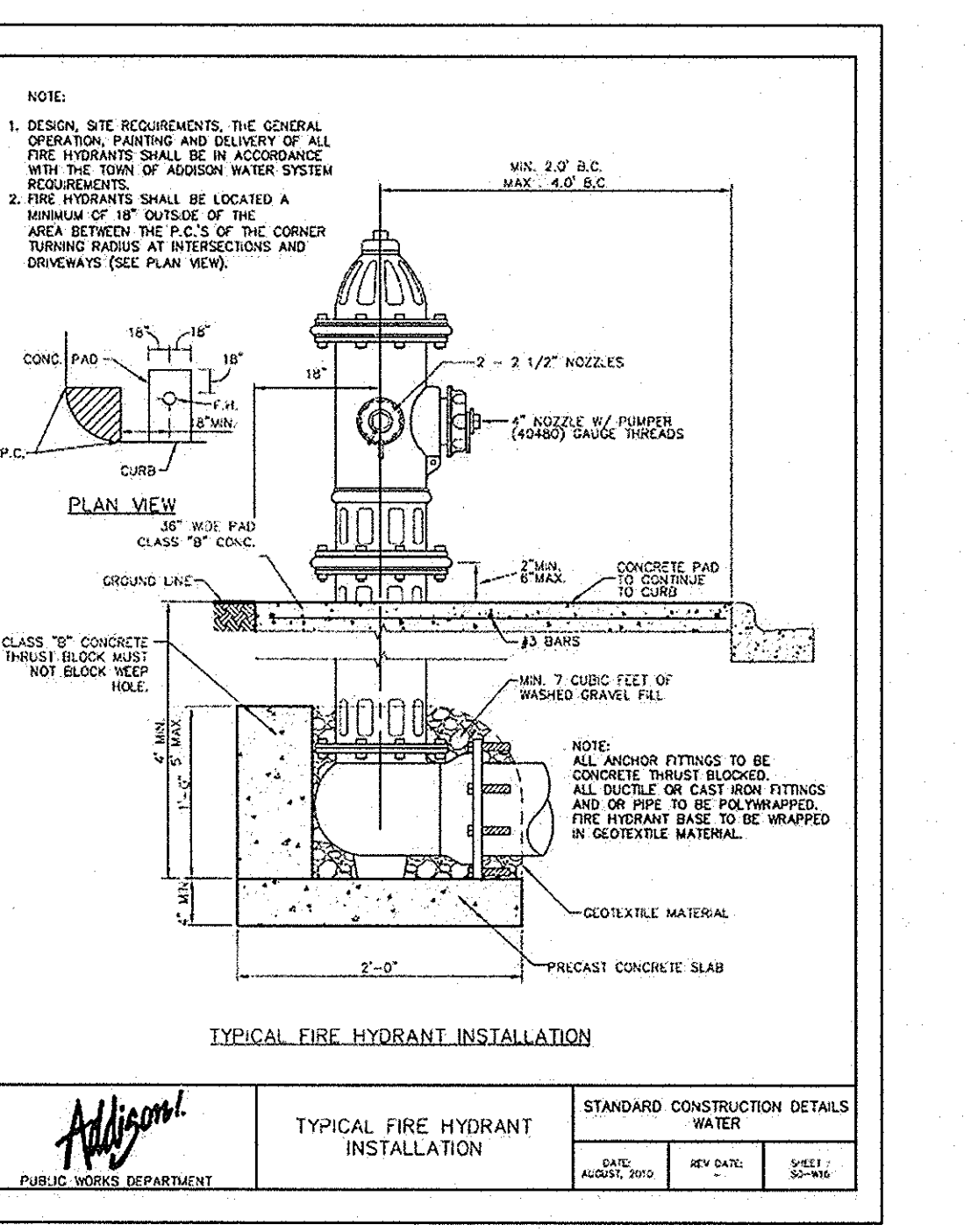
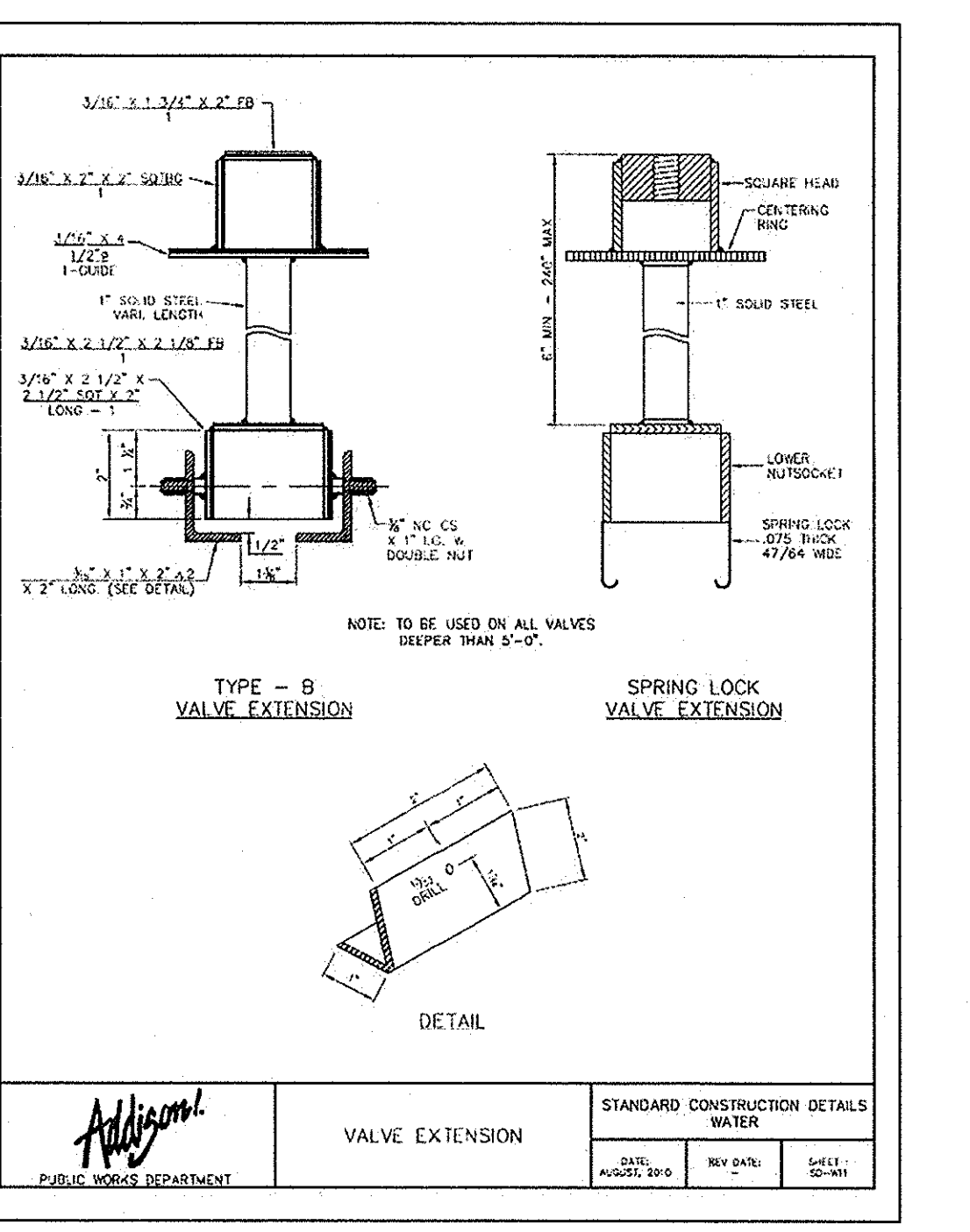
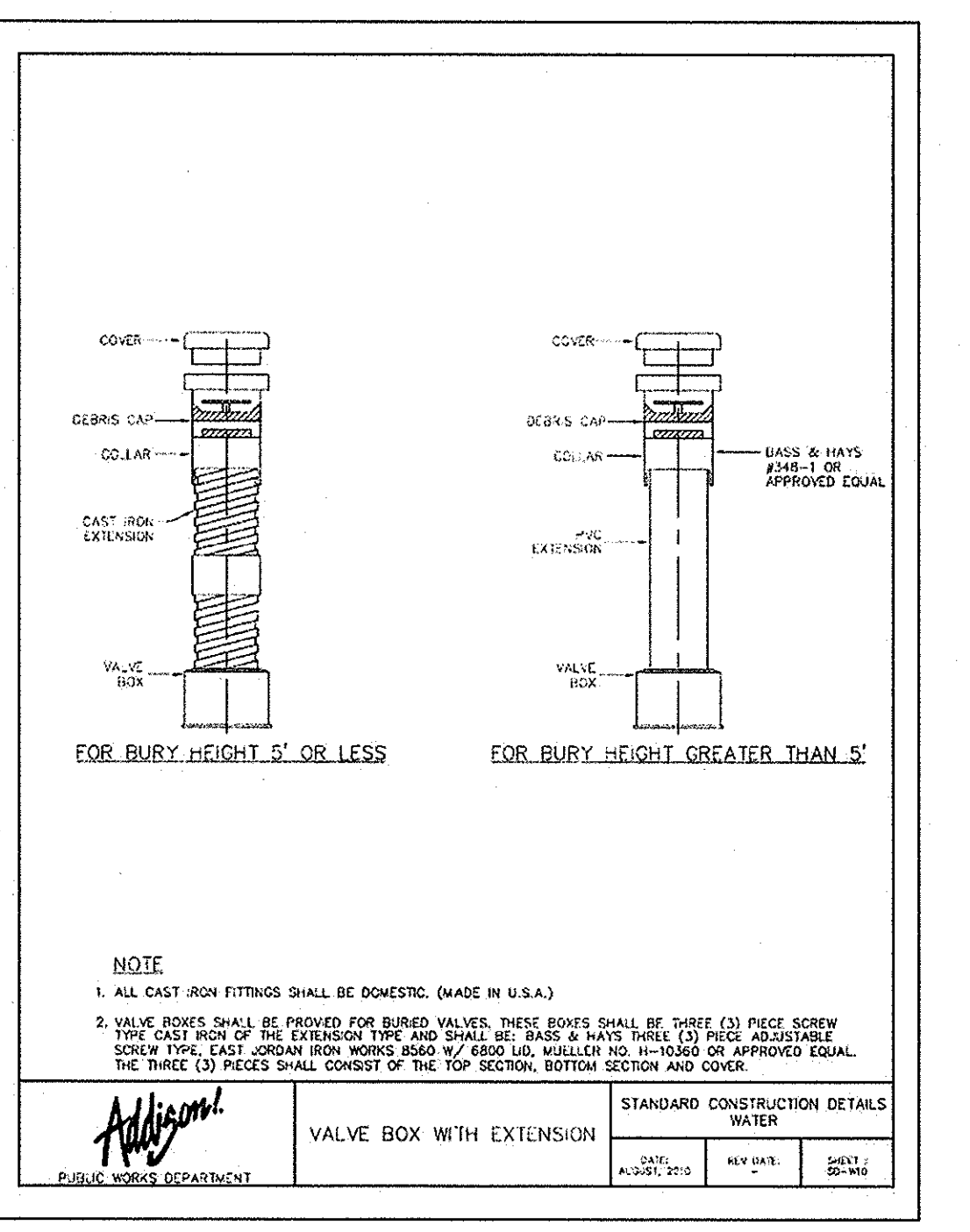
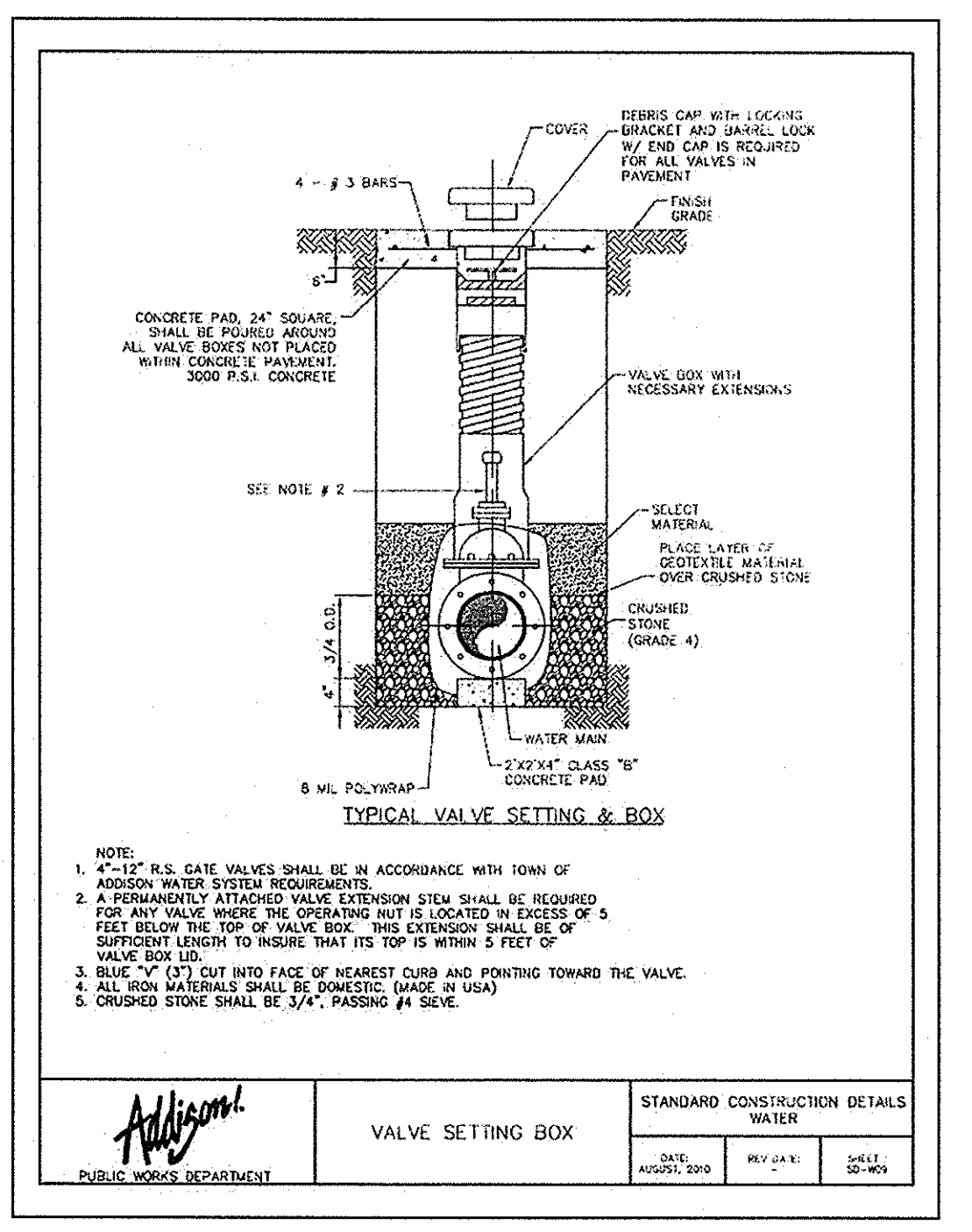
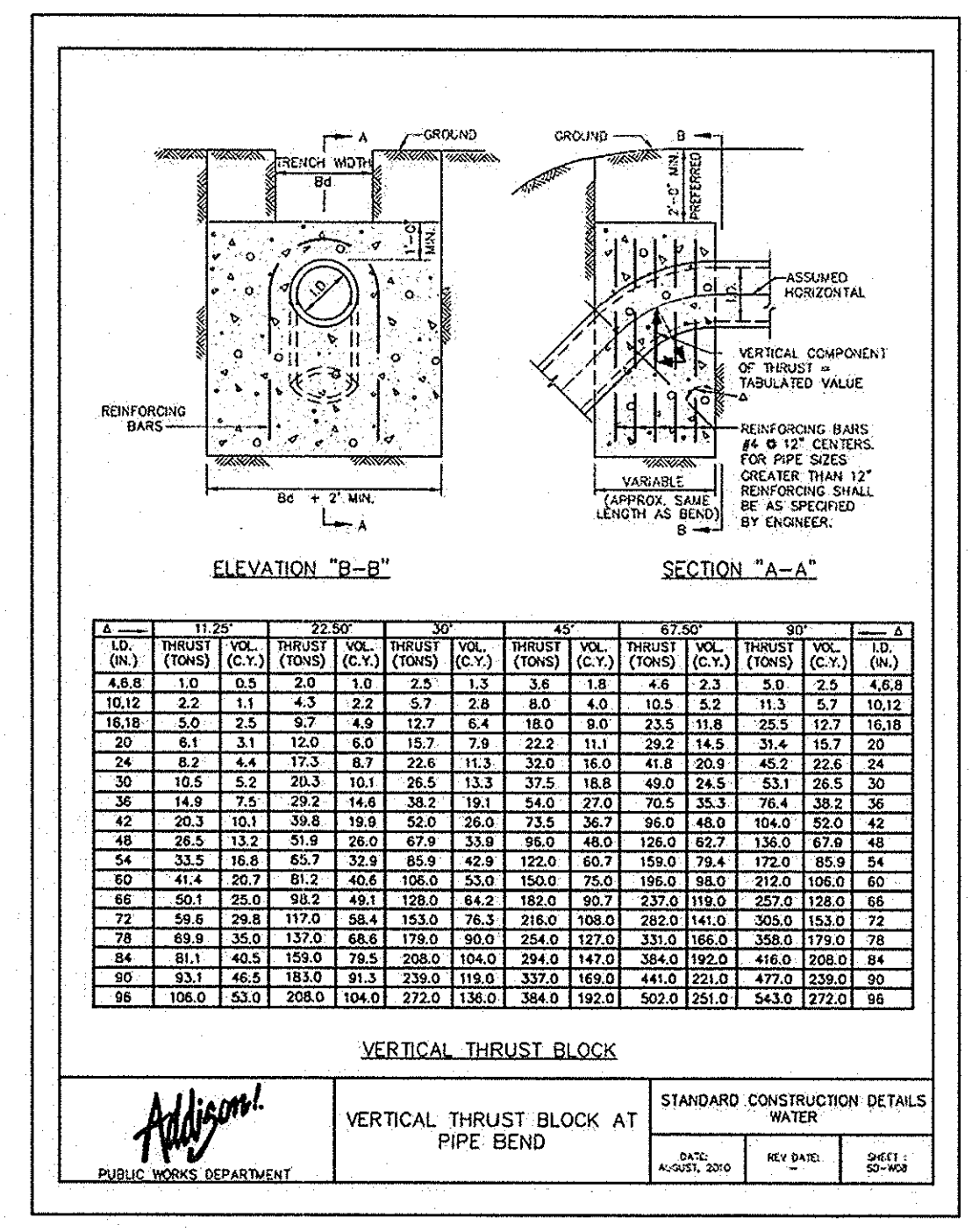
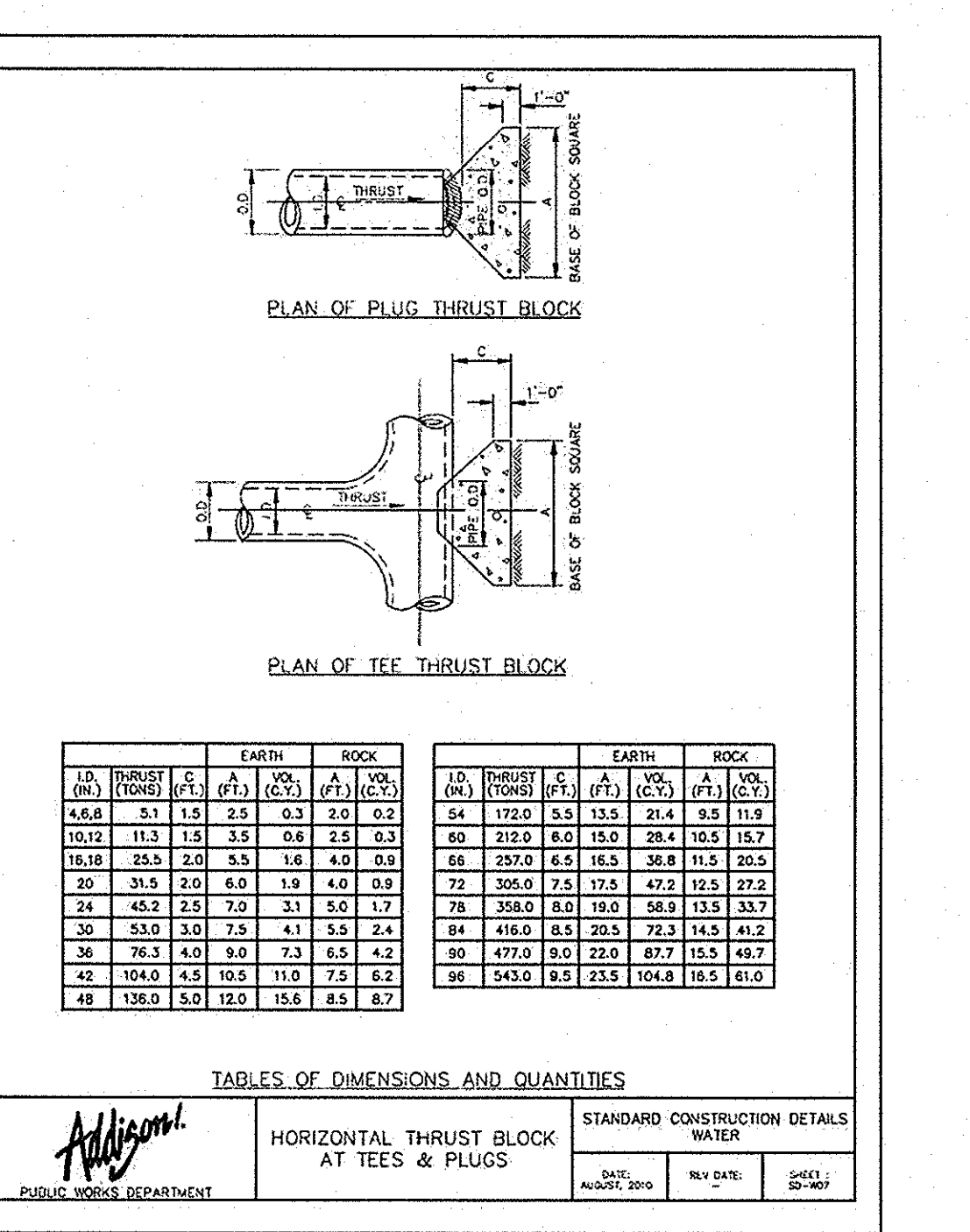
DATE: AUGUST 2010 REV DATE: SHEET: 30-104

**TABLES OF DIMENSIONS AND QUANTITIES**

**HORIZONTAL THRUST BLOCK DIMENSIONS & QUANTITIES**

STANDARD CONSTRUCTION DETAILS WATER

DATE: AUGUST 2010 REV DATE: SHEET: 30-104



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